

I hereby give notice that a hearing by commissioners will be held on:

Date: Monday 27 to Thursday 30 May and
Tuesday 4 to Friday 7 June 2024
(Note: not all days may be required)

Time: 9.30am

Meeting Room: Council Chamber

Venue: Level 1, West Annex,
31-33 Manukau Station Road, Manukau

**TWO NOTICES OF REQUIREMENT FOR THE
TAKAANINI LEVEL CROSSING PROJECT (TLC) AND
FOUR NOTICES OF REQUIREMENT FOR THE SOUTH
FREQUENT TRANSIT NETWORK (FTN) PROJECT**

HEARING REPORT – VOLUME TWO (SUBMISSIONS)

**NOR 1 TAKAANINI LEVEL CROSSINGS (TLC)
– SPARTAN ROAD, MANUIA ROAD, MANUROA
ROAD AND TAKA STREET**

**NOR 2 TAKAANINI LEVEL CROSSINGS (TLC)
– WALTERS ROAD LEVEL CROSSING CLOSURE
AND NEW MULTI-MODAL BRIDGE**

**TE TUPU NGATAHI - SUPPORTING GROWTH
ALLIANCE**

COMMISSIONERS

**Chairperson
Commissioners**

**Dave Serjeant
Nigel Mark-Brown
Basil Morrison**

**Bevan Donovan
KAITOHUTOHU WHAKAWĀTANGA
HEARINGS ADVISOR**

Telephone: 09 890 8056 or 021 325 837
Email: bevan.donovan@aucklandcouncil.govt.nz
Website: www.aucklandcouncil.govt.nz

Note: The reports contained within this document are for consideration and should not be construed as a decision of Council. Should commissioners require further information relating to any reports, please contact the hearings advisor.

WHAT HAPPENS AT A HEARING

Te Reo Māori and Sign Language Interpretation

Any party intending to give evidence in Māori or NZ sign language should advise the hearings advisor at least ten working days before the hearing so a qualified interpreter can be arranged.

Hearing Schedule

If you would like to appear at the hearing please return the appearance form to the hearings advisor by the date requested. A schedule will be prepared approximately one week before the hearing with speaking slots for those who have returned the appearance form. If changes need to be made to the schedule the hearings advisor will advise you of the changes.

Please note: during the course of the hearing changing circumstances may mean the proposed schedule may run ahead or behind time.

Cross Examination

No cross examination by the requiring authority or submitters is allowed at the hearing. Only the hearing commissioners are able to ask questions of the requiring authority or submitters. Attendees may suggest questions to the commissioners and they will decide whether or not to ask them.

The Hearing Procedure

The usual procedure for a hearing is:

- **the chairperson** will introduce the commissioners and will briefly outline the hearing procedure. The Chairperson may then call upon the parties present to introduce themselves. The Chairperson is addressed as Madam Chair or Mr Chairman.
- The Requiring Authority (the applicant) will be called upon to present their case. The Requiring Authority may be represented by legal counsel or consultants and may call witnesses in support of the application. After the Requiring Authority has presented their case, members of the hearing panel may ask questions to clarify the information presented.
- **Submitters** (for and against the application) are then called upon to speak. Submitters' active participation in the hearing process is completed after the presentation of their evidence so ensure you tell the hearing panel everything you want them to know during your presentation time. Submitters may be represented by legal counsel or consultants and may call witnesses on their behalf. The hearing panel may then question each speaker.
 - Late submissions: The council officer's report will identify submissions received outside of the submission period. At the hearing, late submitters may be asked to address the panel on why their submission should be accepted. Late submitters can speak only if the hearing panel accepts the late submission.
 - Should you wish to present written evidence in support of your submission please ensure you provide the number of copies indicated in the notification letter.
- **Council Officers** will then have the opportunity to clarify their position and provide any comments based on what they have heard at the hearing.
- The **requiring authority** or their representative then has the right to summarise the application and reply to matters raised. Hearing panel members may ask further questions. The requiring authority's reply may be provided in writing after the hearing has adjourned.
- **The chairperson** will outline the next steps in the process and adjourn or close the hearing.
- The hearing panel will make a recommendation to the Requiring Authority. The Requiring Authority then has 30 working days to make a decision and inform council of that decision. You will be informed in writing of the Requiring Authority's decision, the reasons for it and what your appeal rights are.

SIX NOTIFIED NOTICES OF REQUIREMENT TO THE AUCKLAND COUNCIL UNITARY PLAN BY TE TUPU NGATAHI - SUPPORTING GROWTH ALLIANCE

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Joy LaNauzel, Reporting officer (Takaanini Level Crossing project)

Reporting on two Notices of Requirement for the Takaanini Level Crossing project

REQUIRING AUTHORITY: TE TUPU NGATAHI - SUPPORTING GROWTH ALLIANCE

Takaanini Level Crossing

NOR1 - SPARTAN ROAD, MANUIA ROAD, MANUROA ROAD AND TAKA STREET

Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1) Auckland Transport. Notice of requirement lodged by Auckland Transport for new multi-modal bridge crossings of the North Island Main Trunk (NIMT) at Manuia Road and Taka Street; and new active mode bridge crossings of the NIMT at Spartan Road and Manuroa Road with two consequential road closures.

NOR2 – WALTERS ROAD LEVEL CROSSING CLOSURE AND NEW MULTI-MODAL BRIDGE

Notice of requirement lodged by Auckland Transport for a new multi-modal bridge crossing of the North Island Main Trunk (NIMT) railway at Walters Road.

SUBMITTERS TAKAANINI LEVEL CROSSING - NOR1 – SPARTAN ROAD, MANUIA ROAD, MANUROA ROAD AND TAKA STREET:

Page 9	Pam Scott Family Trust
Page 11	A1 Auto Panel and Paint
Page 13	Johnstone Properties Partnership
Page 15	Takanini Business Association Inc
Page 24	Brian Hogan
Page 25	Carter Building Supplies
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Page 99	Silverfin Capital Limited
Page 102	Big Rock Commercial Ltd and Matthew Koppens Ltd
Page 112	BP Oil New Zealand Limited
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Page 198	Arborfield Trust, Takanini Home and Trade Limited, and Mitre 10 Mega Takanini Limited
Page 207	Watercare Services Limited
Page 212	Z Energy Limited
Page 225	Kāinga Ora
Page 243	DR Levene & JAG Kearns & MA Levene & DW Tibby and EA Levene (The Levene Foundation)
Page 245	Basil Kuriakose Portrush Lane and 6 Signatories

LATE SUBMITTERS:

Page 249	Takaanini Rentors Limited
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SUBMITTERS TAKAANINI LEVEL CROSSING – NOR2 - WALTERS ROAD LEVEL CROSSING CLOSURE AND NEW MULTI-MODAL BRIDGE:

Page 251	Mead Trusts Holdings Ltd - Carters Takanini
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Page 282	Van Den Brink 254 Limited
Page 289	Brian Hogan
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Page 455	KiwiRail Holdings Limited
Page 461	Jayanta Bhaduri and Sudarshana Bhaduri
Page 465	Auckland Council Parks and Community Facilities
Page 467	Takaanini Childcare Investments Ltd
Page 473	Ministry of Education
Page 477	Takanini Village Limited and Tonea Properties (NZ) Limited
Page 485	Sunlight Holdings Limited and South Auckland Marine Limited
Page 493	Mead Trusts Holdings Limited and Carters Buildings Supplies Limited
Page 502	Arborfield Trust, Takanini Home and Trade Limited, and Mitre 10 Mega Takanini Limited
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Page 529	Kāinga Ora
Page 547	Manpreet Kaur

ATTACHMENT ONE
COPIES OF SUBMISSIONS

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: pamela isabel scott

Organisation name: pam scott family trust

Full name of your agent: pam scott

Email address: pamsc@xtra.co.nz

Contact phone number: 021666184

Postal address:
pamsc@xtra.co.nz
Auckland
Auckland 2018

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
6/6 Taka St Takanini 7/6 Taka St Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

I own the above properties .I am wanting to sell both properties .I am 82 years old .The properties are the reason to sell soon as possible,I am selling them to go into a retirement village as my health is deteriorating .With the notifications from Council how am i going to sell and get a decent price with this in the future .Nobody would be interested .I certainly can not wait until this is sorted . Pam Scott

I or we seek the following recommendation or decision from Auckland Council:

Information to help for this major problem .presume the powers that be will consider buying me out .I do not have time to wait due to age etc

Submission date: 23 November 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Amarjit Singh

Organisation name: a1 auto panel and paint

Full name of your agent: Amarjit Singh

Email address: A1autopanelandpaint@gmail.com

Contact phone number: 0220433606

Postal address:
38 Spartan Road Takanini
Papakura
Auckland 2105

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The reason it will badly affect our business. Client has easy access to our business directly from great South Road to Spartan Road. We get good exposure. Road Closing means, no exposure, means no client, we will be dependent on online business. This is very bad idea. Businesses are already struggling, please do not make it our life harder. Spend this money which is from our GST & Taxes, Please pay to help the business.

I or we seek the following recommendation or decision from Auckland Council:
Overall This is not a Good Idea.

Submission date: 30 November 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

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The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Malcolm Johnstone

Organisation name: Johnstone Properties Partnership

Full name of your agent:

Email address: malcolm@jcl.net.nz

Contact phone number: 021927788

Postal address:

P.O. Box 44

Matakana

Auckland 0948

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

I own 4 Manuia Road.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The very long notice period has already created problem with my tenants. 1. I am having difficulty with getting increased rents through rent reviews due to tenants uncertainty of tenure. 2. I have a tenant who may not renew their lease due to uncertainty of tenure. 3. I will have difficulty re leasing the site if I lose existing tenants. 4. My plans to redevelop the site have been destroyed. 5. Difficulty in getting rentals to current market values results in my site being devalued and compromises my position in the event Council do decide to acquire the site. 6. The notice period is excessively long and in effect denies landowners property rights. 7. Council is forcing landowners to land bank their sites for Council benefit.

I or we seek the following recommendation or decision from Auckland Council:

1. The designation over my land should be limited to 2 years at maximum. 2. Council to underwrite any rental losses arising from notice of requirement.

Submission date: 4 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?

Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Gary Holmes

Organisation name: Takanini Business Association Inc

Full name of your agent:

Email address: admin@takaninibusiness.co.nz

Contact phone number: 0274966283

Postal address:

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The reasons for the TBA being in opposition regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are set out below and include the following: • alternatives • general impacts on businesses • construction effects on transport • operational effects on transport • on street parking • on-site parking • property access • corridor-specific operational effects • construction noise and vibration • lapse period See attached submission

I or we seek the following recommendation or decision from Auckland Council:

The decision the TBA seeks from the Council is to decline the Notice of Requirement. See attached submission for further details regarding relief sought

Submission date: 8 December 2023

Supporting documents

Submission on NOR1 - Takanini Level Crossing (Takanini Business Association).pdf

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

Takanini Business Association Inc

Summary

Although the Takanini Business Association ('TBA') accepts that solutions need to be found to replace the four level crossings that bisect the Takanini community, the TBA opposes the bridge solutions proposed in the Notice of Requirement: Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') [Link](#).

The TBA holds concerns about the following likely negative impacts on businesses or communities accessing businesses from the bridge proposals:

- As properties are acquired for the Project, people will likely move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the area a number of businesses important to the community will likely be lost, including: • a number of early childhood education ('ECE') centres; a skills training and education centre; and a service station (providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity).
- Other businesses will likely be lost, including vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services (including loss of local employment / livelihood).
- There will be construction effects on transport and long term operational effects on transport, especially for freight movements.
- On street parking and on-site parking will be reduced and property access negatively affected.
- Disruption will be caused by construction noise and vibration (along with a reduction in parking availability due to changed road conditions and demand for parking from the construction workforce).
- Loss in revenue for local businesses directly affected by construction as road blockages or disruptive construction redirect regular businesses customers.

The TBA notes the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). The TBA is of the view that the assessment of alternatives is deficient.

However, should the option as proposed of raising the road (i.e. road-over-rail) be preferred, then to avoid, remedy or mitigate the effects of that option, the TBA agrees with the proposals by the applicant to include conditions and detailed plans, including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by the TBA.

Introduction

The Takanini Business Association ('TBA') is an incorporated society (2666951) which was established by local business and commercial property owners and managers in partnership with local Police in 2017 in order to support local businesses with all matters relating to business development and growth, security, networking, collective communication with local authorities and to establish a unique culture within the Takanini business community. [Link](#)

The TBA welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). [Link](#)

NoR1 is part of the Takaanini Level Crossings ('TLC') Project, which involves two new notices of requirement for designations given by Auckland Transport for the construction, operation, maintenance and upgrade of the following grade-separated crossings of the North Island Main Trunk ('NIMT') railway in Takaanini:

- A new multi-modal bridge over the railway at Manuia Road
- New multi-modal bridges to replace existing level crossings at Taka Street and Walters Road; and
- New active mode bridges to replace existing level crossings at Spartan Road and Manuroa Road.

These notices of requirement authorise works to:

- Provide a new bridge over the rail line at Manuia Road, accommodating all transport modes, with a particular focus on providing for heavy vehicles accessing the industrial areas
- Replace current level crossings at Taka Street and Walters Road with new bridges accommodating all transport modes
- Replace existing Spartan Road and Manuroa Road level crossings with new bridges for active modes (ie walking and cycling facilities).

The existing NIMT level crossings at Spartan Road, Manuroa Road, Taka Street and Walters Road will be closed and replaced by these crossings once constructed and operational.

Submissions

The Notice of Requirement being submitted on by the TBA is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

The submission relates to the entire Notice of Requirement.

The TBA is in opposition regarding the Notice of Requirement.

The TBA will not gain an advantage in trade competition through the submission.

The decision the TBA seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for the TBA being in opposition regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are set out below and include the following:

- alternatives
- general impacts on businesses
- construction effects on transport
- operational effects on transport
- on street parking
- on-site parking

- property access
- corridor-specific operational effects
- construction noise and vibration
- lapse period

(1) Alternatives

The TBA notes the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). The TBA says the assessment of alternatives is deficient.

However, should the option as proposed in the NoR1 of raising the road (i.e. road-over-rail) be preferred, then the TBA makes the following submissions regarding that option.

(2) General impacts on businesses

Section 6 of the Social Impact Assessment of the NoR1 identifies the following likely negative impacts on businesses or for communities accessing businesses from the proposal: [Link](#)

- There will be changes to people's way of life, for those living and working in the area and those who access services and businesses.
- As properties are acquired for the Project, people may move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the area, a number of businesses important to the community will potentially be lost, including: • a number of early childhood education ('ECE') centres across the Project area; • the Skills Update Training and Education Centre on Walters Road; and • the BP Service Station on Great South road providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity.¹
- Other businesses which will potentially be lost of the community include vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services.

To avoid, remedy or mitigate these effects, the TBA agrees with the proposals to include detailed plans identified in the TLC - Social Impact Assessment of NoR1 (to be developed in consultation with the Papakura Local Board and the Takanini Business Association) (or equivalents at the time), including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by the TBA.

(3) Construction effects on transport

Section 5 of the Assessment of Transport Effects of the NoR1 [Link](#) notes that construction of the Project includes: • closing existing level crossings at Spartan Road, Manuroa Road, Taka Street and Walters Road (not necessarily simultaneously); • constructing grade-separated multi-modal bridges at Manuia Road, Taka Street and Walters Road; • constructing grade-separated active mode bridges at Spartan

¹ These businesses are considered important to the community as they are included in the access indicator for social deprivation in the NZDep dataset.

Road and Manuroa Road and constructing cul-de-sac turning heads either side of the rail line at these locations; • constructing a cul-de-sac turning head on the northern end of Takanini Road; • constructing accessways along Taka Street and Walters Road; and • constructing the roundabout and signalised intersection either side of the Manuia Road bridge and the signalised intersection at Arion Road.

Section 5 also says that a number of scenarios reflecting construction sequencing of the bridges have been assessed to determine construction effects and especially to identify which construction staging would have significant adverse effects and would therefore need to be avoided or mitigated. These have been grouped under three geographic areas:

- Area 1: • Construction of Manuia Road multi-modal bridge • Testing two scenarios where Manuia Road bridge has not been built yet: o Closure of only Spartan Road level crossing o Closure of only Manuroa Road level crossing
- Area 2: • Closure of Taka Street level crossing and construction of Taka Street multi-modal bridge under the following two scenarios: o Manuia Road bridge built, Spartan Road and Manuroa Road level crossings closed o Spartan Road and Manuroa Road level crossings remain open, Manuia Road bridge not yet built
- Area 3: • Closure of Walters Road level crossing and construction of Walters Road multi-modal bridge

Of particular concern to the TBA are the construction effects on freight movements in each of these three areas.

Area 1: Construction Effects on freight movements

Scenario 1a: Closure of Spartan Road level crossing: Section 5.2.1 of the Assessment of Transport Effects of the NoR1 notes that if the Spartan Road level crossing is closed and Manuia Road has not been built yet, it is expected that freight will be diverted to alternative routes through residential areas such as Manuroa Road, Alfriston Road, and Porchester Road. This is likely to result in a significant potential impact to freight and the surrounding area, though this effect may be mitigated by constructing Manuia Road bridge prior to the closure of Spartan Road level crossing. [Link](#) The TBA asks that these effects on freight movements be avoided, remedied or mitigated.

Scenario 1b: Closure of Manuroa Road level crossing: Section 5.2.2 of the Assessment of Transport Effects of the NoR1 notes that if Manuroa Road is closed and Manuia Road has not been built yet, there may be additional freight (2,500 daily heavy vehicles) having to reroute through alternative routes such as Alfriston Road / Porchester Road, and Taka Street. This will result in diversions that will impact freight operations due to the longer journey time. These diversions will also increase the routing distance of freight through residential areas. This is likely to result in a significant potential impact to freight and the surrounding area, though this effect may be mitigated by constructing Manuia Road bridge prior to the closure of Manuroa Road level crossing. [Link](#) The TBA asks that these effects on freight movements be avoided, remedied or mitigated.

Section 5.2.3.1 of the Assessment of Transport Effects of the NoR1 concludes that: • Manuia Road bridge should be constructed and operational before the level crossings at Spartan Road and Manuroa Road are closed. The Assessment of Transport Effects expects that the Manuia Road bridge will resolve the issues that would otherwise have occurred in Scenario 1a and 1b.

Area 2: Construction Effects on freight movements

Scenario 2a: Manuia Road bridge built: Section 5.3.1 of the Assessment of Transport Effects of the NoR1 notes that there will be a lot more mixing between heavy vehicles and light vehicles on the Manuia Road bridge due to the limited number of east-west connections in this construction scenario. The combination of industrial heavy traffic and general light traffic will result in road safety concerns. The impact heavy vehicles have on smaller light vehicles can be detrimental and will result in serious crash effects on light vehicle users in the case of a collision with a larger, heavier vehicle. Hence, the Assessment of Transport Effects concludes that the combination of the mixture of heavy and light vehicles

will likely result in adverse effects. [Link](#) The TBA asks that these these effects on freight movements be avoided, remedied or mitigated.

A further concern is Appendix 2 of the Assessment of Transport Effects of the NoR1, which suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%). Contrary to this, local businesses have informed the TBA that Hitchcock Road is used by heavy vehicles, with hundreds of containers a year being transported and de-vanned on Hitchcock Road, with multiple trucks doing multiple runs per day delivering to customers.

Scenario 2b: Manuia Road bridge not yet built: Section 5.3.2 of the Assessment of Transport Effects of the NoR1 notes that this construction scenario is not expected to have a significant effect on diversions to freight, as freight can continue to primarily use Spartan Road and Manuroa Road in the 2038 network. The TBA supports scenarios with reduced effects on freight movements.

Section 5.3.3.1 of the Assessment of Transport Effects of the NoR1 concludes that a suitable alternative to facilitate traffic, pedestrian and cyclist movement should be provided for the closure of Taka Street during construction. This could mean the following: • Partial closure with some movements retained on Taka Street; and/or • Reroute traffic to an alternative connection such as Manuroa Road or Manuia Road (with Spartan Road open in either option). This would mean at least three connections are provided in the Takaanini network.

Area 3: Construction Effects on freight movements

Section 5.4 of the Assessment of Transport Effects of the NoR1 notes that the model indicates that this construction scenario is not expected to have a significant effect on diversions to freight as freight can continue to primarily use Spartan Road and Manuroa Road in the 2038 network. The TBA supports scenarios with reduced effects on freight movements.

Section 5.5 of the Assessment of Transport Effects of the NoR1 recommends development of a Construction Traffic Management Plan (CTMP) to mitigate the potential construction traffic effects to be prepared by the nominated contractor prior to works commencing. The TBA agrees and asks that the TBA be involved in development of the CTMP and that businesses be kept informed of construction times and progress.

(4) Operational effects on transport

Section 6.5 of the Assessment of Transport Effects of the NoR1 [Link](#) notes that the current access to and from the Takaanini industrial area is via Spartan Road and Manuroa Road, but since the Project is closing both freight connections and replacing them with Manuia Road, the future freight access will be via the new Manuia Road grade-separated connection.

As noted in Section 6.5, of particular concern is that the current over-dimension route through the Takaanini industrial area traverses along Porchester Road, Popes Road, Takanini School Road, onto Manuroa Road to connect with Great South Road and that the proposed closure of the Manuroa Road level crossing will impact the current over-dimension routing through the Takaanini industrial area.

This is a key effect of the Project, as there will need to be a change in the over-dimension route with a suitable alternative provided to mitigate this effect. The alternative over-dimension route will need to be provided via the future Manuia Road connection to Great South Road. The new over-dimension route will be provided via Porchester Road and then turn onto Manuroa Road, Oakleigh Avenue, and onto the new Manuia Road bridge. This alternative route must have a clear width and height to accommodate over-dimension vehicles and any overhead powerlines may need to be undergrounded. The roundabout at Manuia Road / Oakleigh Avenue intersection must be designed to allow for enough turning space for over-dimension vehicles.

As Manuroa Road is a residential street, it is not ideal to have heavy trucks traversing through this environment.

This is of concern to the TBA.

(5) On Street Parking

Section 6.9 of the Assessment of Transport Effects of the NoR1 [Link](#) notes that all TLC corridors have existing on-street parking and the Manuia Road local street also has existing on-street parking.

The Project will remove all existing on-street parking spaces along the TLC corridors and there will be no on-street parking on the grade-separated bridges. The purpose of the existing on-street parking that will likely be impacted by the Project are highlighted in Table 37 of the Assessment of Transport Effects of the NoR1 and includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(6) On site Parking

Section 6.9 of the Assessment of Transport Effects of the NoR1 [Link](#) also notes that in some locations of the Project areas, the Project intends to widen the existing designation and alter the cross section of the corridor to incorporate separated walking and cycling facilities, provide space for the bridge or to incorporate space for the accessways. As a result, existing car parking provision for properties adjacent to the Project corridor will be affected. This includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(7) Property Access

Section 6.10 of the Assessment of Transport Effects of the NoR1 [Link](#) notes that in some locations of the Project areas, access arrangements to existing properties will be affected. This includes numerous industrial and commercial businesses (Table 40), and particularly the key entry access point to the Takaanini Town Centre development (30 Walters Road).

This is of concern to the TBA.

(8) Corridor-Specific operational effects

Section 7 of the Assessment of Transport Effects of the NoR1 [Link](#) assesses specific transport matters relating to individual corridors as well as measures to avoid, remedy, or mitigate actual or potential adverse effects.

Spartan Road: Routing for northbound trips onto Great South Road and access to SH1 northbound on-ramp are affected due to Spartan Road level crossing closure. The TBA supports discussions with affected landowners being undertaken to understand the operation of their business and typical access movements. The TBA also agrees with the recommendation that prior to the start of construction, a design safe system audit is undertaken for the Project. This will determine if any additional mitigation measures are required to address any safety risks in regards to the specific elements of the Project.

Manuia Road: The proposed design of the existing Manuia Road layout involves an overpass from Great South Road to Oakleigh Avenue with vehicle and active mode provisions. The TBA agrees that the Manuia Road bridge should be designed to accommodate heavy vehicles and should be aligned with future standards/needs.

Manuora Road: The proposed design of the existing Manuora Road pedestrian and road level crossings involves the closure of the existing at grade level crossings and replaced with a grade separated active modes bridge. There is the potential that the design of the active mode bridge is not suitable for pedestrians and cyclists, resulting in reduced uptake of the active mode connection. The TBA agrees with the recommendation that at detailed design, the active mode connection should be designed appropriately and aligned with engineering design standards to ensure the facility accommodates the needs of pedestrians and cyclists.

Taka Street: The proposed design of the existing Taka Street layout involves an overpass from Great South Road to the roundabout involving Takanini School Road and Kauri Heart Avenue with vehicle

and active mode provisions. There are concerns about the gradient of the proposed Taka Street bridge and the effect on freight. The TBA agrees with the recommendation that at the detailed design stage, the Taka Street bridge should be designed to accommodate heavy vehicles and should be aligned with future standards.

Walters Road: The proposed design of the existing Walters Road layout involves an overpass from Great South Road to the four-legged roundabout with Porchester Road with vehicle and active mode provisions. There are concerns about the gradient of the proposed Walters Road bridge and the effect on freight. The TBA agrees with the recommendation that at the detailed design stage, the Walters Road bridge should be designed to accommodate heavy vehicles and should be aligned with future standards.

(9) Construction Noise and Vibration

Although the TLC - Assessment of Construction Noise and Vibration Effects of the NoR1 [Link](#) suggests that predicted noise levels for the majority of works will be able to comply with the relevant daytime limits, which means that effects are generally acceptable inside neighbouring buildings, the TBA has concerns these will, to the contrary, be significant. Where high noise activities would occur (e.g., demolition of close by buildings, piling of bridges or retaining walls, and earthworks), the TBA agrees that these activities should be completed within limited periods (eg weeks) with highest noise levels for only some hours during the workdays. Overall construction of the bridges will take between 1 to 2 years for active mode bridges and 2.5 to 3 years for road bridges. The Assessment of Construction Noise and Vibration Effects notes that effects can be managed through the application of management and mitigation measures through a Construction Noise Vibration Management Plan ('CNVMP').

The Assessment of Construction Noise and Vibration Effects observes that depending on the final construction methodology and receivers in the vicinity, mitigation and management measures may also include the offer of temporary relocation. The appropriate mitigation measures will be determined on a case-by-case basis throughout construction using the CNVMP and/or site-specific schedules as the implementation tool.

Proposed conditions regarding a Construction Noise and Vibration Management Plan are welcomed by the TBA.

(10) Lapse period

Under section 184 of the RMA, the default lapse date for designations is five years unless the designation provides a different lapse period. The Assessment of Effects on the Environment states that a key objective of the Project is to identify and protect land now for future transport networks and that an extended lapse period of 15 years is reasonably necessary to achieve this, as it provides statutory protection of the transport corridors.

The TBA is concerned that the 15-year lapse period is excessive for this Project and creates far longer uncertainty for the business community.

Dear Sirs,

I hereby lodge my objection to this proposal by Auckland Transport to the changes they propose. Auckland Transport have been predetermined, secretive, and casual about this proposal. Over bridges for these crossings will be a blight on the visual aspects of our Takanini community. A T have not given fair regard to the option of under passes which take up far less land, are cost beneficial, less disruptive in both construction and visually. Staff, who are paid employees of Council and thereby us Rate Payers, have not been fair, professional, or willing to listen to independent professional advice. They have been dismissal of alternative options totally, and not given these any credence or consideration.

I as major stakeholder and an inaugural tenant of the new Takanini Town Centre, and I consider this proposal and the casual approach and dismissal attitude of A T is an insult to the investment and effort we at the Centre have put in over the last 12 + years.

Yours Faithfully,

Brian Hogan

Owner of The Blacksmith Restuarant & Bar

--

Brian Hogan

brian_hogan@eml.cc

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Glenn Dyer

Organisation name: Carter Building Supplies

Full name of your agent:

Email address: glenn.dyer@carters.co.nz

Contact phone number: 0273499137

Postal address:
Private Bag 94027
Auckland
Auckland

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
12 Walters Road, Papakura, Takanini 2112

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The proposal reduces the size of the site that we occupy, it takes off a substantial section off the entry and exit from our premises, meaning that the operations would be further constrained making it almost impossible for Carters to be able to operate our business from the site. Walters Road can already be a very busy road during our operating hours, we believe that this will further constrain and slow traffic making it harder for us to be able to operate from the site if these proposed plans are implemented.

I or we seek the following recommendation or decision from Auckland Council:

Exclude the requirement to take any portion of our site and ensure that there is easy exit and entry to our site.

Submission date: 11 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

CAUTION: This email message and any attachments contain information that may be confidential and may be LEGALLY PRIVILEGED. If you are not the intended recipient, any use, disclosure or copying of this message or attachments is strictly prohibited. If you have received this email message in error please notify us immediately and erase all copies of the message and attachments. We do not accept responsibility for any viruses or similar carried with our email, or any effects our email may have on the recipient computer system or network. Any views expressed in this email may be those of the individual sender and may not necessarily reflect the views of Council.

To whom it may concern,

Having looked at your initial plans and then speaking to Josh and James from Supporting Growth about a rough scope of works, we have had a lot to discuss with our tenant (Beststart kindergarten) regarding the impact these works may have on their operation as a kindergarten. As owners of the property and landlords to Best Start, their operation is integral to our properties value to us. They have been a commercial tenant to us for over 20 years running a very successful operation. They are a very reliable tenant for us.

In summary I will list our main thoughts below:

1. We think that initially ending the through road so that no traffic can pass by will be very detrimental to the exposure that the business has had being on a main route. With far less people being able to see the kindergarten we have serious concerns that it will slowly drop away as people become less aware of its existence, and thus kill the business. For this reason we are strongly opposed.

2. We think if the road is ended and the kindergarten is now forced to operate at the end of a cul de sac the access for parent drop off will now be so difficult the kindergarten may no longer be viable. Currently parents are now able to pull in, drop off, and then continue on to work in the same direction if needs be. The new proposal will require parents essentially to drive in, drop off, and then return the way they came and then drive around the block. Parents coming from the other side of the railway track will also have to drive around the block to get the kindergarten. With Auckland's current traffic problems this could potentially add 30 minutes to a commute, thus deterring the kindergarten's potential clients. What is certain is it will be a whole lot more difficult place to access and many parents may look for easier options, alternate schools etc. For this reason we are strongly opposed to the new plans.

3. Parking in relation to a kindergarten is crucial. The Ministry of Education has strict policies regarding the number of parking spaces required for an ECE to operate. The exact number is determined by local council, which for Auckland, states:

"On-Site Car Parking- Early Childhood Education (Preschool)

4. In addition to any car parking required for the school, on-site car parking for early childhood education (preschool) shall be provided at the rate of one car park per every 10 children the facility is licensed or designed to accommodate, plus one per each full time equivalent staff member required for the license or design capacity of the centre, except where the Council accepts, on the basis of a specifically commissioned parking study by an appropriately qualified engineer and/or transportation planner, that a lesser level is appropriate."

There is a ratio of carparks per child. A reduction/change in the number of parks will have a direct impact on future operations of the kindergarten, or deem it unviable altogether. This is a massive concern for the current operators as the proposed plans look to almost obtain half of the current carparks at our property. This will seriously impact the viability of the business and almost certainly result in the tenant terminating the lease. For this reason we are strongly opposed.

4. Any works that are undertaken are going take a substantial time period with a lot of inconvenience for the parents, children and our kindergarten operators. Firstly, there will be difficulties with access driving in and out. There will likely be air pollution with dust etc. Also noise of construction going on etc. With 180 young children adjacent to the proposed construction works for an extended period of time, child attendance numbers may completely drop as parents seek a less chaotic environment. For this reason we are strongly opposed.

There are many more reasons that we could go into but these are our main concerns. In summary our tenant has serious concerns that this will spell the end of their business viability. We as the owners and landlords are feeling this impact immediately, as the tenant is now indicating they may not renew the lease until they have a complete idea of the extent of the works and the impact it will have on them. For us this is a lease that has provided a reliable income for over 20 years with no problems. It would be a very big loss for us.

Regards,

Jack Downer, Portsmouth Family Trust
021 562 078

Form 21

Submission on requirements for designations

To: Auckland Council
Private Bag 92300
Auckland 1142
unitaryplan@aucklandcouncil.govt.nz

Name of submitter: Aotearoa Towers Group (ATG)
Trading as FortySouth
Private Bag 92161
Auckland, 1142

Chorus New Zealand Limited (Chorus)
PO Box 632
Wellington

Connexa Limited (Connexa)
PO Box 91362
Victoria Street West
Auckland, 1142

One New Zealand (One NZ) (formally Vodafone New Zealand Ltd)
Private Bag 92161
Auckland, 1142

Spark New Zealand Trading Limited (Spark)
Private Bag 92028
Auckland, 1010

These parties are making a joint submission and for the purposes of this submission are referred to collectively as the **Telecommunications Submitters**.

The Proposal:

This is a submission on the following notices of requirement by Auckland Transport for transport projects at the North Island Main Trunk rail line in the Takanini area. These are referred to as the Takanini Level Crossing (TLC) projects:

- Notice of Requirement 1: Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street (Auckland Transport)
- Notice of Requirement 2: Takanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge (Auckland Transport)

The Telecommunications Submitters are not trade competitors for the purposes of section 308B of the Resource Management Act 1991.

The specific parts of the notice of requirement that this submission relates to are:

The conditions of the designations that relate to Network Utility Operators.

The Telecommunications Submitters' submission is that:

The Telecommunications Submitters have no position on the overall Takanini Level Crossing package of transport projects but seek to ensure that existing and potential future telecommunications infrastructure in the project corridors are adequately addressed.

The Telecommunications Submitters **oppose** the proposed designations unless the matters outlined in this submission are satisfactorily addressed.

The organisations collectively deliver and manage the majority of New Zealand's fixed line/fibre and wireless phone and broadband services in New Zealand. The network utility operators in the telecommunications sector deliver critical lifeline utility services (as per Schedule 1 to the Civil Defence Emergency Management Act 2002) including infrastructure to support emergency services calls. It is also crucial for supporting social and economic wellbeing and measures to reduce travel demand. The services provide opportunities for work from home/remote work solutions through fast internet connections by fibre and/or wireless means which promotes a lower carbon economy.

The equipment used to deliver this is often located in road corridors which act as infrastructure corridors as well as just transport corridors. The works enabled by the proposed designations will affect existing and potential future infrastructure that will need to be protected and/or relocated as part of the proposed works. The design and construction of the works should take into account any opportunities for new

infrastructure to be installed which is preferable than trying to retrofit necessary telecommunications/ broadband infrastructure later due to disruptions and/ or incompatibility with project design.

Existing Infrastructure

A summary of existing infrastructure located in the project footprints is as follows and is outlined in more details viewable in **Appendix A**:

- **Connexa Facility:** Light pole located at Taka Street and Great South Road Intersection in NoR 1 [project 4: Taka Street project area] (supporting 2degrees network). This facility is also impacted by the South Frequent Transit Network Project (NoR 1) as both designations overlap each other.
- **FortySouth Facility:** Pole on 20 Walters Street in NoR 2 (supporting One NZ network)
- Chorus has extensive fibre and copper lines networks throughout the project area.
- Mobile operators are progressively rolling out roadside equipment and fibre routes in Auckland roads which may be within project corridors when works proceed.

Future Infrastructure Requirements

Network utility operators need to integrate necessary services into infrastructure projects such as transport projects. This is especially significant for future development with the introduction of advanced technology such as 5G infrastructure, which will be crucial to transport infrastructure. It is most efficient to coordinate any such services with the design and construction of a project, rather than trying to retrofit them at a later date. This process does not always run smoothly. To provide a previous example, Spark, 2degrees and Vodafone (now One NZ) had substantial issues trying to negotiate with the Public Private Partnership (PPP) operator of the Transmission Gully project in the Wellington Region to install services to provide telecommunications coverage. This process proved to be very difficult as there was no requirement to consult and work with relevant network utility operators in the designation conditions, and post completion of the project design and PPP contracting, it proved to be very challenging to try to incorporate necessary telecommunications infrastructure into the design of this project.

Spark achieved a more satisfactory outcome through participation as a submitter in the Auckland East West Link and Warkworth to Wellsford (W2W) project designation conditions where there was a specific obligation for the Requiring Authority to consult with network utility operators as part of the detailed design phase of the project to identify opportunities to enable the development of new network utility

including telecommunications infrastructure where practicable to do so¹. While the Telecommunication Submitters are not asking for the exact same outcomes of these examples, it demonstrates mutual benefits with ease of collaboration, communication and cohesive infrastructure development.

This is reflected in more recent times in two separate occasions earlier this year where Auckland Transport and Waka Kotahi agreed to amend their proposed Network Utility Management Plan (NUMP) conditions to involve network utility operators during the design phase, as well as the inclusion of Land Integration Process (LIP) conditions in Auckland Transport designations. Satisfactory conditions in this regard have been agreed with the requiring authorities in the Airport to Botany and Northwest Transport Projects. Those agreed amendments have been applied to the LIP conditions, however, the NUMP condition have not been carried through to the Takanini Level Crossing projects.

Both NoRs include a NUMP condition in the general conditions (listed as 24), which is not the same as the previously and recently agreed upon NUMP condition wording for the other abovementioned projects. The NUMP conditions used in these NoRs do not include the updated clause “(d) *the development of the NUMP shall consider opportunities to coordinate future work programmes with other network utility operator(s) **during detailed design** where practicable.*”

Further, Spark on behalf of the telecommunication companies has had more recent discussions with SGA representatives on how to have more effective conditions for the various NoRs packages. An SGA representative suggested that design stage is not an actual stage but is instead progressive. Accordingly, further changes to the amended NUMP are sought for:

“(d) *the development of the NUMP shall consider opportunities to coordinate future work programmes with other network utility operator(s) **during the further project stages including detailed design** where practicable.*”

This revised wording is proposed to assure the telecommunication companies has the opportunity to be continued to be involved for future project stages.

Whilst there is no direct obligation on the requiring authority to accommodate such works/opportunities, it is reasonable for there to be provisions to ensure the matter is properly considered during the design phase through consultation with network utility operators as it sets appropriate expectations and ensures these opportunities are properly explored. This enables proper consideration of making provision for communications infrastructure that support the function of the roads and/or serves adjacent growth. This

¹ East West Link Condition NU2, W2W Condition 24A

should be a consideration distinct from protecting or relocating existing network utilities affected by the project which has previously been the focus of conditions to manage network utilities.

Consultation with Telecommunications Network Utility Operators

Key to the outcomes the Telecommunications Submitters are seeking is to ensure they are adequately consulted by the requiring authorities over effects on their existing infrastructure, as well as being provided the opportunity to discuss any future requirements so this can be considered in the project design.

The Assessment of Environmental Effects (AEE) for each notice sets out the relevant utility providers who have assets within and around the proposed designations and is listed in the Network Utility Effects section. However, none of the Telecommunication Submitters except Chorus are listed within the affected Utility Providers despite having existing infrastructure within and around the proposed designated boundaries.

Spark and One NZ operate mobile phone/wireless broadband networks that are often located on facilities located in or adjacent to roads, while Chorus operates fixed line assets in roads including fibre. In addition, Spark has sold its fixed mobile asset infrastructure (e.g., their poles) to Connexa who are also acquiring the fixed assets of 2degrees, and similarly One NZ has sold its fixed mobile assets to Aotearoa Towers Group (trading as FortySouth). Accordingly, the operating landscape for telecommunications companies and who may be affected by these projects has become quite complex.

Land Use Integration Process (LIP)

The Auckland Transport NoRs included a satisfactory LIP condition (Condition 3), reflective of previous agreed upon amendments for the Airport to Botany and Northwest Projects NoRs. Specifically, these were changes to clause (f) and (f)(ii), which enable ongoing communication and opportunities for future infrastructure requirements to be integrated into the NoR projects. The Telecommunication Submitters are supportive of these changes.

The Telecommunications Submitters seeks the following decision from the Requiring Authorities:

Amend the NUMP condition for each notice of requirement, as follows:

Network Utility Management Plan (NUMP)

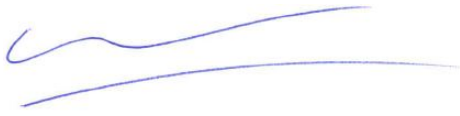
(a) A NUMP shall be prepared prior to the Start of Construction for a Stage of Work.

- (b) The objective of the NUMP is to set out a framework for protecting, relocating and working in proximity to existing network utilities. The NUMP shall include methods to:**
- (i) provide access for maintenance at all reasonable times, or emergency works at all times during construction activities;**
 - (ii) protect and where necessary, relocate existing network utilities;**
 - (iii) manage the effects of dust and any other material potentially resulting from construction activities and able to cause material damage, beyond normal wear and tear to overhead transmission lines in the Project area; and**
 - (iv) demonstrate compliance with relevant standards and Codes of Practice including, where relevant, the NZECP 34:2001 New Zealand Electrical Code of Practice for Electrical Safe Distances 2001; AS/NZS 4853:2012 Electrical Hazards on Metallic Pipelines; and AS/NZS 2885 Pipelines – Gas and Liquid Petroleum.**
- (c) The NUMP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project.**
- (d) The development of the NUMP shall consider opportunities to coordinate future work programmes with other Network Utility Operator(s) during the further project stages including detailed design where practicable.**
- (e) The NUMP shall describe how any comments from the Network Utility Operator in relation to its assets have been addressed.**
- (f) Any comments received from the Network Utility Operator shall be considered when finalising the NUMP.**
- (g) Any amendments to the NUMP related to the assets of a Network Utility Operator shall be prepared in consultation with that asset owner**

Retain the LIP condition.

The Telecommunications Submitters do wish to be heard in support of its submission.

If others make a similar submission, the Telecommunications Submitters will consider making a joint case with them at the hearing.



Signature of submitter
(Chris Horne, authorised agent for the Telecommunications Submitters)

Date: 12 December 2023

Address for service of submitter:

Chris Horne

Incite

PO Box 3082

Auckland

Telephone: 0274 794 980

E-mail: chris@incite.co.nz

Appendix A

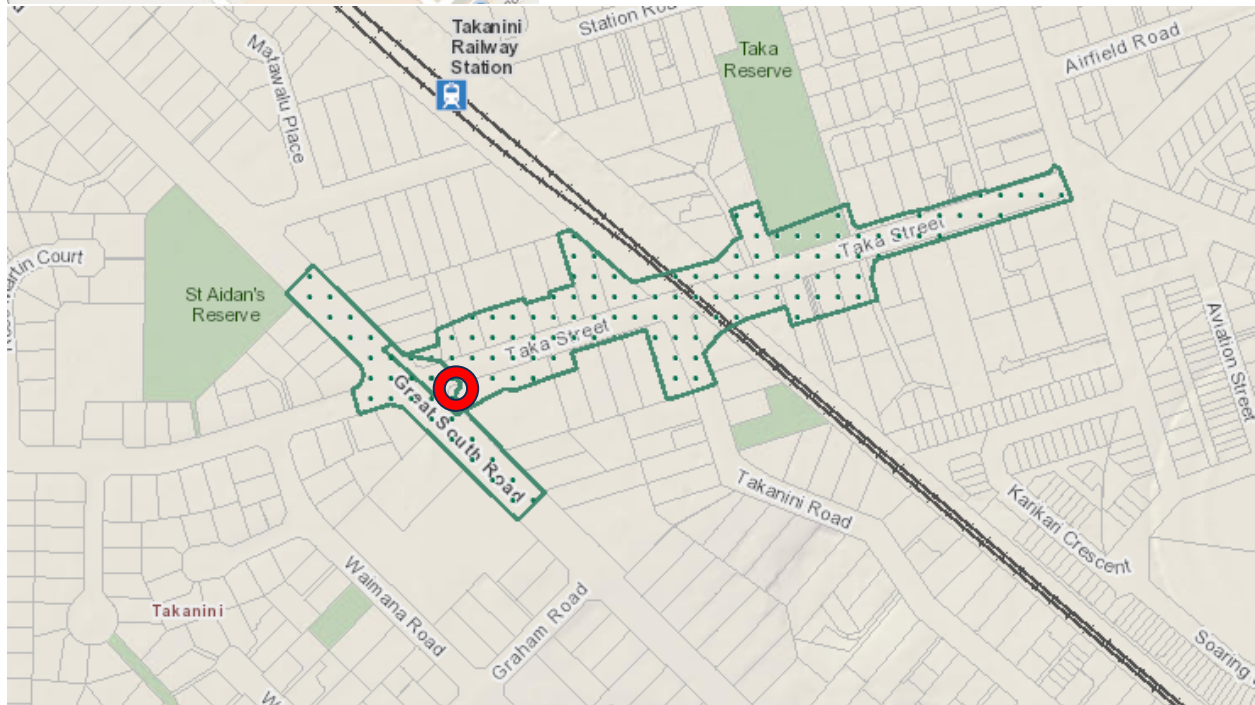
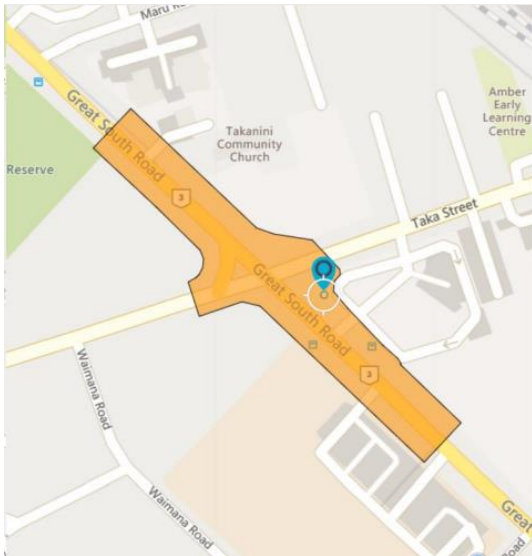
Impacted Telecommunication Facilities

Telecommunication Sites Impacted

Connexa

NoR 1 – Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Spartan Road (Auckland Transport)

- Pole located at Taka Street and Great South Road Intersection in NoR 1 [project 4: Taka Street project area] (supporting 2degrees network)



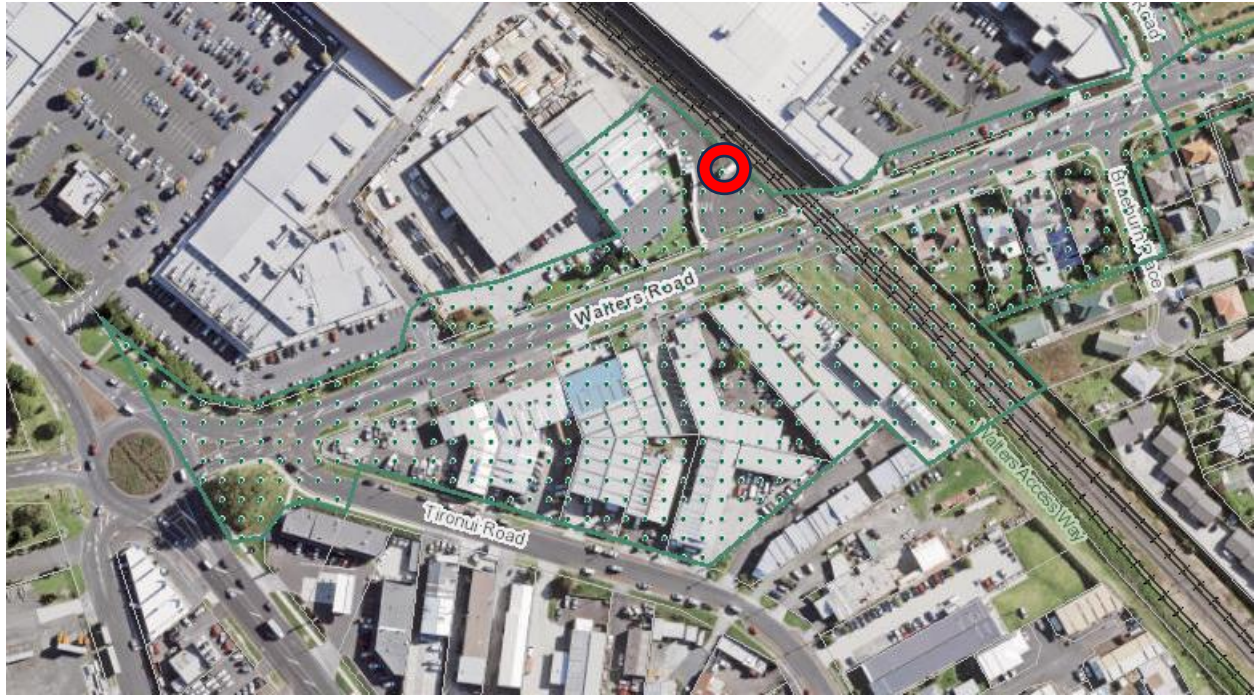


FortySouth

NoR 2: Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (Auckland Transport)

- Pole on 20 Walters Street (supporting One NZ network)





The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Anil Kumar

Organisation name:

Full name of your agent: Anil Kumar

Email address: anidelhi223@gmail.com

Contact phone number: 0212718875

Postal address:
2/25 Takanini Road
Takanini
Auckland 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we support the Notice of Requirement.

The reason for my or our views are:
To provide all the information prior to the work

I or we seek the following recommendation or decision from Auckland Council:
We support the decision of Auckland council

Submission date: 12 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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Before you fill out the attached submission form, you should know:

You need to include your full name, an email address, or an alternative postal address for your submission to be valid. Also provide a contact phone number so we can contact you for hearing schedules (where requested).

By taking part in this public submission process your submission will be made public. The information requested on this form is required by the Resource Management Act 1991 as any further submission supporting or opposing this submission is required to be forwarded to you as well as Auckland Council. Your name, address, telephone number, email address, signature (if applicable) and the content of your submission will be made publicly available in Auckland Council documents and on our website. These details are collected to better inform the public about all consents which have been issued through the Council.

Please note that your submission (or part of your submission) may be struck out if the authority is satisfied that at least one of the following applies to the submission (or part of the submission):

- It is frivolous or vexatious.
- It discloses no reasonable or relevant case.
- It would be an abuse of the hearing process to allow the submission (or the part) to be taken further.
- It contains offensive language.
- It is supported only by material that purports to be independent expert evidence, but has been prepared by a person who is not independent or who does not have sufficient specialised knowledge or skill to give expert advice on the matter.

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification

Sections 168A, 169, 181, 189A, 190, and 195A of the Resource Management Act 1991



FORM 21

Send your submission to unitaryplan@aucklandcouncil.govt.nz or post to :

Attn: Planning Technician
Auckland Council
Level 16, 135 Albert Street
Private Bag 92300
Auckland 1142

For office use only
Submission No:
Receipt Date:

Submitter details

Full Name or Name of Agent (if applicable)

Mr/Mrs/Miss/Ms(Full Name) _____

Organisation Name (if submission is made on behalf of Organisation)

Address for service of Submitter

Telephone: Email:

Contact Person: (Name and designation if applicable)

This is a submission on a notice of requirement:

By: Name of Requiring Authority

Auckland Transport

For: A new designation or alteration to an existing designation

Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

The specific parts of the above notice of requirement that my submission relates to are: (give details including property address):

My submission is:

I or we support of the Notice of Requirement I or we oppose to the Notice of Requirement
I or we are neutral to the Notice of Requirement

The reasons for my views are:

(continue on a separate sheet if necessary)

I seek the following recommendation or decision from the Council *(give precise details including the general nature of any conditions sought).*

- I wish to be heard in support of my submission
- I do not wish to be heard in support of my submission
- If others make a similar submission, I will consider presenting a joint case with them at a hearing



Signature of Submitter
(or person authorised to sign on behalf of submitter)

Date

Notes to person making submission:

If you are making a submission to the Environmental Protection Authority, you should use Form 16B.

You must serve a copy of your submission on the person who gave the notice of requirement as soon as reasonably practicable after you have served your submission on the Council (unless the Council itself, as requiring authority, gave the notice of requirement)

If your submission relates to a notice of requirement for a designation or alteration to a designation and you are a trade competitor of the requiring authority, you may make a submission only if you are directly affected by an effect of the activity to which the requirement relates that:

- (a) Adversely affects the environment, and
- (b) Does not relate to trade competition or the effects of trade competition.

Submission on Notice of Requirement (NoR1) - In Opposition

Takaanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) by Auckland Transport

By: Matthew Koppens & Denise Ibbett

Introduction

Matthew Koppens & Denise Ibbett welcome the opportunity to make a submission on the Takaanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') as we own a property and operate a business located at 26 Oakleigh Avenue, Takanini. This property will be directly and wholly affected by the Manuia Road project area and the proposed designation.

We understand there is a need to find a solution to the congestion issues caused by the current level crossings in the Takanini area as trains become more frequent in the future.

However, we believe there are a number of issues with the TLC project and we do not believe this plan will solve the significant traffic congestion in the area concerned for a number of reasons. We also feel there are better alternatives which;

- would better suit the needs of the local area
- would require less land/properties to be acquired
- would integrate more smoothly with existing and future roading infrastructure.

Therefore, Matthew Koppens & Denise Ibbett *oppose* the grade separation solutions proposed in the Notice of Requirement: Takaanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Submission

Matthew Koppens & Denise Ibbett are submitting on the following Notice of Requirement:

Takaanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

- Our submission relates to the entire Notice of Requirement.
- Matthew Koppens & Denise Ibbett are in opposition regarding the Notice of Requirement.
- Matthew Koppens & Denise Ibbett will not gain an advantage in trade competition through the submission.
- The decision Matthew Koppens & Denise Ibbett seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

Matthew Koppens & Denise Ibbett oppose the Takaanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') for the following reasons:

(1) Traffic Movements & Volumes

- It seems extremely counter-intuitive to close off two busy rail crossings (Spartan Road and Manuroa Road) and funnel all the vehicle movements from both roads (which will likely increase in years to come, with the further development of land east of Takanini School Road) down a minor road (Oakleigh Avenue), around a relatively small proposed roundabout and over a single lane (in each direction) bridge.
- Oakleigh Avenue is not a particularly wide road, it is already overcrowded and is not solely a commercial area – it houses residential properties, a church and kindergarten, among other things. It is not ideally suited to carry a large number of heavy-haul vehicles, wide loads and large truck & trailer units.

- The proposed plan will significantly increase the traffic movements, ground vibration and noise, for the local residents and businesses of Oakleigh Avenue, which, unlike Spartan Road and Manuroa Road, has never been a high-volume thoroughfare. Safety should also be a consideration for the likes of the Counties Manukau Kindergarten Association's Takanini Early Learning and Family Centre, which was purpose built relatively recently.
- Manuroa Road is currently the over-dimension route through the Takanini industrial area (along Porchester Road, Popes Road, Takanini School Road, onto Manuroa Road) connecting with Great South Road. The proposed closure of the Manuroa Road crossing will change the current over-dimension routing through the Takanini industrial area. A new over-dimension route would initially follow Manuroa Road (as above) but would then divert off Manuroa Road onto Oakleigh Avenue, negotiate the proposed roundabout, then cross the Manuia Road bridge to the intersection with Great South Road. This adds extra turns to the route, all of which would need to be well designed in terms of width and height in order to allow for over-dimension vehicles. Overhead powerlines may also need to be considered and moved underground.

(2) Interface with Great South Road

- As a regular user of the current rail crossings and intersections with Great South Road, it is readily apparent that the cause of the congestion on Manuroa Road and Spartan Road is not just due to the rail-vehicle crossings/barrier arms. Although barriers will be lowered for longer periods of time as future train journeys increase, it is actually the traffic light intersections at Great South Road that cause the biggest queues during peak times. This will not change with the addition of the proposed Manuia Road bridge – we predict there will be gridlock over the proposed bridge and along Oakleigh Avenue, which will be exacerbated by the issues highlighted in point (1) above and (3) below.
- We believe there has not been adequate consideration of the way the new bridges will connect with Great South Road. When we raised this issue with the planners at Supporting Growth we were simply told 'that's a separate issue, it's not the focus of this project'. This kind of *'not our problem'* attitude is short sighted and blinkered – it does not provide for 'future-proofing' or for the robust transport network Auckland desperately needs.

(3) Connectivity with State Highway 1 (SH1)

- The closure of the Spartan Road crossing to vehicular traffic will mean that any traffic from business on the west side of the closure (e.g. VTNZ and Halls) that need to head north on SH1 will be required to turn south onto Great South Road, cross over the proposed Manuia Road bridge heading east, make a u-turn around the proposed roundabout at the Oakleigh Avenue intersection, cross the bridge again heading west, then make a right turn at the traffic light intersection with Great South Road and finally merge with SH1. As well as adding time to the journey of those vehicles, this will cause increased volumes of traffic in both directions over the proposed Manuia Road bridge and therefore more congestion.
- The roundabout intersection at Oakleigh Avenue must therefore be designed to allow for 'u-turns' of large truck & trailer units, which, due to the closure of Spartan Road, will need to double back across the Manuia Road bridge in order to access the SH1 heading north. Some businesses may choose to relocate due to the adverse effects on their access to roading infrastructure, meaning the Takanini community/area will likely lose employment opportunities and services, as alternative commercial sites are not readily available in the Takanini area.

(4) Future-proofing for connections with Mill Road

- Similarly to point (2) above, we believe there has not been adequate consideration of the way the new Manuia Road bridge will connect with the potential infrastructure upgrade at Mill Road. During

consultation with Supporting Growth we were informed that there has been no consideration of how the roundabout at Oakleigh Avenue could potentially link to Mill Road (a project that now could be back on the table). Again we are told 'that's a separate issue, it's not the focus of this project'. There seems to be a lack of 'Future-Proofing' to consider how infrastructure projects will potentially connect with each other.

- We believe further investigation should be carried out into location the bridge and vehicle corridor further north in order to better connect with Popes Road, which is a likely traffic corridor east to Mill Road.

(5) Active Mode Bridges at Spartan Road and Manuroa Road.

- The provision of a grade-separated active mode bridge at Spartan Road raises concern for the safety of pedestrians and cyclists entering this area. There are already issues in this area with double parking along the busy roads, frequent passage of commercial heavy vehicles and boy-racers/speeding. It seems unwise to encourage cyclists and pedestrians to frequent this area. We also question the likely uptake of this active mode connection, considering it leads directly to the commercial area with little or no residential housing or community facilities.
- We note that the TLC Project Assessment of Alternatives document broadly shows consideration of other options, but we would advocate for a more detailed investigation of using Manuroa Road as an alternative location of a multi-modal vehicle crossing. This route is already the over-width vehicle route, and it appears there would be less disruption with fewer properties affected if the proposed bridge was situated at Manuroa Road. Even under the current proposed plan heavy vehicles would still run along the majority of Manuroa Road before diverting onto Oakleigh Avenue, so the cost of locating a bridge at Manuia Road does not seem to outweigh the stated benefit of removing heavy vehicles from a residential area. The serious consideration of an underpass or raising the rail lines at this site should also be investigate in more detail to see if this would be a viable solution or could mitigate some of the effects of the proposed project.

(6) Lapse period

- Under the RMA, the default lapse date for designations is five years unless the designation provides a different lapse period. The Assessment of Effects on the Environment states that a key objective of the Project is to identify and protect land now for future transport networks and that an extended lapse period of 15 years is reasonably necessary to achieve this, as it provides statutory protection of the transport corridors.
- However, this extended 15 year time period leaves an unreasonable burden on land owners. Having a long lapse period does not help to mitigate the negative effects of the designation on property owners. A long lapse period *increases* the uncertainty faced by property owners as the timeframes being given for the project are both wide and inconsistent. This actually makes future planning significantly more difficult, leaves the property owner in 'limbo' for an extended period, and leaves a blight on the land in terms of the ability to develop, lease and sell the property in the future.

(7) Impacts on the Submitter

- As an owner/operator of a small family business, this property is a massive investment for now and for the future. Being able to own and occupy the building you operate from is no small feat, and has involved both hard work and sacrifices along the way.
- The added ability of having a separate portion of the building available for lease is a valuable source of secondary income which can offset mortgage payments and support business income where necessary.

- This site was chosen specifically for its location close to the motorway, close to suppliers and close to site locations. The building was designed and built relatively recently (2010) specifically to suit the needs of our business.
- As far as we are aware, our entire section of land will be designated and acquired for the purpose of the planned bridge at Manuia Road. We are extremely concerned that we will not be able to find a like-for-like building within the same area, with good motorway access. We will likely have to leave the area, which will have a negative impact on our business and those we employ.
- We also have an excellent tenant to consider through the process. If we lose this tenant due to the uncertainty of the process/timeframes, or are unable to find a property that suits the needs of our tenant, this will also have a negative impact on our business.
- The extended and uncertain timeframe of this project leaves our property, business and lives in 'limbo'. We feel our options are significantly limited, for example, we cannot afford to purchase a 'back-up' property should one suddenly become available, yet we feel we should not invest in other potential opportunities 'just in case'. We are unable to develop the site if we felt the need to do so in the future. If there was a need to sell the property we feel that we would not achieve true 'market value' yet the process of Early Acquisition is likely to be lengthy. These are just a few examples of the impacts we can envisage, but are likely not the full extent of the issues.
- Additionally, the stress and anxiety being endured on a personal level is immense and impossible to adequately describe. The token offer of a counselling session is completely ineffective when what is needed is some willingness to work flexibly to provide a plan and tailored solutions for those directly affected. These stresses include, but are not limited to;
 - frustrations with the lack of consultation and poor quality of information provided by Supporting Growth and Auckland Transport
 - worries about the potential loss of income and negative effects on our business now and in the future.
 - lack of experience/knowledge of the process and the need to pay lawyers and consultant for advice and support, even at such an early stage of the process.
 - anxiety about the lack of available alternative sites/buildings in the area and the likelihood that a like-for-like property will not be found.
 - The realisation that the 'life-plan' we worked hard to achieve over many years has been significantly changed and is currently no longer in our control.

(8) The Consultation Process

- There have been a number of opportunities to attend public consultation sessions regarding the TLC project and we have attended several of them. However, the information available at these sessions is non-specific, often conflicting and not especially helpful.
- At our request, and that of our immediate neighbours, there have also been several meetings with Supporting Growth / Auckland Transport. These meetings feel like an opportunity to pay 'lip-service' to the statutory requirement to consult with the local community. There is a general disregard and lack of respect for ideas suggested by property owners and no willingness to look at other options that might mitigate some of the negative effects of the TLC project on the properties involved or the local area. Alternative suggestions and potential concerns about the impacts of the project fall on deaf ears and are often not even acknowledged by return email. In our experience documentation from meetings is not provided, must be specifically requested, and is lacking detail when it arrives. Discussion between neighbours has been actively discouraged and until recently, information given has been piece-meal and fragmented under the guise of privacy.

(9) Alternative Options

- We, the submitters, believe there should be a more robust investigation of alternative options that have a better cost-to-benefit outcome. We do not see evidence of a fully comprehensive assessment of the alternative options in the NoR documentation.
- Specifically, we would request that there be further consideration of a grade-separated crossing at Manuroa Road. We would like to see all options carefully investigated in terms of socio-economic impacts, environmental effects, traffic and costs benefits of all options - an underpass, raised rail and overpass.
- We would also request further, more robust investigations of the alternative grade-separated crossings further north, as noted on page 38 of the Assessment of the Effects in the Environment (AEE) - Appendix A – Assessment of Alternatives, particularly option 4.3 (or a variation of this to provide an intersection with Great South Road that is further from SH1).

Outcome Sought

- (a) We the submitters, Matthew Koppens and Denise Ibbett, seek that the NoR's referred to above are declined in their current form.
- (b) In the event that the NoR's are not declined, we would seek the designations are amended to avoid remedy or mitigate the concerns raised in the submission, including but not limited to;
- i. Investigation of other options that avoid a designation over the property at 26 Oakleigh Avenue Takanini
 - ii. Modify the design of the proposed project to minimise the impact on 26 Oakleigh Avenue allowing for contained use of the site and building for its current purpose
 - iii. Minimising and temporary or permanent effects of the TLC project in the ongoing operations of the site, including access and manoeuvring.
- (c) In the event that designations are not amended, we seek the opportunity to enter into discussions with the acquiring authority to seek compensation under the Public Works Act 1981 including, but not limited to;
- i. provision of a like-for-like site and property under the Public Works Act 1981
 - ii. Full loss of income relating to land and lease arrangements including current and potential rental income for the duration of the 15 year implementation timeframe, or beyond should it extend.
 - iii. Toll on personal health and wellbeing as a result of stress relating to the process
 - iv. Injurious effect

We would welcome the opportunity to enter into discussions with Auckland Transport on the concerns raised in this submission and would appreciate the opportunity to be heard in support of this matter.



Matthew Koppens & Denise Ibbett

Address For Service:

PO Box 474,
Drury 2247

Tel: 021 744 747

Email: admin@koppensdrainage.co.nz

Before you fill out the attached submission form, you should know:

You need to include your full name, an email address, or an alternative postal address for your submission to be valid. Also provide a contact phone number so we can contact you for hearing schedules (where requested).

By taking part in this public submission process your submission will be made public. The information requested on this form is required by the Resource Management Act 1991 as any further submission supporting or opposing this submission is required to be forwarded to you as well as Auckland Council. Your name, address, telephone number, email address, signature (if applicable) and the content of your submission will be made publicly available in Auckland Council documents and on our website. These details are collected to better inform the public about all consents which have been issued through the Council.

Please note that your submission (or part of your submission) may be struck out if the authority is satisfied that at least one of the following applies to the submission (or part of the submission):

- It is frivolous or vexatious.
- It discloses no reasonable or relevant case.
- It would be an abuse of the hearing process to allow the submission (or the part) to be taken further.
- It contains offensive language.
- It is supported only by material that purports to be independent expert evidence, but has been prepared by a person who is not independent or who does not have sufficient specialised knowledge or skill to give expert advice on the matter.

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification

Sections 168A, 169, 181, 189A, 190, and 195A of the Resource Management Act 1991



FORM 21

Send your submission to unitaryplan@aucklandcouncil.govt.nz or post to :

Attn: Planning Technician
 Auckland Council
 Level 16, 135 Albert Street
 Private Bag 92300
 Auckland 1142

For office use only
Submission No:
Receipt Date:

Submitter details

Full Name or Name of Agent (if applicable)

Mr/Mrs/Miss/Ms(Full Name) Natasha Rivai (Agent)

Organisation Name (if submission is made on behalf of Organisation)

Oceania Healthcare C/- The Property Group Limited

Address for service of Submitter

PO Box 104 Shortland Street
 Auckland 1010

Telephone: Email:

Contact Person: (Name and designation if applicable)

This is a submission on a notice of requirement:

By: Name of Requiring Authority	Auckland Transport
For: A new designation or alteration to an existing designation	Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

The specific parts of the above notice of requirement that my submission relates to are: (give details including property address):

Designation affecting properties at 9, 11 and 13 Taka Street, Takaanini, and future access to 3 Takanini Road, Takaanini.

My submission is:

I or we support of the Notice of Requirement I or we oppose to the Notice of Requirement
 I or we are neutral to the Notice of Requirement

The reasons for my views are:

Refer to Attached submission.

(continue on a separate sheet if necessary)

I seek the following recommendation or decision from the Council (give precise details including the general nature of any conditions sought).

Refer to Attached submission.

- I wish to be heard in support of my submission
- I do not wish to be heard in support of my submission
- If others make a similar submission, I will consider presenting a joint case with them at a hearing

Natasha Pivai

12/13/2023

Signature of Submitter
(or person authorised to sign on behalf of submitter)

Date

Notes to person making submission:

If you are making a submission to the Environmental Protection Authority, you should use Form 16B.

You must serve a copy of your submission on the person who gave the notice of requirement as soon as reasonably practicable after you have served your submission on the Council (unless the Council itself, as requiring authority, gave the notice of requirement)

If your submission relates to a notice of requirement for a designation or alteration to a designation and you are a trade competitor of the requiring authority, you may make a submission only if you are directly affected by an effect of the activity to which the requirement relates that:

- (a) Adversely affects the environment, and
- (b) Does not relate to trade competition or the effects of trade competition.

**SUBMISSION ON A NOTIFIED NOTICE OF REQUIREMENT FOR DESIGNATION OF LAND BY
AUCKLAND TRANSPORT**

Section 168(2) of the Resource Management Act 1991

To: Auckland Council, Plans and Places
Private Bag 92300 Auckland 1142
Attention: Planning Technician

Oceania Healthcare c/- The Property Group, at the address for service set out below (the “**Submitter**”) makes the following submission in relation to the notice of requirement lodged by **Auckland Transport** in respect of:

- Takaanini Level Crossing (TLC): Taka Street (NoR 1): a new designation, for a multi-modal bridge crossing of the North Island Main Trunk (NIMT) at Taka Street (the “**Taka Street NoR**”).
1. The AEE states that the above NoR is one of the four transport works packages proposed as part of Te Tupu Ngātahi Supporting Growth programme to respond to both existing transport deficiencies as well as provide for the forecasted future growth pressure in the Takaanini area. It is understood that the proposed works include the closure of the existing level crossing, construction of a new bridge with general traffic lanes and walking and cycling facilities across the NIMT rail line and associated works.
 2. The Submitter will be directly affected by the NoRs as it owns the properties at 9, 11 and 13 Taka Street, legally described as Lot 91, 90, 89 DP 9255 (the “**Site**”), parts of which come within the designation boundaries. The Submitter also owns the property at 3 Takanini Road, Takaanini, legally described as Lot 1 DP 314399 that does not appear to be affected by the proposed designation boundaries.
 3. The Submitter has operated the Takaanini site for the last 14 years. It provides rest home, hospital, dementia and palliative level care to its approximately 90 patients/residents, and maintains a relatively high and continuous occupancy level. There is a high need for this healthcare facility due to the large and growing population of the immediate area where there are few of/limited similar facilities.
 4. The patients at this facility are cared for by approximately 96 staff on rotation, in part and full time capacities, for the various requirements of the facility. The facility has three dedicated visitor car

parking spaces onsite, and 21 spaces for staff. Staff also rely on public transport and the adjacent childcare centre.

5. The Submitter acknowledges that consultation has been undertaken by SGA, and whilst this Application is for route protection, the eventual works and structure will have significant adverse impacts on the function and amenity of the healthcare facility.
6. The Submitter is not a trade competitor of the Requiring Authority for the NoRs and could not gain any advantage in trade competition through this submission.
7. The Submitter is not opposed in principle to the NoRs, and supports the Project, but seeks to ensure that:
 - a. For the full duration of the construction phase of works, appropriate management and mitigation is required to reduce the effects of nuisance including but not limited to noise, vibration, dust, and traffic, and on the amenity of the residential environment, specifically the healthcare facility.
 - b. The detailed design of the works incorporates careful consideration of and facilitates the provision of access and parking to the site for staff, visitors, emergency services and loading and unloading facilities.

Reasons for submission

8. The reasons for the submission are as follows and particularly relate to both the impacts during the construction phase and the permanent impacts of the level crossing structure:
 - a. Vehicle access from Taka Street and Takanini Road
 - i. During the construction period, vehicle access for staff, visitors and deliveries to the site will be severed from Taka Street. This will adversely impact on the ease of accessibility for staff and visitors to the healthcare facility.
 - ii. Where the proposed access slip lane connects with the site boundary, this will conflict with buildings within the site and there is no guarantee that two-way vehicle access can be provided without changes to the buildings on site.
 - iii. Access through the slip lane will require significant changes to parking and landscaping areas on site to enable vehicles to turn around. Refuse trucks, delivery trucks, emergency vehicles, visitors and staff vehicles will all need to turn around on site to exit back through the slip lane.

- iv. The proposed slip lane provides no turning head and any traffic that enters the slip lane not relating to the facility is likely to enter the site to turnaround – where onsite manoeuvring may already be reduced.
 - v. The point at which the slip lane connects with Taka Street is very close to the signalised intersection with Great South Road. The slip lane will be continuously blocked by queued vehicles making access and egress from the site extremely difficult. It is anticipated that the slip lane is likely to be limited to left turns only to and from Taka Street, making access for visitors challenging.
 - vi. If Takanini Road is converted to a cul de sac, this will adversely impact on the current accessibility for daily laundry deliveries/collection, waste management collections, and emergency services vehicles.
 - vii. During and after construction, maintaining ease of access for emergency vehicles (i.e. fire trucks, ambulance, police, etc) 24/7 is an essential part of the healthcare facility to maintain the safety, health and wellbeing of staff and patients. This is particularly of concern with respect to maintaining a designated safe evacuation zone for the residents and staff that is away from the driveway access points.
- b. Public transport connections – rail and bus connections
- i. It is not clear how pedestrian access and connection to the railway station for staff and visitors will be maintained for the duration of the works. Staff and visitors at the healthcare facility rely on public transport, and the proposed works will adversely impact on this access and their safety.
 - ii. Similarly, safe and legible pedestrian access and connection to local bus stops need to be maintained throughout the construction phase as well as future-proofed following completion of the construction works. This accessibility to public transport is essential to the staff and visitors of the healthcare facility.
- c. Car parking on site and off site
- i. The construction phase of works will result in a loss of off and on site car parking for staff and visitors to the healthcare facility. Only 50% of staff are able to park on site, but with a reduction in parking, demand for parking on street will increase. Staff working shifts will need to have parking available as public transport is not always available. This will have an adverse impact on the wellbeing of staff and visitors.

- ii. The temporary reduction in area will result in reduced onsite manoeuvring for any available staff and visitors spaces as well as for delivery and emergency vehicles.
- d. Construction impacts
- i. The duration of the construction period is likely to adversely impact on the function and amenity of the healthcare facility from effects of:
 - 1. Construction noise impact the quiet enjoyment of the facility by residents;
 - 2. Vibration impact from piling activities;
 - 3. Dust nuisance which can contribute to health and respiratory issues and forcing windows to be kept closed;
 - 4. The temporary construction fencing impacting visual amenity from the site;
 - 5. Loss of mature trees and landscaping impacting amenity for residents.
- e. Visual impacts from the frontage of the site/Taka Street
- i. The construction works will likely entirely screen the healthcare facility from the Taka Street frontage making the facility illegible and difficult to find for visitors.
 - ii. The healthcare facility will face temporary adverse landscape and visual amenity impacts for the duration of the construction works. The anticipated 3 year construction period is a reasonable duration that does raise concern for the quiet enjoyment of the facility by patients as well as staff.
 - iii. The permanent structure itself will adversely impact on the legibility of the site/facility but also the views from the facility over Taka Street. View Point 12 in the Landscape, Natural Character and Visual Effects Assessment illustrates an approximate height of a substantial bridge structure that will cross directly in front of the site. From the healthcare facility, staff and patients will effectively have an outlook directly into the structure as opposed to the existing situation that is to mature trees and over the street. This outlook to the structure is a significant adverse effect on the residential amenity appreciated from the facility.
 - iv. Further, the site is located to the south of the structure, and the bulk will result in adverse shading effects and reduced daylight across this property and into the buildings.

f. Loss of childcare facility

- i. It is understood that the existing childcare facility on the opposite side of Taka Street will be removed as a result of the works. This is a huge loss for the staff at this healthcare facility that rely on the centre and over time have built a strong connection between the two facilities.

g. Property and financial impacts

- i. The loss of car parking spaces during the construction period will impact access to the facility by staff and visitors. There are costs associated with securing car parking elsewhere for staff and visitors.
- ii. The disconnection with the road frontage and reduced prominence of the facility will impact the visibility and accessibility of the facility. This reduced prominence is also an impact from the bridge structure directly in front of the site. This could have cost implications on the business.
- iii. The land requirement area will impact on future development potential of the site/required area for both the temporary works and permanent structure in terms of setbacks from the structure.
- iv. Noise during construction and from the use of the bridge will have adverse impacts on the enjoyment of the property/facility, potentially making the facility undesirable for existing and new residents.
- v. The requirement, construction and future structure will have adverse amenity impacts in terms of the loss of landscaping, fencing and roadside appeal of the facility.
- vi. The permanent bridge structure will result in loss of natural light, adverse shading and impact on air quality for the facility.

9. Unless the relief sought in this submission is granted, the NoRs will:

- i. Not promote the sustainable management of natural and physical resources;
- ii. Not amount to and promote the efficient use and development of resources;
- iii. Will not enable people and communities to provide for their social and cultural wellbeing or for their health and safety;

- iv. Overall, be inconsistent with the purpose and principles in Part 2 of the Resource Management Act 1991 (“**RMA**”);
- v. Generate significant adverse effects on the environment, and in particular, on the Site; and
- vi. Not warrant being confirmed by Council under section 171 RMA.

10. In particular, but without derogating from the generality of the above, the Submitter generally supports the safety improvements proposed by the NoRs, which seek to remove the rail level crossing at Taka Street. The Submitter is concerned that the construction duration outlined will result in extended adverse impacts on the health and safety of the community of the healthcare facility, residential amenity and ongoing operations of the facility.

Relief sought:

11. The Submitter seeks that the NoR be accepted provided conditions are imposed on the designations to ensure that it:

- a. Maintains the existing level of service for access to the site with potentially new parking and access provided from Takanini Road.
- b. Providing a connection through the site between the proposed slip lane and Takanini Road to improve accessibility for delivery vehicles and emergency service vehicles and in particular provide a dedicated safe evacuation zone.
- c. Implement measures to ensure non-healthcare facility traffic can turn around within the slip lane without needing to enter the site.
- d. Maintain the current number of parking spaces off-street in an accessible location for staff and visitors both during construction and once completed.
- e. Maintain safe and legible access for visitors and staff to public transport stops.
- f. Appropriate management and engagement with the operator prior to and during construction to ensure the safety and comfort of the patients and staff is maintained.
- g. Provision of appropriate landscaping along the Taka Street frontage to maintain the frontage, amenity and street presence of the facility.
- h. Such other conditions, relief or other consequential amendments as are considered appropriate or necessary to address the matters outlined in this submission.

12. If the above relief is not accepted, the Submitter seeks that the NoRs be declined.
13. The Submitter wishes to be heard in support of its submission.
14. If other parties make a similar submission, the Submitter would consider presenting a joint case with them at any hearing.

DATED this 13 December 2023

Oceania Healthcare by its duly authorised agents, The Property Group



Natasha Rivai

Planner Manager, Auckland & Northland

ADDRESS FOR SERVICE:

The Property Group Limited

PO Box 104 Shortland Street

Auckland 1010

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Penelope Savidan

Organisation name: B&F Papers Ltd

Full name of your agent: Grant Hewison

Email address: penelopes@bfpapers.co.nz

Contact phone number: +64 27 736 3567

Postal address:
33 Oakleigh Avenue
Takanini
Auckland 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
The entire NOR

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The reasons for B&F Papers Ltd being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are: (1) Oversight of our property at 33 Oakleigh Avenue, Takanini – and landlocked property We are concerned that the application documents for NoR1 appear not to have identified our company property at 33 Oakleigh Avenue, Takanini (which is located at the end of Hitchcock Road). While the application documents list both our neighbours (15 Oakleigh Avenue and 39 Oakleigh Avenue) as properties whose access will be affected by the construction of the roundabout at Hitchcock Road (see page 119 - Table 40 of the Assessment of Transport Effects of the NoR1 Link), the Assessment appears to have failed to acknowledge our business. B&F Papers Ltd are tucked away at the end of Hitchcock Road and visitors find us difficult to locate (despite our property being 1ha in size). We also hold concerns that the Assessment of Transport Effects document states that the Project cannot landlock any properties. However, we have concerns that our property has been overlooked and may be landlocked. (2) Heavy Vehicle usage of Hitchcock Road Further to our concern that our property has been overlooked is Appendix B of the Assessment of Transport Effects of the NoR1, which suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%). Contrary to this, we import approximately 400 containers a year and de-van these on site and have multiple trucks doing multiple runs per day delivering to customers, so there is, in our view, considerable usage of Hitchcock Road. Not only is our access 100% affected by the construction of the Project, but temporary access will be required by heavy vehicles to our site. (3) Access to Hitchcock Road Further to our concerns that our property has been overlooked in terms of heavy vehicle usage generally, we are also concerned about our access being significantly affected during construction, which will require temporary access during this period. Table 40 of the Assessment of Transport Effects of the NoR1 suggests that access to 15 Oakleigh Avenue and 39 Oakleigh Avenue can be provided via Oakleigh Ave instead of Hitchcock Road during construction, with Hitchcock Road access reinstated after construction, but it is not clear what options are available to us. We must be able to continue to operate during this

construction period and we wish to emphasise that we cannot access our property via any other road, other than Hitchcock Road, despite our address being 33 Oakleigh Avenue. (4) Manuia Crossing The Assessment of Transport Effects notes that in the 'do-nothing' scenario that the vehicle to capacity (V/C) on Spartan Road and Manuroa Road is at 100% and 60-80% respectively. The Assessment of Transport Effects further states that these road users at both existing level crossings will be redirected to the proposed new Manuia Rd bridge. The Assessment further proposes that traffic from Halls Logistics (western side of the railway on Spartan Road) will be re-routed to use the new Manuia bridge in both directions for no other purpose than to use the new Oakleigh Avenue roundabout as a u-turn opportunity. So not only are two level crossings that are either at or close to V/C to be re-routed to a single lane (in each direction) bridge at Manuia, but further pressure will be put on this access by heavy trucks crossing it for no other purpose than to u-turn. Furthermore, this route is being proposed for inclusion as an over-dimension route when it wasn't before. This will add further pressure to the proposed Manuia bridge crossing. We cannot see how the proposed Manuia bridge can handle the traffic from two closed level crossings, that are also being incrementally used for u-turning, particularly if the interchange is deemed unsafe to do so by a future safety study, and is also now being included in the over-dimension traffic planned routes. The Assessment of Transport Effects also notes that these are 2038 projections. However, it is clear to us and anyone who uses these roads now that these roads are at capacity during peak travel times in 2023. The vast majority of traffic on Manuroa Road turns right (north) during peak travel times, thus making Taka Road (to the South) an unlikely alternative as not only is it to the south, but there isn't a direct link due to the Takanini Railway Station. Furthermore, Alfriston Road (to the north) is substantially farther away requiring a lot more suburban travel time and is unlikely to be an alternative route for users of the Takanini Industrial area. (5) Modified Stream Channel Page 47 of the Assessment of the Effects on the Environment highlights that there is a "modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area." This stream channel crosses our property at 33 Oakleigh Avenue and was paid for by us as part of our development consents. It is also outside the Project area so it is not clear how this will be impacted by the Project. Page 72 of the Assessment of the Effects on the Environment notes that the Project will "improve existing culverts capacities and/or provide new stormwater infrastructure which improve ponding and stream flow in the area", but it is not made clear whether this will affect our property. (6) Assessment of construction noise and vibration effects Our property at 33 Oakleigh Avenue, accessible via Hitchcock Road, has not been included in the Assessment of Construction Noise and Vibration Link. We are very concerned that Figure 4 on page 35 ('Indicative piling locations and designation overlay (Manuia Road)') does not even show 33 Oakleigh Avenue on the map of affected properties, despite our very close proximity to the construction of both the Manuia bridge and the new roundabout at the intersection of Oakleigh Avenue and Hitchcock Road (our building is approximately 20m from the end of Hitchcock Road).

I or we seek the following recommendation or decision from Auckland Council:
Refuse the application

Submission date: 13 December 2023

Supporting documents
20231211 Submission on NOR - Takanini Level Crossing - BF Papers [F].pdf

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

B&F Papers Ltd

Introduction

B&F Papers Ltd is a company located at 33 Oakleigh Avenue, Takanini.

B&F Papers Ltd welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). [Link](#)

Submissions

The Notice of Requirement being submitted on by B&F Papers Ltd is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

B&F Papers Ltd is in opposition regarding the Notice of Requirement.

B&F Papers Ltd will not gain an advantage in trade competition through the submission.

The decision B&F Papers Ltd seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for B&F Papers Ltd being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

(1) Oversight of our property at 33 Oakleigh Avenue, Takanini – and landlocked property

We are concerned that the application documents for NoR1 appear not to have identified our company property at 33 Oakleigh Avenue, Takanini (which is located at the end of Hitchcock Road).

While the application documents list both our neighbours (15 Oakleigh Avenue and 39 Oakleigh Avenue) as properties whose access will be affected by the construction of the roundabout at Hitchcock Road (see page 119 - Table 40 of the Assessment of Transport Effects of the NoR1 [Link](#)), the Assessment appears to have failed to acknowledge our business.

B&F Papers Ltd are tucked away at the end of Hitchcock Road and visitors find us difficult to locate (despite our property being 1ha in size).

We also hold concerns that the Assessment of Transport Effects document states that the Project cannot landlock any properties. However, we have concerns that our property has been overlooked and may be landlocked.

(2) Heavy Vehicle usage of Hitchcock Road

Further to our concern that our property has been overlooked is Appendix B of the Assessment of Transport Effects of the NoR1, which suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%).

Contrary to this, we import approximately 400 containers a year and de-van these on site and have multiple trucks doing multiple runs per day delivering to customers, so there is, in our view, considerable usage of Hitchcock Road. Not only is our access 100% affected by the construction of the Project, but temporary access will be required by heavy vehicles to our site.

(3) Access to Hitchcock Road

Further to our concerns that our property has been overlooked in terms of heavy vehicle usage generally, we are also concerned about our access being significantly affected during construction, which will require temporary access during this period.

Table 40 of the Assessment of Transport Effects of the NoR1 suggests that access to 15 Oakleigh Avenue and 39 Oakleigh Avenue can be provided via Oakleigh Ave instead of Hitchcock Road during construction, with Hitchcock Road access reinstated after construction, but it is not clear what options are available to us.

We must be able to continue to operate during this construction period and we wish to emphasise that we cannot access our property via any other road, other than Hitchcock Road, despite our address being 33 Oakleigh Avenue.

(4) Manuia Crossing

The Assessment of Transport Effects notes that in the 'do-nothing' scenario that the vehicle to capacity (V/C) on Spartan Road and Manuroa Road is at 100% and 60-80% respectively.

The Assessment of Transport Effects further states that these road users at both existing level crossings will be redirected to the proposed new Manuia Rd bridge. The Assessment further proposes that traffic from Halls Logistics (western side of the railway on Spartan Road) will be re-routed to use the new Manuia bridge in both directions for no other purpose than to use the new Oakleigh Avenue roundabout as a u-turn opportunity. So not only are two level crossings that are either at or close to V/C to be re-routed to a single lane (in each direction) bridge at Manuia, but further pressure will be put on this access by heavy trucks crossing it for no other purpose than to u-turn.

Furthermore, this route is being proposed for inclusion as an over-dimension route when it wasn't before. This will add further pressure to the proposed Manuia bridge crossing. We cannot see how the proposed Manuia bridge can handle the traffic from two closed level crossings, that are also being incrementally used for u-turning, particularly if the interchange is deemed unsafe to do so by a future safety study, and is also now being included in the over-dimension traffic planned routes.

The Assessment of Transport Effects also notes that these are 2038 projections. However, it is clear to us and anyone who uses these roads now that these roads are at capacity during peak travel times in 2023. The vast majority of traffic on Manuroa Road turns right (north) during peak travel times, thus making Taka Road (to the South) an unlikely alternative as not only is it to the south, but there isn't a direct link due to the Takanini Railway Station. Furthermore, Alfriston Road (to the north) is substantially farther away requiring a lot more suburban travel time and is unlikely to be an alternative route for users of the Takanini Industrial area.

(5) Modified Stream Channel

Page 47 of the Assessment of the Effects on the Environment highlights that there is a "modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area."

This stream channel crosses our property at 33 Oakleigh Avenue and was paid for by us as part of our development consents. It is also outside the Project area so it is not clear how this will be impacted by the Project. Page 72 of the Assessment of the Effects on the Environment notes that the Project will "improve existing culverts capacities and/or provide new stormwater infrastructure which improve ponding and stream flow in the area", but it is not made clear whether this will affect our property.

(6) Assessment of construction noise and vibration effects

Our property at 33 Oakleigh Avenue, accessible via Hitchcock Road, has not been included in the Assessment of Construction Noise and Vibration [Link](#). We are very concerned that Figure 4 on page 35 ('Indicative piling locations and designation overlay (Manuia Road)') does not even show 33

Oakleigh Avenue on the map of affected properties, despite our very close proximity to the construction of both the Manuia bridge and the new roundabout at the intersection of Oakleigh Avenue and Hitchcock Road (our building is approximately 20m from the end of Hitchcock Road).

Penelope Savidan
Managing Director
B&F Papers Ltd.
Auckland - 0800 263 727 ext. @bfpapers
Mobile - +64 27 736 3567

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1158] Notice of Requirement online submission - Miriam Chisnall
Date: Wednesday, 13 December 2023 2:01:05 pm
Attachments: [Submission on NOR1 - Takanini Level Crossing - Unit 3 24 Oakleigh Avenue.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Miriam Chisnall

Organisation name: The Runciman Trust

Full name of your agent: Miriam Chisnall

Email address: stuartandmiriam@gmail.com

Contact phone number: 021433483

Postal address:
PO Box 2721111
Papakura
Auckland 2244

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
3/24 Oakleigh Avenue Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
Please see attached file

I or we seek the following recommendation or decision from Auckland Council:
Reconsider alternative options. Do not put NoR's on properties before the Proposal hearing

Submission date: 13 December 2023

Supporting documents
[Submission on NOR1 - Takanini Level Crossing - Unit 3 24 Oakleigh Avenue.pdf](#)

Attend a hearing

Do you wish to be heard in support of your submission? Yes

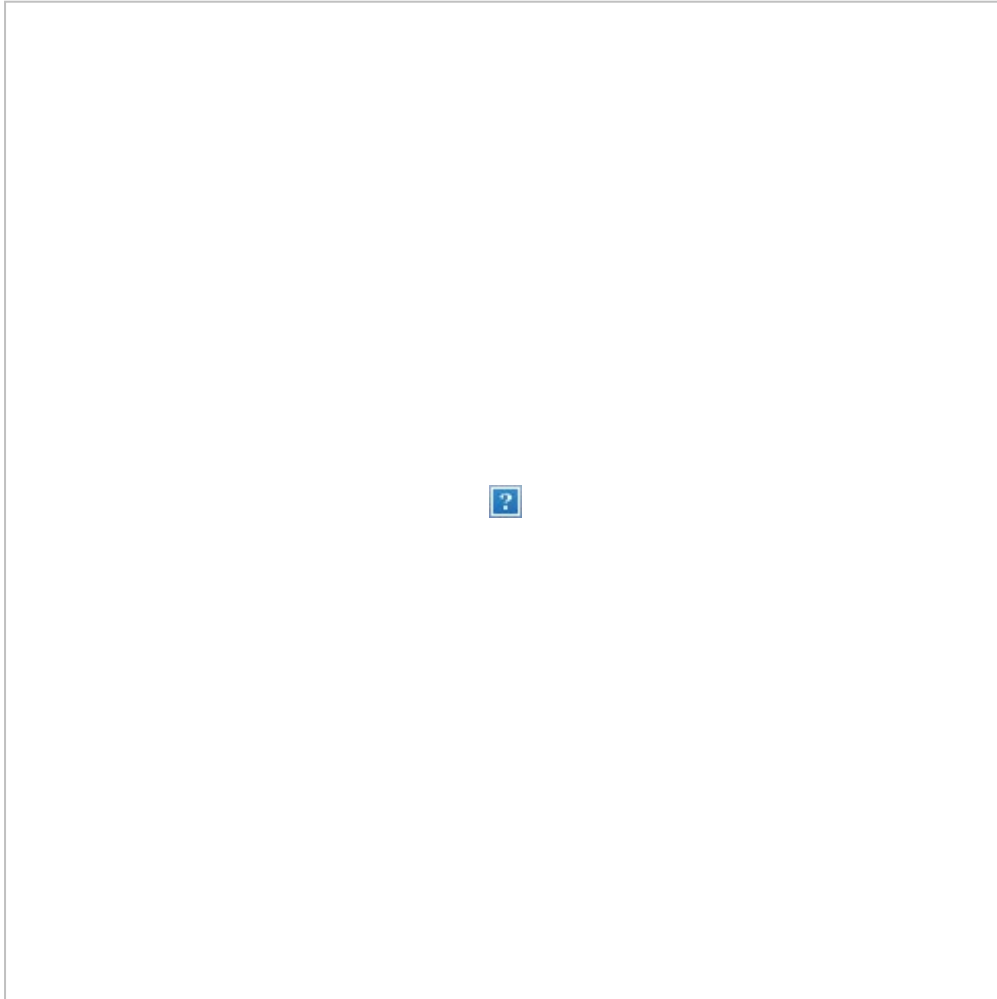
Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

The Runciman Trust

Introduction

The Runciman Trust owns the building located at 3/24 Oakleigh Avenue, Takanini, and leases it to an industrial tenant.

The Runciman Trust welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Submissions

The Notice of Requirement being submitted on by Atsource is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

The Runciman Trust is in strong opposition regarding the Notice of Requirement.

The Runciman Trust will not gain an advantage in trade competition through the submission.

The decision The Runciman trust seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for the Runciman Trust being strongly opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

- There is no merit in designating an area before any decision is made, and also 8 to 15 years before the project is proposed to happen whilst impacting all business with major uncertainty. We believe this is a breach of rights of local business and property owners by the Government and Auckland Council, and we will be seeking further advice and potentially redress with the local Member of Parliament.
- Very stressful for us having purchased this property for long term tenure to lease to busy and successful companies based in Takanini.
- Destroys our long term investment/livelihood in terms of capital value and potential rental income. This is a very significant issue at our age.
- It is difficult for traffic to move along Oakleigh Avenue now without the proposed plan creating less provision for road traffic simply to increase train services. Trucks are parked on the road now, minimising parking for our tenants overflow. This is a major traffic and pedestrian hazard.
- There is no suitable building to relocate to in the area, being close to the motorway which is an advantage for our employees, suppliers, and our service contracts with Government essential services.
- Zero return on our investment if our tenants are forced to move further away
Less rates to maintain services in the Takanini area
- Local businesses will suffer from less people in the area that have supported them.
- We believe the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). We believe the view of the assessment of alternatives is deficient.
- Our tenant, like many in the Takanini area, has freight deliveries/sendings from major trucking companies with truck and trailer units that will seriously be affected.
- Our tenant has many employees and contractors that must access Oakleigh Avenue in order to acquire the materials and equipment they need to carry out their work.

The proposed changes can only increase their travel time, and therefore their costs in carrying out their business.

- Oakleigh Avenue is currently an unsafe road with heavy vehicles parked on both sides (because there is nowhere else to go). It is not possible for two vehicles to pass between trucks parked on the side of the road.

This can only become a greater risk hazard and a cause of delays under the proposed plan.

We cannot see any benefit in this proposed plan but rather we see very significant downsides, and increased risk to businesses in the area.

These roads also feed significant and growing residential areas. The downsides and risks apply equally to residential properties.

There must also be increased risk to children in the area because there will be increased traffic passing schools, childcare facilities and local shopping areas.

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1159] Notice of Requirement online submission - Stuart Chisnall
Date: Wednesday, 13 December 2023 2:16:00 pm
Attachments: [Submission on NOR1 - Takanini Level Crossing - Unit 4 24 Oakleigh Avenue.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Stuart Chisnall

Organisation name: AtSource

Full name of your agent: Stuart s Chisnall

Email address: stuart@atsource.co.nz

Contact phone number: 021933713

Postal address:
stuart@atsource.co.nz
Auckland
Auckland 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The plan as put forward by Auckland Council will totally gridlock Takanini residential and industrial areas with serious adverse effects on residents and business in the area. This will have serious financial and emotional impacts on business owners and residents. It will have serious negative impacts of community facilities in the area, including schools, pre-schools and shopping areas.

I or we seek the following recommendation or decision from Auckland Council:
Please cancel this proposal. Give urgent consideration to liaising with the NZ Government and other involved organisations to open the Mill Road alternative route. This will provide meaningful improvements to the greater Auckland traffic, the Takanini area traffic as well as reducing traffic on railway crossings along the total southern route.

Submission date: 13 December 2023

Supporting documents
[Submission on NOR1 - Takanini Level Crossing - Unit 4 24 Oakleigh Avenue.pdf](#)

Attend a hearing

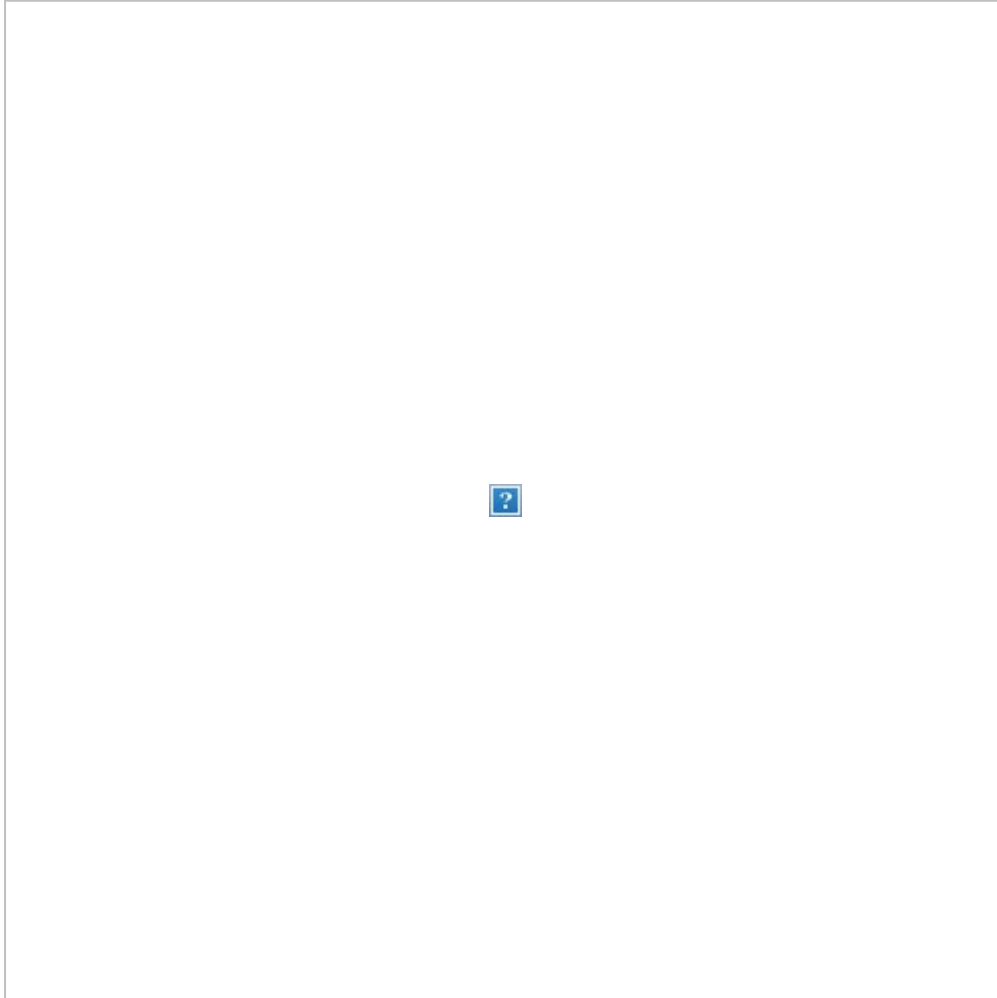
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

Atsource Extraction Systems

Introduction

Atsource is a company located at 4/24 Oakleigh Avenue, Takanini

Atsource welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Submissions

The Notice of Requirement being submitted on by Atsource is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

Atsource is in strong opposition regarding the Notice of Requirement.

Atsource will not gain an advantage in trade competition through the submission.

The decision Atsource seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for Atsource being strongly opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

- There is no merit in designating an area 8 to 15 years before the project is proposed to happen whilst impacting our business with major uncertainty. We believe this is a breach of rights of local business and property owners by the Government and Auckland Council, and we will be seeking further advice and potentially redress with the local Member of Parliament.
- Very stressful for us having purchased this property for long term tenure to operate our business from. Supporting Growth Alliance have the impudence to provide a national 0800 anxiety line for anyone that suffers from anxiety when this is a major stress that will not be solved by taking a deep breath.
- Destroys our long term investment/livelihood in terms of capital value and potential rental income. This is a very significant issue at our age.
-
- It is difficult for traffic to move along Oakleigh Avenue now without the proposed plan creating less provision for road traffic simply to increase train services. Trucks are parked on the road now, minimising parking for our overflow. This is a major traffic and pedestrian hazard.
- There is no suitable building to relocate to in the area, being close to the motorway which is an advantage for our employees, suppliers, and our service contracts with Government essential services.
- Added cost to our business if we are forced to move further away
- Less rates to maintain services in the Takanini area
- Local businesses will suffer from less people in the area that have supported them.
- We believe the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). We believe the view of the assessment of alternatives is deficient.
- Atsource has freight deliveries/sendings from major trucking companies with truck and trailer units that will seriously be affected
- Atsource does service work for New Zealand Emergency Services and therefore proximity to the motorway for essential servicing is crucial to our service delivery

- Oakleigh Avenue is currently an unsafe road with heavy vehicles parked on both sides (because there is nowhere else to go). It is not possible for two vehicles to pass between trucks parked on the side of the road.
This can only become a greater risk hazard and a cause of delays under the proposed plan.

We cannot see any benefit in this proposed plan but rather significant downsides, and increased risk to businesses in the area.

These roads also feed significant and growing residential areas. The downsides and risks apply equally to residential properties.

There must also be increased risk to children in the area because there will be increased traffic passing schools, childcare facilities and local shopping areas.

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1160] Notice of Requirement online submission - Richard Scarlett
Date: Wednesday, 13 December 2023 2:16:08 pm
Attachments: [Submission on NOR1 - Takanini Level Crossing - Aintree Group \[F1\].pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Richard Scarlett

Organisation name: Aintree Group Ltd

Full name of your agent: Grant Hewison

Email address: Richard@aintree.co.nz

Contact phone number: 021 228 4573

Postal address:
10 Rawson Way
Takanini
Auckland 2105

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
The entire NoR

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

Submission on Notice of Requirement (In Opposition): Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport Aintree Group Ltd Introduction Aintree Group Ltd is a company located at 37-39 Oakleigh Avenue, Takanini. Aintree Group Ltd welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). Link Submissions The Notice of Requirement being submitted on by Aintree Group Ltd is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). Our submission relates to the entire Notice of Requirement. Aintree Group Ltd is in opposition regarding the Notice of Requirement. Aintree Group Ltd will not gain an advantage in trade competition through the submission. The decision Aintree Group Ltd seeks from the Council is to decline the Notice of Requirement. Reasons for being in opposition regarding the Notice of Requirement Link By way of introduction, we understand from the Assessment of Effects on the Environment Link that the following effects have been identified for Oakleigh Avenue: • Reconstruction of existing cul-de-sac at Hitchcock Road (east of the NIMT) to tie into the new intersection at Oakleigh Avenue / Manuia Road / Hitchcock Avenue and upgrade with footpath (page 16); • New roundabout intersection at Oakleigh Avenue / Manuia Road / Hitchcock Avenue with active mode facilities and tie in works (page 16); • On the south-eastern corner of Spartan Road and Oakleigh Ave, there is a stormwater pond / modified natural wetland (and works should avoid any wetlands as much as possible, especially this identified wetland at the corner of Spartan Road and Oakleigh Avenue)(pages 44 and 119); • Modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area (page 44); • Some feedback on the proposal did not think Oakleigh Avenue has the capacity to support increased traffic movements should both crossings potentially close (page 62); • Routing from the

businesses on Spartan Road (west of the railway) northbound onto Great South Road and access to SH1 northbound on-ramp will be affected due to Spartan Road level crossing closure (page 76); • An alternative over-dimension route from Porchester Road to Great South Road, through the industrial area is planned to be provided via Manuroa Road, Oakleigh Avenue and the Manuia Road connection (page 83); The reasons for Aintree Group Ltd being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

- (1) Onsite Parking The Assessment of Transport Effects Link notes that in some locations of the Project areas, the Project intends to widen the existing designation and alter the cross section of the corridor to incorporate separated walking and cycling facilities, provide space for a bridge or to incorporate space for the accessways. As a result, existing car parking provision for properties adjacent to the Project corridor will be affected. The indicative number of the on-site parking spaces which are affected by the Project are summarised in Table 39: Summary of affected on-site parking spaces (pages 116-118). Table 39 notes that for 39 Oakleigh Ave (identified as an Industrial site (name unknown)) approximately 7 on-site parks will be affected on the edge of the property. We hold concerns for these effects.
- (2) Property Access to Oakleigh Avenue The Assessment of Transport Effects Link acknowledges that the Project will have impacts on a number of existing access arrangements to surrounding properties (pages 118-121). With regard to the Manuia Road multi-modal bridge, the Assessment of Transport Effects observes that for our site at 39 Oakleigh Ave (Industrial site (name unknown)), access is off Hitchcock Road on the eastern arm of the proposed roundabout and access will need to be reinstated after the roundabout is implemented (Table 40, page 119). This suggests that access to 39 Oakleigh Avenue can be provided via Oakleigh Ave instead of Hitchcock Road during construction, with Hitchcock Road access reinstated after construction, but it is not clear how this will work for all vehicles that need to access our site. In particular, we ask that the current truck access points into our property at 39 Oakleigh Avenue be maintained. Please see the plan in Appendix 1 which shows truck access point 1 (which is where trucks access the property) and the exit at point 2. Point 3 is for the use of office vehicles. We are also concerned about our access being significantly affected during construction, which will likely require temporary access during this period.
- (3) Heavy Vehicle usage of Hitchcock Road We are also concerned that Appendix B of the Assessment of Transport Effects suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%). Contrary to this, we are aware of significant heavy vehicle usage, with multiple trucks doing multiple runs per day.
- (4) Manuia Crossing The Assessment of Transport Effects notes that in the 'do-nothing' scenario that the vehicle to capacity (V/C) on Spartan Road and Manuroa Road is at 100% and 60-80% respectively. The Assessment of Transport Effects further states that these road users at both existing level crossings will be redirected to the proposed new Manuia Rd bridge. The Assessment further proposes that traffic from Halls Logistics (western side of the railway on Spartan Road) will be re-routed to use the new Manuia bridge in both directions for no other purpose than to use the new Oakleigh Avenue roundabout as a u-turn opportunity. So not only are two level crossings that are either at or close to V/C to be re-routed to a single lane (in each direction) bridge at Manuia, but further pressure will be put on this access by heavy trucks crossing it for no other purpose than to u-turn. Furthermore, this route is being proposed for inclusion as an over-dimension route when it wasn't before. This will add further pressure to the proposed Manuia bridge crossing. We cannot see how the proposed Manuia bridge can handle the traffic from two closed level crossings that are also being incrementally used for u-turning, particularly if the interchange is deemed unsafe to do so by a future safety study, and is also now being included in the over-dimension traffic planned routes. The Assessment of Transport Effects also notes that these are 2038 projections. However, it is clear to us and anyone who uses these roads now that these roads are at capacity during peak travel times in 2023. The vast majority of traffic on Manuroa Road turns right (north) during peak travel times, thus making Taka Road (to the South) an unlikely alternative as not only is it to the south, but there isn't a direct link due to the Takanini Railway Station. Furthermore, Alfriston Road (to the north) is substantially farther away requiring a lot more suburban travel time and is unlikely to be an alternative route for users of the Takanini Industrial area. We are of the view that there needs to be two lanes exiting the bridge onto Oakleigh Avenue to cope with increased traffic, one lane to turn left only and one lane to turn right (or for traffic moving straight ahead into the site for B&F Papers Ltd). We believe there needs to be a free right turn into Spartan Road once Spartan Road is terminated at the crossing. We also believe Spartan Road should be left open until the new bridge at Manuroa Road is operating.
- (5) Modified Stream Channel Page 47 of the Assessment of the Effects on the Environment highlights that there is a "modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area." Page 72 of the Assessment of the Effects on the Environment notes that the Project will "improve existing culverts capacities and/or provide new

stormwater infrastructure which improve ponding and stream flow in the area”, but it is not made clear whether this will affect properties nearby. (6) Assessment of construction noise and vibration effects Our property at 39 Oakleigh Avenue does not appear to have been included in the Assessment of Construction Noise and Vibration Link. We are concerned that Figure 4 on page 35 (‘Indicative piling locations and designation overlay (Manuia Road)’) does not appear to have identified 39 Oakleigh Avenue on the map of affected properties, despite our very close proximity to the construction of both the Manuia bridge and the new roundabout at the intersection of Oakleigh Avenue and Hitchcock Road.

I or we seek the following recommendation or decision from Auckland Council:
That the NoR be refused

Submission date: 13 December 2023

Supporting documents
Submission on NOR1 - Takanini Level Crossing - Aintree Group [F].pdf

Attend a hearing

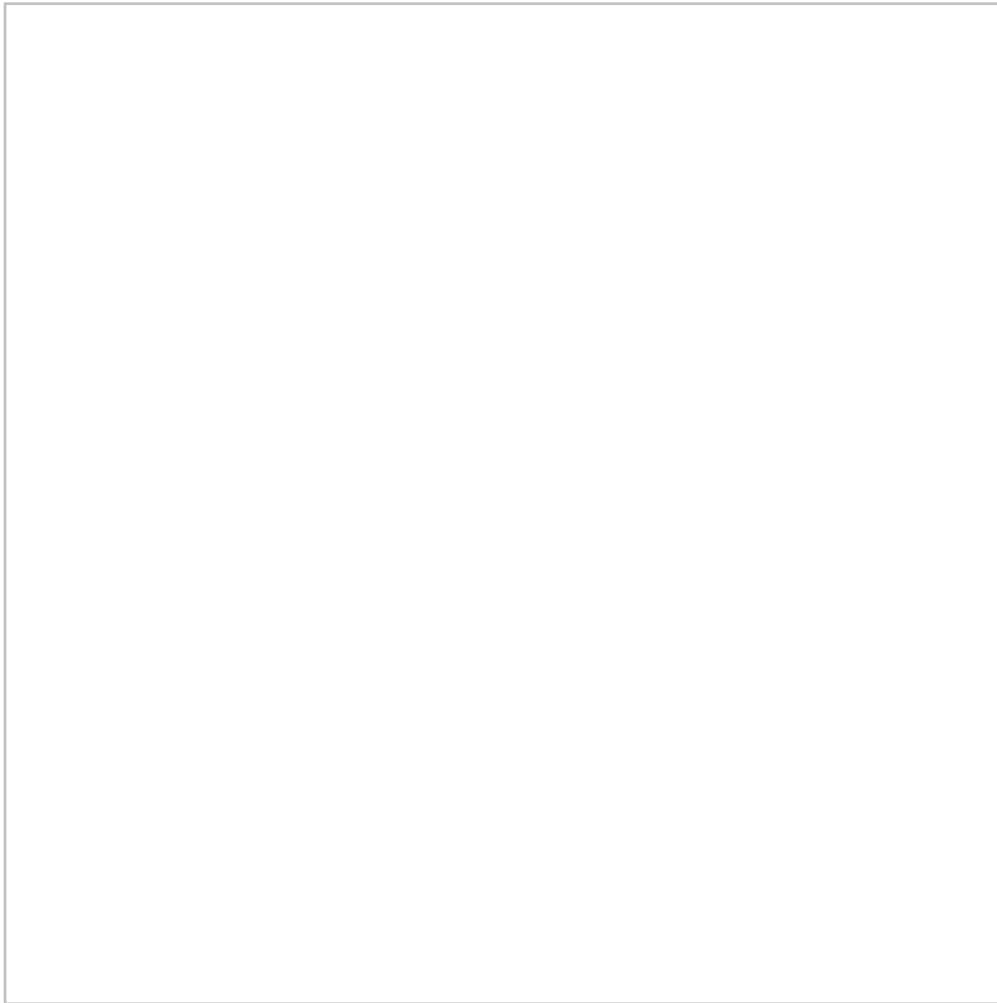
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

Aintree Group Ltd

Introduction

Aintree Group Ltd is a company located at 37-39 Oakleigh Avenue, Takanini.

Aintree Group Ltd welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). [Link](#)

Submissions

The Notice of Requirement being submitted on by Aintree Group Ltd is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

Aintree Group Ltd is in opposition regarding the Notice of Requirement.

Aintree Group Ltd will not gain an advantage in trade competition through the submission.

The decision Aintree Group Ltd seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

By way of introduction, we understand from the Assessment of Effects on the Environment [Link](#) that the following effects have been identified for Oakleigh Avenue:

- Reconstruction of existing cul-de-sac at Hitchcock Road (east of the NIMT) to tie into the new intersection at Oakleigh Avenue / Manuia Road / Hitchcock Avenue and upgrade with footpath (page 16);
- New roundabout intersection at Oakleigh Avenue / Manuia Road / Hitchcock Avenue with active mode facilities and tie in works (page 16);
- On the south-eastern corner of Spartan Road and Oakleigh Ave, there is a stormwater pond / modified natural wetland (and works should avoid any wetlands as much as possible, especially this identified wetland at the corner of Spartan Road and Oakleigh Avenue)(pages 44 and 119);
- Modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area (page 44);
- Some feedback on the proposal did not think Oakleigh Avenue has the capacity to support increased traffic movements should both crossings potentially close (page 62);
- Routing from the businesses on Spartan Road (west of the railway) northbound onto Great South Road and access to SH1 northbound on-ramp will be affected due to Spartan Road level crossing closure (page 76);
- An alternative over-dimension route from Porchester Road to Great South Road, through the industrial area is planned to be provided via Manuroa Road, Oakleigh Avenue and the Manuia Road connection (page 83);

The reasons for Aintree Group Ltd being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

(1) Onsite Parking

The Assessment of Transport Effects [Link](#) notes that in some locations of the Project areas, the Project intends to widen the existing designation and alter the cross section of the corridor to incorporate separated walking and cycling facilities, provide space for a bridge or to incorporate space for the accessways. As a result, existing car parking provision for properties adjacent to the Project corridor will be affected. The indicative number of the on-site parking spaces which are affected by the Project are summarised in Table 39: Summary of affected on-site parking spaces (pages 116-118).

Table 39 notes that for 39 Oakleigh Ave (identified as an Industrial site (name unknown)) approximately 7 on-site parks will be affected on the edge of the property. We hold concerns for these effects.

(2) Property Access to Oakleigh Avenue

The Assessment of Transport Effects [Link](#) acknowledges that the Project will have impacts on a number of existing access arrangements to surrounding properties (pages 118-121).

With regard to the Manuia Road multi-modal bridge, the Assessment of Transport Effects observes that for our site at 39 Oakleigh Ave (Industrial site (name unknown)), access is off Hitchcock Road on the eastern arm of the proposed roundabout and access will need to be reinstated after the roundabout is implemented (Table 40, page 119). This suggests that access to 39 Oakleigh Avenue can be provided via Oakleigh Ave instead of Hitchcock Road during construction, with Hitchcock Road access reinstated after construction, but it is not clear how this will work for all vehicles that need to access our site.

In particular, we ask that the current truck access points into our property at 39 Oakleigh Avenue be maintained. Please see the plan in Appendix 1 which shows truck access point 1 (which is where trucks access the property) and the exit at point 2. Point 3 is for the use of office vehicles.

We are also concerned about our access being significantly affected during construction, which will likely require temporary access during this period.

(3) Heavy Vehicle usage of Hitchcock Road

We are also concerned that Appendix B of the Assessment of Transport Effects suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%). Contrary to this, we are aware of significant heavy vehicle usage, with multiple trucks doing multiple runs per day.

(4) Manuia Crossing

The Assessment of Transport Effects notes that in the 'do-nothing' scenario that the vehicle to capacity (V/C) on Spartan Road and Manuroa Road is at 100% and 60-80% respectively.

The Assessment of Transport Effects further states that these road users at both existing level crossings will be redirected to the proposed new Manuia Rd bridge. The Assessment further proposes that traffic from Halls Logistics (western side of the railway on Spartan Road) will be re-routed to use the new Manuia bridge in both directions for no other purpose than to use the new Oakleigh Avenue roundabout as a u-turn opportunity. So not only are two level crossings that are either at or close to V/C to be re-routed to a single lane (in each direction) bridge at Manuia, but further pressure will be put on this access by heavy trucks crossing it for no other purpose than to u-turn.

Furthermore, this route is being proposed for inclusion as an over-dimension route when it wasn't before. This will add further pressure to the proposed Manuia bridge crossing. We cannot see how the proposed Manuia bridge can handle the traffic from two closed level crossings that are also being incrementally used for u-turning, particularly if the interchange is deemed unsafe to do so by a future safety study, and is also now being included in the over-dimension traffic planned routes.

The Assessment of Transport Effects also notes that these are 2038 projections. However, it is clear to us and anyone who uses these roads now that these roads are at capacity during peak travel times in 2023. The vast majority of traffic on Manuroa Road turns right (north) during peak travel times, thus making Taka Road (to the South) an unlikely alternative as not only is it to the south, but there isn't a direct link due to the Takanini Railway Station. Furthermore, Alfriston Road (to the north) is substantially farther away requiring a lot more suburban travel time and is unlikely to be an alternative route for users of the Takanini Industrial area.

We are of the view that there needs to be two lanes exiting the bridge onto Oakleigh Avenue to cope with increased traffic, one lane to turn left only and one lane to turn right (or for traffic moving straight ahead into the site for B&F Papers Ltd). We believe there needs to be a free right turn into Spartan Road once Spartan Road is terminated at the crossing. We also believe Spartan Road should be left open until the new bridge at Manuroa Road is operating.

(5) Modified Stream Channel

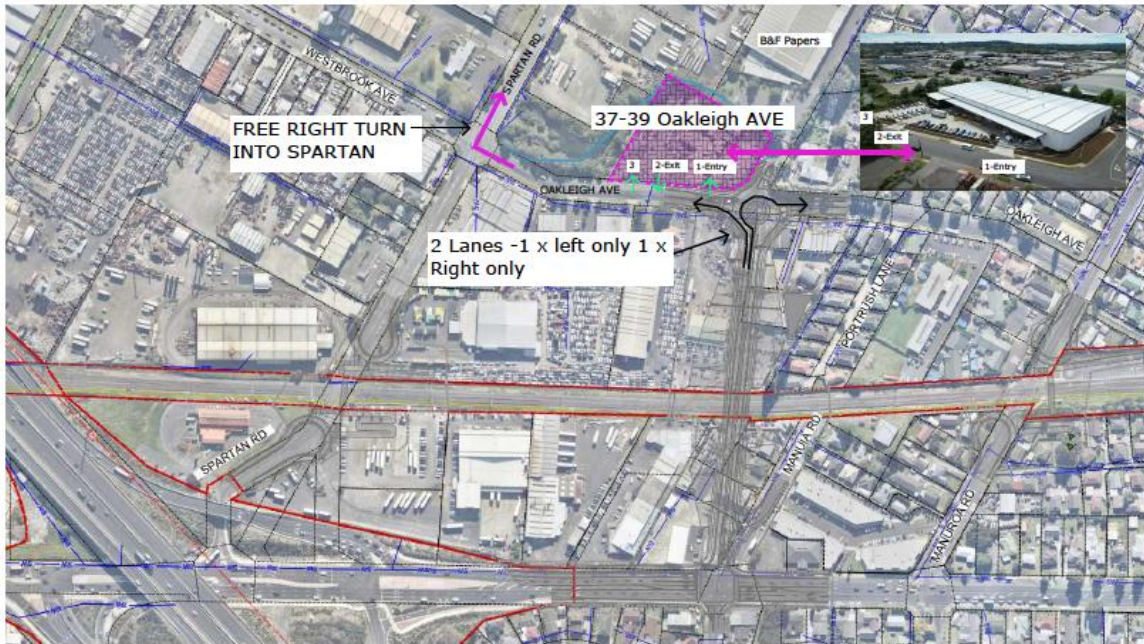
Page 47 of the Assessment of the Effects on the Environment highlights that there is a "modified stream channel that connects Oakleigh Ave stormwater pond / modified natural wetland and Scott Field Drive modified natural wetland adjacent to Manuia Road project area."

Page 72 of the Assessment of the Effects on the Environment notes that the Project will "improve existing culverts capacities and/or provide new stormwater infrastructure which improve ponding and stream flow in the area", but it is not made clear whether this will affect properties nearby.

(6) Assessment of construction noise and vibration effects

Our property at 39 Oakleigh Avenue does not appear to have been included in the Assessment of Construction Noise and Vibration [Link](#). We are concerned that Figure 4 on page 35 ('Indicative piling locations and designation overlay (Manuia Road)') does not appear to have identified 39 Oakleigh Avenue on the map of affected properties, despite our very close proximity to the construction of both the Manuia bridge and the new roundabout at the intersection of Oakleigh Avenue and Hitchcock Road.

Appendix 1



From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1163] Notice of Requirement online submission - Peter Robinson
Date: Wednesday, 13 December 2023 3:46:02 pm
Attachments: [CCF_001985.pdf](#)
[CCF_001985_20231213153254.727.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Peter Robinson

Organisation name: By Design Concrete and Paving Limited

Full name of your agent:

Email address: peter@bydesignconcrete.co.nz

Contact phone number:

Postal address:

Takanini.
Takanini. 2110

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
unit 1/ 24 Oakleigh Ave Takanini South Auckland

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

We as the owners of unit 1 / 24 Oakleigh Ave Takanini and are running a hard landscape company from this building we own. The location for our business is only 5 minutes onto the Takanini on and off ramp. The cost to move from here and try and find another building will be well out of our price range. Fuel cost and time traveling to get onto a motorway out south further is going to put stress and extra cost and time wasted in travel to our sites daily. This is also our retirement fund and we are worried we won't be able to sell this building for a fair price and when we want to sell it not when you want to purchase it from us. The plan to have that volume of traffic coming onto Oakleigh Ave is just crazy! The road won't cope with that share volume daily. We just want to stay here and run our business right here and NOT MOVE.

I or we seek the following recommendation or decision from Auckland Council:

To give me a firm date when you can buy our property as we are heading into retirement in the next few years. Will you buy our building and we can rent back off you? How is this going to affect our value on this building

Submission date: 13 December 2023

Supporting documents
[CCF_001985.pdf](#)
[CCF_001985_20231213153254.727.pdf](#)

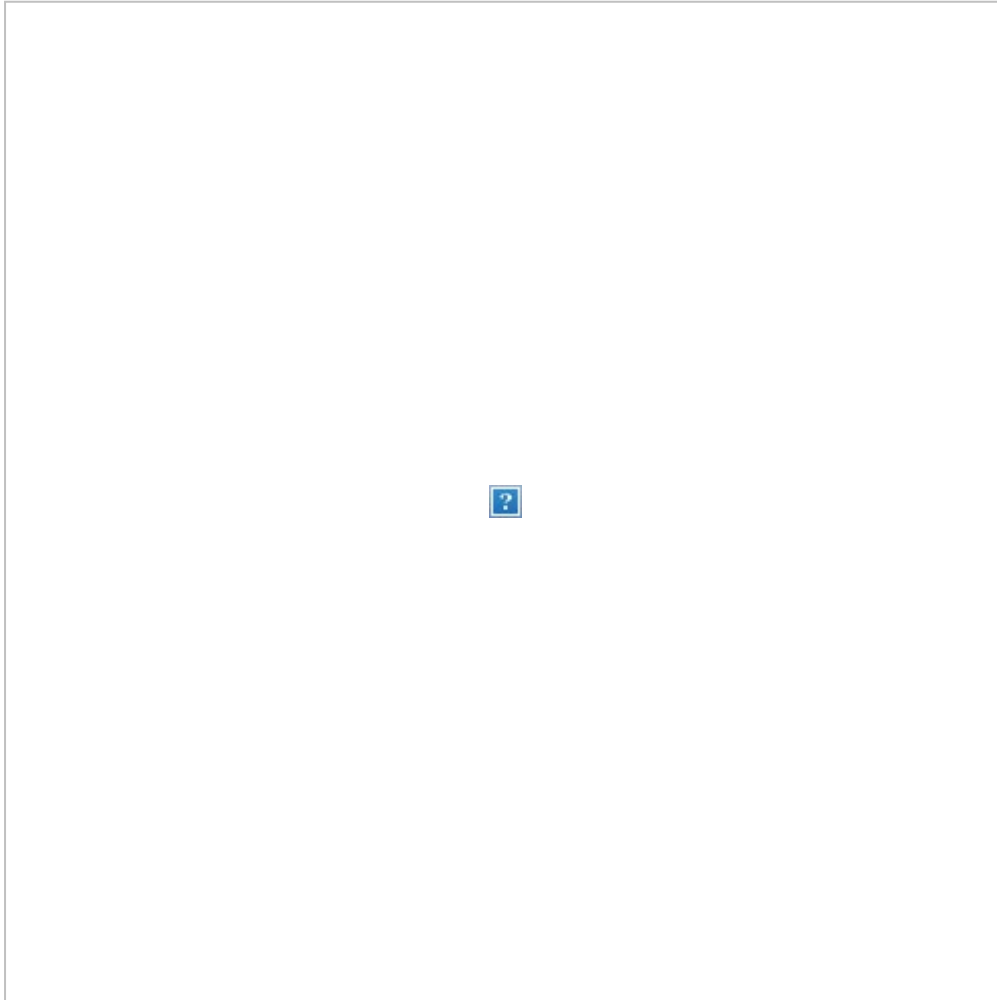
Attend a hearing

Do you wish to be heard in support of your submission? No

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka
Street (NoR1) Auckland Transport**

By Design Concrete and Paving Limited

Introduction

By Design Concrete and Paving Limited

is a company located at unit 1 / 24 Oakleigh Ave Takanini

By Design Concrete and Paving Limited

welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). [Link](#)

Submissions

The Notice of Requirement being submitted on by **By Design Concrete and Paving Limited** is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

By Design Concrete and Paving Limited

is in opposition regarding the Notice of Requirement.

By Design Concrete and Paving Limited

will not gain an advantage in trade competition through the submission.

The decision **By Design Concrete and Paving Limited**

seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for **By Design Concrete and Paving Limited** being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

We as the owners of unit 1 / 24 Oakleigh Ave Takanini and are running a hard landscape company from this building we own. The location for our business is only 5 minutes onto the Takanini on and off ramp. The cost to move from here and try and find another building will be well out of our price range. Fuel cost and time traveling to get onto a motorway out south further is going to put stress and extra cost and time wasted in travel to our sites daily. This is also our retirement fund and we are worried we won't be able to sell this building for a fair price and when we want to sell it not when you want to purchase it from us.

The plan to have that volume of traffic coming onto Oakleigh Ave is just crazy! The road won't cope with that share volume daily.

We just want to stay here and run our business right here and NOT MOVE.

From
Peter Robinson
Oakleigh Road Trust
021346666
09-8694168

**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka
Street (NoR1) Auckland Transport**

By Design Concrete and Paving Limited

Introduction

By Design Concrete and Paving Limited

is a company located at unit 1 / 24 Oakleigh Ave Takanini

By Design Concrete and Paving Limited

welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). [Link](#)

Submissions

The Notice of Requirement being submitted on by **By Design Concrete and Paving Limited** is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

By Design Concrete and Paving Limited

is in opposition regarding the Notice of Requirement.

By Design Concrete and Paving Limited

will not gain an advantage in trade competition through the submission.

The decision **By Design Concrete and Paving Limited**

seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for **By Design Concrete and Paving Limited** being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

We as the owners of unit 1 / 24 Oakleigh Ave Takanini and are running a hard landscape company from this building we own. The location for our business is only 5 minutes onto the Takanini on and off ramp. The cost to move from here and try and find another building will be well out of our price range. Fuel cost and time traveling to get onto a motorway out south further is going to put stress and extra cost and time wasted in travel to our sites daily. This is also our retirement fund and we are worried we won't be able to sell this building for a fair price and when we want to sell it not when you want to purchase it from us.

The plan to have that volume of traffic coming onto Oakleigh Ave is just crazy! The road won't cope with that share volume daily.

We just want to stay here and run our business right here and NOT MOVE.

From

Peter Robinson

Oakleigh Road Trust

021346666

09-8694168

SUBMISSION ON A REQUIREMENT FOR A DESIGNATION OR AN ALTERATION TO A DESIGNATION SUBJECT TO FULL OR LIMITED NOTIFICATION

Sections 168A,169, 181, 189A, 190, and 195A of the Resource Management Act 1991

FORM 21

Name of Requiring Authority: Auckland Transport

A new designation or alteration to an existing designation: Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

Submission emailed to unitaryplan@aucklandcouncil.govt.nz

SUBMITTER DETAILS

Submitter: DDI Takanni Investments Limited, Attention: Matthew Gibson (as owners of the site at 72-86 Great South Road, Takaanini)

Agent: (for Service/Correspondence): DCS, Attention: Brooke Dales, Director/Planner

Email: brooke@dcs.gen.nz, Phone: 027 432 4283

The specific provisions that this submission relates to are:

DDI Takanni Investments Limited are the owners of the site at 72-86 Great South Road, Takanini currently occupied by Fairfax Industries Limited.

DDI Takanni Investments Limited **do not support** the proposed notice of requirement, based on the limited information and details provided as to the details of this proposed designation as it affects the site at 72-86 Great South Road, Takanini, and in particular the need for this site's land to give effect of the proposed designation, and the extent of and location of the area of land that has been delineated for the designated purpose. DDI Takanni Investments Limited consider, in particular, that the extent of the area could be reduced while still fulfilling the purpose of the designation.

This submission opposes the Notice of Requirement.

The reason for the submission are:

DDI Takanni Investments Limited are not satisfied, based on the limited information and details provided as to the details of this proposed designation as it affects the site at 72-86 Great South Road, Takanini and the need for this site's land to give effect of the proposed designation, and the extent of and location of the area

of land that has been delineated for the designated purpose. DDI Takanni Investments Limited consider, in particular, that the extent of the area could be reduced while still fulfilling the purpose of the designation.

DDI Takanni Investments Limited **oppose** the land being designated without limitation, when that land appears (based the limited information supplied) to be only required for the construction phase of the designation. In this case, these areas of land could be secured for this use on a temporary basis rather than permanently designated as set out in the notice of requirement. DDI Takanni Investments Limited is willing to discuss the provision of a licence to occupy to provide for this construction time period.

DDI Takanni Investments Limited has significant concerns that the loss of this land could significantly adversely affect the ongoing operation of the property, noting, among other matters, the location of the building in relative close proximity to the road boundary. Furthermore, DDI Takanni Investments Limited have operational concerns, including but are not limited to, the maintenance of easy, safe and efficient access to and from the property, future limitations on the safe and efficient manoeuvring of trucks and machinery within the site as a result of the loss of this land as proposed and more generally limitation of the site's useability for a range of permitted uses into the future.

DDI Takanni Investments Limited seek the following recommendation or decision from Auckland Council:

- Reduce the extent of the designation area so that it does not encompass any of the site at 72-86 Great South Road, Takanini; or
- Reduce the extent of the designation area as it affects the site at 72-86 Great South Road, Takanini to the minimum required to enable the road widening works to occur; and/or
- If additional land is required during the construction phase and is not required in the operational phase in perpetuity, then the duration should be limited to reflect this situation.

DDI Takanni Investments Limited **wish to be heard** in support of this submission.

If others make a similar submission, then DDI Takanni Investments Limited **will consider presenting a joint case** with them at a hearing.



Brooke Dales
Consultant Planner (DCS)
Resource Consents

Date: 13 December 2023

(person authorised to sign on behalf of submitter DDI Takanni Investments Limited)

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1172] Notice of Requirement online submission - Amylee Smits
Date: Wednesday, 13 December 2023 5:31:05 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Amylee Smits

Organisation name: Dealership Properties Limited

Full name of your agent:

Email address: admin@dealershipproperties.co.nz

Contact phone number: 0274443204

Postal address:
1242 PO Box

Taupo 3351

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
106-162 Great South Rd, Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
We have a RC currently in process that the proposed alteration conflicts with.

I or we seek the following recommendation or decision from Auckland Council:
We do not wish the proposed alteration to be approved from council.

Submission date: 13 December 2023

Attend a hearing

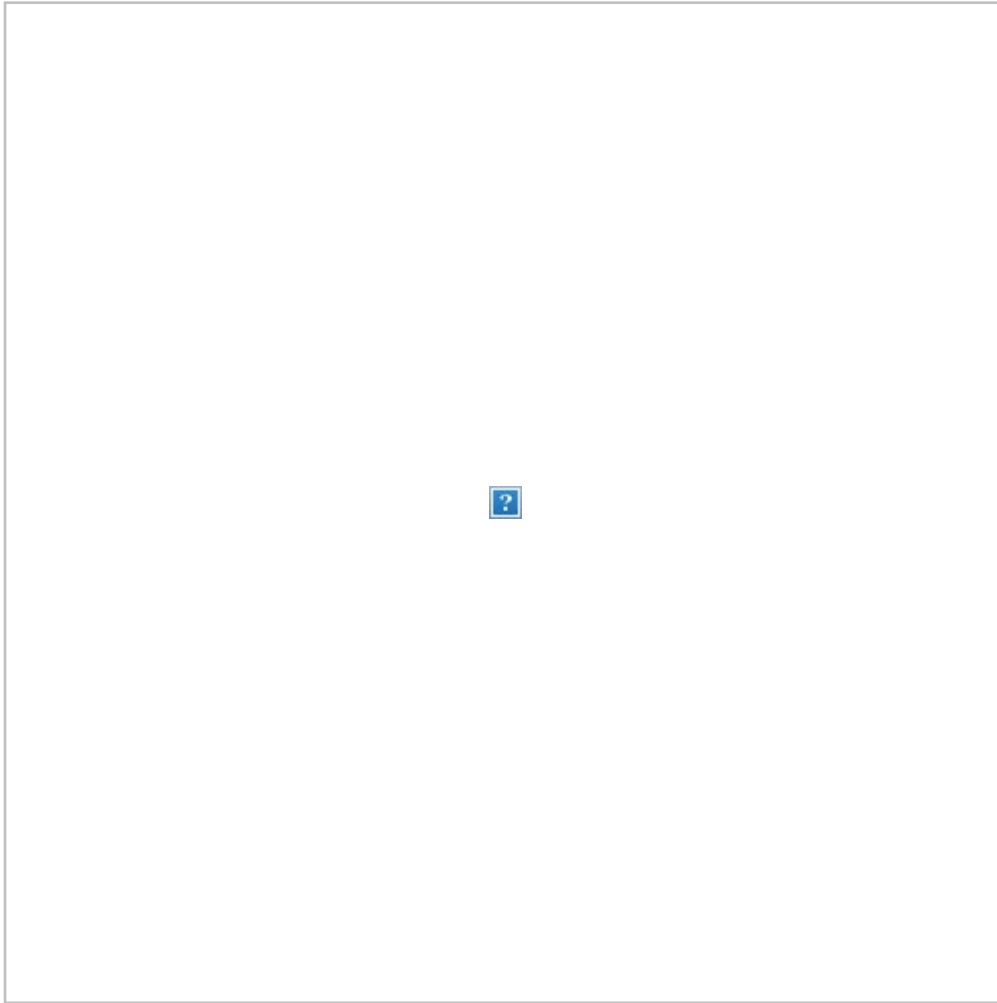
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1182] Notice of Requirement online submission - Miriam Chisnall
Date: Wednesday, 13 December 2023 9:01:02 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Miriam Chisnall

Organisation name:

Full name of your agent: Miriam Chisnall

Email address: miriam@atsource.co.nz

Contact phone number: 021433483

Postal address:
miriam@atsource.co.nz

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
3 & 4 Oakleigh Avenue Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

We purchased these properties as small investors to enable businesses to operate out of and provide us an income as we headed towards retirement and for the future of our children and grandchildren. Now we have an NoR on each property while Auckland Council debates if this proposal will work, meaning our buildings will be demolished and causing us the stress of finding another property to lease out to SME businesses trying to make a living, helping NZ's economy. My husband has cancer and is requiring very expensive non funded treatment to lengthen his life. The stress this has caused us both is very unfair when there is an extended lapse period. Auckland Council needs to stop and realise what this is doing to hard working people. Extra trains will not pay rates. Many businesses will close, livelihoods will be lost. Does Auckland Council care about this? Because we, the local landlords and leasees care and are hugely affected. The Mill Road project should go ahead because previous Governments have purchased property, that project is ready to go, instead of hanging out many more owners for a proposed designation. I believe this project is untenable and will have disastrous effects on this community.

I or we seek the following recommendation or decision from Auckland Council:
Designation to not proceed

Submission date: 13 December 2023

Attend a hearing

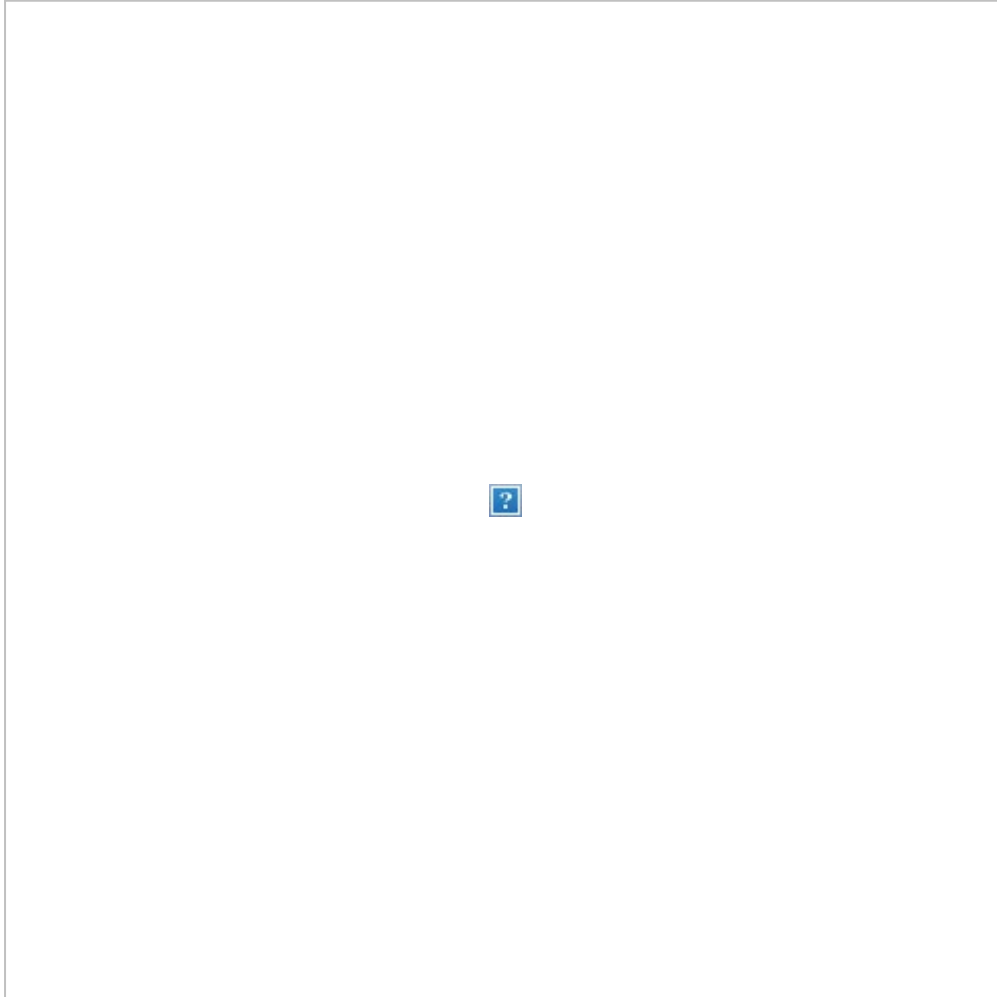
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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Attn: Planning Technician
Auckland Council
Level 16, 135 Albert Street
Private Bag 92300
Auckland 1142

14 December 2023

Re: Submission on notice of requirement for Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

To whom it may concern,

Please find attached the submission of New Zealand Steel Limited to the notice of requirement by Auckland Transport for the Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street project.

New Zealand Steel Limited, through its wholly owned subsidiary Steltech, occupies a portion of the site at 16 Spartan Road.

Yours sincerely

A handwritten signature in black ink, appearing to read "Amy Hill".

Amy Hill
Environment Manager
New Zealand Steel Limited

Form 21

Submission on requirement for designation or heritage order or alteration of designation or heritage order that is subject to public notification or limited notification by a territorial authority *Sections 168A, 169, 181, 189A, 190, and 195A, Resource Management Act 1991*

To: **Auckland Council**
Level 16, 135 Albert Street
Private Bag 92300
Auckland 1142
Attention: Planning Technician
unitaryplan@aucklandcouncil.govt.nz

Name of submitter: New Zealand Steel Limited (“NZ Steel”)

1. INTRODUCTION

- 1.1 This is a submission on the notice of requirement from Auckland Transport for a designation for the Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street Project (the NoR).
- 1.2 NZ Steel is not a trade competitor for the purposes of section 308B of the Resource Management Act 1991.
- 1.3 The specific parts of NoR 1 that NZ Steel’s submission relates to are:

NoR 1 in its entirety, and in particular the proposed works at Spartan Road, including the construction activity, the permanent closure of Spartan Road in the location of the North Island Main Trunk railway (NIMT), and the active modes bridge over Spartan Road.

2. NEW ZEALAND STEEL’S SUBMISSION

- 2.1 This document is NZ Steel’s submission to Auckland Council (AC) regarding NoR 1 from Auckland Transport (AT), for new multi-modal bridge crossings of the North Island Main Trunk (NIMT) railway at Manuia Road and Taka Street; and new active mode bridge crossings of the NIMT at Spartan Road and Manuroa Road with two consequential road closures.
- 2.2 NZ Steel is New Zealand’s largest steel making facility and the only plate steel manufacturer in the country. Owned by BlueScope Steel Limited, NZ Steel is a global pioneer in the process of producing steel from natural iron sand.
- 2.3 Steltech, a wholly owned subsidiary of NZ Steel Limited, introduced optimised steel beams to New Zealand in the early 1990s and has continuously been involved in designing and manufacturing for various range of projects across New Zealand and the Pacific Islands. In 1986, operations commenced in Spartan Road, where roll-formed sheet metal components for buildings were initially manufactured, subsequently leading the way in introducing various roll-forming techniques to the New Zealand market. Further, Steltech developed a custom welded beam system and invested in three splicing work stations to splice shorter lengths of plates together.

2.4 The Steltech facility (**Figure 1**), located at 16 Spartan Road (the Steltech site), is adjacent to the NIMT line on its western boundary and Spartan Road on its southern boundary. The proposed designation includes part of the property at 16 Spartan Road affecting the road frontage and site vehicle access. Steltech holds a lease over a portion of the site, as indicated approximately by the orange line.



Figure 1: Steltech site (orange outline) at 16 Spartan Road (blue outline) and area of designation (green dots)

2.5 NZ Steel's interest in the NoR relates to potential adverse effects the proposed designation will have on accessibility to the Steltech site during both the construction and operational stages of the proposed works, as a result of the proposed closure of the existing level crossings at Spartan Road and as a result of application of a designation to approximately 285 m² of the Steltech site¹. In particular, NZ Steel seeks to ensure that the operation of the Steltech site is not compromised by the proposed works to be undertaken on Spartan Road, both during construction and operation.

Closure of railway crossing

2.6 Currently, the site is accessed by vehicles through Spartan Road. This accessway serves as the only means for vehicles to enter and exit the site. Operations at the site heavily rely on consistent and convenient vehicular access for delivery and logistics. Steel beams are often large and heavy, requiring large vehicles to transport unique designs and sizes based on

¹ Refer Form 18 Notice of Requirement, Attachment B (Schedule of Directly Affected Properties), Property ID 602801.

specific customer requirements. Movement of trucks in and out of the site can vary from 30 to 50 occurrences per day.

- 2.7** Large trucks (flat decks and trombone trailers) are utilised to transport goods in and out of the Steltech site. Trucks entering the site transport goods that are in lengths of up to 18m long. Incoming goods are transported from the New Zealand Steel Glenbrook Steel Mill, travelling via the Auckland – Hamilton Motorway, exiting at exit 453 (Takaanini) before traversing Great South Road, making a left turn onto Spartan Road, and crossing the NIMT at the existing Spartan Road railway crossing. Upon reaching the Steltech site (approximately 28 m from the railway crossing), heavy vehicles carrying the goods turn left and proceed to navigate in an anti-clockwise direction.
- 2.8** Goods leaving the site can have a maximum length of 24 m. For departing vehicles, the process involves continuous anti-clockwise manoeuvring to exit the site, with the only available turn being left (eastward) onto Spartan Road.
- 2.9** The use of large vehicles for delivery and logistics by Steltech is also driven by safety considerations, and the need for efficiency in their supply chain and production processes.
- 2.10** The proposed cul-de-sacs for the eastern and western portions of Spartan Road, either side of the NIMT, will significantly limit accessibility to the site at 16 Spartan Road for large vehicles and this will adversely impact daily operations.
- 2.11** It is noted that the Assessment of Transport Effects included with NoR 1 assesses operational effects on property access at Section 6.10. However, 16 Spartan Road is not identified as one of the properties where access is directly affected, and no assessment has been undertaken of potential effects on property access to 16 Spartan Road².

Designation of portion of 16 Spartan Road

- 2.12** The proposed designation includes part of the property at 16 Spartan Road (285 m² at the road frontage). The potential impact of this land requirement will be a bottleneck restriction between the vehicle access and the existing buildings at the Steltech site. This impact may be so significant that it is no longer tenable for the Steltech business to operate from the site. In this case, NZ Steel will have no choice but to relocate to a different location.
- 2.13** Relocation would not only cause substantial disruptions to the business operations and incur significant costs, but would also pose substantial challenges in locating an alternative site given the shortage of heavy industrial zoned land in Auckland. Such disruption and challenges to ongoing operations have the potential to significantly impact on structural steel supply, resulting in consequential effects on planned and future New Zealand infrastructure projects.

Active mode bridge

- 2.14** NZ Steel's interest in the NoR also relates to the active mode bridge across the NIMT, enabling increased pedestrian movement. It is considered that this has the potential to exacerbate anti-social behaviour in the area surrounding the Steltech site where such behaviour (e.g., vandalism of private property) already occurs.

² Refer Table 40, Takaanini Level Crossings Assessment of Transport Effects, October 2023, Version 1.0.

3. RECOMMENDATIONS SOUGHT FROM THE COUNCIL:

3.1 NZ Steel seeks the following recommendations from Auckland Council:

- a Confirm the Notice of Requirement only in the event that appropriate conditions are included in the designation to ensure that the site at 16 Spartan Road can be accessed by large vehicles throughout construction and operation of the proposed works, subject to any further or other relief, or consequential amendments considered appropriate and necessary to address the concerns identified in NZ Steel’s submission.
- b Confirm the Notice of Requirement only in the event that appropriate conditions are included in the designation to ensure that effective urban design is incorporated at the detailed design stage to contribute to the reduction of anti-social behaviour, such as consideration of sight lines and the implementation of adequate lighting in areas with pedestrian activity.

3.2 NZ Steel wishes to be heard in support of its submission.

3.3 If others make a similar submission, NZ Steel will consider presenting a joint case with them at a hearing.



.....
Signature of person authorised
to sign on behalf of submitter

14th December 2023

.....
Date

Electronic address for service of submitter: Amy.Hill@bluescopesteel.com

Telephone: +64 9 375 8175 Mobile: 021 199 3886

Postal address: 131 Mission Bush Road, Glenbrook, Private Bag 92121, Auckland 1142

Contact person: Amy Hill - Environment Manager

14-Dec-23

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14 December 2023

To: Auckland Council

Attn: Planning Technician

Level 24, 135 Albert Street

Private Bag 92300

Auckland 1142

Submission on a Notice of Requirement for a Designation

SUBMITTER DETAILS

Name of Submitter: Silverfin Capital Limited ("**Silverfin**")

1. Silverfin Capital Limited on behalf of Hall's Portfolio Nominees Limited ("**Silverfin**") makes this submission on a new designation for the construction of four new grade-separated bridge crossings of the North Island Main Trunk rail line to replace three existing road-rail level crossing in the Takanini area ("**NoR 1**"). The application for NOR 1 was lodged by Auckland Transport through Te Tupu Ngatahi Supporting Growth in accordance with Sections 168A, 181, 189A, and 195A of the Resource Management Act (RMA) 1991.
2. Silverfin could not gain an advantage in trade competition through this submission.
3. Silverfin is directly affected by effects of the subject matters of the submission that –
 - a. Adversely affect the environment; and
 - b. Do not relate to trade competition or the effects of trade competition
4. Silverfin wishes to be heard in support of their submission.
5. If any other submitters make a similar submission, Silverfin would consider presenting a joint case with them at the hearing.

Overview of Silverfin site

6. Silverfin is a property syndicator, whose nominee companies own properties across New Zealand.
7. Silverfin is the owner of a 2.1 ha site at 1 Spartan Road, Takanini ("**the Site**"), currently tenanted by Halls Group Limited, who provide cool-storage and cold-chain logistics solutions throughout the country. This includes the use of a range of refrigerated trucks including both truck and trailer and b-train trucks. The Site is an important asset due to its proximity to arterial roading infrastructure including State Highway 1. Currently the Site has efficient and easy access to and from both the north and south.

8. Silverfin has an interest in the proposed NoR 1 that is greater than the interest of the general public as part of the Site is subject to NOR1 and the Site will be directly affected by the proposed NoR 1. The proposed designation has the potential to give rise to adverse effects that would directly affect Silverfin given the part of the Site is within the area proposed to be designated by NoR 1.

Grounds for Submission

9. The submission relates to NoR 1 as it relates to changes to the Spartan Road rail crossing.
10. Silverfin opposes NoR 1 for the reasons below which include but are not limited to:
 - a. Silverfin opposes NoR 1 including the proposed closure of the Spartan Road rail crossing, as it will severely compromise the efficient operation of the existing use on the Site. The proposal will remove the ability of heavy vehicles to easily and directly access the motorway at the Takanini interchange from and to the north. The consequence is that heavy vehicles seeking to take such routes will either need to travel through extensive residential areas to the Manurewa (Grand View / Hill Road) interchange or to carry out challenging and potentially dangerous turning manoeuvres within the surrounding road network.
 - b. Silverfin opposes the extent of the designation boundary because it is considered unnecessary and has the consequential effect of unduly restricting the future development potential of a significant portion of land owned by Silverfin because no person may do anything in relation to the designated land without the written consent of the requiring authority as section 176 of the RMA would apply.
 - c. There has been inadequate consideration of alternative sites, routes or methods for the proposed grade-separated crossing. There does not appear to be a consideration of alternative arrangements that will:
 - i. Provide a better and more efficient design with smaller footprints within the industrial sites; and
 - ii. Take appropriate account of the need for cycle infrastructure in this industrial area, given that additional cycling infrastructure will be provided in nearby residential areas by the NoR.
 - d. Pursuant to section 184(1)(c) of the RMA, Auckland Transport proposes an extended lapse period for implementation of the proposed designation. Silverfin opposes the lapse date proposed of 15 years. The extension of 10 years to the statutory lapse period proposed is excessive and will prevent future development opportunities progressing in a cohesive and integrated manner. Sterilising the land until funding is allocated does not represent the sustainable management of a natural and physical resource, and therefore would not meet the sustainable management purpose of the RMA 1991.

Relief sought

11. Silverfin seeks the following relief on NoR 1:
 - a. That NoR 1 be declined, and removed from the site at 1 Spartan Road.

- b. That the Spartan Road at-grade rail crossing remains in place pending development and implementation of an alternative access arrangement for the Site which enables safe, convenient and efficient access for heavy goods vehicles to the arterial and motorway heading both south and north.
- c. If the NOR is approved, that the extent of the designation boundary of NoR 1 be reviewed and reduced to minimise the required land take, and reflect the actual and reasonable area of land that is needed to accommodate the appropriate design for the future rail crossing. Alternative arrangements of the pedestrian and cyclist overpass should be considered to reduce the requirement for the permanent acquisition of private land and to account for the need for cycle infrastructure within the industrial area given that additional cycling infrastructure will be provided in nearby residential areas by the NoR.
- d. If the NOR is approved, that the lapse date is reviewed and reduced to be consistent with section 184(1) of the RMA. The lapse date should be 5 years after the date on which the NoR is included in the district plan unless it is given effect to, substantial progress or effort has been made to give effect to, or a different period is specified when incorporated into the plan.
- e. If the NOR is approved, that the designation boundary be amended to show the operational extent around what will be the legal road reserve, and the construction extent (two separate designation boundaries).

Address for Service:

Barker & Associates Limited
Attn: Nick Roberts / Pamela Santos

PO Box 1986

Shortland Street

Auckland 1140

Contact Number: 029 666 8330 / 021 306 026

Email: nickr@barker.co.nz / pamelas@barker.co.nz

From the planner



14 December 2023

Auckland Council
Private Bag 92300
Auckland 1142

Attention: Manager, Plans and Places, Auckland Council
Via email: unitaryplan@aucklandcouncil.govt.nz

RE: NOTICE OF REQUIREMENT BY AUCKLAND TRANSPORT FOR THE TAKAANINI LEVEL CROSSING PROJECT

We act for Big Rock Commercial Ltd and Matthew Koppens Ltd, the owners and partial occupiers, of 26 Oakleigh Avenue, Takanini property. As the Notice of Requirement (NoR) directly affects their site, Big Rock Commercial Ltd and Matthew Koppens Ltd have a direct and obvious interest in the proposed designations, and the proposed works that this may enable.

Auckland Transport (AT), as requiring authority under Section 167 of the Resource Management Act 1991(RMA), has given a NoRs for works associated safe and reliable east west connections across the North Island Main Trunk Line (NIMT) in Takanini, which incorporate the anticipated KiwiRail planned expansion of the NIMT from two to up to four tracks, and the City Rail Link enabling works.

NoR 1 is for the construction, operation, maintenance and upgrade, as well as associated works, of transport infrastructure on and around Spartan Road, Manuia Road, Manuroa Road and Taka Street which includes:

- the closure of the existing level crossings at Spartan Road, Manuroa Road and Taka Street;
- new bridges with general traffic lanes and walking and cycling facilities across the NIMT line at Manuia Road and Taka Street;
- new bridges with walking and cycling facilities across the NIMT line at Spartan Road and Manuroa Road.

In particular, this submission relates to the Manuia Road project area, which involves the construction of a new bridge with general traffic lanes and walking and cycling facilities across the NIMT line, in the vicinity of the intersections of Oakleigh Avenue with Hitchcock Road and the intersection with Great South Road.

Big Rock Commercial Ltd is the land owner of 26 Oakleigh Avenue, Takanini (the site). A portion of the property is leased to the submitters business, *Matthew Koppens Ltd*, an earthworks and drain laying business with the remaining portioning being leased to *Not Only Transmissions Ltd*, an auto transmissions business.

481 Parnell Road, Parnell | PO Box 37964, Parnell, Auckland 1151

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The Submitter will not gain an advantage in trade competition through this submission, and in any case are directly affected by the NoR.

Our submission

The Submitter supports the principle of removing existing transport deficiencies as well as providing for the forecasted future growth in the area, but opposes the NoR in regard to the proposed layout across their 26 Oakleigh Avenue site as it will significantly adversely affect the operation of the business to the point that it is can not operate from this location and is untenable.

The submitters opposition is on the basis that:

- (a) The project does not promote the sustainable management of natural and physical resources as is required by Part 2 of the RMA;
- (b) The project does not enable people and communities to provide for their social, economic, and cultural well-being and for their health and safety;
- (c) The project does not promote the efficient use and development of urban land and development infrastructure;
- (d) The Assessment of Environmental Effects is inadequate and does not address the significant adverse effects of the works in sufficient detail to address matters under section 171(1) of the RMA;
- (e) The potential adverse effects on the Submitter have been inadequately identified, considered, or avoided, remedied or mitigated;
- (f) The nature and extent of the benefits of the project have not been demonstrated to outweigh the potentially significant adverse effects of the project;
- (g) The adverse effects of the project are not sufficiently mitigated, including managing the effects of the NoR on adjacent activities;
- (h) The project will generate significant adverse social and economic impacts, including on the Submitters business;
- (i) The proposed conditions do not adequately address the potential; for adverse effects, including significant adverse effects for the project for which the designation is sought.

Reasons for the Submission

To understand the Submitters position, details regarding the specific operation at the site are provided below, together with their reasons for the submission:

1. The existing business at 26 Oakleigh Avenue (Lot 3 DP199386, 1/5 Share Lot 6 DP199386 (NA128A/443) occupies the 936m² site, which is currently zoned Business – Light Industry. The site is occupied by an approximately 8m height, 580m² purpose built concrete industrial warehouse and ancillary office building (circa 2010) and forecourt. The property is separated into two industrial units (by a party wall).
2. In part of the building, the submitter operates Matthew Koppens Ltd Drainage and Earthworks from the site, having built the business up over a number of years, with employees who live locally and rely on this work for the livelihood. This business provides drainage services to

commercial and domestic clients, including the installation of stormwater and wastewater systems, land preparation and earthworks. The site, as well as being the commercial business premises provides storage of the business's machinery and materials, as well as providing workshop facilities.

3. The remainder of the building is leased to *Not Only Transmissions Ltd*, who operate a business specialising in the importation, supply and service agency for bus transmissions. The tenant has a lease for two years, with two further right of renewals.
4. The site is accessed via a shared ROW from Oakleigh Avenue, which services the site and four other industrial units at 24 Oakleigh Avenue and one at 22 Oakleigh Avenue, Takanini. The property is located 275m from the intersection of Oakleigh Avenue and Spartan Road and 233m from the intersection of Manuroa Road and Oakleigh Avenue, which currently connects to the Great South Road and the Southern Motorway (and both of which are affected by the proposed changes purported in the NoR). The property is also located to the east of the NIMT.
5. Figure 1 below depicts the existing layout of the business on the site.



Figure 1: The 26 Oakleigh Avenue, Takanini site (source: Geomaps)

6. Figure 2 below illustrates the significant changes that are proposed on and adjacent to the 26 Oakleigh Avenue site as a result of the NoR. The full extent of NoR 1 and the proposed designation boundaries are shown below:

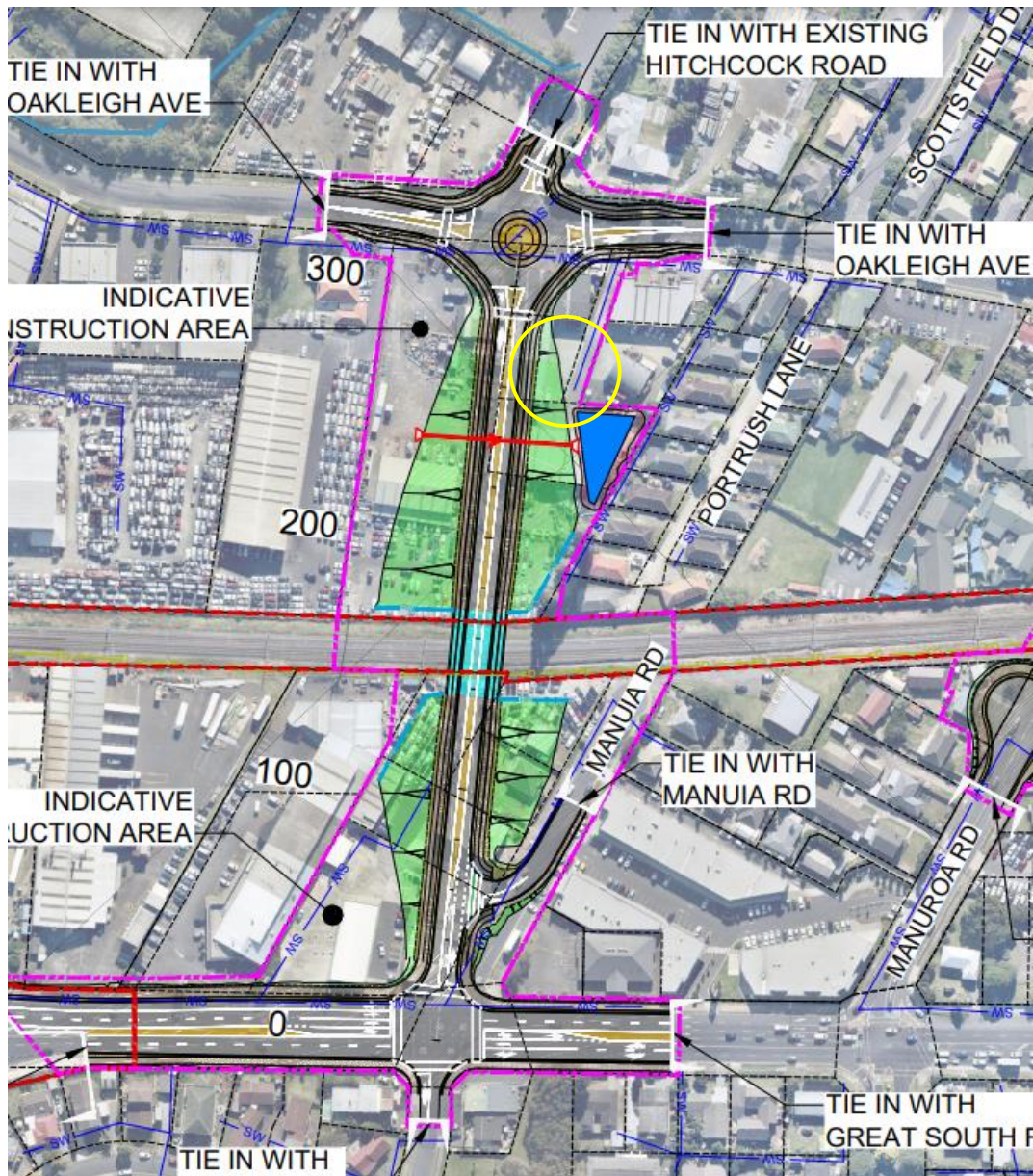


Figure 2 General Arrangement Plan NoR1 with location of the site highlighted in yellow (Source Auckland Transport)

7. These changes proposed include the following:
- Closure of the existing road corridor to vehicular traffic across the NIMT;
 - Construction of a new 2 lane (one each direction) lane vehicle bridge, including active modes of transport across the NIMT;
 - Construction of a new 2 lane road corridor connecting to a signalled intersection at Great South Road and roundabout on Oakleigh Avenue, adjacent the Hitchcock Road intersection;
 - Ramps and stairs will connect to the bridge on either side of (east and west) of the NIMT and will tie into existing streets; and
 - Establishment of proposed embankments adjacent the new transport corridor.

8. To facilitate these works, AT seeks the designation boundary to encompass the entire 26 Oakleigh Avenue site, as well as neighbouring properties (shown as the pink dashed line).

Level of investment

9. The Submitter's family has been instrumental in the design and construction of the purpose-built facility on the property in 2010, injecting significant investment into the site, without any prior advice from Auckland Transport or Auckland Council that there was a likelihood of a designation change or changes to the current roading infrastructure in the locale of their property.
10. Had they been aware of the AT project, it would definitely have reconsidered such an investment in this location, and potentially avoided the loss of property and business value that this NoR will inflict, as well as the overwhelming stress this has caused personally.

Inadequate Consultation

11. The AEE states that directly affected landowners have been contacted to discuss the project and expectations. It appears significant steps have been made by AT to progress the designation and associated works through the NoR process, however there has been very little detailed information given as part of consultation of the proposed works and how the owners of 26 Oakleigh Avenue site were likely to be affected, other than to convey information regarding the PWA acquisition process.
12. The NoR package also does not include targeted information on how their property is to be affected by the proposed works, or whether alternative alignments are feasible for this property and its neighbours. Although a meeting was undertaken between AT and the Submitter, no discussion, or resolution was reached, regarding how changes could be incorporated to address the impacts on the property, even when alternatives were suggested by the Submitter. Consequently, the Submitter does not consider that they have had detailed conversations with the Requiring Authority, how the effects of the NoR can be avoided or mitigated on their business and property.
13. As a result, the Submitter considers that there has been ineffective consultation with the general public, and particularly them, as an adversely affected land and businessowner. In many ways the submitter feels that AT have paid "lip service" to consultation, where while their opinion and thoughts were recorded, no feedback or response to the suggestions and alternatives have ever been given by AT. This is very distressing to the Submitter, considering their property and livelihood are significant adversely affected by the proposal and it is causing a huge amount of stress/anxiety.

Impact on the Submitter and their business

14. The proposed works associated with the Manuia Road project, as currently set out in the NoR will severely impact the *Matthew Koppens Ltd* business, as well as that of *Not only Transmissions Ltd*, to a point where they will not be able to operate from the property in any

form, given that the proposed alignment of the designation encompasses their entire property.

Lapse period

15. The implementation of the entire TLC project (as the past experience with other AT projects has shown us) is likely to be undertaken in stages, and may end up being undertaken over a longer period of time than envisaged, which will have sustained impacts on the submitters ability to operate their business, or to retain tenants and a market rental for the leases. There is, also potential for funding and delivery time blowouts, lack of community engagement and a total disregard on the impact to people's business and livelihoods.
16. AT contend that extension to the lapse period enables them to undertake detail design, obtain necessary consents, procure funding, undertake tendering and procurement as well as property and access negotiations. It is noted that within the AEE there is little detail of funding of the projects (noting that this set of NoRs are likely reliant on initial funding earmarked for Auckland wide level crossing projects, which are yet to be prioritized for implementation).
17. Within the AEE, AT notes that long lapse period before active property acquisition or construction will ensure that businesses are informed and have time to undertake their own business planning. This is in our view is hardly an effective mitigation measure for managing the socio-economic and property impacts and particular the significant emotional and mental health effects of the proposed NoRs. On the contrary, having such a loose timeframe actually makes it more difficult to develop a suitable 'business plan'.
18. AT have requested to extend the lapse period to 15 years for the project and as such it is appropriate that the Council balance the need for the lapse extension with the potential "blighting" effects for existing landholders and leaseholders. In our view the further uncertainty generated by the NoR and its 15-year implementation time period, will further negatively impact the submitters business, causing significant hardship and potential difficulty selling the business as a going concern in the years before the TLC works are realised.

Lack of detailed design

19. As we have noted above, there is a lack of detailed design surrounding the proposed works (including changes to access, landscape works or construction management details) and therefore puts all landowners and leaseholders "on notice", with no certainty as to how they will be affected at a per site basis or "macro" level, and for how long. The lack of detailed discussion with the owners of 26 Oakleigh Avenue site, as to how their business may be accommodated through changes to the alignment further reinforces the hopelessness that they feel.

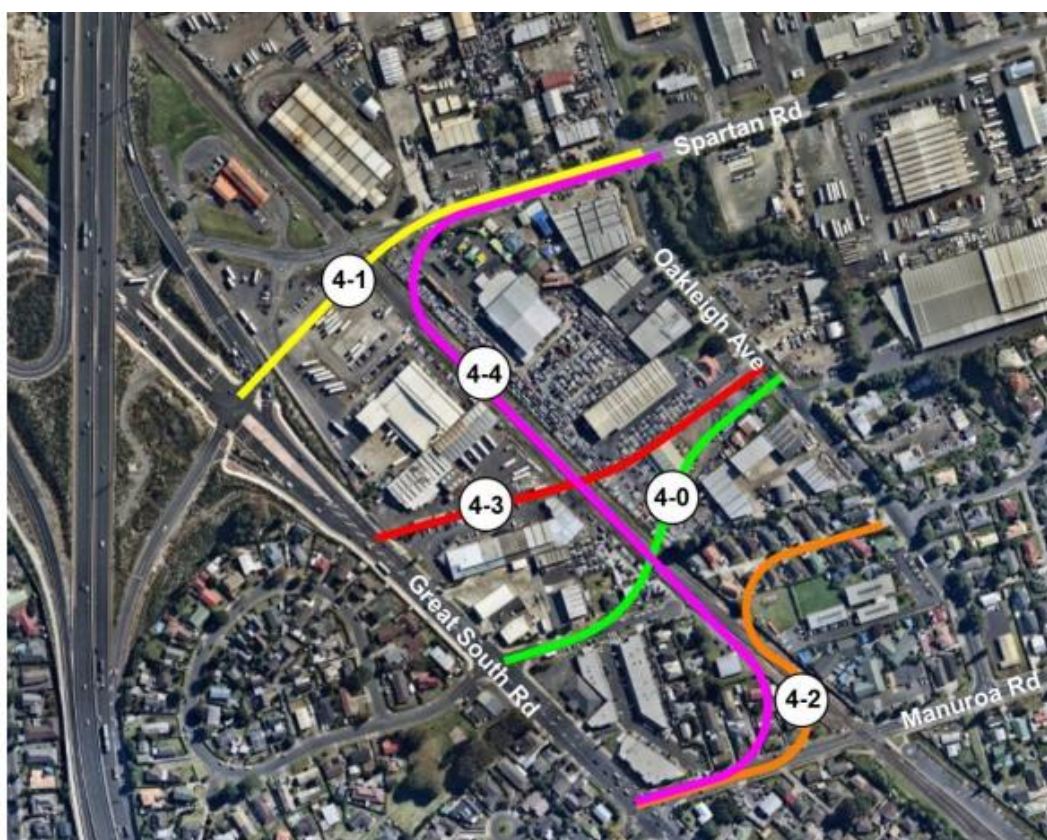
Potential loss of livelihood

20. Any future expansion or development plans for the site, possible opportunities for marketing the business for sale, and affecting retirement plans, are therefore likely to be compromised.

21. Relocating to another property with a similar purpose-built building in such a good location, without additional financial outlay also seems unlikely in Auckland's commercial property environment.
22. In addition, given that a portion of the site is leased, Big Rock Commercial is also at risk of losing the rental income because of the uncertainty of the proposed works in the shorter term.

Consideration of Alternatives

23. Section 171(1)(b) of the RMA requires that AT must undertake a robust assessment of alternatives, particularly when they do not currently own the land required for the Takanini Level crossing project. Section 5.4 of the AEE notes that the preferred network layout for the NoRs has been based on "a comprehensive and robust optioneering process considering specialist assessment and feedback from Mana whenua, stakeholders and landowners and the community", and summarises that "As such it is concluded that adequate consideration has been given to alternative sites, routes, and methods for undertaking the work, satisfying the requirements of section 171(1)(b) of the RMA".
24. The five options considered in the AEE are shown in Figure 3 below and summarised from the AEE.



No.	Option	Description
4-0	Connection using Manuia Road	Connection from Oakleigh Avenue through to Manuia Road and onto Great South Road (GSR) to reduce industrial / heavy traffic through residential area.
4-1	Option to connect to SH1 off-ramp intersection	Spartan Road alternative to resolve turn restriction issue with existing intersection with GSR. Intersection with GSR is moved and consolidated with SH1 southbound (SB) offramp (exact arrangement to be determined during route refinement).
4-2	Manuroa Road to Portrush Lane	Portrush Lane to Manuroa Road (west) to reduce industrial/heavy traffic through residential zoning. Involves a lengthened structure located over the NIMT corridor. Maintains existing intersection with GSR and Oakleigh Avenue.
4-3	Connection from Hitchcock Road to Great South Road	New road alignment through industrial zone at interface with SH1 interchange. New intersection with GSR and Oakleigh Avenue.
4-4	Manuroa Road to Spartan Road	Spartan Road to Manuroa Road (west) to maintain Spartan Road as industrial/heavy traffic through route. Maintains existing intersection with GSR and Spartan Road alignment. Involves a lengthened structure located over the NIMT corridor. Maintains existing intersection with GSR and Oakleigh Avenue.

Figure 3: Option Assessment for Manuia Road (source TLC – AEE (Appendix A))

25. It is our submission as an affected landowner, that AT has not adequately assessed alternative sites and routes, as is required by s171(1)(b) RMA, and that the Further Options Assessment in Table 7.6 of Section 7.2.4 of the AEE lacks the detail for affected parties to ascertain the weighting of effects and what the analysis entailed.
26. In particular the AEE, notes that Scenario 4-3 (connection from Hitchcock Road to Great South Road) could also deliver the outcomes sought in providing industrial traffic without diversion through the residential area. The AEE concludes that both scenarios “*did not have any anticipated highly adverse effects unlike the other additional scenarios, or the previous scenarios assessed. However, as Scenario 4-0 with the connection at Manuia Road would be located further away from the Takanini interchange it was considered to provide slightly greater network resilience and greater traffic benefits. Accordingly, Scenario 4-0 is the preferred scenario.*”
27. From the diagrams provided in the NoR Scenario 4-3 appears to avoid the Submitter’s property and a number of others around it, and could be aligned to better reduce the impact on private property by avoiding existing buildings and structures. As it is unclear within the documentation the weighting given to the social-economic impacts on business owners and operators within the Options Assessment, who will lose their premises and find it difficult or impossible to find an appropriate place to relocate to.
28. In our opinion, given that AT are wanting to take private property for this project, the consequential social and emotional impact on personal and property rights, health and wellbeing, and other socio-economic impacts (as described in section 6.2.2 of the AEE, and graded from moderate to extreme in the Appendix E of the AEE), should be given more weight.

29. In our view every effort to avoid these impacts should be explored and considered by AT, before referencing the PWA as a mitigation and remediation solution. In cases where there are potentially viable alternatives which would minimise the effects on affected property owners, this is disingenuous and not in our opinion, consistent with the statutory tests.

Option refinement through detailed design

30. A number of options for the Manuia Road Route are considered in Table 10.2 – 10.4 of Appendix A – Assessment of Alternatives within the AEE., with the Manuia Road Option 2 alignment (shown in Figure 4 below) being considered by the Requiring Authority as the preferred option,



Figure 4: AT Preferred Manuia Road Route (Option 2):Source: TLC – AEE (Appendix A)

31. In the absence of any further detailed design provided within the AEE, it would in our opinion be appropriate for AT to engage in further discussion with the Submitter as to whether the designation boundary, design of the bridge, and supporting earthworks (shown in Figure 4 above) could be further refined to allow for the continued use and occupation of the 26 Oakleigh Avenue property. This could be, for example, investigation the potential for realigning the proposed bridge, earthworks and roundabout to the north (either through moving or a slight skew) to avoid the 26 Oakleigh Avenue property, given that adjoining property which is already affected, appears to have more space and less built development than the Submitters site.
32. This would be preferred outcome for the Submitter, rather than compulsory property acquisition and demolition of the buildings and infrastructure on their property, under the PWA.

Relief sought

- (a) The Submitter seeks that the NoRs in their current form are declined;
- (b) In the event that the NoRs are not declined, the Submitter seeks that the designations are amended to avoid, remedy or mitigate all matters of concern raised in this submission, but not limiting to:
- Further investigation of Option 4.3 Connection of Hitchcock Avenue to Great South Road to avoid the impact on 26 Oakleigh Avenue, Takanini;

- Minimising the encroachment of the designation into the Submitters site at 26 Oakleigh Avenue, Takanini through detailed design development and potential modification, to enable the continued operation from the site;
 - Ensuring that any temporary or permanent effects on the ongoing operation of the site, including access and egress and manoeuvring are minimised.
- (c) If the designations can not be amended, the Submitter seeks:
- The opportunity for AT and Supporting Growth enter into formal negotiations with the Submitter to formalise compensation under the Public Works Act for remedies including but not limited:
 - Full replacement value for acquisition of a “like for like” site and built development;
 - Full loss of income relating to land and lease arrangements, including current and potential rental income for the duration of the 15 year implementation timeframe, or further should it extend;
 - Injurious effect
 - Toll on personal health and wellbeing as a result of stress relating to this process
- (d) in support of their submission, we also ask that all updates and notices are also served to us as agents on the email and postal address noted below.
- (e) The Submitter also welcome the opportunity to enter into discussions with AT in respect of the issues raised, with a view to ensuring that their concerns are appropriately addressed.

Keren McDonnell

Keren McDonnell – As Authorised Agent and Planner for Mt Hobson Group

Address for Service:

Mt Hobson Group
PO Box 37964
Parnell
Auckland 1151
T: 09 950 5100

Email: keren@mhg.co.nz

481 Parnell Road, Parnell | PO Box 37964, Parnell, Auckland 1151

keren@mhg.co.nz | 09 950 5100

www.mhg.co.nz

Before you fill out the attached submission form, you should know:

You need to include your full name, an email address, or an alternative postal address for your submission to be valid. Also provide a contact phone number so we can contact you for hearing schedules (where requested).

By taking part in this public submission process your submission will be made public. The information requested on this form is required by the Resource Management Act 1991 as any further submission supporting or opposing this submission is required to be forwarded to you as well as Auckland Council. Your name, address, telephone number, email address, signature (if applicable) and the content of your submission will be made publicly available in Auckland Council documents and on our website. These details are collected to better inform the public about all consents which have been issued through the Council.

Please note that your submission (or part of your submission) may be struck out if the authority is satisfied that at least one of the following applies to the submission (or part of the submission):

- It is frivolous or vexatious.
- It discloses no reasonable or relevant case.
- It would be an abuse of the hearing process to allow the submission (or the part) to be taken further.
- It contains offensive language.
- It is supported only by material that purports to be independent expert evidence, but has been prepared by a person who is not independent or who does not have sufficient specialised knowledge or skill to give expert advice on the matter.

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification

Sections 168A, 169, 181, 189A, 190, and 195A of the Resource Management Act 1991



FORM 21

Send your submission to unitaryplan@aucklandcouncil.govt.nz or post to :

Attn: Planning Technician
 Auckland Council
 Level 16, 135 Albert Street
 Private Bag 92300
 Auckland 1142

For office use only
Submission No:
Receipt Date:

Submitter details

Full Name or Name of Agent (if applicable)

Mr/Mrs/Miss/Ms(Full Name) Jarrold Dixon

Organisation Name (if submission is made on behalf of Organisation)

bp Oil New Zealand Limited (Attention: Fergus Leo)

Address for service of Submitter

PO Box 99 873 Auckland 1149

Telephone: Email:

Contact Person: (Name and designation if applicable)

This is a submission on a notice of requirement:

By: Name of Requiring Authority	Auckland Transport
For: A new designation or alteration to an existing designation	Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

The specific parts of the above notice of requirement that my submission relates to are: (give details including property address):

Refer attached submission

My submission is:

I or we support of the Notice of Requirement I or we oppose to the Notice of Requirement
 I or we are neutral to the Notice of Requirement

The reasons for my views are:

Refer attached submission

(continue on a separate sheet if necessary)

I seek the following recommendation or decision from the Council (give precise details including the general nature of any conditions sought).

Refer attached submission

- I wish to be heard in support of my submission
- I do not wish to be heard in support of my submission
- If others make a similar submission, I will consider presenting a joint case with them at a hearing

Jarrod Dixon
 Signature of Submitter
 (or person authorised to sign on behalf of submitter)

12/14/2023
 Date

Notes to person making submission:

If you are making a submission to the Environmental Protection Authority, you should use Form 16B.

You must serve a copy of your submission on the person who gave the notice of requirement as soon as reasonably practicable after you have served your submission on the Council (unless the Council itself, as requiring authority, gave the notice of requirement)

If your submission relates to a notice of requirement for a designation or alteration to a designation and you are a trade competitor of the requiring authority, you may make a submission only if you are directly affected by an effect of the activity to which the requirement relates that:

- (a) Adversely affects the environment, and
- (b) Does not relate to trade competition or the effects of trade competition.

**SUBMISSION ON NOTICE OF REQUIREMENT: TAKANINI LEVEL CROSSING (TLC): SPARTAN
ROAD, MANUIA ROAD, MANUROA ROA AND TAKA STREET (NOR 1)**

AUCKLAND TRANSPORT

To: Plans and Places
Auckland Council
Private Bag 92300
Auckland 1142

Via email: unitaryplan@aucklandcouncil.govt.nz

Submitter: Bp Oil New Zealand Limited
PO Box 99 873
AUCKLAND 1149

Address for Service: SLR Consulting Limited
201 Victoria St West
PO Box 911310, Victoria St West,
AUCKLAND 1142

Attention: Jarrod Dixon

Phone: 021 948 073

Email: jarrod.dixon@slrconsulting.com

A. Introduction

1. This is a submission on a notice of requirement (*NOR*) lodged by Auckland Transport for four new bridge crossings over the railway line in Takaanini, Auckland. The project is also referred to as the Takaanini Level Crossings project (*NOR 1*).
2. Auckland Transport (*AT*), as a requiring authority under Section 167 of the Resource Management Act 1991 (*RMA*), has given a *NOR* for works, including for new multi-modal bridge crossings of the North Island Main Trunk (*NIMT*) at Manuia Road and Taka Street, new active mode bridge crossings of the *NIMT* at Spartan Road and Manuroa Road, and for two consequential road closures. This submission relates to the proposed multi-modal crossing adjacent to Manuia Road that connects Great South Road to Oakleigh Avenue.
3. bp Oil New Zealand Limited (*bp* or *the Submitter*) operates the existing service station at 102 Great South Road, Takaanini (*bp Takaanini*) which is directly affected by *NOR 1*.
4. bp could not gain an advantage in trade competition through this submission.

B. Submission

5. The Submitter supports the principle of improving transport connections and safety in the Takaanini area but opposes this *NOR* as its service station, referred to above, is proposed to be decommissioned over the proposed construction period with no assurance that an operational service station can be reestablished on a site that has been significantly reduced in size.
6. The submitter's opposition is on the basis that:
 - The project does not promote the sustainable management of natural and physical resources as required by Part 2 of the *RMA*;
 - The project does not enable people and communities to provide for their social, economic, and cultural well-being and for their health and safety;
 - The Assessment of Environmental Effects is inadequate and does not address the significant adverse effects of the works in sufficient detail to address matters under section 171(1) of the *RMA*;
 - The potential adverse effects on the Submitter have been inadequately identified, considered, or avoided, remedied, or mitigated;
 - The nature and extent of the benefits of the project have not been demonstrated to outweigh the potentially significant adverse effects of the project;
 - The adverse effects of the project are not sufficiently mitigated, including manging the effects of the *NOR* on adjacent activities;
 - The project will generate significant adverse social and economic impacts, including on the Submitter's business; and
 - The proposed conditions do not adequately address the potential for adverse effects, including significant adverse effects.

C. Reasons for Submission

7. To understand the Submitters' position, key details regarding the specifics of its operation at the site are provided below.
8. Bp Takaanini is located at 102 Great South Road, (legally described as Lot 1 DP 176910) and comprises eight refuelling lanes (with canopy), EV charging station, carwash, truck stop and retail shop. The site also contains signage, landscaping, parking and manoeuvring areas. Ingress and egress to the site is provided via two vehicle crossings off Great South Road and one vehicle crossing off Manuia Road.
9. The service station site also comprises a range of infrastructure necessary for their daily operation including underground fuel storage tanks, fill points, underground fuel lines and stormwater infrastructure (e.g. oil and water separators). In addition to any specific resource consent requirements, the Submitter is also required to operate its retail fuel outlets in accordance with other legislation including the Hazardous Substances and New Organisms Act 1996 (*HSNO*) and Health and Safety at Work Act 2015 (*HSWA*). Such operational requirements include providing adequate access and manoeuvring areas for tanker trucks to enter the site, access fill points and then navigate and exit the site safely.
10. Figures 1 and 2 below are sourced from the NOR 1 application and illustrates how the proposed NOR works will impact the bp Takaanini site, including:
 - Proposed bridge crossing and associated fill batters situated across the southern portion of the site in the location of the existing retail shop, truck stop and Manuia Road vehicle crossing.
 - Proposed berm and fill batter extending around the western boundary of the site where it adjoins Great South Road.
 - Site shown as an 'Indicative Construction Area' for the project.

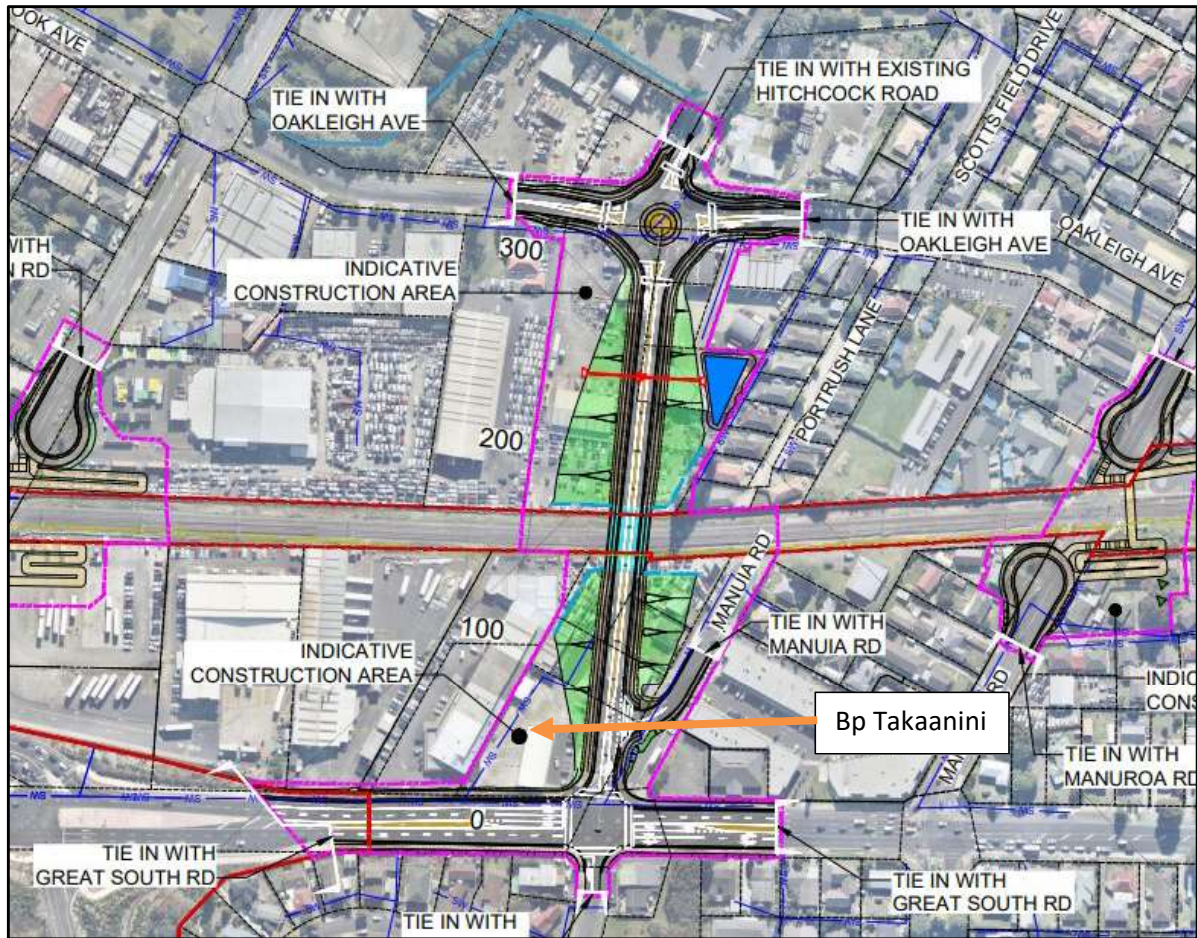


Figure 1: Excerpt from General Arrangement Plan

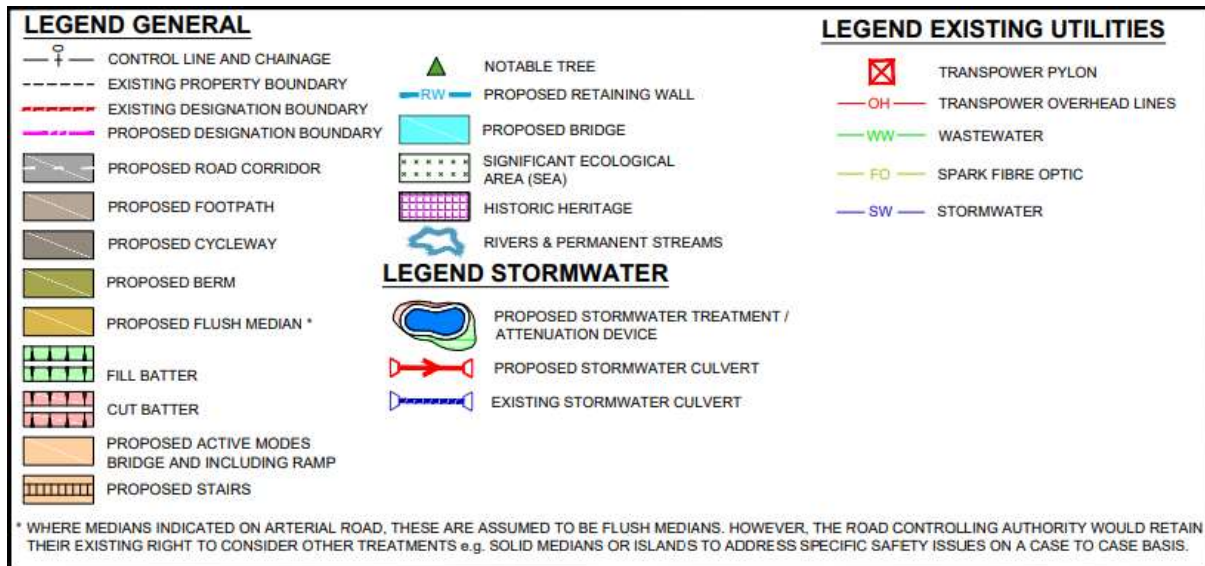


Figure 2: Excerpt from General Arrangement Plan (legend)

11. The proposal will result in significant adverse effects on the Submitter as application shows the proposed bridge and associated fill batter occupying the southern portion of the site and suggests that the entirety of the site will be needed for construction purposes (Figure 1). As such, the service station will need to be decommissioned for the duration of the construction period which is understood to be between 3.5 – 5 years.
12. The application also states that the bp service station will '*potentially be lost*¹' which suggests that, at this stage, the application does not envisage the service station re-opening on the site. The proposal is therefore likely to generate significant temporary and permanent adverse effects on the Submitter through the permanent closure of the service station which, in turn, will have adverse effects on the local and wider community.
13. The Submitter seeks an opportunity to reestablish the service station on the site, albeit an amended layout and reduced size, following the completion of construction. The Submitter therefore seeks that the encroachment of the fill batter on the southern and western boundaries and berm into the site be reduced or amended to an alternative option, such as retaining, to enable the greatest possible site size to provide a workable service station. To ensure said workability, the Submitter wishes to confirm that heavy vehicle access to the proposed multi modal connection can be provided. The Submitter therefore seeks ongoing communication and engagement with AT to understand the timing and design of the project to inform whether a site redesign is possible.
14. As set out in Paragraph 9, bp is required to operate its site in accordance with other legislation including requirements around the transport, storage and distribution of hazardous substances. Even if the extent of project is minimised as much as practicable within the site, there is no guarantee that the service station can be reestablished on the site that complies with the relevant legislative and operational requirements, noting that a new resource consent or a variation to the existing consent will also need to be obtained by the Submitter.

D. Relief Sought

15. The Submitter seeks that NOR 1 in its current form is declined.
16. In the event that NOR 1 is not declined, the submitter seeks that the designation is amended to avoid, remedy, or mitigate all matters of concern raised in this submission, including, but not limited to the following:
 - Communicate project milestones with the Submitter including the likely construction timeframe.
 - Engage with the Submitter through the design process to discuss the possibility of re-establishing the service station on the site, including minimising the encroachment of the project within the site and site access.

¹ Page 111 of the AEE

Signed on and behalf of bp Oil New Zealand Limited as authorised signatory.



.....

Jarrold Dixon
Senior Planner

Dated this day of 14 December 2023

To:

Planning Technicians
Plans and Places
Auckland Council
PB 92300
Auckland 1142
By email: unitaryplan@aucklandcouncil.govt.nz

Supporting Growth - Notice of Requirement (NoR) 1 – Rail Crossing at 14 Taka St – Takanini**From:**

Bhupinder Singh & Navpreet Kaur
BNAP Holdings Ltd
14 Taka Street
Takanini

This is a submission on a NoR:

By: Auckland Transport

For: Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

The specific parts of NoR that our submission relates to are:

Property address 14 Taka Street Takanini. Our land is subject to the NoR to take the whole property to put an overbridge over the railway. Currently, we run a very successful childcare business on the land.

Our submission is:

We oppose to the NoR.

1. The general reasons for our submission are:

We understand that the main reason for the NoR is to find solutions to the current transport issues and remove the safety risks associated with rail crossings.

However, there are several issues that we have with the proposal.

In general terms, the NoR does not:

- meet the Purpose and Principles in Part 2 of Act;
- avoid, remedy and mitigate the significant adverse effects that the NoR will have on our social and economic wellbeing;
- meet the requirements of a NoR including the provisions in the Act, including s171, higher order national policy statements, and the AUP-RPS;
- adequately evaluate alternative sites, routes or methods, for undertaking the works;

- meet the threshold for being work that is reasonably necessary;
- place sufficient weight on the social and economic costs to landowners and businesses in the overall AEE assessment; and
- demonstrate sound resource management practice.

Without limiting the general scope of the concerns raised above, the following specific concerns are also raised.

2. Economic and Social Impacts

As mentioned, our property at 14 Taka Street is where we run our well established and successful Childcare business, that serves the community. It is not easy to run a centre that complies with all the increasingly stringent regulatory requirements.

One of our biggest concerns is that we will not be able to get a Ministry of Education licence under new Network Management Law after we lose our current one which is based on the current site. Not having a licence will significantly impact on our livelihood.

Therefore, it is not simply a matter of relocating to another physical site, even if a suitable site were available.

There is also the issue of being able to obtain to another suitable site for a centre within the area. If relocation poses issues, we will not only lose our property, but our livelihood and income.

However, of equally significant concern to us, is that the NoR could jeopardise the longstanding relationships we have with our children and their parents. Many people in the community rely on us to provide quality and safe childcare, and understandably, earning the trust of parents is not easy and has taken many years of hard work by our skilled staff.

We are of course also very concerned about the wellbeing of our dedicated and well-trained staff. Childcare is a challenging area to secure and retain quality staff, and we have a team of 14 that we have worked very hard build up over many years. If the centre closes and there is no continuity with any new centre, it is inevitable that most of our team will be lost to other centres, and we will have to start again.

It is unclear to us how the Supporting Growth project and will properly compensate us for the social and economic effects that our business, parents, and children will suffer with the taking of the land.

It is also unclear what the form of assistance may be to secure another site, relocate to it, and very importantly, ensure that all of the necessary licencing and regulatory requirements are met. We refer to the meeting notes with Julie Boucher from the SIA Team.

3. Necessity of the Works

It is submitted that the works are not reasonably necessary, at least currently. It is noted that one of the main original strategic justifications for that works was to service the growth that was planned in the AUP for Future Urban Zoned land in Takanini and areas further south such as Drury.

However, while some parts of these area are proceeding with plan changes from live zoning, the recently confirmed Auckland Council Future Development Strategy, removes some FUZ zoning in Takanani due to flooding risks, and defers the timing for the development of other Southern areas. Therefore, it is not clear that these rail upgade works are necessary to be undertaken at this time.

4. Timing and Uncertainty

Even at this relatively early stage of the NoR process, the uncertainty created by the proposed taking of our land is affecting the childcare centre and the levels of service we are offering. For example, with such a high use facility, there is of course an on-going programme of works to update, renovate and maintain the facilities. Our next major focus was on refurbishing the now dated outdoor play area. However, we have put those plans on hold now because we are unsure if it is worth investing the resources because we would normally expect the new works to be there for 10 years or so. If we don't do the work, then parents can get the impression that the facility is not being maintained and is not as attractive as other centres.

Also, because this is a public process, we are already getting parents asking when the Centre will be closing.

As to be expected, parents undertake a lot of investigations before committing to a childcare facility. This is particularly the case where they have, or are planning to have, more than one child and understandably want the continuity and convenience of attendance at one centre over many years. This provides more stability for the children and fosters deeper relationships between children and their parents and the staff.

The lapse time for designations under s 184 of the RMA is 5 years unless the designation provides a different period.

It is noted that a designation was deemed the most logical and effective method due to the certainty it provided all parties including affected landowners (e.g., on timing).

However, contrary to the reasoning above, for this project, the time frame has been increased to 15 years. This significant period, 3 times the statutory guidance, is not certain for us in any sense.

This is particularly the case at our age because we may want to retire after a couple of years, or later for any personal reasons, meaning that the increase in time is too long and uncertain. If the project could take up to 15 years and we need to sell, the existence of the

designation and the uncertainty it creates will have a significant adverse impact on our economic wellbeing.

We want to reserve the right for early settlement with AT and the notice period duration for early settlement should be 6 Months.

Landowner/developer negotiations should have been an important part of the process considering our land and business is being appropriated from us.

We do not consider it acceptable, and consistent with the overall purpose of the powers of designation, to, for example, confirm a designation, which effectively sterilises the land for sale and changing/maintaining the activities, when there is no planned capital budget to undertake the works. This means that the project is effectively “speculative”, and may in fact, never be implemented. Government and Council/AT priorities could change.

Therefore, if the Panel, were to grant such a long period of time as 15 years for the NoR before it lapses, the question is who should bear the effects of the uncertainty during this period, the landowners, or the requiring authority, that is exercising arguably draconian powers to take private property?

It is submitted that it is only fair and reasonable that it is the agency that should bear the consequences of the uncertainty created by the NoR/acquisition process, because they have control over the timing and outcomes.

For example, if our business was compromised for 12 years because we do not invest in it, and parents prefer other centres because ours could close at relatively short notice, but then the project is cancelled or changed at year 13, that would be very unfair. There are usually waitlists for the better centres, and parents would not want to compromise care at a lower quality facility, so are likely to try and avoid that risk, by not committing to our centre due to uncertainty.

The only reasonable approach is that if the full works are not funded in the next 5 years, at least the budget for land acquisition should be funded. If then the works are not undertaken at year 13, the agency can then resell the land that they had acquired. The designating authority is responsible for the timing, and it should bear the costs of any losses with reselling rather than the business and landowners bearing this social and economic burden for an unreasonable period.

5. Alternatives Not Property Considered

Section 171(1)(b) of the RMA requires that adequate considerations should be given to alternative sites, routes, or methods when making a recommendation on a NoR when the requiring authority does not have interest in the land.

We do not believe that all the alternatives have been adequately considered in the process by AT in order to limit the amount of land being taken and used.

It is noted that Appendix A did assess the alternatives to a degree. However, several options did not progress past the initial considerations, and should have been more fully investigated.

Furthermore, options were not properly assessed against the impact that it would have on the surrounding people's property/land.

One option which could have limited the amount of land used and taken would have been rail-under-road trench. When comparing the rail-under-road trench and road-over-rail bridge summary they both had the same investment objectives, cost, and safety and design. A rail-under-road trench method of achieving the grade separation had one higher technical and two higher consentability requirements and was a bit of a longer time period.

Regardless, this option should have been explored more due to it potentially impacting the livelihoods of people in the surrounding housing areas less, for reasons including, avoiding, remedying, and mitigating:

- The adverse noise effects of the whole rail corridor on residents flanking the corridor.
- The adverse amenity effects of the unsightly gantry and cable infrastructure to provide power to the trains.
- The adverse visual amenity and dominance and shading effects of large concrete overbridge structures on the residential dwellings and public street and pedestrian spaces.

Further assessment of the rail-under-road trench could have found solutions to limit disruptions to the rail operations.

Even if the rail-under-road trench could not have been applied at Spartan Road (stated in Appendix A) there could have been several options considered to ensure that the right option was applied depending on the needs of the different areas.

6. Adverse Social and Economic Effects

If the NoR is confirmed, it is essential that the requirements of s 5 of the Act, in terms of enabling people to provide for their social and economic wellbeing is achieved (171(1)) is subject to Part 2).

We want fair compensation for our business, property, staff and services to the community that are at risk. Once the Demand Notice will be approved by Auckland Council, then it will be listed on our title and after that our business and property prices will be greatly impacted. This will lead to lower confidence of parents in the centre and less revenue so less investment in the physical infrastructure and services, so amenity values drop, and this risks becoming a negative self-reinforcing decline.

While it is understood that the acquisition process is separate, it is requested that the Panel carefully consider what provisions should be put in place to ensure that landowners are properly compensated for adverse impacts on their social and economic wellbeing.

7. We seek the following recommendation or decision from the Council.

That the NoR be declined.

In the alternative, that the conditions of the NoR, including the duration of the approval, and process of acquisition, be shortened to 5 years to address the uncertainty for landowners and enable people to continue to provide for their social and economic wellbeing.

We wish to be heard in support of our submission.

Filed on behalf of:

Bhupinder Singh & Navpreet Kaur
BNAP Holdings Ltd
14 Taka Street
Takanini

By their Counsel:



Peter Fuller
Barrister
Quay Chambers

Address for Service:

PO Box 106215, Auckland City 1143
peter.fuller@quaychambers.co.nz
021 635 682

Date: 13 Dec 2023

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1209] Notice of Requirement online submission - Gregory Paul Brumby
Date: Thursday, 14 December 2023 2:15:38 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Gregory Paul Brumby

Organisation name: Takanini Residents Action Group

Full name of your agent:

Email address: greg57ptk@xtra.co.nz

Contact phone number:

Postal address:
27 Pateke Drive
Takanini 2112
Takanini
Papakura 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
Spartan Road, Manuia Road, Manuroa Road and Taka Street

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

1. We do not consider there has been adequate consultation with the community and affected property owners. Some property owners were only aware of the significance of the grade separation project when they were served with the NOR. Council should consider using social media, community groups, business organizations, churches, temples etc. to increase early and extensive community engagement. 2. There have been no designs or artists impressions of the pedestrian/cyclist overbridges in Spartan and Manuroa Roads shown to the community. We have not been given adequate information on these structures and have concerns that they may not be suitable for people with mobility issues.

I or we seek the following recommendation or decision from Auckland Council:
The NOR closing date should be extended to allow for more community consultation.

Submission date: 14 December 2023

Attend a hearing

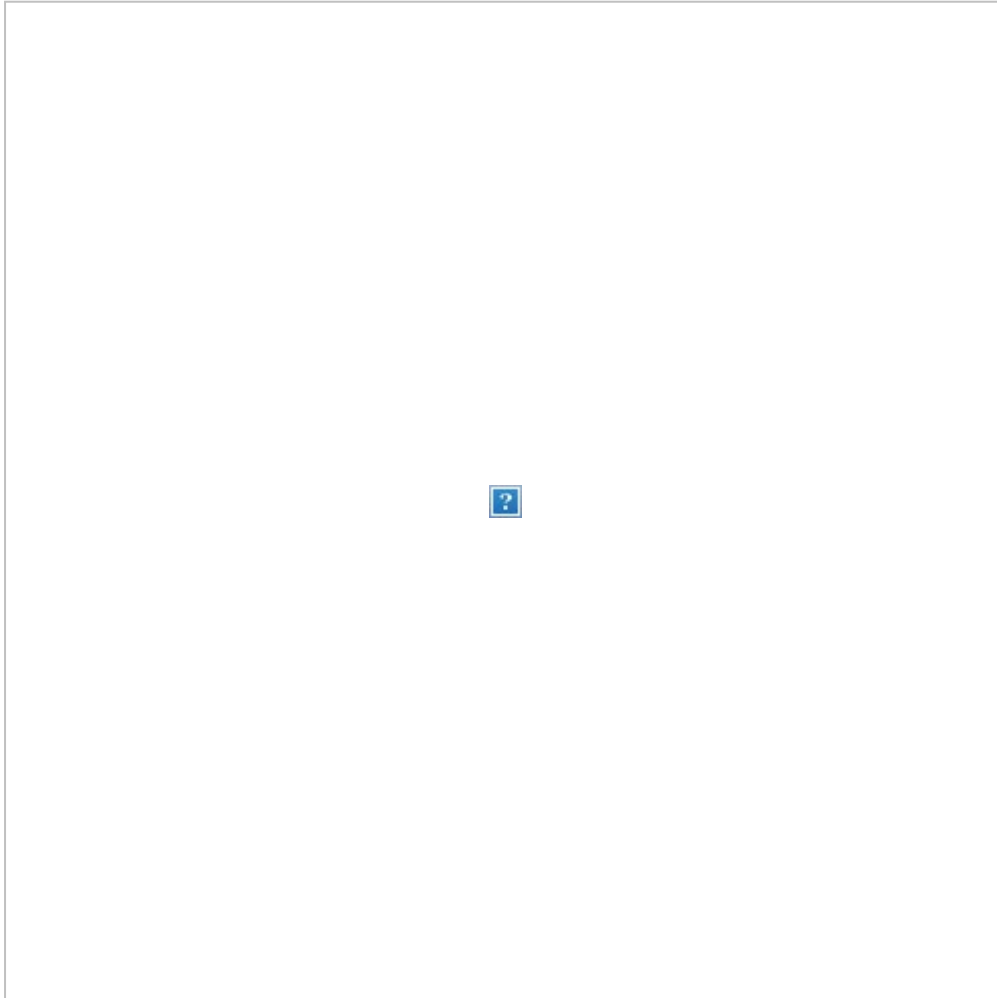
Do you wish to be heard in support of your submission? No

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal

- details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1211] Notice of Requirement online submission - DALJIT SINGH
Date: Thursday, 14 December 2023 2:30:45 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: DALJIT SINGH

Organisation name: Supreme Sikh Society of NZ

Full name of your agent: DALJIT SINGH

Email address: supremesikhstetynz@gmail.com

Contact phone number: 2

Postal address:
70 TAKANINI SCHOOL ROAD - TAKANINI 2112
Auckland
Auckland 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
SIKH TEMPLE 70 TAKANINI SCHOOL ROAD TAKANINI 2112

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
7,000 people weekly visits Takanini Gurdwara and closing Taka Street, Manuroa and Spartan Road same time means 75% of our congregation will not able to come which is great loss to a charity which helping wider community

I or we seek the following recommendation or decision from Auckland Council:
The work should be undertake one by one road instead all three shutting down together.

Submission date: 14 December 2023

Attend a hearing

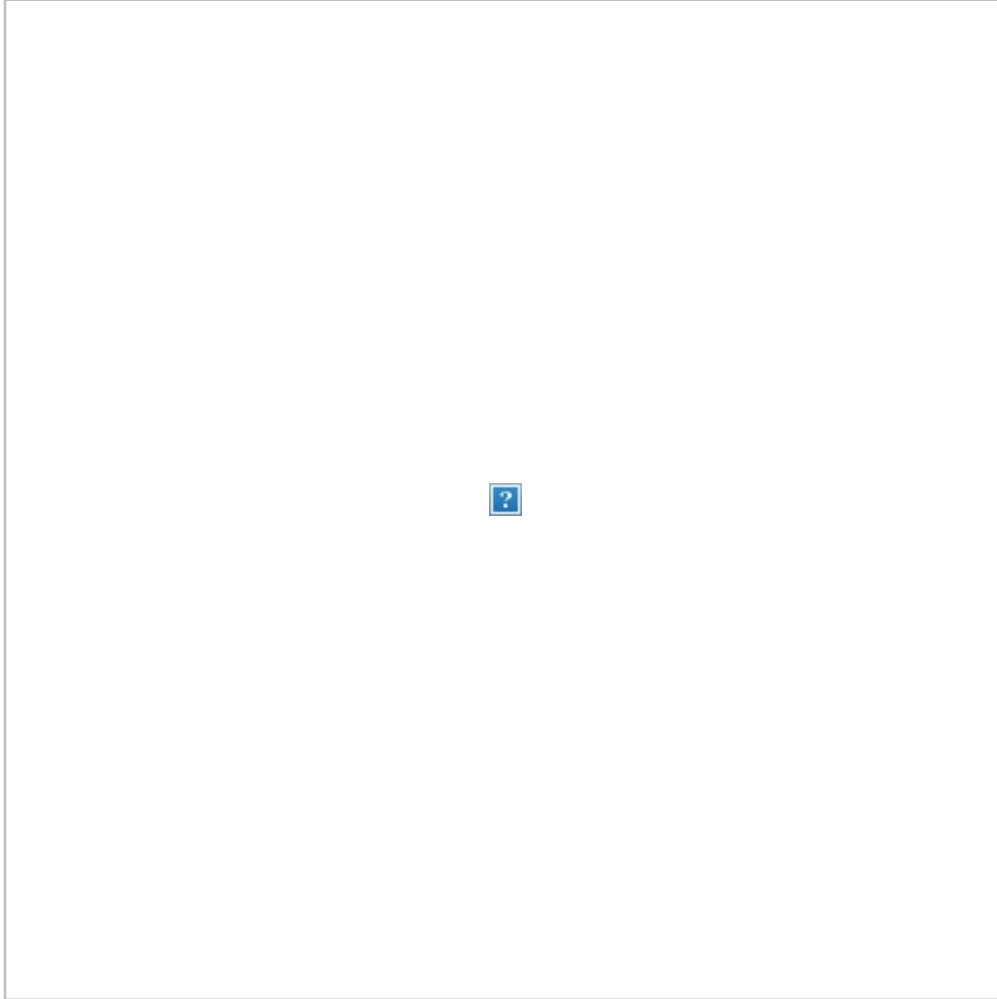
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1212] Notice of Requirement online submission - Natasha brownlee
Date: Thursday, 14 December 2023 2:45:38 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Natasha brownlee

Organisation name: H2O Pipelines Ltd

Full name of your agent:

Email address: accounts@hpl.co.nz

Contact phone number: 0275769351

Postal address:
PO Box 251 Drury
Drury
auckland 2247

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
22 Oakleigh Ave Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

We will be right next to the off ramp / over bridge. The noise and vehicle pollution that is going to directly affect our property is going to be unbearable. I feel that there possibly will be extra crime caused to my belongings and all my property will be in direct sight from the over bridge. My driveway entrance is on the round about which will be extremely dangerous to entre and leave my property

I or we seek the following recommendation or decision from Auckland Council:

I seek the council to decline the notice

Submission date: 14 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

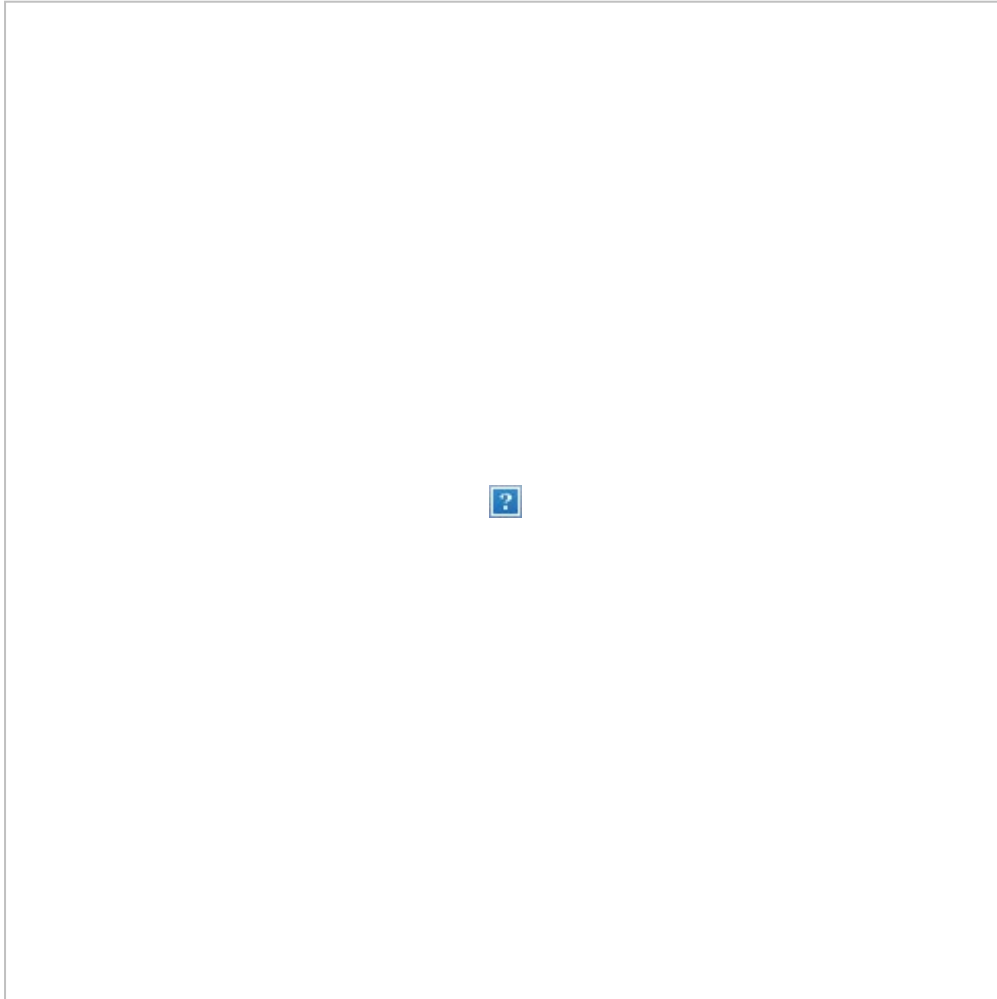
Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1213] Notice of Requirement online submission - Andrew Stewart
Date: Thursday, 14 December 2023 3:00:48 pm
Attachments: [TAKANINI CROSSING.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Andrew Stewart

Organisation name: On Track Trust

Full name of your agent: Andrew Stewart

Email address: andrew@vertexlubricants.co.nz

Contact phone number: 021333426

Postal address:

andrew@vertexlubricants.co.nz

Auckland

Auckland 2105

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
120 Takanini School Rd

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The Council proposes to limit the Vehicle crossings in the Takanini area from 3 two lane crossings to 2 this will cause unprecedented congestion Takanini is a rapidly growing commercial area that has constant congestion problems at present with the present 3 crossings being Spartan rd ,Manuroa rd and Taka st looking to the future with far larger traffic volumes from takanini and beyond trying to access the Motorway. All future bridges should be active mode bridges including Heavy vehicles on all bridges including the new Manuia rd bridge. No road should be closed at all until the bridges are completed otherwise congestion will be unworkable. The construction should be done one bridge at a time so we still have a functioning (all be it reduced) infrastructure though this extended time period .

I or we seek the following recommendation or decision from Auckland Council:

We would like open discussions with council to come up with the best solution to keep maximum traffic flows during these changes over the coming years.

Submission date: 14 December 2023

Supporting documents

TAKANINI CROSSING.pdf

Attend a hearing

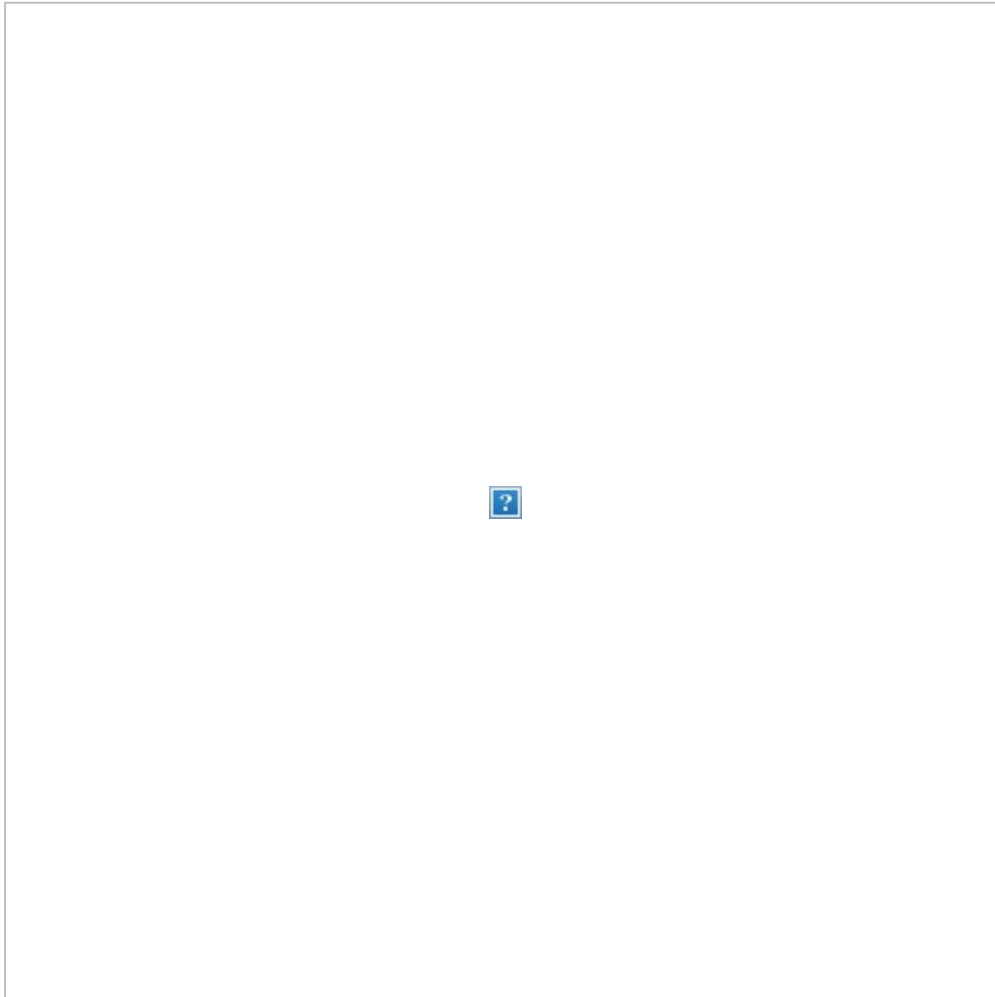
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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- on-site parking
- property access
- corridor-specific operational effects
- construction noise and vibration
- lapse period

(1) Alternatives

The TBA notes the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). The TBA says the assessment of alternatives is deficient.

However, should the option as proposed in the NoR1 of raising the road (i.e. road-over-rail) be preferred, then the TBA makes the following submissions regarding that option.

(2) General impacts on businesses

Section 6 of the Social Impact Assessment of the NoR1 identifies the following likely negative impacts on businesses or for communities accessing businesses from the proposal: Link

- There will be changes to people's way of life, for those living and working in the area and those who access services and businesses.
- As properties are acquired for the Project, people may move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the area, a number of businesses important to the community will potentially be lost, including:
 - a number of early childhood education ('ECE') centres across the Project area;
 - the Skills Update Training and Education Centre on Walters Road; and
 - the BP Service Station on Great South road providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity.¹
- Other businesses which will potentially be lost of the community include vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services.

To avoid, remedy or mitigate these effects, the TBA agrees with the proposals to include detailed plans identified in the TLC - Social Impact Assessment of NoR1 (to be developed in consultation with the Papakura Local Board and the Takanini Business Association) (or equivalents at the time), including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by the TBA.

(3) Construction effects on transport

Section 5 of the Assessment of Transport Effects of the NoR1 Link notes that construction of the Project includes:

- closing existing level crossings at Spartan Road, Manuroa Road, Taka Street and Walters

¹ These businesses are considered important to the community as they are included in the access indicator for social deprivation in the NZDep dataset.

Road (not necessarily simultaneously); • constructing grade-separated multi-modal bridges at Manuia Road, Taka Street and Walters Road; • constructing grade-separated active mode bridges at Spartan Road and Manuroa Road and constructing cul-de-sac turning heads either side of the rail line at these locations; • constructing a cul-de-sac turning head on the northern end of Takanini Road; • constructing accessways along Taka Street and Walters Road; and • constructing the roundabout and signalised intersection either side of the Manuia Road bridge and the signalised intersection at Arion Road.

Section 5 also says that a number of scenarios reflecting construction sequencing of the bridges have been assessed to determine construction effects and especially to identify which construction staging would have significant adverse effects and would therefore need to be avoided or mitigated. These have been grouped under three geographic areas:

- Area 1: • Construction of Manuia Road multi-modal bridge • Testing two scenarios where Manuia Road bridge has not been built yet: o Closure of only Spartan Road level crossing o Closure of only Manuroa Road level crossing
- Area 2: • Closure of Taka Street level crossing and construction of Taka Street multi-modal bridge under the following two scenarios: o Manuia Road bridge built, Spartan Road and Manuroa Road level crossings closed o Spartan Road and Manuroa Road level crossings remain open, Manuia Road bridge not yet built
- Area 3: • Closure of Walters Road level crossing and construction of Walters Road multi-modal bridge

Of particular concern to the TBA are the construction effects on freight movements in each of these three areas.

Area 1: Construction Effects on freight movements

Scenario 1a: Closure of Spartan Road level crossing: Section 5.2.1 of the Assessment of Transport Effects of the NoR1 notes that if the Spartan Road level crossing is closed and Manuia Road has not been built yet, it is expected that freight will be diverted to alternative routes through residential areas such as Manuroa Road, Alfriston Road, and Porchester Road. This is likely to result in a significant potential impact to freight and the surrounding area, though this effect may be mitigated by constructing Manuia Road bridge prior to the closure of Spartan Road level crossing. [Link](#) The TBA asks that these these effects on freight movements be avoided, remedied or mitigated.

Scenario 1b: Closure of Manuroa Road level crossing: Section 5.2.2 of the Assessment of Transport Effects of the NoR1 notes that if Manuroa Road is closed and Manuia Road has not been built yet, there may be additional freight (2,500 daily heavy vehicles) having to reroute through alternative routes such as Alfriston Road / Porchester Road, and Taka Street. This will result in diversions that will impact freight operations due to the longer journey time. These diversions will also increase the routing distance of freight through residential areas. This is likely to result in a significant potential impact to freight and the surrounding area, though this effect may be mitigated by constructing Manuia Road bridge prior to the closure of Manuroa Road level crossing. [Link](#) The TBA asks that these these effects on freight movements be avoided, remedies or mitigated.

Section 5.2.3.1 of the Assessment of Transport Effects of the NoR1 concludes that: • Manuia Road bridge should be constructed and operational before the level crossings at Spartan Road and Manuroa Road are closed. The Assessment of Transport Effects expects that the Manuia Road bridge will resolve the issues that would otherwise have occurred in Scenario 1a and 1b.

Area 2: Construction Effects on freight movements

Scenario 2a: Manuia Road bridge built: Section 5.3.1 of the Assessment of Transport Effects of the NoR1 notes that there will be a lot more mixing between heavy vehicles and light vehicles on the Manuia Road bridge due to the limited number of east-west connections in this construction scenario. The combination of industrial heavy traffic and general light traffic will result in road safety concerns. The impact heavy vehicles have on smaller light vehicles can be detrimental and will result in serious

crash effects on light vehicle users in the case of a collision with a larger, heavier vehicle. Hence, the Assessment of Transport Effects concludes that the combination of the mixture of heavy and light vehicles will likely result in adverse effects. Link The TBA asks that these effects on freight movements be avoided, remedied or mitigated.

A further concern is Appendix 2 of the Assessment of Transport Effects of the NoR1, which suggests that the Heavy Vehicle usage of Hitchcock Road is zero percent (0%). Contrary to this, local businesses have informed the TBA that Hitchcock Road is used by heavy vehicles, with hundreds of containers a year being transported and de-vanned on Hitchcock Road, with multiple trucks doing multiple runs per day delivering to customers.

Scenario 2b: Manuia Road bridge not yet built: Section 5.3.2 of the Assessment of Transport Effects of the NoR1 notes that this construction scenario is not expected to have a significant effect on diversions to freight, as freight can continue to primarily use Spartan Road and Manuroa Road in the 2038 network. The TBA supports scenarios with reduced effects on freight movements.

Section 5.3.3.1 of the Assessment of Transport Effects of the NoR1 concludes that a suitable alternative to facilitate traffic, pedestrian and cyclist movement should be provided for the closure of Taka Street during construction. This could mean the following: • Partial closure with some movements retained on Taka Street; and/or • Reroute traffic to an alternative connection such as Manuroa Road or Manuia Road (with Spartan Road open in either option). This would mean at least three connections are provided in the Takaanini network.

Area 3: Construction Effects on freight movements

Section 5.4 of the Assessment of Transport Effects of the NoR1 notes that the model indicates that this construction scenario is not expected to have a significant effect on diversions to freight as freight can continue to primarily use Spartan Road and Manuroa Road in the 2038 network. The TBA supports scenarios with reduced effects on freight movements.

Section 5.5 of the Assessment of Transport Effects of the NoR1 recommends development of a Construction Traffic Management Plan (CTMP) to mitigate the potential construction traffic effects to be prepared by the nominated contractor prior to works commencing. The TBA agrees and asks that the TBA be involved in development of the CTMP and that businesses be kept informed of construction times and progress.

(4) Operational effects on transport

Section 6.5 of the Assessment of Transport Effects of the NoR1 Link notes that the current access to and from the Takaanini industrial area is via Spartan Road and Manuroa Road, but since the Project is closing both freight connections and replacing them with Manuia Road, the future freight access will be via the new Manuia Road grade-separated connection.

As noted in Section 6.5, of particular concern is that the current over-dimension route through the Takaanini industrial area traverses along Porchester Road, Popes Road, Takaanini School Road, onto Manuroa Road to connect with Great South Road and that the proposed closure of the Manuroa Road level crossing will impact the current over-dimension routing through the Takaanini industrial area.

This is a key effect of the Project, as there will need to be a change in the over-dimension route with a suitable alternative provided to mitigate this effect. The alternative over-dimension route will need to be provided via the future Manuia Road connection to Great South Road. The new over-dimension route will be provided via Porchester Road and then turn onto Manuroa Road, Oakleigh Avenue, and onto the new Manuia Road bridge. This alternative route must have a clear width and height to accommodate over-dimension vehicles and any overhead powerlines may need to be undergrounded. The roundabout at Manuia Road / Oakleigh Avenue intersection must be designed to allow for enough turning space for over-dimension vehicles.

As Manuroa Road is a residential street, it is not ideal to have heavy trucks traversing through this environment.

This is of concern to the TBA.

(5) On Street Parking

Section 6.9 of the Assessment of Transport Effects of the NoR1 Link notes that all TLC corridors have existing on-street parking and the Manuia Road local street also has existing on-street parking.

The Project will remove all existing on-street parking spaces along the TLC corridors and there will be no on-street parking on the grade-separated bridges. The purpose of the existing on-street parking that will likely be impacted by the Project are highlighted in Table 37 of the Assessment of Transport Effects of the NoR1 and includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(6) On site Parking

Section 6.9 of the Assessment of Transport Effects of the NoR1 Link also notes that in some locations of the Project areas, the Project intends to widen the existing designation and alter the cross section of the corridor to incorporate separated walking and cycling facilities, provide space for the bridge or to incorporate space for the accessways. As a result, existing car parking provision for properties adjacent to the Project corridor will be affected. This includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(7) Property Access

Section 6.10 of the Assessment of Transport Effects of the NoR1 Link notes that in some locations of the Project areas, access arrangements to existing properties will be affected. This includes numerous industrial and commercial businesses (Table 40), and particularly the key entry access point to the Takaanini Town Centre development (30 Walters Road).

This is of concern to the TBA.

(8) Corridor-Specific operational effects

Section 7 of the Assessment of Transport Effects of the NoR1 Link assesses specific transport matters relating to individual corridors as well as measures to avoid, remedy, or mitigate actual or potential adverse effects.

Spartan Road: Routing for northbound trips onto Great South Road and access to SH1 northbound on-ramp are affected due to Spartan Road level crossing closure. The TBA supports discussions with affected landowners being undertaken to understand the operation of their business and typical access movements. The TBA also agrees with the recommendation that prior to the start of construction, a design safe system audit is undertaken for the Project. This will determine if any additional mitigation measures are required to address any safety risks in regards to the specific elements of the Project. **LEAVE OPEN**

Manuia Road: The proposed design of the existing Manuia Road layout involves an overpass from Great South Road to Oakleigh Avenue with vehicle and active mode provisions. The TBA agrees that the Manuia Road bridge should be designed to accommodate heavy vehicles and should be aligned with future standards/needs.

Manuora Road: The proposed design of the existing Manuora Road pedestrian and road level crossings involves the closure of the existing at grade level crossings and replaced with a grade separated active modes bridge. There is the potential that the design of the active mode bridge is not suitable for pedestrians and cyclists, resulting in reduced uptake of the active mode connection. The TBA agrees with the recommendation that at detailed design, the active mode connection should be

designed appropriately and aligned with engineering design standards to ensure the facility accommodates the needs of pedestrians and cyclists. ~~2 VEHICLES~~

Taka Street: The proposed design of the existing Taka Street layout involves an overpass from Great South Road to the roundabout involving Takanini School Road and Kauri Heart Avenue with vehicle and active mode provisions. There are concerns about the gradient of the proposed Taka Street bridge and the effect on freight. The TBA agrees with the recommendation that at the detailed design stage, the Taka Street bridge should be designed to accommodate heavy vehicles and should be aligned with future standards.

Walters Road: The proposed design of the existing Walters Road layout involves an overpass from Great South Road to the four-legged roundabout with Porchester Road with vehicle and active mode provisions. There are concerns about the gradient of the proposed Walters Road bridge and the effect on freight. The TBA agrees with the recommendation that at the detailed design stage, the Walters Road bridge should be designed to accommodate heavy vehicles and should be aligned with future standards.

(9) Construction Noise and Vibration

Although the TLC - Assessment of Construction Noise and Vibration Effects of the NoR1 Link suggests that predicted noise levels for the majority of works will be able to comply with the relevant daytime limits, which means that effects are generally acceptable inside neighbouring buildings, the TBA has concerns these will, to the contrary, be significant. Where high noise activities would occur (e.g., demolition of close by buildings, piling of bridges or retaining walls, and earthworks), the TBA agrees that these activities should be completed within limited periods (eg weeks) with highest noise levels for only some hours during the workdays. Overall construction of the bridges will take between 1 to 2 years for active mode bridges and 2.5 to 3 years for road bridges. The Assessment of Construction Noise and Vibration Effects notes that effects can be managed through the application of management and mitigation measures through a Construction Noise Vibration Management Plan ('CNVMP').

The Assessment of Construction Noise and Vibration Effects observes that depending on the final construction methodology and receivers in the vicinity, mitigation and management measures may also include the offer of temporary relocation. The appropriate mitigation measures will be determined on a case-by-case basis throughout construction using the CNVMP and/or site-specific schedules as the implementation tool.

Proposed conditions regarding a Construction Noise and Vibration Management Plan are welcomed by the TBA.

(10) Lapse period

Under section 184 of the RMA, the default lapse date for designations is five years unless the designation provides a different lapse period. The Assessment of Effects on the Environment states that a key objective of the Project is to identify and protect land now for future transport networks and that an extended lapse period of 15 years is reasonably necessary to achieve this, as it provides statutory protection of the transport corridors.

The TBA is concerned that the 15-year lapse period is excessive for this Project and creates far longer uncertainty for the business community.

From the planner



14 December 2023

Auckland Council
Private Bag 92300
Auckland 1142

Attention: Manager, Plans and Places, Auckland Council
Via email: unitaryplan@aucklandcouncil.govt.nz

RE: NOTICE OF REQUIREMENT BY AUCKLAND TRANSPORT FOR THE TAKAANINI LEVEL CROSSING PROJECT

We act for the Halls Transport as the leasee of 1 and 15 Spartan Road, Takanini. As the Notice of Requirement directly affects their entire site and critical transport connections, Halls Transport have a direct and obvious interest in the proposed designations, and the proposed works that this may enable.

AT, as requiring authority under Section 167 of the Resource Management Act 1991(RMA), has given a NoRs for works associated safe and reliable east west connections across the North Island Main Trunk Line (NIMT) in Takanini , which incorporate the anticipated KiwiRail planned expansion of the NIMT from two to up to four tracks, and the City Rail Link enabling works.

NoR 1 is for the construction, operation, maintenance and upgrade, as well as associated works, of transport infrastructure on and around Spartan Road, Manuia Road, Manuroa Road and Taka Street which includes:

- the closure of the existing level crossings at Spartan Road, Manuroa Road and Taka Street;
- new bridges with general traffic lanes and walking and cycling facilities across the NIMT line at Manuia Road and Taka Street;
- new bridges with walking and cycling facilities across the NIMT line at Spartan Road and Manuroa Road.

Halls Transport is the leasee of the property at 1 – 15 Spartan Road, Takanini (the site) which operates a refrigerated and frozen transport and storage business from the site, in connection with their other sites on Spartan Road, which is directly affected by the NoR.

The Submitter will not gain an advantage in trade competition through this submission, and in any case are directly affected by the NoR.

Our submission

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keren@mhg.co.nz | 09 950 5100
www.mhg.co.nz

The Submitter supports the principle of removing existing transport deficiencies as well as providing for the forecasted future growth in the area, but opposes the NoR in regard to the changes in layout and surroundings of the site that will significant adversely affect the operation of the logistics and storage business.

The submitters opposition is on the basis that:

- (a) The project does not promote the sustainable management of natural and physical resources as is required by Part 2 of the RMA;
- (b) The project does not enable people and communities to provide for their social, economic, and cultural well-being and for their health and safety;
- (c) The project is not an appropriate way of meeting the objectives and policies of the National Policy Statement for Urban Development or the Auckland Unitary Plan;
- (d) The project does not promote the efficient use and development of urban land and development infrastructure;
- (e) The Assessment of Environmental Effects is inadequate and does not address the significant adverse effects of the works in sufficient detail to address matters under section 171(1) of the RMA;
- (f) The potential adverse effects on the Submitter have been inadequately identified, considered, or avoided, remedied or mitigated;
- (g) The nature and extent of the benefits of the project have not been demonstrated to outweigh the potentially significant adverse effects of the project;
- (h) The adverse effects of the project are not sufficiently mitigated, including managing the effects of the NoR on adjacent activities;
- (i) The project will generate significant adverse social and economic impacts, including on the Submitters business;
- (j) The proposed conditions do not adequately address the potential; for adverse effects, including significant adverse effects; and
- (k) The proposed works are not reasonably necessary for achieving AT's objectives for the project for which the designation is sought.

Reasons for the Submission

To understand the Submitters position, details regarding the specific operation at the site are provided below.

1. The existing business at 1 and 15 Spartan Road (Pt Lot 1 DP38856, Pt Lot 4 DP 12143, Pt Lot 2 DP38856, Sec 19 BLK XIV Survey Dis Otahuhu NA75C/747, and Pt Lot 1 DP 38856, Sec 8 BLK XIV Survey Dis Otahuhu, NA2074/3, NA1811/24) occupies the 2.1152Ha site.
2. Onsite parking for a fleet of purpose-built vehicles for line haul, short haul and metro services, in conjunction with 5000m² of refrigerated storage and cool store, for storing and loading containers for domestic and international export are all provided for on this site.
3. The site lies adjacent to Great South Road and the Takanini Off Ramp to the west, Spartan Road to the north and the NIMT to the east. To the south are other industrial properties accessed from Great South Road.

4. The existing Halls Transport business operating from the site comprises facilities where they can transport, store, pickup and dispatch chilled, frozen and dry goods by road, rail and sea. At the Spartan Road site, they provide an end-to-end service incorporating integrated refrigeration and frozen transport, cold storage, cross docking, warehousing and distribution.
5. The site which is accessed from Spartan Road includes ancillary offices, motor vehicle and truck trailer parks. The site is landscaped against its northern and western boundary and includes signage relating to the activities on the site. Halls Group Ltd also operate from 58 and 81 Spartan Road, Takanini, with these properties providing training facilities (58 Spartan Road), truck and trailer storage, container storage and loading facilities (81 Spartan Road), amongst other functions.
6. The business is a vehicle-oriented activity and access to the 1 and 15 Spartan Road site is gained via Spartan Road. Trucks and other vehicles enter and leave the site via the Spartan Road vehicle crossing. This access point connects the site with Great South Road and the Southern Motorway. Figure 1 below depicts the existing layout of the Halls Transport business on the site.



Figure 1: The affected site, 1 and 15 Spartan Road, Takanini (source: Geomaps)

7. The figure below is sourced from the NoR application details and helps depict the significant changes that are proposed on and adjacent to the Halls Transport site as a result of the NoR.
8. These include the following:
 - Closure of the existing road corridor to vehicular traffic across the NIMT;
 - Construction of an active mode bridge across the NIMT;
 - Construction of cul-de-sacs (accommodating footpaths) and works to tie in the existing corridor on either side of the NIMT on Spartan Road;

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- Ramps and stairs will connect to the bridge on either side of (east and west) of the NIMT and will tie into the cul-de-sacs.
9. To facilitate these works, AT seeks the designation boundary to extend into the 1 and 15 Spartan Road site. Notably this extends 2728m2 along practically the entire site frontage, including both of the site's access points, the entire landscaped frontage, carparking area and a portion of an existing storey cool storage warehouse). The proposed designation boundary also runs along the southeast property boundary adjacent the NIMT.
 10. There is little detail given of the proposed works, nor the need for such an elaborate ramping and turnback system for cyclists and pedestrians.

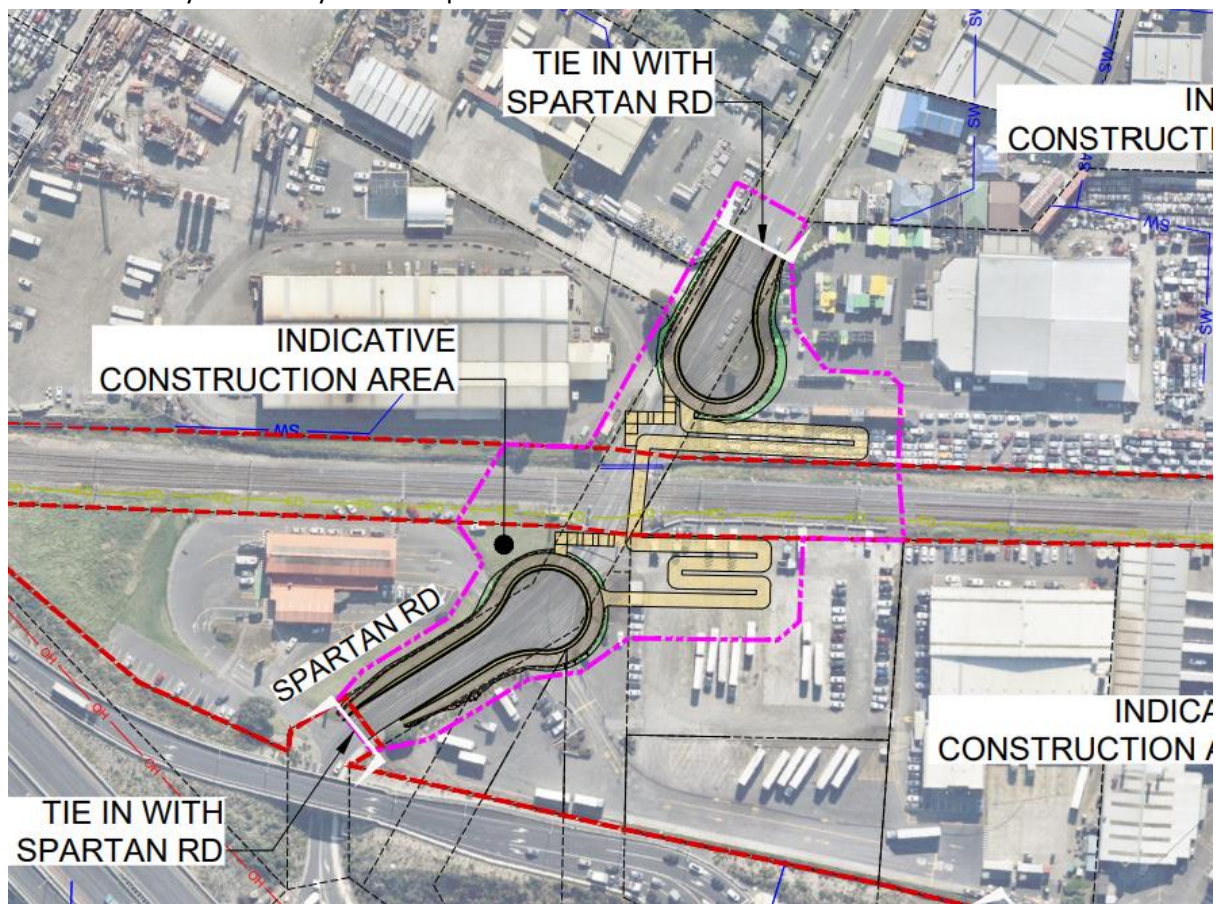


Figure 2: Spartan Road project area (source: NoR TLC General Arrangement Plan)

11. There is a lack of detailed evidence of the complexity and range of potentially significant adverse effects on the Submitter site and wider network being adequately assessed (with S92 comments from Council specialists noting that some data appears approximated and underestimated), including journey times, added congestion and issues with accessibility onto SH1. Indeed, the aerial photographs used in the documents appear to be outdated and do not show the extent of existing buildings or landscaping on the site.

Safe and convenient access and egress

12. The existing logistics business requires a safe and convenient access to ensure that all vehicles can appropriately enter and exit the site, whilst allowing for the continued manoeuvrability of large trucks. The level of accessibility is important to maintain transport efficiency and safety, as well as the economic performance of the site. The proposed changes have potentially significant adverse effects on the safe and convenient access to the site, particularly as:
- The exit and entry point to pedestrian cycling ramp is located within approximately 10m of the existing vehicle crossing;
 - The existing vehicle crossing appears to be removed and replaced with footpath and landscaped bunding, with no new vehicle crossing appearing to be shown on the above plan.

In the absence of further detail, there is no certainty that the proposed changes will:

- Retain the ability for vehicles enter and exit the site;
- Provide the ability for pedestrians and cyclists to safely and conveniently access the active mode bridge.

Effects on frontages and built development.

13. The proposed designation boundary encompasses the frontage of the site, and encapsulates the existing landscaping and front yard signage, together with carparking areas and a portion of the existing cool storage facility. This will result in a reduction in street amenity and require a revaluation of the appropriate location for signage on the site, as well as the replacement of landscaping to remain compliant with the zoning requirements for Business light Industry zone.

Effects on the continued operation of the business

14. The Spartan Rd railway crossing road closure, together with the others proposed within the NoRs forces the rerouting of Halls trucks when entering and exiting the site and severs this site from the 58 and 81 Spartan Road sites. It is anticipated that approximately 90 truck movements per day would require to be redirected, via suboptimal routes, given no right turn onto Spartan Road, with increases in the operational costs of the business. The change in routes is also likely to result in increased pressure on ancillary roads that pass through urbanised areas (i.e. schools).
15. The proposed designation area materially impacts the onsite operations with a minimum of 30 trucks and 18 car parks displaced. The business cannot efficiently operate without those trucks onsite and are likely to incur a direct cost of needing to lease additional land.
16. This may result in the potential loss of employment and livelihood for owners and employees of this business, to which AT notes that long timeframes before active property acquisition or construction will ensure that businesses are informed and have time to undertake their own business planning – which is hardly an effective mitigation measure for managing the socio-economic impacts of the proposal.

Effects on potential business expansion

17. AT have requested to extend the lapse period to 15 years for the project (which would apply to all NoRs) and as such it is appropriate to balance the need for the lapse extension with the potential “blighting” effects for existing landholders and leaseholders. Halls have occupation rights on the site until 2048 and are exploring opportunities for building development.
18. AT contend that the timeframe extension enables them to undertake detailed design, obtain necessary consents, procure funding, undertake tendering and procurement as well as property and access negotiations. It is noted that within the AEE there is no detail of the staging of the various NoRs, nor is there any detailed design surrounding the proposed works (including changes to access, landscape works or construction management details) and therefore puts both landowners and leaseholders “on notice”, with no certainty as to how they will be affected at a per site basis or “micro” level, when they will be affected and for how long. Any future expansion or development plans for the site are therefore compromised.

Construction effects

19. The AEE purports a construction period of 1 – 2 years for the Spartan Road project, with little information on the timing of this or the other project areas. During construction there are likely to be periods where access to the site is disrupted, as well as the wider transport environment around Takanini. The extent to which the works will compromise the Submitter’s business are not clearly defined, nor is the ability of the Submitter to influence the detail of how works will be managed to minimise adverse impacts on their business operation. AT appear to have limited involvement of parties affected to the lip service and hand holding of the Stakeholder and Community Engagement Plan (which is essentially an information dissemination tool for AT) and Community Health and Wellbeing Strategy, rather than engaged and detailed discussion regarding potential mitigation and management of effects.

Relief sought

- (a) The Submitter seeks that the NoRs in their current form are declined;
- (b) In the event that the NoRs are not declined, the Submitter seeks that the designations are amended to avoid, remedy or mitigate all matters of concern raised in this submission, but not limiting to:
 - Minimising the encroachment of the designation into the Submitters site;
 - Ensuring that any temporary or permanent effects on the ongoing operation of the site, including access and egress, manoeuvring, parking and landscaping are minimised.
 - Retain safe and convenient entry and exit via Spartan Road including the ability to right turn out of the site for Halls Transport, as was provided prior to works commencing on the overbridge.
 - Ensure that the works are appropriately managed through conditions to avoid, remedy or mitigate adverse effects on the submitter. This includes requirements

to engage with the Submitter and appropriately address matters raised by the Submitter in relation to both temporary construction effects and the final design of the crossing.

- (c) The Submitter would like involvement in the finalisation of the following Management Plans:
- Construction Environment Management Plan (CEMP)
 - Construction Noise and Vibration Management Plan (CNVMP)
 - Construction Traffic Management Plan (CTMP)
 - Stakeholder and Community Engagement Management Plan (SCEMP)
- (d) Require Council certification that management plans achieve the specified outcomes and are not simply submitted for information.
- (e) The Submitter wishes to be heard at the hearing in support of their submission, and also ask that all updates and notices are also served to us as agents on the email and postal address noted below.
- (f) The Submitter also welcome the opportunity to enter into discussions with Auckland Transport in respect of the issues raised, with a view to ensuring that their concerns are appropriately addressed.



Keren McDonnell – As Authorised Agent and Planner for Mt Hobson Group

Address for Service:

Mt Hobson Group
PO Box 37964
Parnell
Auckland 1151
T: 09 950 5100

Email: keren@mhg.co.nz

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1236] Notice of Requirement online submission - Tahua Partners Limited
Date: Thursday, 14 December 2023 5:15:41 pm
Attachments: [Submission - TLC NoR 1 - Tahua Partners Limited.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Tahua Partners Limited

Organisation name:

Full name of your agent: SLR Consulting New Zealand Limited (c/o Thomas Trevilla)

Email address: thomas.trevilla@slrconsulting.com

Contact phone number: 02040006702

Postal address:
201 Victoria Street West
Auckland Central
Auckland 1010
Auckland Central
Auckland 1010

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
Please refer to the attached submission.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
Please refer to the attached submission.

I or we seek the following recommendation or decision from Auckland Council:
Please refer to the attached submission.

Submission date: 14 December 2023

Supporting documents
[Submission - TLC NoR 1 - Tahua Partners Limited.pdf](#)

Attend a hearing

Do you wish to be heard in support of your submission? Yes

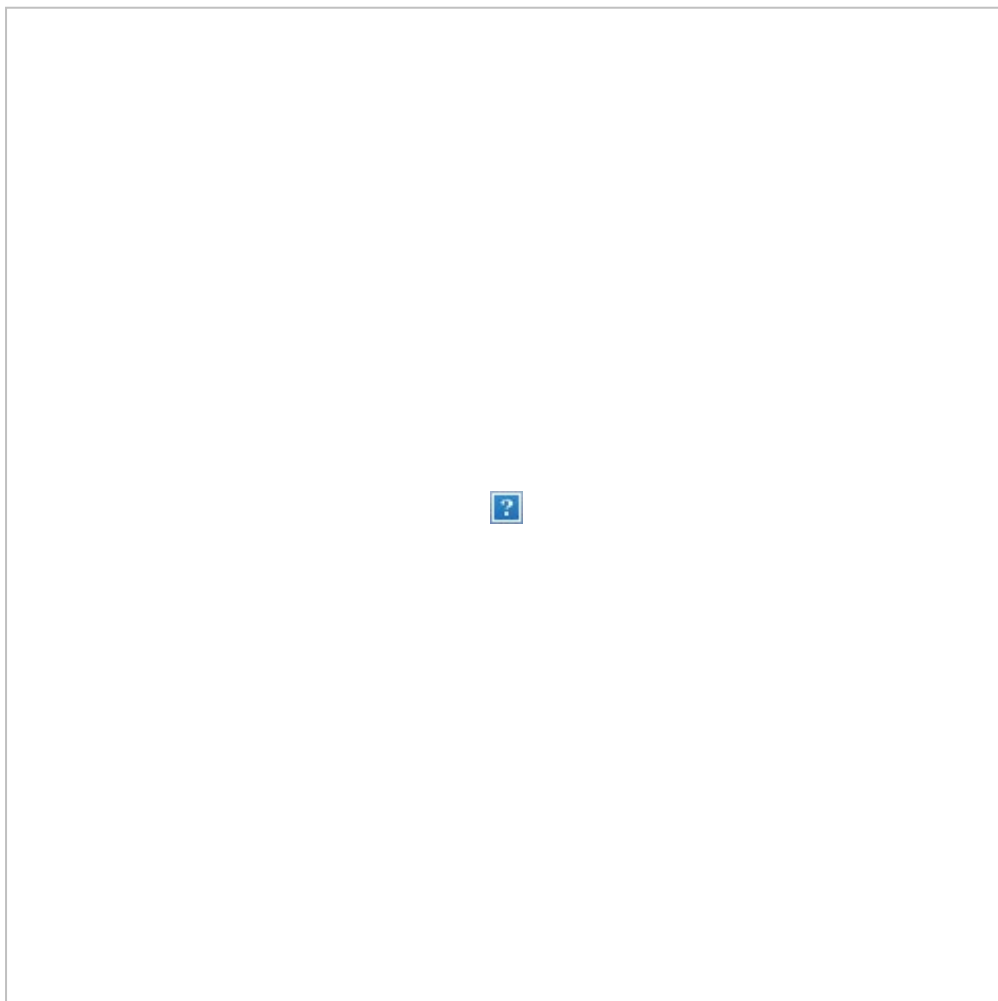
Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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SUBMISSION ON A NOTICE OF REQUIREMENT FOR A PUBLIC WORK SUBJECT TO FULL NOTIFICATION UNDER THE RESOURCE MANAGEMENT ACT 1991

Territorial authority: Auckland Council
Level 16, 135 Albert Street
Private Bag 92300
Auckland 1142

Attention: Planning Technician

Email: unitaryplan@aucklandcouncil.govt.nz

Submission on: A notice of requirement from Auckland Transport for a designation for a public work subject to full notification: Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1)

Submitter: Tahua Partners Limited
Level 2, 1-7 The Strand
Takapuna
Auckland 0622

Address for service: SLR Consulting New Zealand
201 Victoria Street West
Auckland Central
Auckland 1010

Attention: Thomas Trevilla

Phone: 020 400 06702
Email: thomas.trevilla@slrconsulting.com

Date: 14 December 2023

SLR Ref No.: Submission - TLC NoR 1 - Tahua Partners Limited

SLR Project No.: 16275

INTRODUCTION

1. Under section 168(2) of the Resource Management Act 1991 (**the RMA**), Auckland Transport has given a notice of requirement to the Auckland Council (**the Council**) for a designation for a public work: Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1) (**the Project**).
2. This is the submission of Tahua Partners Limited (**Tahua**) on the Project. Tahua is a New Zealand retail and hospitality investment company with a portfolio of business units that includes Starbucks, Hannahs Shoes, Number One Shoes, Burger King and Popeyes Louisiana Kitchen in New Zealand.
3. Tahua owns 106 Great South Road, Takanini (Lot 1 DP 46142) which is a property situated at a prime location at the corner of Great South Road and Manuia Road. Tahua is currently constructing a restaurant with dine-in and drive-through facilities at their property. The development will maintain the existing vehicle access to Manuia Road and reinstate the existing vehicle access to Great South Road. This was specifically designed to avoid new or amend the existing Great South Road vehicle access with the aim of prioritising and improving pedestrian amenity and reduce potential traffic effects on Great South Road.

SCOPE OF THIS SUBMISSION

4. This submission relates to the Project as a whole, but has a particular focus on:
 - 4.1. the proposed designation boundary for the Manuia Road project area and the proposed road layout / alignment;
 - 4.2. the actual and potential adverse effects on the environment resulting from works in the Manuia Road project area during construction and once operational; and
 - 4.3. the proposed designation lapse period and conditions.

REASONS FOR THIS SUBMISSION

5. Manuia Road is a two-lane cul-de-sac, terminating by the North Island Main Trunk railway line (**NIMT**). Manuia Road provides access to Tahua's property, the Takanini Gateway shopping mall, a bp service station and truck stop, other vehicle associated businesses, and dwellings. The Great South Road and Manuia Road intersection is unsignalised; stop signs control traffic from Manuia Road and Challen Close turning onto Great South Road.
6. Works which the Project seeks to undertake within the Manuia Road project area, in the vicinity of Tahua's property, includes the following:
 - 6.1. signalisation of the Great South Road and Manuia Road intersection;
 - 6.2. construction of a new grade-separated road crossing, a bridge, with general traffic lanes and walking and cycling facilities over the NIMT (connecting to the Oakleigh Avenue and Hitchcock Road intersection further north);
 - 6.3. construction of abutment walls on either side of the bridge and landscaping; and
 - 6.4. reconfiguration of Manuia Road into an access lane for remaining properties with connection to the proposed road and not Great South Road.
7. The Project estimates a construction period of 2.5 years to 3 years for the Manuia Road project area. The Project proposes a designation lapse period of 15 years.
8. The Project's designation boundary for the Manuia Road project area adjoins Tahua's property. Based on the Project's notified plans and visualisations prepared by Te Tupu

Ngātahi Supporting Growth (**Te Tupu Ngātahi**), it appears that the only vehicle access to Taha's property from Manuia Road will be eliminated due to the realignment of Manuia Road and the bridge's abutment walls, battered slopes and landscaped areas. Taha's property is identified in **Figure 1** and **Figure 2** below.

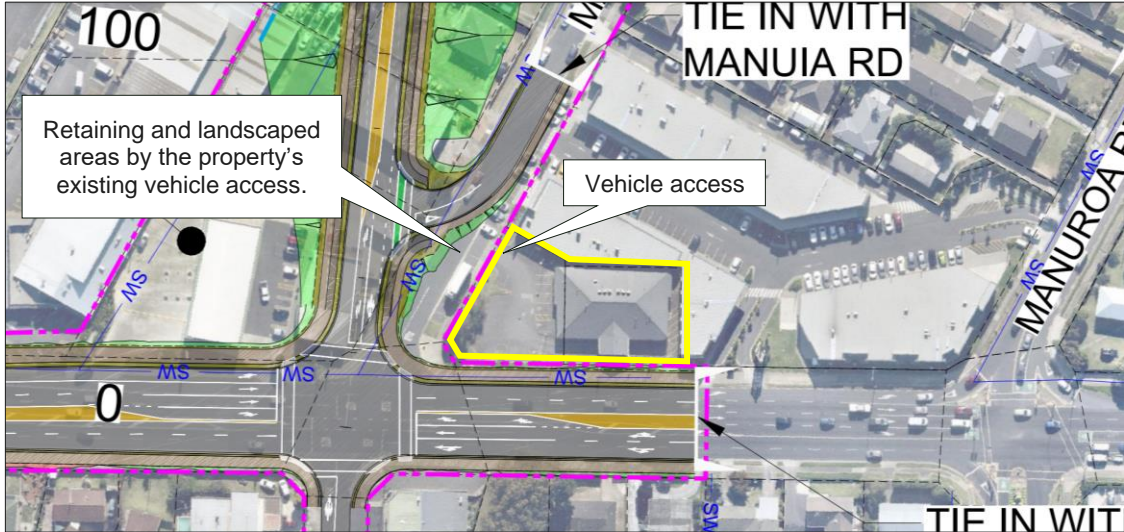


Figure 1: Taha's property identified in yellow on the Manuia Road project area of the General Arrangement Plan (Source: Te Tupu Ngātahi¹).

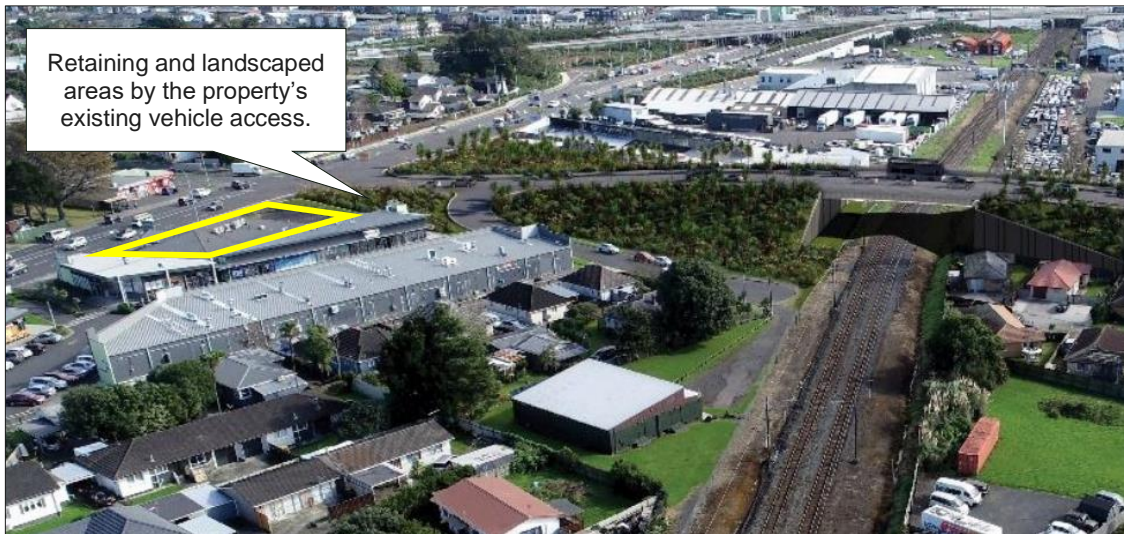


Figure 2: Taha's property identified in yellow on the visualisation of the Manuia Road grade separation (Source: Te Tupu Ngātahi²).

9. Taha recognises the overarching outcomes sought by the Project, including the benefits that it, and the other associated notice of requirement, proposes in terms of transport connectivity and the impact that level crossings have on traffic delays in the

¹ Plan prepared by Te Tupu Ngātahi and titled 'GENERAL ARRANGEMENT LAYOUT PLAN NoR1 SPARTAN ROAD, MANUIA ROAD, MANUROA ROAD, AND TAKA STREET', SGA-DRG-STH-005-GE-11000, Revision C, dated 26 May 2023.

² Figure 10-7 of the report prepared by Te Tupu Ngātahi titled 'Volume 2 - Takaanini Level Crossings: Assessment of Effects on the Environment', Version 1.0, dated 13 October 2023.

Takanini area. However, Tahua opposes the Project in its current form due to several concerns and adverse effects, including:

- 9.1. if the restaurant's only vehicle access is to be permanently eliminated by the Project, the adverse social and economic effects on Tahua and its ability to continue operations as vehicle access and the drive-through facility are critical to the restaurant;
 - 9.2. if the restaurant had to be redeveloped to have vehicle access from Great South Road instead, this would result in traffic directly from and onto Great South Road (which they sought to avoid due to pedestrian amenity and traffic effects);
 - 9.3. the Project's additional adverse social and economic effects resulting from the loss of a business and jobs in the community due if the restaurant has to close;
 - 9.4. the Project's adverse construction-related effects on the community, over an estimated construction period of 2.5 years to 3 years, including:
 - a. traffic;
 - b. access and wayfinding;
 - c. noise and vibration;
 - d. dust; and
 - 9.5. the general uncertainty that Tahua and the community will experience as to whether or not the Project will ultimately progress due to the proposed designation lapse period of 15 years, which is beyond the standard lapse period of 5 years under section 184 of the RMA.
10. If the Project will, or is able to be modified to, maintain the Manuia Road vehicle access to Tahua's property, in such a way that vehicle entry and exit is not hindered by the layout of the proposed road and proximity to the new intersection of the re-aligned Manuia Road and proposed road (a particular requirement is to ensure vehicles can turn right in and right out of the site), Tahua still maintain their concerns set out in paragraphs 9.4 and 9.5 above as well as the ability for their customers to easily access the site as a result of the roading changes.

RELIEF SOUGHT

11. Tahua seeks that the Council recommends that:
 - 11.1. the Project is withdrawn in its current form; or
 - 11.2. the Project is modified or made subject to conditions to address all of the concerns raised in this submission.
12. Tahua also seeks any alternative, further or consequential relief that may be required to address all of the concerns raised in this submission.

PROCEDURAL MATTERS

13. Tahua could not gain an advantage in trade competition as a result of this submission.
14. Tahua wishes to be heard in support of this submission.
15. If others make similar submissions, Tahua may be prepared to consider presenting a joint case with them at any hearing.

SLR Consulting New Zealand

201 Victoria Street West Auckland 1010, New Zealand



Signed on behalf of Tahua Partners Limited:

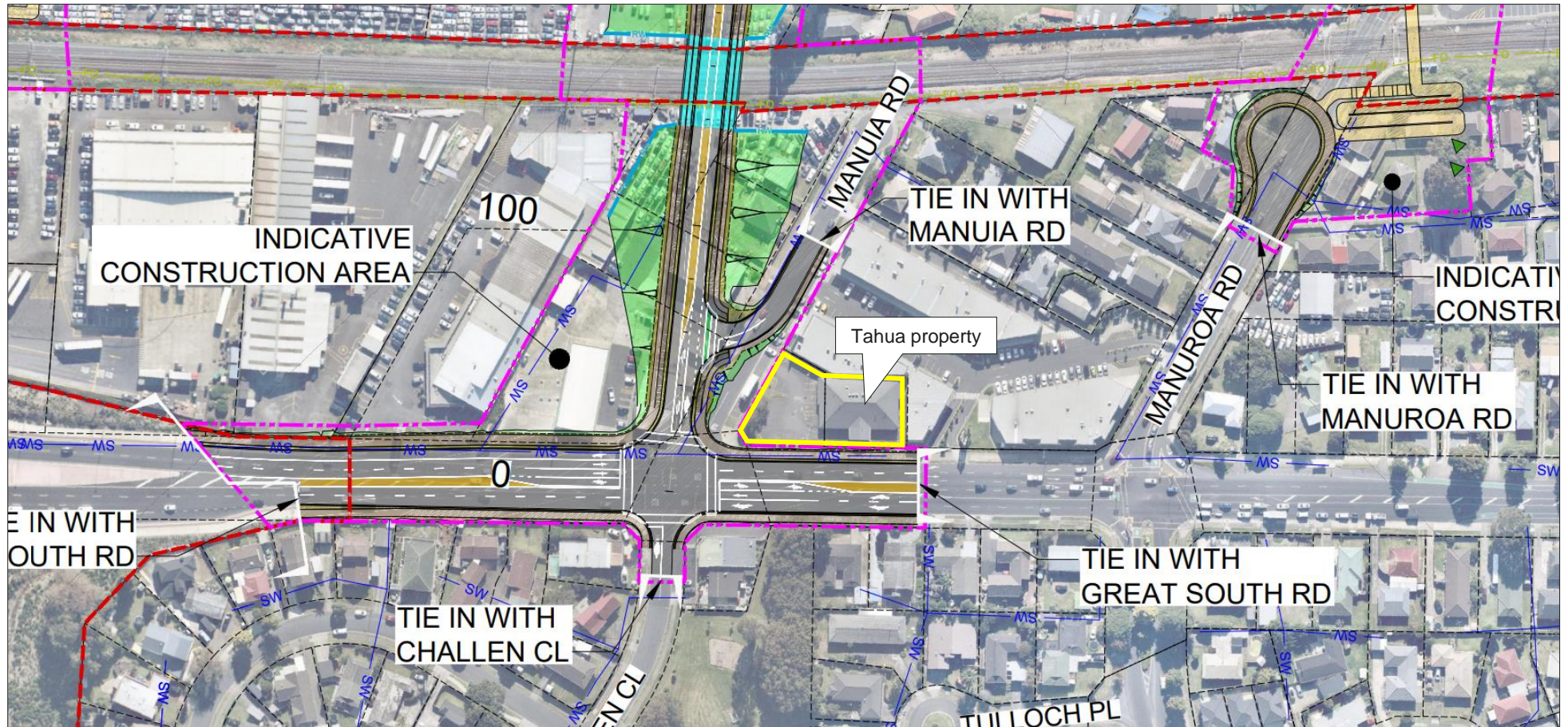
A handwritten signature in black ink, appearing to read 'Thomas Trevilla'.

Thomas Trevilla

Planner

SLR Consulting New Zealand

Attachment A



Excerpt of the Manuia Road project area of the General Arrangement Plan.

Source: Plan prepared by Te Tupu Ngātahi and titled 'GENERAL ARRANGEMENT LAYOUT PLAN NoR1 SPARTAN ROAD, MANUIA ROAD, MANUROA ROAD, AND TAKA STREET', SGA-DRG-STH-005-GE-11000, Revision C, dated 26 May 2023.



Visualisation of the Manuia Road grade separation.

Source: Figure 10-7 of the report prepared by Te Tupu Ngātahi titled 'Volume 2 - Takaanini Level Crossings: Assessment of Effects on the Environment', Version 1.0, dated 13 October 2023.

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1237] Notice of Requirement online submission - Vivienne Jamieson
Date: Thursday, 14 December 2023 5:15:44 pm
Attachments: [Submission on Notice of Requirement Road closures Takanini.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Vivienne Jamieson

Organisation name: Vertex Lubricants

Full name of your agent: Viv Jamieson

Email address: viv@vertexlubricants.co.nz

Contact phone number: 021990507

Postal address:
22 Marphona Crescent
Takanini
Auckland 2105

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

To summarize the proposed closing of Main Roads - Spartan Road and Manuroa Road to vehicles will create chaos throughout both the business district and the residential district. I strongly suggest that the railway is either tunneled under the roads or bridged over the roads and keep all roads open.

I or we seek the following recommendation or decision from Auckland Council:

We strongly recommend that the railway is either positioned over or under the roads.

Submission date: 14 December 2023

Supporting documents

[Submission on Notice of Requirement Road closures Takanini.pdf](#)

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?

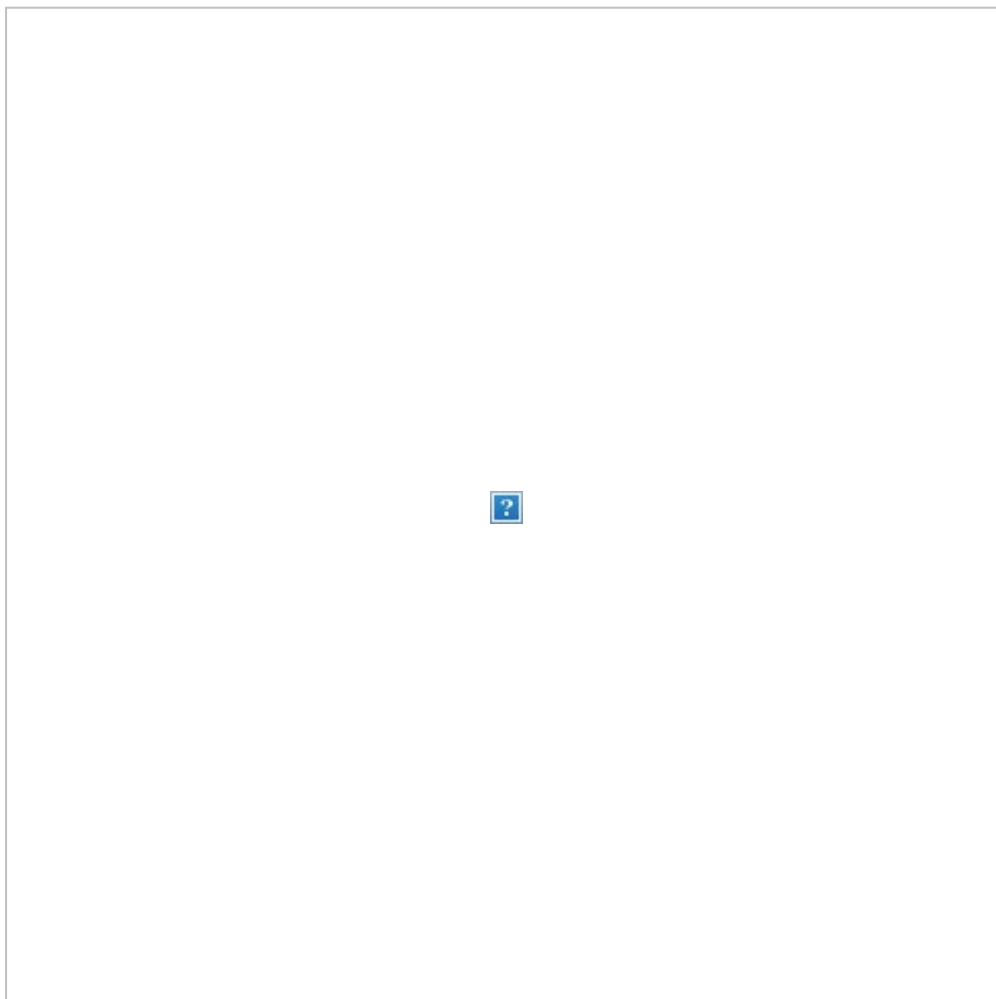
Yes

Declaration

I accept and agree that:

by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport**

We accept that solutions need to be found to replace the four level crossings that bisect the Takanini community, We oppose the bridge solutions proposed in the Notice of Requirement: Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') [Link](#).

We have concerns about the following likely negative impacts on businesses or communities accessing businesses from the bridge proposals:

- As properties are acquired for the Project, people will likely move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the area a number of businesses important to the community will likely be lost, including: • a number of early childhood education ('ECE') centres; a skills training and education centre; and a service station (providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity).
- Other businesses will likely be lost, including vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services (including loss of local employment / livelihood).
- There will be construction effects on transport and long term operational effects on transport, especially for freight movements.
- On street parking and on-site parking will be reduced and property access negatively affected.
- Disruption will be caused by construction noise and vibration (along with a reduction in parking availability due to changed road conditions and demand for parking from the construction workforce).
- Loss in revenue for local businesses directly affected by construction as road blockages or disruptive construction redirect regular businesses customers.

We note the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of **raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail)**. We are of the view that the **assessment of alternatives is deficient**.

However, should the option as proposed of raising the road (i.e. road-over-rail) be preferred, then to avoid, remedy or mitigate the effects of that option, the we agree with the proposals by the applicant to include conditions and detailed plans, including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by Vertex Lubricants.

Submissions

The Notice of Requirement being submitted on by Vertex Lubricants is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

The submission relates to the entire Notice of Requirement.

Vertex Lubriants is in opposition regarding the Notice of Requirement.

Vertex Lubricants will not gain an advantage in trade competition through the submission.

The decision Vertex Lubricants seeks from the Council is to decline the Notice of Requirement.

Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR1) Auckland Transport

Durmast Holdings Ltd

Introduction

Durmast Holdings is a company located at **30-34 Oakleigh Avenue**

Durmast Holdings Ltd welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1'). Link

Submissions

The Notice of Requirement being submitted on by Durmast Holdings is the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1').

Our submission relates to the entire Notice of Requirement.

Durmast Holdings Ltd is in opposition regarding the Notice of Requirement.

Durmast Holdings Ltd will not gain an advantage in trade competition through the submission.

The decision **Durmast Holdings Ltd** seeks from the Council is **to decline the Notice of Requirement**.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for Durmast Holdings Ltd being opposed regarding the Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street ('NoR1') are:

It is nonsensical to close the access to Great South Rd from both Sparton Rd and Manuroa Rd and redirect two major traffic streams to a new railway crossing proposed at Manuia Rd, the consolidation of the two traffic flows will result in a concentration of both traffic streams firstly at a roundabout and then over the crossing, it will also result in an additional two turning manoeuvres for vehicles from both the Sparton Rd and Manuroa Rd access directions while making their way to the new crossing location, inevitably leading to increased traffic congestion and increased traffic incidents and accidents

The addition of the requirement to negotiate a roundabout to access Great South Road will greatly hinder truck access from the Takanini industrial area which currently has two functioning large truck access points.

The proposed new rail crossing at Manuia Rd seems an absolute waste of currently utilised productive land, the proposed construction of a crossing utilising a bulk fill causeway which is approximately 70m wide seems more suited to a bridge or rail crossing in a rural area, not in a built up productive industrial location. Surely if this is the only suitable rail crossing location (which we do not believe it is) between Myres Rd and Taka St, then a much better engineered slimmer footprint structure should be proposed

In our opinion a much more sensible, efficient and fit for purpose solution would be to maintain and widen the existing Manuroa Rd access location utilising a construction solution similar to the proposed Taka St and Walters Rd upgrades.

I strongly feel the designations must be amended to avoid the issues I have raised and further investigation of other options is researched. Should this not occur, discussions must be held with myself regarding compensation for losses that will be endured.

WH Hurlstone

DURMAST HOLDINGS LTD

Postal: 1B/120 St Aubyn Street, New Plymouth

Ph: 0274 490 124

Email: b.hurlstone@xtra.co.nz

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1253] Notice of Requirement online submission - Pam Butler Senior RMA Advisor KiwiRail
Date: Thursday, 14 December 2023 8:30:44 pm
Attachments: [KiwiRail reasons for submissions Takanini Level Crossings_20231214202438.470.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Pam Butler Senior RMA Advisor KiwiRail

Organisation name: KiwiRail Holdings Limited

Full name of your agent:

Email address: Pam.butler@kiwirail.co.nz

Contact phone number: 0275708571

Postal address:
Private Bag 92138
Auckland 1142

Auckland 1142

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:
North Island Main Trunk Line designation ref 6302 in the AUP in the Takaanini area as affected by works at Spartan, Manuia, Manuroa Roads and Taka Street and level crossing closures (except Manuia)

Do you support or oppose the Notice of Requirement? I or we support the Notice of Requirement.

The reason for my or our views are:
Please see attached 'Reasons for Submission'

I or we seek the following recommendation or decision from Auckland Council:
KiwiRail supports Takanini Level Crossing NoR's One and Two and seeks that the Notices of Requirement be recommended for approval subject to the comments provided and the applicant's proposed conditions.

Submission date: 14 December 2023

Supporting documents
[KiwiRail reasons for submissions Takanini Level Crossings_20231214202438.470.pdf](#)

Attend a hearing

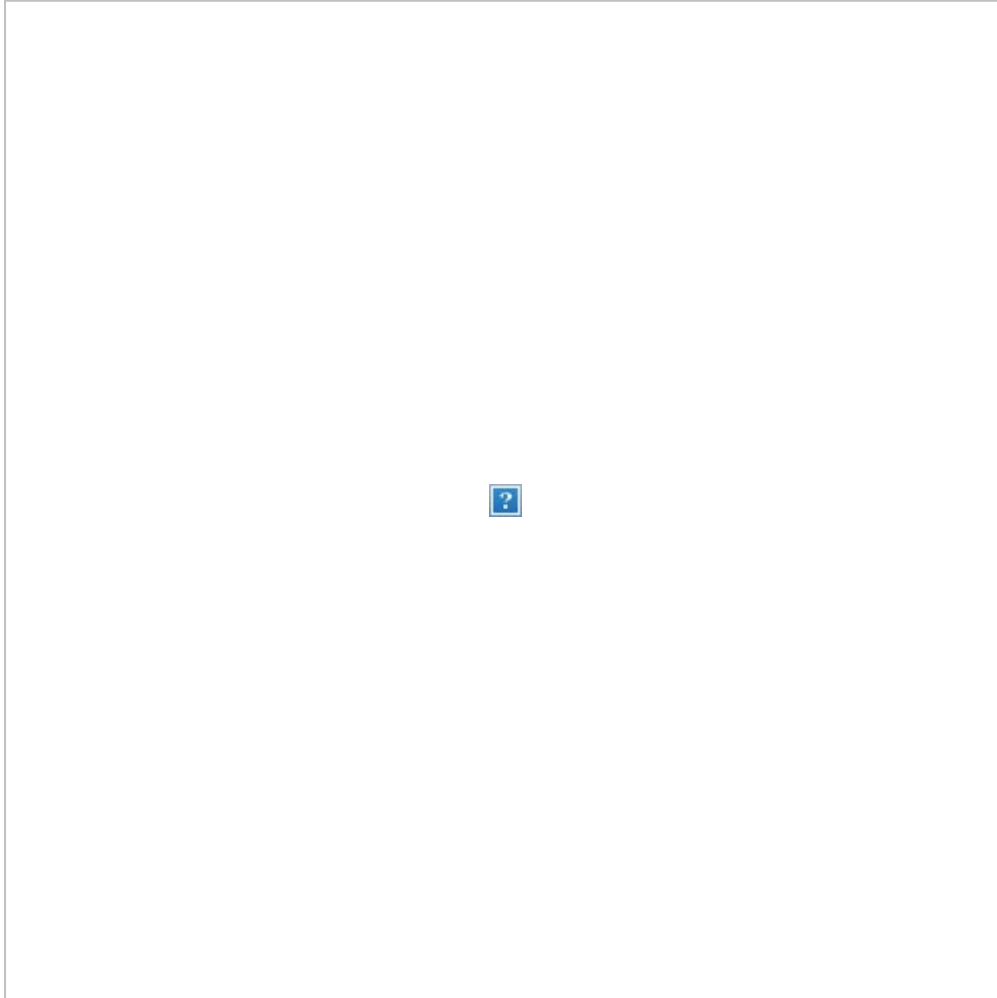
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
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Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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KiwiRail submission in support of Takanini Crossings NoRs 1 and 2

NoR1

Construction of a new bridge with general traffic lanes and/or walking and cycling facilities across the NIMT and associated works at Spartan, Manuia, Manuroa Roads and Taka Street and level crossing closures (except Manuia)

NoR 2

Construction, operation, maintenance and upgrade of transport infrastructure on Walters Road, Takaanini, which includes the closure of the existing level crossing on Walters Road, a new bridge with general traffic lanes and walking and cycling facilities across the North Island Main Trunk (NIMT) line, as well as local road connections and all associated works

Reason for submission

Level crossing safety

KiwiRail is part of Auckland's wider transport family and supports initiatives improving efficiency and safety on the rail corridor. Removing level crossings will enable a more efficient and productive freight network and improve safety for both users of the road network and the rail corridor. KiwiRail is committed to supporting level crossing safety, both in Auckland and across the network. This ranges from supporting closures to mitigating risks at existing private and public crossings, to support for TrackSafe and public education programmes.

International best practice has long established that physically separating rail, road, and pedestrian traffic creates optimum network outcomes from a safety and capacity perspective. Strategically, we need to move more people and goods on trains in the future than we do today. Working against this, rail capacity and spacing of the signalling system block sections is constrained by the level crossings, limiting the number of train movements on the existing network.

KiwiRail role

KiwiRail Holdings Limited (KiwiRail) is the State-Owned Enterprise responsible for the management and operation of the national railway and Interislander ferry services. In many places, the rail network has been in place for over 100 years and remains crucially important to the economic and social development of the areas it services. The rail network serves two functions as a metropolitan public transport service in Auckland and Wellington primarily, and a route for freight and other services nationally.

The land upon which the rail network operates is owned by the New Zealand Railways Corporation and leased to KiwiRail. KiwiRail owns the rail infrastructure (including rails, sleepers, sidings, and depots) and is a railway operator for the purposes of the Railways Act



2005. It is also the licensed Access Provider under the Railways Act, which provides KiwiRail with broad powers to safely control and restrict the use of railway assets and entry onto railway land.

While not formally part of Te Tupu Ngātahi/Supporting Growth (Te Tupu Ngātahi), KiwiRail is working closely with Auckland Transport (AT) and Waka Kotahi (WK) to develop the strategic transport network to support Auckland's growth areas, particularly in the south.

KiwiRail owns and maintains Auckland's Metro track network and is currently well into delivering major southern projects including electrification between Papakura and Pukekohe and, three new Drury stations (Drury, Ngākōroa and Paerātā), and will shortly embark on work to add capacity to the NIMT (North Island Main Trunk).

A functioning and efficient freight network is critical to the productivity of the nation's supply chain. KiwiRail also operates New Zealand's rail freight network and tourism passenger rail services between Auckland and Wellington and the Te Huia Hamilton – Auckland passenger service, which began in April 2021. Further interregional passenger growth is predicted. KiwiRail therefore has a significant interest in planning to enable the efficient flow of imports, exports, and domestic goods within and through the region. Freight tonnage is forecast to treble to, from and through the region over the next 25 years.

Aligned with its broader national role, KiwiRail and AT have developed a 30-year strategic plan to meet the needs of the network, its users and increasing demand. The Strategic Rail Programme plans to increase capacity, improve future passenger and freight levels of service to drive increases in rail mode share. This will be underpinned by greater network reliability and resilience through a step change in maintenance and renewals (without having to close lines for extended periods). The removal of level crossings is a key component of planned future investment outlined in the Strategic Rail Programme.

As KiwiRail is the Requiring Authority for the earlier designation/s, approval under s177 of the Resource Management Act 1991 (RMA) is required for any secondary requiring authority seeking to undertake works within the railway corridor.

KiwiRail acknowledges that the NoR(s) Assessment of Effects on the Environment (AEE) identifies that further engagement with KiwiRail will continue as the Project is developed and that bridge designs require particular attention. This future work will need to reference (and incorporate) the KiwiRail Engineering Principles and Standards applying at the time. Future construction methodologies will also need to prioritize the need to limit the operational impacts on the NIMT - which is acknowledged in the AEE.

Specific areas that are of greatest interest to KiwiRail and around which further detail will be required prior to granting any s177 approval, include:

That growing the capacity and resilience of the NIMT through the provision of additional tracks (as outlined in the Strategic Rail Programme) is acknowledged and accommodated as far as possible in the development and design of the Project

NoR alignments which restrict or challenge emerging rail corridor options are addressed in advance of starting detailed design

Evidence that the KiwiRail’s Engineering Principles and Standards are met

Construction methodologies that reduce the need for, or duration of, any proposed full closure of the NIMT

All safety and operational concerns arising from structures over and adjacent to the rail corridor are addressed, including but not limited to ongoing effects on corridor stability.

Table One below sets out several specific design areas for future discussions. KiwiRail considers that these can be managed to meet both parties’ objectives provided there is early and ongoing engagement.

Table One

NoR potential constraint at named level crossings and suggested approach

NoR	Issue	Resolution
<p>NoR 1</p> <p>Construction of a new bridge with general traffic lanes and/or walking and cycling facilities across the NIMT and associated works at Spartan, Manuia, Manuroa Roads and Taka Street and level crossing closures (except Manuia)</p>	<p>Allows for an increase of track capacity however potentially limits provision of maintenance access to improve resilience</p> <p>All bridge structure abutments and pier locations will need to be confirmed in future design.</p> <p>The proposed location of the cul de sac at the end of Takanui Road (Southwest corner) is very close to the existing mains so rail capacity implications need to be worked through with KiwiRail</p>	<p>Ongoing dialogue and engagement before detailed design starts and throughout design process (applies to all issues)</p>

NoR	Issue	Resolution
	<p>Station access from the bridge at Taka Street and from the footbridge at Manuroa Road needs to be well considered in detailed design.</p> <p>The proposed ramps at Manuroa and Spartan Roads need careful attention due to proximity to the rail corridor and overhead electric systems.</p> <p>Future swale and overland flow solutions will require a coordinated approach by AT and KiwiRail.</p> <p>At Spartan Road the major drainage swale/overland flow path in the rail corridor may conflict with footbridge. This needs to be considered in future design work including the reprovision of drainage infrastructure to prevent overland flow into the rail corridor.</p> <p>The location of the footbridge at Spartan Road needs to accommodate the potential capacity enhancement on the eastern side of the existing tracks.</p> <p>Provision required for on-track access from the eastern side</p>	<p>Identification of opportunities for future connection to the platform</p>
<p>NoR 2</p> <p>Construction, operation, maintenance and upgrade of transport infrastructure on Walters Road, Takaanini, which includes the closure of the existing level crossing on Walters Road, a new bridge with general traffic lanes and walking and cycling facilities across the North Island Main Trunk (NIMT) line, as well as local road connections and all associated works</p>	<p>Allowance for future capacity upgrades of rail system including access for construction, operation and maintenance needs to be considered.</p> <p>The bridge structure abutment and pier locations need to be confirmed in future design.</p>	<p>Ongoing dialogue and engagement before detailed design starts and during design process.</p>

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1254] Notice of Requirement online submission - Parks and Community Facilities
Date: Thursday, 14 December 2023 8:45:42 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Parks and Community Facilities

Organisation name: Auckland Council

Full name of your agent: Bianka Griffiths

Email address: bianka.griffiths@aucklandcouncil.govt.nz

Contact phone number: 027 337 3218

Postal address:
Auckland House - Level 12
135 Albert Street
Auckland
Auckland 1010

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 1 Spartan Road, Manuia Road, Manuroa Road and Taka Street

The specific provisions that my submission relates to are:

The direct effects of NoR 1 on the properties at: 24R Taka Street; 2R Challen Close; 8 Takanini Road; as well as the indirect effects of NoR 1 on the properties at 103R Manuroa Road; 12 Challen Close; 16R Reding Street; 2 Popes Road; 20W Challen Close; 354F Porchester Road; 35R Spartan Road; 48R Rangī Road; 50R Rangī Road; 8 Takanini Road; and, 8R Scotts Field Drive.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The submitter is strongly opposed to NoR 1 to the extent that a significant part of 24R Taka Street will be acquired by the NoR, as well as the remaining properties subject of this submission will be affected both directly and indirectly by NoR 1 including the applicable indicative construction areas. The loss of public open space and park land at 24R Taka Street is of most concern to the submitter.

I or we seek the following recommendation or decision from Auckland Council:

Avoiding all effects on the properties subject of this submission, so that the properties' public open features and the submitter's assets are preserved and maintained, and/or fully reinstated to the same or a better condition than they were prior to any works associated with the proposed designation. Further, that if the extent of effects of NoR 1 on 24R Taka Street cannot be avoided, then the requiring authority must mitigate or remedy the loss of public open space caused by NoR 1 so that the same or more public open space is provided in a strategic location that is in proximity the area taken by NoR 1.

Submission date: 14 December 2023

Attend a hearing

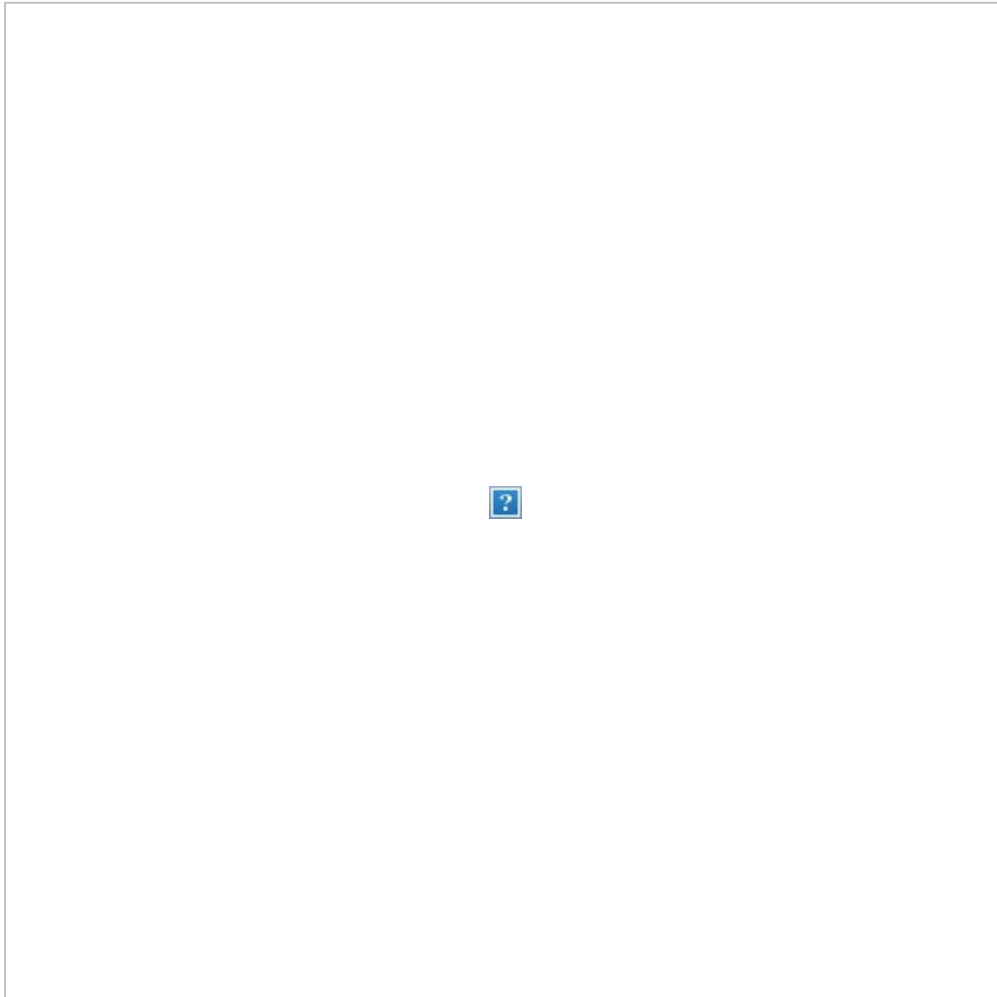
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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Form 21

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification under Section 168A, 169, 181, 189A, 190 and 195A of the Resource Management Act 1991

Date: 14 December 2023

To: Auckland Council

Name of Submitter: Te Tāhuhu o te Mātauranga | Ministry of Education

Address for Service: Woods
8 Nugent Street
Grafton, Auckland

Attention: Emma Howie, General Manager – Planning & Urban Design

Phone: 027 572 2220

Email: Emma.howie@woods.co.nz

Submission on two Notices of Requirement for Te Tupu Ngātahi Supporting Growth for the Takaanini Level Crossing Project

SUMMARY

- 1) The Ministry of Education (“**the Ministry**”) is the Government’s lead advisor on the New Zealand education system, shaping direction for education agencies and providers and contributing to the Government’s goals for education.
- 2) Te Tupu Ngātahi Supporting Growth Alliance (“**Te Tupu Ngātahi**”) has lodged two Notices of Requirement (“**NoR**”) for the Takaanini Level Crossing Project (“**TLC**”):
 - NoR 1 – Takaanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street - Auckland Transport
 - NoR 2 – Takaanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge – Auckland Transport
- 3) This submission relates to both NoRs lodged by Te Tupu Ngātahi on behalf of Auckland Transport.
- 4) The Takaanini Level Crossings Project is located in proximity to a range of designated schools within Takaanini, Papakura, and Manurewa. There is potential for these existing schools, or any future schools developed in this area, to be affected by traffic, noise and other nuisance effects arising from future construction works of this transportation network. The Ministry is seeking to ensure that appropriate conditions are included in the designations to mitigate any adverse effects associated with the construction of the TLC.
- 5) The Ministry supports the provision of active transport modes (walking and cycling) as proposed through the TLC.
- 6) Overall, the Ministry’s submission is neutral on the NoRs subject to changes being made to the conditions as set out in this submission.
- 7) The Ministry wishes to be heard in support of its submission.

OVERVIEW OF THE MINISTRY'S RESPONSIBILITIES & LAND INTERESTS

- 8) The Ministry is the Government's lead advisor on the New Zealand education system. The Education and Training Act 2020 sets out the obligations and responsibilities of the Ministry. The Ministry have responsibility for the education outcomes of students across the full spectrum of the education sector, including pre-school, primary and secondary levels.
- 9) The Ministry assesses population changes, school roll fluctuations and other trends and challenges impacting education provision at all levels of the education network to identify changing needs within the network so the Ministry can respond effectively.
- 10) The Minister of Education is a Requiring Authority under the Resource Management Act 1991 ("RMA") and has over 400 education purposes designations in the Auckland Unitary Plan: Operative in Part ("AUP:OP").
- 11) The Ministry has responsibility for all education property owned by the Crown. This involves managing the existing property portfolio, upgrading and improving the portfolio, purchasing and constructing new property to meet increased demand, identifying and disposing of surplus State school sector property and managing teacher and caretaker housing.
- 12) The Ministry is therefore a considerable stakeholder and social infrastructure provider in terms of activities that may impact existing and future educational facilities and assets in the Auckland region.
- 13) The Ministry has multiple education sites within the area (a 3km radius from NoR 1 or 2) including:
 - Alfriston College (4901)
 - Greenmeadows Intermediate School (4929)
 - Manurewa East School (4952)
 - Manurewa South School (4955)
 - Randwick Park School (4981)
 - Conifer Grove School (5013)
 - Cosgrove Primary School (5014)
 - Edmund Hillary School (5016)
 - Kelvin Road Primary School (5018)
 - Papakura Central School (5020)
 - Papakura High School (5021)
 - Papakura Intermediate School (5022)
 - Papakura Normal School (5023)
 - Takanini School (5028)
 - Kauri Flats School (5056)
- 14) The location of each NoR in relation to the Ministry's existing assets is shown in **Figure 1**.

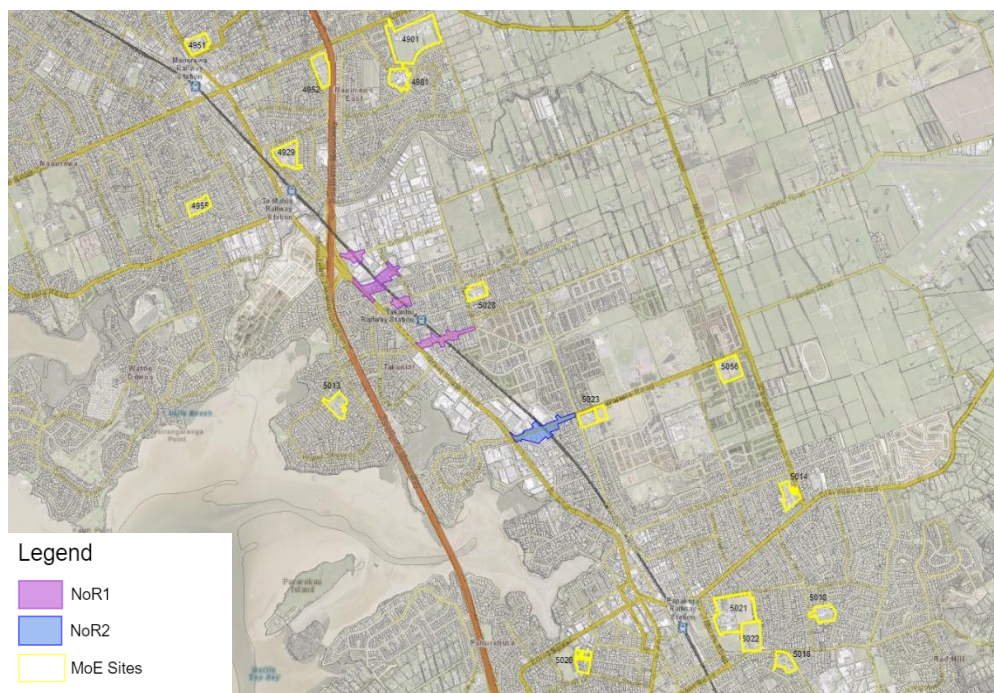


Figure 1: Project Overview – Location of two NoRs (identified in purple and blue) in relation to the Ministry of Education's School Network (outlined in yellow)

MINISTRY OF EDUCATION'S SUBMISSION

- 15) Under the RMA, decision-makers must have regard to the health and safety of people and communities. Furthermore, there is a duty to avoid, remedy or mitigate actual and potential adverse effects on the environment.
- 16) The two NoRs will designate land to respond to both existing transport deficiencies as well as provide for the forecasted future growth pressures in the area. There are currently four public road level crossings along the North Island Main Trunk ("NIMT") line in the Takaanini area at Spartan Road, Manuroa Road, Taka Street, and Walters Road. Each of these currently experience congestion, severance, and an elevated level of safety risk stemming from the operation of the level crossings at grade. The project supports improved walking and cycling, public transport, and general traffic connections. The key reasons for this investment are to improve safety, better integrate transport and land use, improve accessibility, transport resilience, and promote travel choice. This is in the form of new bridges with general traffic lanes and walking and cycling facilities across the NIMT line.
- 17) The Ministry broadly supports the Project's aim to plan transport investment in Auckland's future urban zoned areas. The project will improve active mode facilities, enhancing the safety of students walking and cycling to and from school.
- 18) The Ministry supports the provision of construction, operation, maintenance and upgrade of transport infrastructure that will provide safe access to the current and future wider school network. Encouraging mode shift will provide significant health benefits for students and staff, reducing traffic generation at pick-up and drop-off times. Schools should be well serviced by safe and accessible pedestrian and cycling links as well as public transportation facilities, and it is considered that the proposed upgrades will provide adequate cycling and walking infrastructure to the surrounding area.
- 19) The Takaanini project is a large programme of work. The quantum of construction required to deliver the projects will likely have temporary adverse effects on the surrounding environment. There are several schools in proximity to the NoRs. There is potential for these schools to be affected by traffic, noise and other nuisance effects arising from future construction works. The construction timing and staffing is yet to be determined, so there is uncertainty regarding the construction methodology, including the routes for construction vehicles and the location of construction laydown areas.
- 20) The Ministry seeks to appropriately address and manage construction-related effects and the ongoing potential effects the project may have on the operation and management of the schools and any future schools for NoRs 1 and 2, as the project has a lapse date of 15 years.
- 21) The key issues that the Ministry has concerns about in relation to the NoRs include construction traffic effects and stakeholder engagement which are outlined below. Consequential changes are also sought to the acronyms/terms and conditions of the NoRs for consistency with other Te Tupu Ngātahi designations.

Construction traffic effects

- 22) The surrounding schools (and any future schools) will potentially be affected by an increased volume of heavy vehicles to access the construction area of the NoRs. This is a traffic safety concern for students walking and cycling to school at peak pick-up and drop-off times.
- 23) Condition 18 requires the preparation of a CTMP prior to the start of construction. The Ministry supports the inclusion of this condition but requests that specific reference is made to education facilities to address the estimated numbers, frequencies, routes and timing of traffic movements, including any specific non-working or non-movement hours (for example on roads servicing educational facilities during pick-up and drop-off times) to manage vehicular and pedestrian traffic near educational facilities or to manage traffic congestion.

Construction noise and vibration effects

- 24) The surrounding schools (and any future schools) will potentially be affected by an increased volume of construction noise and vibration due to the construction of these NoRs.
- 25) Conditions 21 and 22 requires the preparation of a CNVMP prior to the start of construction. The Ministry supports the inclusion of this condition but requests that specific reference is made to education facilities to ensure they are taken into consideration as part of the development of this plan as a key stakeholder. In addition, the Ministry requests that any construction activities that could be expected to significantly exceed the permitted noise and/or vibration levels are undertaken outside of study and exam periods to minimise disruptions to students' learning.

Stakeholder engagement

- 26) The Ministry supports the establishment of Stakeholder Communication and Engagement Management Plan (SCEMP) (condition 9). However, the Ministry considers that they are a key stakeholder in this Project, and specific engagement is required to manage construction effects on the schools.
- 27) Amendments made to conditions are requested to ensure consistency with the changes made to the Te Tupu Ngātahi Warkworth NoR conditions as included in the Strategic Planning & Conditions Rebuttal Evidence prior to the Council hearing. This includes the requirement that at least 6 months prior to construction, the requiring authority shall identify a list of stakeholders and properties and identify methods to engage with stakeholders and submit this record with any Outline Plan of Works for the relevant stage of work. The inclusion of a new condition that addresses this, is consistent with other conditions agreed through Te Tupu Ngātahi designations.

Acronym/Terms

- 28) The Ministry seeks that acronyms and terms used in the NoRs are consistent with those agreed through other Te Tupu Ngātahi NoRs. As these terms are continuously evolving through hearings on NoRs, a summary of the terms supported is provided below:
- Education facility
 - Stakeholder

RELIEF SOUGHT

- 29) In principle, based on the above, the Ministry supports the proposed walking and cycling facilities proposed in each NoR application providing improved active mode connectivity is essential to provide existing and future communities with a sustainable means of accessing education facilities in Takaanini area.
- 30) To ensure effects associated with the NoRs on the Ministry are appropriately managed, it is requested that amendments to conditions as set out above are adopted by Te Tupu Ngātahi. The amendments sought include:
- Inclusion of the Ministry as the primary contact for schools in the SCEMP
 - Amend the CNVMP condition
 - Amend the CTMP condition
 - Amendments to conditions, and acronym/terms to be consistent with other Te Tupu Ngātahi NoRs
- 31) Such other consequential amendments to the NoRs may be necessary to give effect to the relief sought through this submission.
- 32) Overall, the submission is neutral subject to the above changes being made to the designation conditions.
- 33) The Ministry wishes to be heard in support of its feedback.

SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Takanini Village Limited and Tonea Properties (NZ) Limited

1. Takanini Village Limited (**TVL**) and Tonea Properties (NZ) Limited (**TPL**) make this submission on the two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About TVL and TPL

2. TVL owns the leasehold interest in the land for Takanini Town Centre at 30 Walters Road, Takanini. TPL is the freehold landowner for Takanini Town Centre. Takanini Town Centre is situated on 5.4 hectares and has a gross floor area of over 19,000m². It is anchored by The Warehouse and integrates a diverse offering of more than 45 stores comprising retail, food and beverage, commercial offices, a (soon to be opened) Silky Otter cinema, medical centre and Te Paataka Koorero o Takaanini, the Takaanini Library and Community Hub.
3. The single landholding comprising Takanini Town Centre is owned and operated by TVL.
4. TVL and TPL (and therefore the tenants) will be significantly impacted by the Project, which includes works from NoR2 on Walters Road immediately south of the site, as well as tie-ins to portions of Arion Road, Braeburn Place, Tironui Road, the Porchester Road roundabout, and the Great South Road roundabout. The Project will have a direct impact on the one-way access into the town centre including the main access to the underground car park. The Project will also reduce the parking spaces in the southern carpark that serve the town centre and sever existing pedestrian connections both to the town centre and its surrounds.

5. The Submitters also have concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre.

Submission

5. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
6. TVL and TPL generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects. However, TVL and TPL oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on TVL and TPL and significant adverse effects on the surrounding environment including:
 - a. Adverse effects on the operation and safety of access from Walters Road to the Takanini Town Centre;
 - b. Potential adverse effects on the Walters Road/Arion Road intersection;
 - c. Parking effects during construction of the Project and following completion of the Project;

- d. Adverse effects on the interface with the Takanini Town Centre, including landscape effects and urban design considerations;
- e. Construction noise and vibration effects;
- f. Adverse geotechnical effects on existing buildings and infrastructure, including on the underground basement to the building in the southeast corner of the site;
- g. Adverse impacts on existing service connections to the site including water, fibre, gas, power and impacts on stormwater networks both piped and overland flows; and
- h. Adverse impacts on existing pedestrian and vehicular access and carparking including the loss of pylon signage, carparks, electric vehicle charging infrastructure, gardens and irrigation.

7. The Submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

- 8. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
- 9. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network

have not been given adequate consideration. Furthermore, the Requiring Authority's assessment of alternatives is cursory and falls short of "adequate".

10. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:

- a) Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
- b) Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
- c) Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
- d) Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
- e) The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g.. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
- f) Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
- g) Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
- h) A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase

and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

11. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:
- a) The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
 - b) An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
 - c) There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
 - d) The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
 - e) There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on Takanini Town Centre and other surrounding properties and businesses.
 - f) There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for Takanini Town Centre and its surrounding sites.
 - g) An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Traffic Safety Effects

12. The Requiring Authority proposes to temporarily (for a period of 2.5 – 3 years) restrict access from Walters Road to the Takanini Town Centre. Critically this will place sole dependency on the remaining access into the site from Arion Road (a local residential road) during the

construction period and it will not be able to operate safely or efficiently. Based on the limited information provided it is not clear if the one-way in to the Town Centre from Walters Road will be lost permanently. This will result in significant adverse traffic effects on the surrounding transport network, including the signalised Walters Road / Arion Road intersection and on the safety of all transport users. This will also have an impact on the Arion Road entrance into the Town Centre and will adversely impact on traffic flows within the Town Centre itself. The Requiring Authority has not provided adequate information to support the design or its operation. The Submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

13. During the construction period the Requiring Authority also seeks to acquire carparking land which serves the Takanini Town Centre. The Project does not detail how adequate car parking will be maintained for the Town Centre during the construction period. The Project will result in a shortfall of carparking, and it will result in a direct contravention of a lease agreement to supply minimum carparking ratios to The Warehouse.
14. The NoR2 Project will also remove the electric vehicle charging stations that are located within the southern carpark.

Landscape and Urban Design Effects

15. The Project has not adequately demonstrated that an appropriate interface will be provided to the Takanini Town Centre. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Town Centre (and for nearby residential dwellings).

Construction Noise and Vibration Effects

16. The Project will result in an increase in noticeable noise levels and for some landowners it will result in significant adverse noise and vibration effects over long durations, and at high frequencies.
17. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

18. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
19. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
20. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

21. In the event that NoR1 and NoR2 are confirmed, the Submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.

Conditions should be imposed to ensure the minimum practicable impact on Takanini Town Centre especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

21. The Submitters seek that Auckland Council:
 - a) Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b) Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
22. The Submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

23. The Submitters could not gain an advantage in trade competition through this submission.

24. The Submitters wish to be heard in support of this submission. TVL and TPL would consider presenting a joint case at the hearing.

Date: 14 December 2023

Address for service: Jeremy Brabant / Shannon Darroch
Foundry Chambers
Level 4, Vulcan Building
Cnr Queen Street and Vulcan Lane
PO Box 1502, Shortland St
Auckland

Mobile: 021 494 506 / 021 077 8497

Email: jeremy@brabant.co.nz
shannon@brabant.co.nz

SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Sunlight Holdings Limited and South Auckland Marine Limited

1. Sunlight Holdings Limited (**SHL**) and South Auckland Marine Limited (**SAML**) make this submission on two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. SHL owns the land at 1-3 Walters Road, Takanini (**Site**). Originally SAML was established at 11 Walters Road before moving to the Site in 2002. SAML owns and operates the marine business undertaken on the Site. The Site is accessed from Walters Road and is located opposite the Southgate Shopping Centre.
3. SAML is a family owned and run business which has been operating in Takanini for almost 40 years. SAML provides goods and services to the marine industry, specialising in all major engine and boating brands. The Site includes a marine retail shop which specialises in boat products, trailer parts, fishing gear and bait supplies, stainless and rigging hardware.
4. In addition to marine retail, SAML are certified Yamaha and Mercury service agents providing engine diagnostic and servicing. SAML also provide services to repair trailers, boat wiring, battery setups, boat repairs and installations. Both the retail and the servicing undertaken on the Site require effective and safe access for vehicles towing boats or trailers. In addition, manoeuvring room on Site and display and/or storage room on Site for vessels for sale or being serviced are critical to the operation of the business.

5. SHL and SAML (as the tenant) will be significantly impacted by NoR2 as:
 - a. The proposed designation covers the entirety of the Site;
 - b. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (located between Walters Road and Tironui Road) cuts through a significant portion of the Site's existing building, making the operation unviable;
 - c. SAML's business requires a highly visible and exposed site with good connections to foot traffic and the public. The Site is well-equipped and is purpose-built for the goods and services provided by SAML;
 - d. The Project will have a direct impact on the one-way access and manoeuvrability into the Site from Walters Road. SAML has large boat and towing vehicles which require sufficient yard turning space; and
 - e. Together, the alignment of the new access lane and proposed extent of the designation boundary mean that continued operation of SAML's business would not be viable. Furthermore, post-construction the rump land will be an inefficient and unusual shape for development or use for the purposes for which it is zoned, have compromised access, and poor amenity due to the size, height and proximity of the proposed new bridge.
6. The Submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

7. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;

- c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
8. SHL and SAML generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
9. However, SHL and SAML oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on SHL and SAML and significant adverse effects on the surrounding environment including:
- a. SAML will not be able to continue its operation. The business will likely need to relocate;
 - b. Any rump land not used for the access road or for construction will be inefficient and unusually shaped which will make use of that land challenging;
 - c. Loss of yard space will severely impact SAML's ability to store and manoeuvre large boats and towing vehicles;
 - d. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;
 - e. Potential adverse effects on the Walters Road/Tironui Road intersection;
 - f. Parking effects during construction of the Project and following completion of the Project;
 - g. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and

- h. Construction noise and vibration effects.

10. The submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

- 11. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
- 12. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority’s assessment of alternatives is cursory and falls short of “adequate”.
- 13. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. This is particularly the case here where SHL’s entire landholding will be subject to the designation. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:

- a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
- b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
- c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
- d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
- e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
- f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
- g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
- h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

14. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.

- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

- 15. The Project will have significant and irreversible adverse effects on SHL and SAML. The location of the proposed overbridge and alignment of the access lanes in combination with construction setbacks will result in either the complete loss of the buildings on Site (such that the SAML operation is no longer viable) or an outcome where a small portion of the Site is retained with limited or no access.
- 16. To the extent that the rump of the Site is retained, the following adverse effects are noted.

Traffic Safety Effects

- 17. The Requiring Authority proposes to temporarily restrict access to sections of Walters Road. This is particularly relevant to the surrounding Takanini Village Centre and Southgate Shopping Centre. This will result in significant adverse traffic effects on the surrounding transport

network and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently. There is also a lack of detail as to how the Site can be accessed during construction.

Landscape and Urban Design Effects

18. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to tenants and customers.

Construction Noise and Vibration Effects

19. The Project will result in an increase in noticeable noise levels and for some landowners it will result in significant adverse noise and vibration effects over long durations, and at high frequencies.
20. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

21. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
22. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
23. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

24. In the event that NoR1 and NoR2 are confirmed, the submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini

Town Centre and the surrounding business and residential landowners including SHL and SAML.

25. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

26. The submitters seek that Auckland Council:
- a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
27. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

28. The submitters could not gain an advantage in trade competition through this submission.
29. The submitters wish to be heard in support of this submission. SHL and SAML would consider presenting a joint case at the hearing.

Date:	14 December 2023
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SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Mead Trusts Holdings Limited and Carters Buildings Supplies Limited

1. Mead Trusts Holdings Limited (**MTHL**) and Carters Buildings Supplies Limited (**Carters**) make this submission on two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. MTHL owns the land at 12 Walters Road, Takanini (**Site**). Carters leases and occupies the Site from which it operates the Carters Papakura.
3. The Site has a long history in the timber industry. Carters has been operating as a timber merchant from the Site since the 1960s. Prior to that, timber was sold as part of a timber mill operation that was located on the Site.
4. Carters operates a “to trade” timber merchant business providing building materials to the greater South Auckland region. As part of this operation:
 - a. Carters stores timber and a wide variety of building products both in the yard and inside the warehouse/showroom.
 - b. Carters receive bulk deliveries of timber and construction materials transported via large, long-haul B train trucks.
 - c. Carters delivers products from the Site using small to medium sized trucks.
 - d. Customers can visit the Site to pick up materials.

- e. The Carters buildings are purpose-built, with the entirety of the Site covered in reinforced concrete to suit the business operations undertaken.
5. Currently, Carters Papakura has 26 staff members based in this location who are onsite 6 days a week.
6. MTHL and Carters (as the tenants) will be significantly impacted by the Project (particularly NoR2) as:
 - a. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (fronting Walters Road) cuts through a significant portion of the Site's yard, restricting access to the Site;
 - b. NoR2 will have a direct impact on the sole access to the Site from Walters Road. Deliveries of timber and construction materials by large truck and trailers will not be possible under NoR2;
 - c. Essential truck access will be restricted. Truck access is crucial for the operation of Carters' business;
 - d. NoR2 will reduce the 7,000m² Site by 1,500m². The reduction in site size removes a significant portion of the parking area used by staff and customers. It also results in the removal of adjacent roadside parking;
 - e. The Site has been designed and developed specifically for Carters' building supplies operation. The effects of NoR2's reduction in yard space and the removal of essential access and parking have adverse implications for the Site's fitness for purpose.
7. The Submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

8. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
9. MTHL and Carters generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
10. However, MTHL and Carters oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on MTHL and Carters and significant adverse effects on the surrounding environment including:
 - a. Significant reduction in the Site's yard size which has consequences for the continued operation of Carters on the Site;
 - b. NoR2 directly impacts and restricts the sole access to the Site from Walters Road. Deliveries of timber and construction materials by large truck and trailers will not be possible under NoR2. This will have critical adverse effects on Carters' operation and trade.
 - c. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;

- d. Potential adverse effects on the Walters Road/Great South Road intersection;
- e. Parking effects during construction of the Project and following completion of the Project;
- f. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and
- g. Construction noise and vibration effects; and
- h. Adverse geotechnical effects on existing buildings and infrastructure.

11. The Submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

12. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
13. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority’s assessment of alternatives is cursory and falls short of “adequate”.

14. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:
- a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
 - b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
 - c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
 - d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
 - e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
 - f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
 - g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
 - h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

15. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

16. The Project will have significant effects on the Site's access from Walters Road and there are concerns that during the construction period the restricted access will not be able to operate safely or efficiently. The Site is accessed solely from Walters Road. Carters relies on that access for transport and deliveries of timber and building supplies from large truck and trailers. Removal, limitation or restriction of that access will have fatal consequences on Carters' operations.

17. Particularly when combined with the limitation of access on other businesses on Walters Road, this will result in significant adverse traffic effects on the surrounding transport network, including the roundabout at the Walters Road / Great South Road intersection and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The Submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

18. During the construction period the Requiring Authority also seeks to acquire carparking land which serves Carters (both staff and customer parking). The Project does not detail how adequate car parking will be maintained for the Site during the construction period. The Project will result in a shortfall of carparking, and it will impact staff and customers.

Landscape and Urban Design Effects

19. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Site.
20. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

21. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
22. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
23. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

24. In the event that NoR1 and NoR2 are confirmed, the submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.
25. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

26. The submitters seek that Auckland Council:
- a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
27. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

28. The submitters could not gain an advantage in trade competition through this submission.
29. The Submitters wish to be heard in support of this submission. MTHL and Carters would consider presenting a joint case at the hearing.

Date: 14 December 2023

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SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Arborfield Trust, Takanini Home and Trade Limited, and Mitre 10 Mega Takanini Limited

1. The Arborfield Trust (**Trust**), Takanini Home and Trade Limited (**THTL**), and Mitre 10 Mega Takanini Limited (**Mitre 10**) make this submission on two Notices of Requirement (**NoR**) for the Takanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. This submission relates to two adjoining parcels of land located at 230 Great South Road and 20A Walters Road (collectively, the **Site**). By way of context:
 - a. The land at 20A Walters Road is owned by the Trust and is leased to THTL trading as Mitre 10. 20A Walters Road stores timber overstock and receives container deliveries of timber. Mitre 10 customers are able to click and collect for retail bulk goods.
 - b. The land at 230 Great South Road Takanini (Southgate Shopping Centre) is owned by Centuria Group. Mitre 10 (the lessee) is the anchor tenant of the Southgate Shopping Centre. This site is sub-leased to THTL. Mitre 10 is a large-format hardware store which supplies customers (including trade customers) with building supplies, home improvement supplies, and garden retail.

3. The Mitre 10 Group is a co-operative that has been operating in New Zealand for almost 50 years. Mitre 10 (Takanini) has been operating from 230 Great South Road for the last 20 years. A measurable portion of Mitre 10's customer base is from the Addison/Botany/Flat Bush and Clevedon areas. Good site access and transport links are essential both from an operational and delivery perspective, and also to service the longer-distance customer base.
4. The submitters will be significantly impacted by the Project (particularly by NoR2 at 20A Walters Road) as:
 - a. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (fronting Walters Road) results in the removal of the left turn option into the Site from Walters Road.
 - b. NoR2 will have a direct impact on the sole access into 20A from Walters Road;
 - c. 20A Walters Road is critical to the safe operation of Mitre 10's heavy vehicles, both for deliveries and the dispatching of goods from the Site.
 - d. As proposed, part of the NoR2 designation extends into a portion of the existing building at 20A Walters Road. The building is currently used for unloading containers and for bulk warehousing of timber and building supplies. It is an important component of Mitre 10's operation. The building will be rendered unusable if the land is required for construction of the Walters Road overbridge. 20A Walters Road is the submitters' MPI approved location for the loading and unloading of sea freight. Impacts on that site will have significant flow-on effects for distribution.
 - e. 20A Walters Road is specifically designed to enable deliveries (including container deliveries by large truck and trailers), unloading, and storage of timber. Use of the Site's front carpark for these activities is not a viable alternative as the carpark is not designed for large trucks for example, the trucks cannot turn around, the carpark cannot support tare weight, there is no storage facility, and there will be significant health and safety risks to staff and customers.

- f. The use of B train heavy vehicle combinations has become a regular vehicle of choice in logistics and supply chain. As proposed, NoR2 will limit Mitre 10's ability to use B trains.
5. The submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

6. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
7. The submitters generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
8. However, the submitters oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on the submitters and significant adverse effects on the surrounding environment including:
 - a. Construction effects and the potential use of 20A Walters Road for construction purposes which will affect or remove part of the existing building on that site. This

will result in flow-on consequences for the operation and supply of materials for Mitre 10;

- b. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;
- c. Potential adverse effects on the Walters Road/Great South Road intersection;
- d. Parking effects during construction of the Project and following completion of the Project;
- e. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and
- f. Construction noise and vibration effects; and
- g. Adverse geotechnical effects on existing buildings and infrastructure.

9. The submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

10. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.

11. The submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority's assessment of alternatives is cursory and falls short of "adequate".
12. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:
 - a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
 - b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
 - c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
 - d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
 - e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
 - f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
 - g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and

- h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

13. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

14. The Project will have significant effects on the Site's access from Walters Road and there are concerns that during the construction period the restricted access will not be able to operate safely or efficiently. Particularly when combined with the limitation of access on other businesses on Walters Road, this will result in significant adverse traffic effects on the surrounding transport network, including the roundabout at the Walters Road / Great South Road intersection and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

15. During the construction period the Requiring Authority also seeks to use part of 20A Walters Road which infringes on an existing building and potentially part of the yard. Additionally, the submitters are concerned that the limitations imposed on Walters Road will have flow-on effects for the Southgate Shopping Centre – particularly with respect to parking and access. The Project does not detail how adequate car parking will be maintained for the Site during the construction period. The Project will negatively impact staff and customers.

Landscape and Urban Design Effects

16. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Site.
17. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

18. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.

19. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
20. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

21. In the event that NoR1 and NoR2 are confirmed, the Submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.
22. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

23. The submitters seek that Auckland Council:
 - a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
24. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

25. The submitters could not gain an advantage in trade competition through this submission.
26. The submitters wish to be heard in support of this submission. The Trust, THTL, and Mitre 10 would consider presenting a joint case at the hearing.

Date: 14 December 2023

Address for service: Jeremy Brabant / Shannon Darroch
Foundry Chambers
Level 4, Vulcan Building
Cnr Queen Street and Vulcan Lane
PO Box 1502, Shortland St
Auckland

Mobile: 021 494 506 / 021 077 8497

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**Submission on the Two Notices of Requirement for the Takanini Level Crossing lodged by
Auckland Transport as requiring authority under the
Resource Management Act 1991**

TO: Attn: Planning Technician Auckland Council Level 24, 135 Albert
Street Private Bag 92300 Auckland 1142

SUBMISSION ON: Notices of Requirement ("**NoRs**") for the Takanini Level Crossing
("**TLC**")

FROM: Watercare Services Limited ("**Watercare**")

ADDRESS FOR SERVICE: Mark Bishop
Regulatory & Policy Manager
Watercare Services Ltd
Private Bag 92 521
Wellesley Street
AUCKLAND 1141
Phone:022 010 6301
Email: Mark.Bishop@water.co.nz

DATE: 14 December 2023

1. INTRODUCTION

- 1.1 Watercare is pleased to have the opportunity to make a submission on the two NoRs for the Takanini Level Crossing TLC lodged by Auckland Transport as a requiring authority under the Resource Management Act 1991 ("**RMA**").
- 1.2 Watercare neither supports or opposes these NoRs (ie it is neutral as to whether the NoRs are confirmed or not). Watercare seeks to ensure that any decisions made on the NoRs respond to the issues raised in this submission and avoids, remedies, or mitigates potential adverse effects on Watercare's ability to provide water and wastewater services now and in the future.
- 1.3 Watercare could not gain an advantage in trade competition through this submission.

2. WATERCARE – OUR PURPOSE AND MISSION

- 2.1 Watercare is New Zealand's largest provider of water and wastewater services. We are a substantive council-controlled organisation under the Local Government Act 2002 ("LGA") and are wholly owned by Auckland Council ("Council"). Watercare has a significant role in helping the Council achieve its vision for the city. Our services are vital for life, keep people safe and help communities to flourish.
- 2.2 Watercare provides integrated water and wastewater services to approximately 1.7 million people in the Auckland region. Over the next 30 years, this could increase by another 520,800 people, potentially requiring another 200,000 dwellings along with associated drinking water, stormwater and wastewater infrastructure. The rate and speed of Auckland's population growth puts pressure on our communities, our environment, and our housing and infrastructure networks. It also means increasing demand for space, infrastructure, and services necessary to support this level of growth.
- 2.3 Under both the LGA and the Local Government (Auckland Council) Act 2009, Watercare has certain obligations. For example, Watercare must achieve its shareholder's objectives as specified in our statement of intent, be a good employer, and exhibit a sense of social and environmental responsibility.¹
- 2.4 Watercare must also give effect to relevant aspects of the Council's Long-Term Plan, and act consistently with other plans and strategies of the Council, including the Auckland Unitary Plan and the recently adopted Auckland Council Future Development Strategy.
- 2.5 Watercare is also required to manage our operations efficiently with a view to keeping overall costs of water supply and wastewater services to our customers (collectively) at minimum levels, consistent with effective conduct of the undertakings and maintenance of long-term integrity of our assets.²

3. PLANNED AND EXISTING WATERCARE ASSETS

- 3.1 The Assessment of Effects on the Environment for the NoRs does not identify any Watercare assets within either of the NoR project areas.³ However, some of the project areas for the NoRs are within areas where Watercare has planned for future infrastructure development, as detailed at paragraph [3.2].
- 3.2 Specific commentary regarding known projects within Watercare's Asset Management Plan to service growth at a bulk level is outlined below. Solutions and alignments/locations are subject to change as we learn more, progress our projects and the area develops. There is also potential for new needs to surface, necessitating further bulk infrastructure. Ongoing engagement is critical to maintain alignment.

a) Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1)⁴

¹ LGA, s 59.

² Local Government (Auckland Council) Act 2009, s 57.

³ Assessment of Effects on the Environment for the Takaanini Level Crossings (dated October 2023) at Table 11-13.

⁴ For new multi-modal bridge crossings of the North Island Main Trunk (NIMT) at Manuia Road and Taka Street; and new active mode bridge crossings of the NIMT at Spartan Road and Manuroa Road with two consequential road closures.

- Watercare has no planned projects at this time that intersect with NoR 1, although it may have future needs that may change due to influences outside its control.

b) NoR Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR 2)⁵

- Watercare’s Takanini Cross-Connection project, which involves a new Bulk Supply Point and watermain, has the potential to intersect with proposed designation on Walters Road between Great South Road and Porchester Road.

4. SUBMISSION POINTS AND RELIEF SOUGHT

4.1 This is a submission on both NoRs (detailed above) that were publicly notified on 16 November 2023.

4.2 As noted previously, Watercare neither supports or opposes these NoRs (ie it is neutral as to whether the NoRs are confirmed or not). Watercare seeks to ensure that any decisions made on the NoRs responds to the issues raised in this submission and avoids, remedies, or mitigates potential adverse effects on Watercare’s ability to provide water and wastewater services now and in the future.

Early engagement

4.3 Watercare seeks to ensure that there is a live and continual process planned forward to recognise that asset management and construction plans are constantly updating and changing.

4.4 Watercare acknowledges the proactive approach to engagement shown by the requiring authority to date. Watercare has been in discussions with the Supporting Growth Alliance, and the preceding ‘future urban land use strategy’ project work, as well as independent engagement with AT during the development of these NoR’s.

4.5 Watercare supports in depth collaboration and consultation (including information, data sharing and identification of opportunistic works) across infrastructure providers on the development (or redevelopment) of urban environments and wishes to ensure that there is ongoing and timely engagement and collaboration as these projects develop.

4.6 As noted, Watercare seeks early engagement from the requiring authority for future planning and construction works including prior to detailed design and during implementation of construction works. Early and fulsome engagement with Watercare, along with other infrastructure providers, can enable opportunities to plan and future proof the delivery of assets to provide for well-functioning urban environments. For Watercare, this includes applying for, in a timely manner, “Works Over” Approvals, in compliance with Watercare’s “*Water Supply and Wastewater Network Bylaw 2015*” (updated 2021).

4.7 Watercare seeks to ensure the NoRs do not impact its wastewater and water services in the NoR project areas in the future (these planned projects are detailed in paragraph [3.2] above). Watercare wishes to ensure it maintains access to its assets 24 hours a day, 7

⁵ For a new multi-modal bridge crossing of the North Island Main Trunk (NIMT) railway at Walters Road.

days a week for maintenance, safety, and efficient operation of its services and that it is consulted on any works undertaken by the requiring authorities that may impact Watercare's services.

Specific amendments to conditions

- 4.8 Watercare has filed evidence, and attended, recent NoR hearings for other Supporting Growth Alliance projects (the North West Strategic Network, and the Airport to Botany Bus Rapid Transit Project). The conditions proposed for the NoRs by the requiring authorities for these NoRs are similar to those which have been proposed at the recent North West Strategic Network hearing (in rebuttal evidence).
- 4.9 Watercare supports the intention of conditions proposed by the requiring authority which seek to ensure that there is engagement with relevant stakeholders during the development of both NoRs (ie the conditions which require a Network Utility Management Plan ("**NUMP**"), Stakeholders Communication and Engagement Management Plan ("**SCEMP**"), and Land use Integration Process ("**LIP**")).
- 4.10 That said, Watercare considers further amendments to the conditions are required to address matters raised in this submission, so that the conditions for both NoRs adequately provide for engagement with network utilities, in particular during the feasibility and detailed design stage.
- 4.11 Watercare seeks that a new condition requiring the preparation of a "Network Utility Strategic Outcomes Plan" be added to both NoRs to futureproof assets in consultation with network utility operators such as Watercare:

Network Utility Strategic Outcomes Plan (NUSOP)

- (a) A NUSOP shall be prepared in the project feasibility stage or as early as practicable.
- (b) The objective of the NUSOP is to set out a strategic framework for asset resilience that includes consideration of growth, corridor protection, and asset renewals over time.
- (c) The NUSOP shall:
- i. consider expected asset life of existing assets;
 - ii. consider expected asset capacity increases or changes; and
 - iii. demonstrate how city and national strategic plans are considered.
- (d) The NUSOP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project, including Watercare.
- (e) The NUSOP shall describe how strategic plans from the Network Utility Operators in relation to its assets have been addressed.
- (f) Any comments received from the Network Utility Operator shall be considered when finalising the NUSOP.
- (g) Any amendments to the NUSOP related to the assets of a Network Utility Operator shall be prepared in consultation with that asset owner.

4.12 If the above condition is not included in the NoRs, Watercare seeks the following amendments (shown in underline) to the NUMP condition both of the NoRs:

(a) A NUMP shall be prepared after consultation with Network Utility Operator(s) including during the feasibility and detailed design phases, and prior to the lodgement of an Outline Plan of Works for a stage of construction Start of Construction for a Stage of Work.

...

(c) The NUMP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project and shall include any s177 consents required for works affecting prior Designations and Watercare 'Works Over Approvals'.

...

(h) The Requiring Authority shall consult with Network Utility Operators during the feasibility and detailed design phases to identify opportunities to enable, or not preclude, the development of new network utility facilities including access to power, water services and ducting within the Project, where practicable to do so. The consultation undertaken, opportunities considered, and whether or not they have been incorporated into the detailed design, shall be summarised in the Outline Plan or Plans prepared for the Project.


5. RECOMMENDATION SOUGHT

5.1 Watercare seeks that the Council recommend:

- (a) amendments to the NoRs, including by way of conditions to ensure any adverse effects on Watercare's assets and operations are avoided, remedied or mitigated and to address the concerns set out above; and / or
- (b) such further other relief or other consequential amendments as considered appropriate and necessary to address the concerns set out above.

5.2 Watercare wishes to be heard in support of this submission.

5.3 If others make a similar submission, consideration would be given to presenting a joint case with them at any hearing.



Steve Webster
Chief Infrastructure Officer
Watercare Services Limited

14 December 2023

Attn: Planning Technician
Submission on Notice of Requirements Lodged by Auckland
Transport: Takaanini Level Crossings Project (NoR 1 and NoR 2)

SLR Ref No.: Takaanini Level Crossings
NOR_Z Energy Submission_v1.0

14 December 2023

SLR Ref No.: Takaanini Level Crossings NOR_Z Energy Submission_v1.0

Attn: Planning Technician
Auckland Council
Level 16,
135 Albert Street
Private Bag 92300
Auckland 1142

By email: unitaryplan@aucklandcouncil.govt.nz

**RE: Submission on Notice of Requirements Lodged by Auckland Transport:
Takaanini Level Crossings Project (NoR 1 and NoR 2)**

Submitter:

Z Energy Limited
PO Box 2091
Wellington 6140

Address for Service

SLR Consulting New Zealand
PO Box 911310
Victoria St West
Auckland 1142

Attention: Phil Brown

Phone: 027 467 1566

Email: philip.brown@slrconsulting.com

A. Introduction

- 1 This is a submission on two notice of requirement (*NoRs*) applications which were lodged in association with the Takaanini Level Crossings (*TLC*) project. The Supporting Growth Alliance (*SGA*) which includes Auckland Transport and Waka Kotahi, as a requiring authority under Section 167 of the Resource Management Act 1991 (*RMA*), is seeking *NoR* approval to enable this work.
- 2 The *TLC* project involves the construction, operation, maintenance, and upgrade of five grade-separated bridge crossings of the North Island Main Trunk. *NoR* 1 comprises the section of works in Spartan Road, Manuia Road, Manuroa Road, and Taka Street. *NoR* 2 comprises the section of works in Walters Road.
- 3 Z Energy (*the Submitter*) has an interest in the following sites which are the subject of this submission:
 - a) *NoR* 1: The Z Takanini service station, located at 166 Great South Road, Takanini. (Note that the Z Takanini service station is also affected by the South Frequent Transit Network *NoR* project; this is addressed in a separate Z submission).
 - b) *NoR* 2: The Z Papakura North service station, located at 254 Great South Road, Takanini.
- 4 The Submitter could not gain an advantage in trade competition through this submission and the submission does not raise matters that relate to trade competition or the effects of trade competition.

B. Summary of Submission

- 5 The Submitter supports the principle of improving road safety at level crossings and encouraging multi-modal transport options but opposes the *NoRs* insofar as they relate to (a) the significant loss of site features and facilities, and (b) the consequential changes to the layout of the sites.
- 6 The *NoR* application drawings generally show a setback between the permanent road corridor changes (e.g. new footpaths, berms, cycleways) and the designation boundary. While Z understands that *SGA*'s intention is to 'pull back' the designation boundary at implementation stage so that it is aligned with the finalised permanent works corridor, this submission focuses on all potential effects, understanding that permanent road upgrade works could potentially be undertaken by *SGA* up to the designation boundary if it is confirmed at its current location during and following detailed design.
- 7 The Submitter's opposition is on the basis that:
 - a) The project does not promote the sustainable management of natural and physical resources as required by Part 2 of the *RMA*;
 - b) The project does not enable people and communities to provide for their social, economic, and cultural well-being and for their health and safety;
 - c) The project does not promote the efficient use and development of urban land and development infrastructure;
 - d) The Assessment of Environmental Effects is inadequate and does not address the significant adverse effects of the works in sufficient detail to address matters under section 171(1) of the *RMA*;
 - e) The potential adverse effects on the Submitter have been inadequately identified, considered, or avoided, remedied, or mitigated;

- f) The nature and extent of the benefits of the project have not been demonstrated to outweigh the potentially significant adverse effects of the project;
- g) The adverse effects of the project are not sufficiently mitigated, including managing the effects of the NORs on adjacent activities;
- h) The project will generate significant adverse social and economic impacts, including on the Submitter's business;
- i) The proposed conditions do not adequately address the potential for adverse effects, including significant adverse effects; and
- j) The proposed works are not reasonably necessary for achieving SGA's objectives for the project for which the designation is sought.

C. Characteristics of Service Stations

- 8 Service stations are complex land use activities. Their design and layout require careful consideration of a wide range of matters and adherence to industry standards to ensure that they can operate safely and efficiently. This includes in relation to:
- a) The transfer, storage and handling of hazardous substances. Key hazardous substances components of service stations, including underground and aboveground fuel storage, remote fill points, associated underground fuel lines, and above ground fuel pumps, must be carefully designed and sited in accordance with industry legislation to ensure that potential adverse environmental effects and health and safety risks are appropriately managed. This includes the Hazardous Substances and New Organisms Act 1996 (*HSNO*) and Health and Safety at Work Act 2015 (*HSWA*).
 - b) Stormwater. Stormwater discharges at service stations are managed in accordance with the 'Environmental guidelines for water discharges from petroleum industry sites in New Zealand', Ministry for the Environment, 1998 (*The MfE Guidelines*). 'At risk' areas (i.e. locations where fuel products are being transferred, such as the refuelling forecourt and remote fill points), to be Guideline compliant, must be graded and directed to treatment devices such as an oil-water separator and treated separately (usually) than the 'balance' stormwater.
 - c) Road frontages, including signage. Service station road frontages are critical to their safe and efficient operation. Frontages typically contain landscaping comprising low-level shrubs and grasses, which ensures clear visibility of the forecourt for motorists to safely enter / exit the site whilst also providing an amenity buffer between the site and streetscape. The frontage also contains signage, which provides for early identification of the site to promote safe access, including the prime sign which is required under the Fuel Industry Regulations 2021 to clearly display fuel pricing to road users.
 - d) Location. As service stations are heavily reliant on the road network for their customers and the agglomeration of business in the surrounds, they are predominantly located on key arterial routes. Accordingly, the relationship between a site and the road environment is critically important to the location and operation of a service station.
 - e) Traffic access and safety. Service stations are 24/7 vehicle orientated activities which accommodate customers as well as fuel tanker trucks (*tankers*) which frequently deliver fuel to their sites. Having a layout that ensures safe and convenient access and manoeuvring for tankers and customers entering, exiting and moving within the site is a critical component of service stations.

D. Reasons for Submission

Background

- 9 Z Energy and its consultants have had multiple meetings with SGA to discuss the impacts of the TLC project on Z Takanini:¹
- a) During the initial meetings, Z expressed major concerns regarding the proposal to establish a wetland within the site (as shown in the lodged NoR application drawings).
 - b) SGA subsequently made a formal amendment to the lodged TLC NoR 1 application², which removed the proposed wetland and reduced the extent of the NoR boundary encroachment into the site. However, the amended drawings retain the proposal to establish a 'road corridor' through the site.
 - c) During the more recent meetings, Z expressed major concerns in relation to the proposed road through the Z site for the reasons addressed in this submission. SGA acknowledged these concerns, but as far as Z is aware, has not made any further amendments to the application.

For the avoidance of doubt, this submission relates to the revised NoR 1 layout (per SGA's letter to Auckland Council dated 9/11/23) which now forms part of the NoR application and supersedes the lodged drawing layout.

- 10 Z Energy and its consultants also discussed the TLC project's potential impact on Z Papakura North during their meeting with SGA on 12/12/23.

Z Takanini

- 11 The existing Z Takanini service station (Lot 1 DP 150630) is located on the south-eastern corner of the intersection between Great South Road and Taka Street. Key site features are depicted in **Figure 1** below and include the refuelling forecourt and canopy, convenience store, carwash and water tanks, landscaped frontages, signage, remote fill points, car and trailer parking, underground oil-water separator, underground fuel tanks³, aboveground LPG storage tank. The site also accommodates a Burger King premise, with its restaurant attached to the Z convenience store building and its drive-through facility wrapping around the site's eastern boundary. Access to / from the site is gained via Taka Street (one vehicle crossing) and Great South Road (two vehicle crossings). Tankers access the site via a left turn from Great South Road and exit via a left turn back onto Great South Road.
- 12 NoR 1 of the TLC proposes, per the amended NoR drawing, the following next to Z Takanini:
- a) A 'proposed road corridor' through the north-eastern corner of the site. Z understands that SGA's intention is for this road corridor to provide access to Taka Street from the church at 7 Taka Street and the care centre at 9 Taka Street;

¹ Meetings between Z and SGA to discuss Z Takanini were held on 28/9/23, 25/10/23, 2/11/23, 15/11/23, 12/12/23.

² Letter from SGA to Auckland Council dated 9 November 2023: '*Minor Alteration to Notified Extent of Auckland Transport's Notice of Requirement – Takaanini Level Crossings NoR 1 – Taka Street Project Area (West)*'. https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/takaanini_level_crossing_te_tupu_nq%C4%81tahi_-_minor_alteration_to_notified_nor_1_extent.pdf

³ It is noted that the underground 50,000 litre diesel fuel tank located next to the site's Taka Street access point was recently removed by Z Energy.

- b) A new berm, footpath and cycleway on Taka Street and Great South Road (in part), most of which are within the Z site's existing boundaries;
 - c) A raised or flush median⁴ on Taka Street;
 - d) A proposed wetland on the opposite side of Taka Street along with a stormwater culvert immediately adjacent to the existing Z carwash;
 - e) A proposed bridge crossing in the road reserve outside the site.
- 13 Z understands that the Taka Street changes will tie into the South Frequent Transit (*South FTN*) project under a separate NoR. The impacts of the South FTN project on Z Takanini are addressed in a separate Z submission. However, there are cumulative effects on Z Takanini when considering the South FTN, which together result in further significant adverse effects beyond the impacts of the TLC discussed in this submission. It is therefore recommended that the two submissions are reviewed in tandem.
- 14 To facilitate the works under NoR 1, SGA proposes to designate 755m² of the Z site.⁵ The permanent road corridor upgrades and designation boundary encroach beyond a number of critical features within the Taka Street frontage including the vehicle crossing, the carwash entrance and exit, the carwash rainwater tanks, landscaping, signage, vehicle parking and the Burger King drive thru entrance. The designation boundary also runs along the eastern edge of the forecourt and canopy. The extent to which site features are impacted by the NoR is depicted in **Figure 1** below. It is noted that the designation boundary also extends beyond infrastructure including a transformer on Taka Street (next to the vehicle crossing), a stormwater line connecting to the oil-water interceptor, and a wastewater line that connects to the carwash interceptor.

⁴ The NoR drawing notes that these could either be flush or raised medians.

⁵ Refer to Table 2 of the Letter from SGA to Auckland Council dated 9 November 2023: '*Minor Alteration to Notified Extent of Auckland Transport's Notice of Requirement – Takaanini Level Crossings NoR 1 – Taka Street Project Area (West)*'.

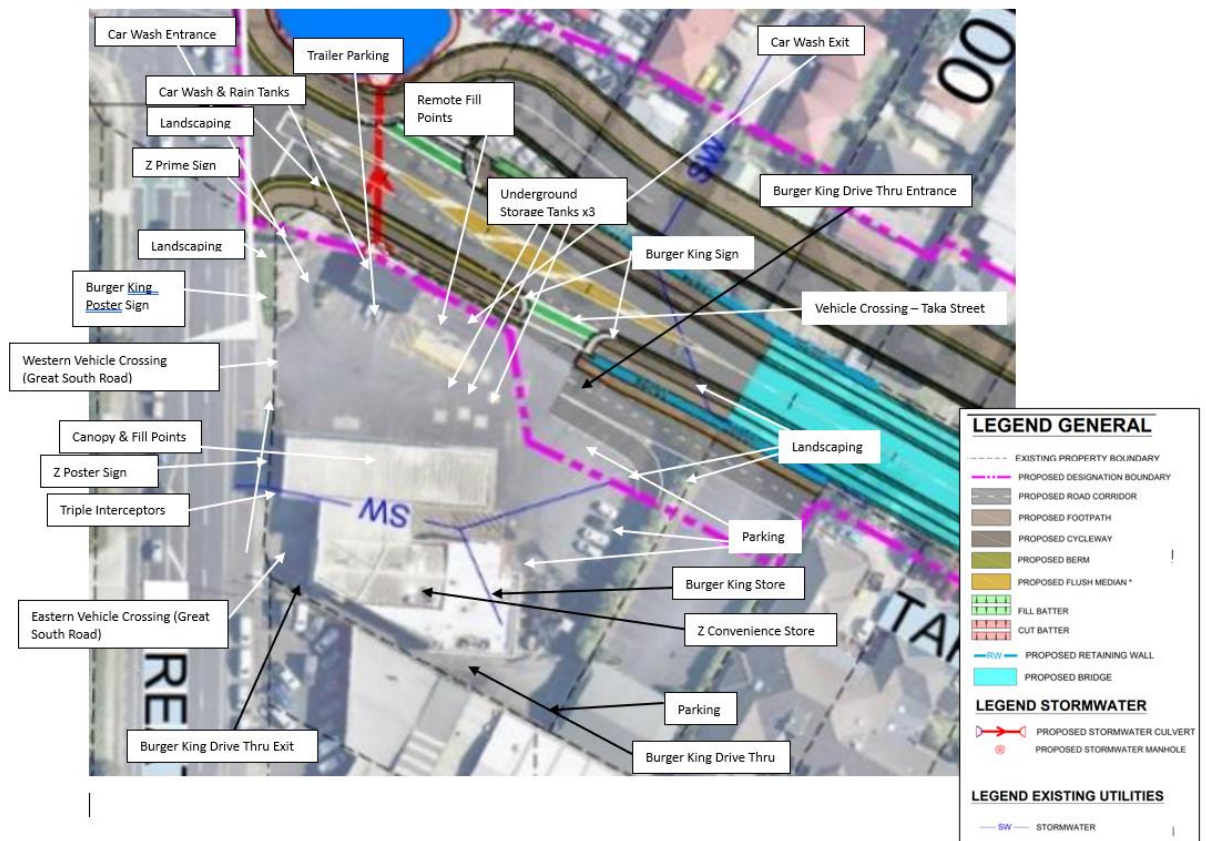


Figure 1: Z Takanini Site Features in Relation to Proposed TLC NoR 1 Concept Design Drawing (Source: Letter from SGA to Auckland Council dated 9 November 2023⁶)

- 15 The proposed changes will result in significant adverse effects on Z Takanini in terms of hazardous substances, traffic, stormwater, landscaping, signage, construction, social and economic considerations, and will almost certainly require a complete redesign of the site. Further, Z has concerns as to whether a redesign of the site will even be viable due to the NoR vastly reducing the site's footprint. While Z appreciates the challenges of assessing effects at a site-specific level for such a large-scale project, there is little to no evidence of the complexity and range of potentially significant adverse effects on the Z Site being adequately assessed.
- 16 Z is opposed to the proposal to establish a road corridor through its site as part of the TLC project. This is likely to result in significant adverse traffic effects, not only for the Z site but also for members of the public using this road. The proposed layout appears to essentially create an informal intersection where a number of vehicles will meet including service station customers, people entering / exiting the retirement village at 9 Taka St, people entering / exiting the church at 7 Taka St, trucks servicing these sites (e.g. rubbish trucks, service vehicles, emergency services accessing the retirement village), fuel tankers at the service station, Burger King drive thru customers, carwash customers, traffic travelling down Taka Street. Further, any queuing on Taka Street will lead to queuing at this informal intersection for exiting vehicles and will create major issues; not only in terms of congestion within the Z site and impeding tankers deliveries (discussed below), but also limited access to / from the church or retirement village in the event of an emergency within these sites. In

⁶ Website source:

https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/takaanini_level_crossing_te_tupu_nq%C4%81tahi_-_minor_alteration_to_notified_nor_1_extent.pdf

addition, the location of the “informal intersection” and the potential effects described above, could create serious safety concerns for users of the dedicated cycleway and footpaths immediately to the east of the informal road / intersection. In the absence of detailed design, Z considers that this appears to be an incredibly dangerous traffic arrangement, and such a layout has the potential to significantly implicate the ability of Z to safely operate its site.

- 17 The proximity of the proposed road corridor to the tanker deliver route is also a significant concern to Z. Refer to the tanker delivery route shown in **Figure 2** below. Tankers deliver up to 30,000 of petroleum product to the site on a regular basis. The site layout has been carefully designed to accommodate fuel tanker deliveries, including the location of fill points to accommodate their tracking curves within the site. When fuel tankers enter the site to deliver fuel, they park to the right of the remote fill points, noting that they must fill from the left side of the tanker. Tankers cannot be impeded by any vehicles and must be protected from any unnecessary movements / manoeuvres of other vehicles. Tankers cannot, under any circumstances, reverse manoeuvre. The proposed NoR layout does not appear to have factored in these safety issues, noting its proximity to the tanker route.



Figure 2: Z Takanini Tanker Delivery Route (Source: Z Energy)

- 18 Z considers that this informal intersection is likely to result in significant safety concerns, not just on tanker movements and traffic, but on the safety of customers, the tanker driver, and people in the immediate vicinity. For example, when vehicles are entering and exiting the access lane to / from the church and retirement village and entering and exiting the service station, at the same time as the tanker is refuelling the site, the intersection becomes incredibly busy and appears to be uncontrolled. Any unnecessary risks to health and safety in this regard are entirely unacceptable.
- 19 Notwithstanding the potentially significant adverse effects that the proposed road corridor arrangement poses, SGA is yet to articulate how this arrangement will actually work in practice. For instance, it is unclear as to how much of the land will be vested road, or whether traffic safety upgrades are proposed (e.g. traffic lights, stop

signs, blind spot mirrors). These additional components could further implicate operations within the Z site. Further, SGA does not appear to have presented any assessment to justify the appropriateness of this arrangement, for instance through trip generation modelling or tracking curves, nor has it demonstrated in its lodged documents any consideration of alternative options to provide access to these two neighbouring sites (7 and 9 Taka Street). Given the potentially significant adverse effects that this proposed road corridor results in, and the absence of an assessment of alternatives in relation to access options for 7 and 9 Taka Street, Z considers the information provided by SGA is inappropriate and does not meet the tests of s171 of the RMA. The taking of private land in this circumstance may be reasonably necessary for providing access to other private sites, but Z does not consider that this is reasonably necessary for the purpose of achieving the objectives of the designation project (per s171(c) RMA).

- 20 Z also has concerns regarding the proximity of the designation boundary and proposed road corridor to areas within the site where the handling, use of transfer of hazardous substances occurs, including the underground fuel storage tanks and remote fill points in the northern portion of the site proximal to Taka Street. HSNO and HSWA legislation dictates that these hazardous area setbacks cannot under any circumstances fall within the road reserve as this is a significant public safety risk.
- 21 The proposed designation boundary crosses into the Z site and encompasses landscaping and existing front yard signage (poster boards, directional signage, prime sign⁷). As noted previously, low lying frontage landscaping is an important element in service stations (and required under the AUP) as it provides a safety buffer, ensures the site is visible for motorists, and contributes to amenity values. Site signage is also located within the landscaped frontage, as there is limited space elsewhere in the site and noting that pricing must be clearly visible to motorists in accordance with industry requirements. If this signage is removed, this will necessitate a comprehensive re-evaluation of the location for necessary signage.
- 22 The proposed designation boundary encroaches into the carwash entrance, partially into the carwash itself and its water tanks, and partially into the carwash exit. These changes resulting from the NoR are highly likely to render the carwash inoperable; for instance the carwash exit cannot simply be reorientated to the south as this would result in collisions with tankers. Given that there is unlikely to be sufficient space within the new site boundaries to relocate the carwash, it will likely need to be closed.
- 23 The NoR is likely to result in the loss of a number of car parking spaces within the Z site, unless they can be accommodated elsewhere in the site (which is unlikely).
- 24 It is also likely that the proposed designation will impact on the site drainage layout (which is subject to the MfE Guidelines for stormwater discharges), including the catchment of 'at risk' discharges from parts of the site such as the remote fill points (noting that these may need to be relocated).
- 25 The NoR will also require the removal or relocation of other infrastructure and structures along the Taka Street frontage associated with the Z site, including wastewater drainage, fencing, and a shed next to the carwash. A transformer is also located next to the Burger King drive thru.
- 26 From the above discussion, it is clear that the NoR will not only significantly adversely affect the Z site, but it may also require relocation of various features, many of which are critical to the site's operation. Some of the features needing relocation would include the remote fill points (to accommodate tanker movements),

⁷ It is difficult to tell whether the prime sign falls within the TLC project NoR. Notwithstanding, the prime sign does fall within the proposed NoR boundary for the South FTN project (as addressed in a separate Z submission).

stormwater infrastructure, landscaping and signage, the carwash, the Burger King drive thru, vehicle parking. Relocation of the underground fuel tanks may also be required. Without a complete redesign of the entire site, it is highly unlikely that there will be sufficient space within the future site boundaries to accommodate these features, noting that SGA proposes to designate 755m² of the Z Takanini site and that the AUP requires a 2m landscaping frontage. A complete redesign of the site may even be difficult given the limited size of the site, and this is without accounting for the implications of the separate South FTN project.

- 27 The application anticipates a total construction duration of 2-3 years for Taka Street and seeks a lapse period of 15 years for NoR 1. These lengthy timeframes, coupled with the 'indicative design' approach of the NoR permanent works corridor in relation to the designation boundary, presents significant uncertainties for Z and its ability to plan for the site, with significant corresponding adverse social and economic effects. It is noted that Z Energy holds a long-term lease for the site.
- 28 Z also has concerns regarding significant adverse construction effects, noting the 2-3 year construction timeframe. As a 24/7 vehicle-oriented activity, maintenance of convenient and safe access for passing traffic to the service station is critical for the duration of the works. The proposed conditions rely heavily on a range of further information, including and via the submission of management plans, to address effects. While some management plan conditions appear effective, not all appear to clearly require meaningful consultation with affected parties or establish outcomes to be achieved. As such, the extent to which the works will affect and/or compromise the Submitter's business cannot be ascertained and the ability of the Submitter to influence the detail of how the works are managed to minimise effects as far as practicable are extremely limited.
- 29 *Z Papakura North*
- 30 The existing Z Papakura North service station is located on 1-3 Tironui Road (Lot 1-2 Deposited Plan 44494) and is located to the eastern corner of the roundabout, between Tironui Road and Great South Road. Key site features are depicted in **Figure 3** below. The site has two vehicle crossings on Great South Road.
- 31 NoR 2 of the TLC proposes to implement a designation boundary along the Z Papakura North site boundary. It also proposes permanent road upgrades including a new footpath in the road reserve. Refer to **Figure 4** below.



Figure 3: Z Papakura North in Relation to Proposed TLC NoR 2 Designation (Source: Auckland Council GeoMaps)

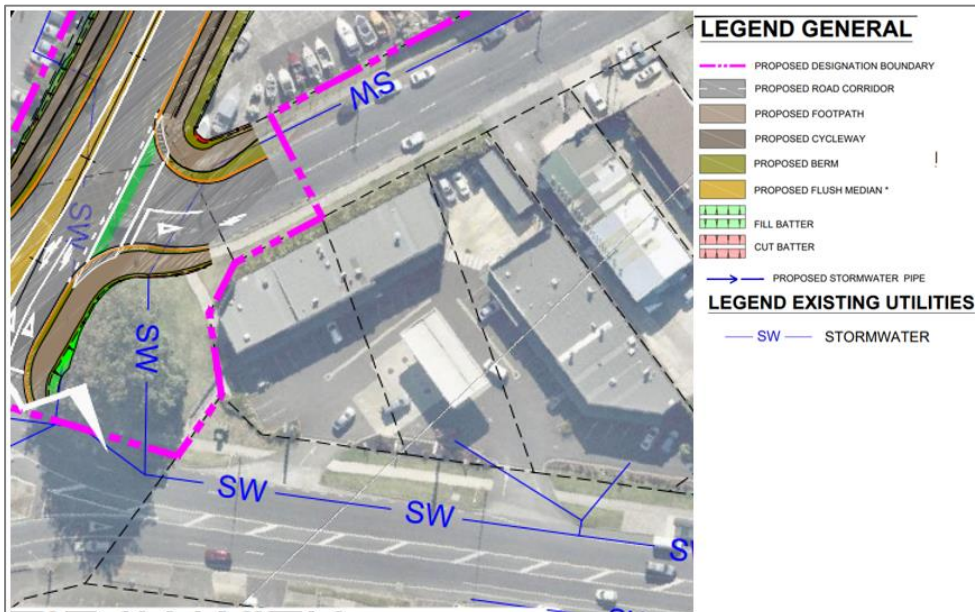


Figure 4: TLC NoR 2 Concept Design Next to Z Papakura North (Source: TLC NoR 2 General Arrangement Plan)

- 32 The designation boundary does not extend into the Z site boundaries and the concept plan shows the permanent works corridor to be reasonably setback from the Z site. Furthermore, the proposed designation area and works corridor does not extend down Great South Road in front of the site. As such, Z does not consider that the NoR in its current format is likely to result in any permanent operational impacts on Z Papakura North. However, this is subject to detailed design and ensuring that appropriate conditions are in place.
- 33 Z seeks to ensure:
- a) That the NoR does not propose permanent road changes that adversely affect traffic movements to / from and within the service station, including in relation to the Great South Road access points.
 - b) That the site's existing landscaping, signage, hazardous substance storage / transfer / use layout, and infrastructure (including stormwater) will not be affected by the NoR.
 - c) That the designation boundary does not encroach into the site, and that the designation boundary is not amended to extend along the site's Great South Road frontage.
- 34 Z does have a particular interest in ensuring that potential adverse construction effects on Z Papakura North are appropriately managed, noting that (a) a 2.5 to 3 year construction timeframe is proposed for the Walters Road section of road, and (b) the concept level design plans and designated area could change to suit SGA's future plans. As a 24/7 vehicle-oriented activity, maintenance of convenient and safe access for passing traffic to the service station is critical for the duration of the works. In line with the above comments relating to Z Takanini, Z considers that changes to the conditions are necessary to ensure that they achieve appropriate outcomes for stakeholders.

Submission on Conditions

- 35 Z Energy has reviewed the proposed conditions contained in Attachment C of Form 18 – NoR 1 - TLC. Failure to factor in the site-specific issues identified in this submission early on at the detailed design process has potentially significant adverse effects and may render the sites inoperable. Z Energy is therefore of the opinion that the conditions should provide more specific recognition of matters that need to be addressed at detailed design stage, rather than leaving this ambiguous and open-ended. To address this outcome, and broadly comment on the suite of conditions, Z Energy submits that:
- a) Not all management plan conditions require consultation with affected landowners and occupiers.
 - b) Some conditions refer only to landowners, where it may be the occupier who is just as adversely affected.
 - c) The extent to which feedback from consultation with affected landowners, leaseholders or occupiers is taken into account is unclear and should be acknowledged as a priority through conditions. i.e.: there is no clear requirement that feedback be considered and implemented to the extent practicable by the requiring authority.
 - d) The conditions do not go far enough to demonstrate that effects on the Z Energy sites will be avoided, remedied or mitigated, such that effects may be significant.

Relief Sought

- 36 The Submitter seeks that the NoR 1 in its current form is declined. However, in the event that NoR 1 is not declined, relief sought is set out below.

- 37 The Submitter is neutral regarding NoR 2.
- 38 The Submitter seeks that the designations are amended to avoid, remedy, or mitigate all matters of concern raised in this submission, including but not limited to the following:
- a) Minimising the encroachment of the designation boundary into the Z sites and ensuring that any temporary or permanent effects do not impact their ability to safely operate, including with regard to access/egress, all-vehicle manoeuvring, parking, stormwater treatment and drainage, the storage and use of hazardous substances (including tanks, remote fills, vents, dispensers), the forecourt canopy, signage, and landscaping.
 - b) Retaining safe and convenient entry and exit crossings, including the ability to right turn in and out of the site.
 - c) Retaining safe and convenient tanker access to and from the site and the remote fill points.
 - d) Retaining safe and convenient on-site manoeuvring.
 - e) Ensuring that any resultant changes will not result in Z Energy being unable to operate its sites lawfully.
 - f) Ensuring that works are appropriately managed through conditions to avoid, remedy, or mitigate adverse effects on the Submitter, per the below.
 - g) Amending the lapse period to 5 years, consistent with s184 of the RMA.
 - h) That the following condition changes are implemented (at a minimum):
 - i. **Proposed Condition 4 (Designation Review)** – The use of the phrase “or as soon as otherwise practicable” is unclear in the context of this condition and leaves the requirement to roll back too open. Amend the condition so that it states: *The Requiring Authority shall as soon as practicable, and otherwise within 12 months of Completion of Construction for each Stage of the Project...*
 - ii. **Proposed Condition 8 (Management Plans)** – As drafted, the condition does not require the requiring authority to incorporate feedback from stakeholders. Rather, it simply directs the requiring authority to summarise feedback and state whether the feedback has been incorporated or not. Amend the condition to require that, the summary of comments received (required by (8(a)(iv)) demonstrates how, as far as practicable, the feedback from stakeholders has been incorporated.
 - iii. **Proposed Condition 9 (SCEMP)** – The condition should be amended to include the requirement to prepare a schedule of sites affected and site-specific matters identified in the schedule to be addressed through consultation (refer to the Joint Witness Statement (Planning – Conditions) dated 20 September 2023 submitted to the Hearing Panel for the NW NORs). It is assumed that Z Energy will be a stakeholder to be engaged and listed under 9(b)(i)B.
 - iv. **Proposed Condition 12(d) (ULDMP)** – Z Energy supports this condition.
 - v. **Proposed Condition 14 (Existing property access)** – This condition should also refer to occupiers and leaseholders. It is not always just a landowner who may be affected.
 - vi. **Conditions 15 and 18 (CEMP and CTMP)** – These two conditions do not require consultation or engagement with any party in their preparation. Further, it is unclear from the SCEMP condition if these Management Plans

are to be prepared in consultation with affected parties. Z Energy seeks that AT either amends these conditions to require affected parties to be engaged with to participate in the drafting of these management plan OR amends the SCEMP condition so that this requirement is clear.

39 Z Energy would be pleased to meet with SGA to discuss this submission.

Signed on behalf of Z Energy Limited

Regards,

SLR Consulting New Zealand

A handwritten signature in black ink, appearing to read 'Phil Brown', with a stylized, cursive script.

Phil Brown

Senior Planner

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**SUBMISSION ON AUCKLAND TRANSPORT'S NOTICES OF REQUIREMENT FOR THE
TAKAANINI LEVEL CROSSINGS PROJECT BY KĀINGA ORA - HOMES AND
COMMUNITIES**

TO: Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1010

Submission via email: unitaryplan@aucklandcouncil.govt.nz

KĀINGA ORA - HOMES AND COMMUNITIES (“Kāinga Ora”) at the address for service set out below makes the following submission on the Notices of Requirement (“**NoRs**”) for the Takaanini Level Crossings Project (“**the Project**”) (Requiring Authority – Auckland Transport).

Background

1. Kāinga Ora - Homes and Communities (“Kāinga Ora”) is a Crown Entity and is required to give effect to Government policies. Kāinga Ora has a statutory objective that requires it to contribute to sustainable, inclusive, and thriving communities that:
 - (a) provide people with good quality, affordable housing choices that meet diverse needs;
 - (b) support good access to jobs, amenities and services; and
 - (c) otherwise sustain or enhance the overall economic, social, environmental and cultural well-being of current and future generations.
2. The Project sits within the Papakura Local Board area in Auckland. Within this area, Kāinga Ora manages 1702 rental properties¹ and there are 501 applicants on the public housing waitlist² as at September 2023. Kāinga Ora is therefore interested in this Project

¹ Kāinga Ora – Housing Statistics – Managed Stock – September 2023.
https://kaingaora.govt.nz/en_NZ/publications/oia-and-proactive-releases/housing-statistics/

² Ministry of Social Development – Housing Register – September 2023.
<https://www.msd.govt.nz/about-msd-and-our-work/publications-resources/statistics/housing/housing-register.html>

and its impacts for communities, noting that it has a number of properties and tenants which will be directly affected:

- (a) 6 Kāinga Ora properties are subject to full acquisition;
 - (b) 20 Kāinga Ora properties are subject to partial acquisition, with their front yard and/or access proposed to be partially removed; and
 - (c) 32 Kāinga Ora properties are considered '*Protected Premises and Facilities*' (**PPFs**), with 5 of these Kāinga Ora properties expected to receive a traffic noise level of over 55 dB $L_{Aeq(24h)}$.
3. However, as per the statutory objectives above, Kāinga Ora also has interests beyond its role as a public housing provider. This includes a role as a landowner and developer of residential housing and as an enabler of quality urban developments through increasing the availability of build-ready land across the Auckland region. Kāinga Ora is interested in all issues that may affect the supply and affordability of housing. This includes the provision of services and infrastructure.

Wider Context

4. In addition to the above, Kāinga Ora will play a greater role in urban development in New Zealand. The legislative functions of Kāinga Ora, as outlined in the Kāinga Ora Act, illustrate this broad mandate and outline two key roles of Kāinga Ora in that regard:
- a) initiating, facilitating and/or undertaking development not just for itself, but in partnership or on behalf of others; and
 - b) providing a leadership or coordination role more generally.
5. Notably, the statutory functions of Kāinga Ora in relation to urban development extend beyond the development of housing (which includes public housing, affordable housing, homes for first time buyers, and market housing) to the development and renewal of urban environments, as well as the development of related commercial, industrial, community, or other amenities, infrastructure, facilities, services or works.

The Government Policy Statement on Housing and Urban Development 2021 (“GPS-HUD”)

6. The GPS-HUD sets a direction for housing and urban development in New Zealand. Its overarching vision is that everyone in New Zealand lives in a home and a community that meets their needs and aspirations. The four main things it sets out to achieve are:
- (a) **Thriving and resilient communities** – The places where people live are accessible and connected to employment, education, social and cultural opportunities. They grow and change well within environmental limits, support our culture and heritage and are resilient.
 - (b) **Wellbeing through housing** – Everyone lives in a home, whether it’s rented or owned, that is warm, dry, safe, stable and affordable, with access to the support they need to live healthy, successful lives.
 - (c) **Māori housing through partnership** – Māori and the Crown work together in partnership so all whānau have safe, healthy, affordable and stable homes. Māori housing solutions are led by Māori and are delivered locally. Māori can use their own assets and whenua Māori to invest in and support housing solutions.
 - (d) **An adaptive and responsive system** – Land-use change, infrastructure and housing supply is responsive to demand, well planned and well regulated.

The National Policy Statement on Urban Development (“NPS-UD”) and the Resource Management (Enabling Housing Supply and Other Matters) Amendment Act 2021 (the “RMAA 2021”)

7. The NPS-UD aims to ensure councils better plan for growth and remove overly restrictive barriers to development to allow growth in locations that have good access to services, public transport networks and infrastructure. The NPS-UD’s intensification policies require councils to enable greater heights and densities in areas that are well-suited to growth, such as in and around urban centres and (existing and proposed) rapid transit stops. The RMAA 2021 introduced the Intensification Streamlined Planning Process for Tier 1 councils to implement the intensification policies and additionally required these councils to introduce the Medium Density Residential Standards.
8. Together, the NPS-UD and RMAA 2021 are intended to ensure New Zealand’s towns and cities are well-functioning urban environments that support housing supply and affordability, accessibility to jobs and services, and emissions reduction.

Scope of Submission

9. The submission relates to the two NoRs for the Takaanini Level Crossings Project in their entirety.

The Submission is:

10. Kāinga Ora **supports** the NoRs for the Project **in part**, subject to the relief Kāinga Ora seeks being granted and matters raised in its submission being addressed.
11. The NoRs seek to replace several at-grade level crossings of the North Island Main Trunk (NIMT) railway with bridges and undertake associated works. The purpose of the Project is to respond to network issues experienced by the corridors such as congestion, severance, safety risk from at-grade level crossings and inefficiency from the operation of barrier arms. The NoRs are broken down into the following sections with the key changes outlined:
 - (a) **NoR 1 – Spartan Road:** Closing the existing railway level crossing and replacing it with a walking and cycling bridge across the railway.
 - (b) **NoR 1 – Manuia Road:** Constructing a new multi-modal bridge across the railway.
 - (c) **NoR 1 – Manuroa Road:** Closing the existing railway level crossing and replacing it with a walking and cycling bridge across the railway.
 - (d) **NoR 1 – Taka Street:** Closing the existing railway level crossing and replacing it with a multi-modal bridge across the railway.
 - (e) **NoR 2 – Walters Road:** Closing the existing railway level crossing and replacing it with a multi-modal bridge across the railway.
12. In particular, but without limiting the generality of the above:
 - a) Kāinga Ora supports the outcomes to be derived from the Project particularly as they overall relate to the delivery of transportation infrastructure that improves the safety measures for walking and cycling as well as the reduction of traffic in some residential environments. Nevertheless, Kāinga Ora supports the proposed NoRs for the Project in part.
 - b) Kāinga Ora generally supports the proposed conditions of the designation and the use of the mechanisms outlined to avoid, remedy, or mitigate potential adverse

effects and to regularly communicate with the community, including but not limited to: the submission of an Outline Plan of Works, the conditions in relation to designation review and existing property access, Mana Whenua Kaitiaki Forum, Stakeholder Communication and Engagement Management Plan, Land Use Integration Process, Construction Environmental Management Plan, Cultural Monitoring Plan, Construction Traffic Management Plan, Construction Noise and Vibration Management Plan, Construction Noise and Vibration Management Schedule, Tree Management Plan, and the Network Utility Management Plan.

13. Notwithstanding the general support of the Project, Kāinga Ora seeks relief in the following aspects of the proposal:
- (a) Providing safer, more direct and more attractive connections for walking and cycling
 - (i) Consideration of alternative options including underpass design;
 - (ii) Provision of direct accessible routes including provision of an accessible elevator and steps where overbridges are provided; and
 - (iii) Connectivity and safety in and around the Takaanini rail station.
 - (b) Reducing the lapse period from 15 years to 10 years
 - (c) Operational noise and vibration
 - (i) Best Practical Option (BPO) for PPFs identification and assessment;
 - (ii) Low noise road surface to be implemented on all roads within the designation; and
 - (iii) Building modification mitigation for all PPFs to 40 dB $L_{Aeq(24h)}$ inside buildings.

In some cases, amendments to the designation conditions and/or the design of the Project are sought to address the concerns expressed in this submission. The concerns of Kāinga Ora are further discussed below.

Accessibility and safety for walking and cycling

Consideration of Alternative Options including underpasses design

14. Kāinga Ora acknowledges that the Project seeks to provide safer east-west active mode facilities for walking and cycling. There will also be a net gain of one walking and cycling crossing over the railway. However, Kāinga Ora requests consideration of more direct and accessible walking and cycling connections at Spartan Road and Manuroa Road. Justification is also sought as to why walking and cycling bridges have been proposed when active mode underpasses would confer the following advantages:
- (a) When designed properly, underpasses offer a better level of service for walking and cycling than overbridges. They are shorter, more direct, more intuitive and safer.
 - (b) Bridges that cross railway lines with overhead electrification need a minimum clearance of 5.5m above track level³. For underpasses designed for cycling and walking, a minimum height clearance of only 2.4m⁴ is required.
 - (c) A height difference of 5.5m or more will require long ramps. E.g. The designation for Manuroa Road shows a narrow arrangement for the ramps with repeated 180° turns and a total length of approximately 250m (assuming a 1:20 gradient). Kāinga Ora considers that a more direct crossing in the form of an underpass of approximately 20m would result in a better connectivity outcome.
 - (d) By minimising the height difference of a grade-separated foot and cycle crossing, the amount of effort required for a person to use the crossing is reduced. This is consistent with a universal design approach, which seeks to design infrastructure that can be used easily by as many people as possible.
 - (e) By providing direct in-line ramps, there is no deviation from the desire line, and the land requirements are also reduced.

³ Figure 3-2 of the Requiring Authority's Assessment of Environmental Effects.
https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/03_tlc_nor1_aee.pdf

⁴ Waka Kotahi – Grade Separation.

<https://www.nzta.govt.nz/walking-cycling-and-public-transport/walking/walking-standards-and-guidelines/pedestrian-network-guidance/design/crossings/grade-separation/>

Provision of accessible routes

15. The project proposes long ramp structures for walking and cycling bridges to enable accessibility for people with limited mobility; however, this will be harder to use than the existing level crossings, because of the gradients and increased distances.
16. If bridges are to be built, Kāinga Ora requests that elevators and staircases are provided to enable more direct routes over the railway. This is requested for Manuroa Road as a priority given its proximity to the Takaanini rail station.

Connectivity and Safety in and around the Takaanini Rail Station

17. It is a basic tenet of public transport planning that the walking and cycling level of service is prioritised at rail stations and bus stops. Kāinga Ora requests that Auckland Transport works with KiwiRail on the following aspects to improve pedestrian and cyclist connectivity in and around the Takaanini rail station.
 - (a) Manuroa Road – Kāinga Ora notes that the safety and connectivity from the proposed walking and cycling bridge to the Takaanini rail station platform in its current form is poor. Kāinga Ora is concerned that there is no segregated path to the platform on the eastern nor western side of the railway line. On the eastern side of the railway, people would need to go through the parking lot (approx. 300m in length) where vehicles would be manoeuvring, exposing active mode users to safety hazards and being at-risk of accidents. It is requested that a more direct, well-designed and safe route is provided.
 - (b) Taka Street – Kāinga Ora notes that active mode users would be able to reach the Takaanini rail station from the western side of the railway line via the existing walking and cycling path. However, it is requested that this path is widened for a more user-friendly shared path. Similar to Manuroa Road, it is noted that there is no segregated path to the platform on the eastern side of the railway line. It is requested that a more direct, well-designed and safe route is provided.

Changes to ULDMP conditions

18. In addition to the outcomes described in Paragraphs 14 to 16 above, Kāinga Ora also seeks changes to the Condition 12 Urban and Landscape Design Management Plan (f) as follows (Requiring Authority Notified Conditions in Black; Kāinga Ora Recommended Changes in **Red**, ~~strikethrough~~ and underline):

To achieve the objective, the ULDMP(s) shall provide details of how the project:

...

(ii) Provides ~~appropriate direct, efficient and high-quality~~ walking and cycling ~~connectivity connections~~ to, and interfaces with, existing or proposed adjacent land uses, public transport infrastructure and walking and cycling connections;

(iii) ~~Promotes~~ Provides inclusive access (~~where appropriate~~); and

...

Reducing the lapse period from 15 years to 10 years

19. Kāinga Ora recognises that the proposed lapse period for the NoRs need to balance flexibility, and acquisitions and construction time required by the Requiring Authority with the uncertainty and planning blight for the adjacent landowners and occupants. Kāinga Ora considers that the lapse date should be amended from 15 years to 10 years, noting that this would still be a longer period than the statutory 5 years lapse period.
20. Kāinga Ora seeks a lapse period of 10 years to encourage the project to be actioned with more urgency, noting that this is an important project for the surrounding communities in terms of better east-west connectivity and safety, particularly walking and cycling. The sooner the project is implemented, the sooner there will also be other positive impacts to communities such as alleviating congestion, reducing inefficiency from the operation of barrier arms and supporting both planned increases in train frequency and the expected urban growth in the Takaanini area.
21. For the reasons in Paragraphs 19-20 above, Kāinga Ora requests that Condition 5 Lapse be amended as follows (Requiring Authority Notified Conditions in Black; Kāinga Ora Recommended Changes in Red, ~~strikethrough~~ and underline):
 - (a) *In accordance with section 184(1)(c) of the RMA, this designation shall lapse if not given effect to within ~~15~~ 10 years from the date on which it is included in the AUP.*

Operational Noise and Vibration

Assessment of adverse effects

22. It is acknowledged that transport infrastructure support the enablement of a well-functioning urban environment, and that a degree of noise and vibration emissions are

expected. However, it must be recognised that significant noise emissions may have potential adverse effects on surrounding residential environments and the health and well-being of people living nearby. Therefore, Operational Noise and Vibration requires careful consideration to ensure that the effects are appropriately avoided, remediated or mitigated in accordance with Section 16 and 17 of the RMA.

23. Kāinga Ora is concerned that the Project does not fully assess the health effects associated with traffic noise of the Project. While the Project assesses the traffic noise effects in the context of NZS6806, Kāinga Ora is concerned that the standard does not fully capture the potential health effects of the proposal on its land and tenants.

Mitigation at-source to within 55 dB $L_{Aeq(24h)}$

24. Kāinga Ora notes that Auckland Transport identified as part of Private Plan Change 51 (PPC51) that activities subjected to an operational noise level of 55 dB $L_{Aeq(24h)}$ require mitigation to address potential adverse health effects. Kāinga Ora requests that operational noise levels for this project to not exceed 55 dB $L_{Aeq(24h)}$ beyond the boundaries of the designation or, where exceeded at a sensitive receiver, mitigation is provided.
25. This operational noise level was the baseline utilised within Auckland Transport's Acoustic Expert Evidence by Claire Drewery for **PPC51**⁵, who considered that there are adverse health effects in relation to road traffic, referencing both the World Health Organisation (WHO) Environmental Noise Guidelines for the European Region (2018) and enHealth's The Health Effects of Environmental Noise (2018). The WHO's guidelines are (in part) copied below:

WHO guidelines for Community Noise 1999 states the following in relation to dwellings

[page xiii]

... The effects of noise in dwellings, typically, are sleep disturbance, annoyance and speech interference. For bedrooms the critical effect is sleep disturbance. Indoor guideline values for bedrooms are 30 dB L_{Aeq} for continuous noise and 45 dB L_{Amax} for single sound events. Lower noise levels may be disturbing depending on the nature of the noise source. At night-time, outside sound

⁵ Paragraphs 6.7 and 6.9 of Statement of Evidence of Claire Drewery on behalf of Auckland Transport – Acoustic, dated 24 August 2021 for Private Plan Change 51 – Drury 2 Precinct.

levels about 1 metre from facades of living spaces should not exceed 45 dB LAeq, so that people may sleep with bedroom windows open. This value was obtained by assuming that the noise reduction from outside to inside with the window open is 15 dB. To enable casual conversation indoors during daytime, the sound level of interfering noise should not exceed 35 dB LAeq. To protect the majority of people from being seriously annoyed during the daytime, the outdoor sound level from steady, continuous noise should not exceed 55 dB LAeq on balconies, terraces and in outdoor living areas. To protect the majority of people from being moderately annoyed during the daytime, the outdoor sound level should not exceed 50 dB LAeq. Where it is practical and feasible, the lower outdoor sound level should be considered the maximum desirable sound level for new development.

WHO Environmental Noise Guidelines for the European Region (2018) states the following:

[page xiii]

Environmental noise is an important public health issue, featuring among the top environmental risks to health. It has negative impacts on human health and well-being and is a growing concern among both the general public and policy-makers in Europe.

[page xvi]

For average noise exposure, the Guideline Development Group (GDG) strongly recommends reducing noise levels produced by road traffic below 53 decibels (dB) Lden, as road traffic noise above this level is associated with adverse health effects.

Based on the above, Ms Drewery adopted 55 dB L_{Aeq(24h)} as the noise level above which potential health effects could occur and made subsequent recommendations for PPC51. Kāinga Ora considers that it is appropriate that a similar baseline is utilised for the Project.

26. Kāinga Ora considers that it is appropriate that the Requiring Authority is required to ensure that measures are undertaken to reduce noise and vibration at source, and where these can not be mitigated at source undertakes receiver mitigation to ensure infrastructure is not have adverse effects on the health and well being of existing communities.

Low noise road surface

27. Kāinga Ora requests that Condition 25 (Low Noise Road Surface) is amended to require the use of low noise and vibration road surfaces, for *all* road surfaces within this designation, unless further information confirms that this is not warranted from a health and safety perspective.

Mitigation inside buildings

28. Kāinga Ora submits that there may be circumstances whereby existing dwellings that experience increased exposure to noise and vibration require further mitigation in the form of building modifications, including but not limited to wall insulation, double glazing, forced ventilation and temperature controls.
29. In addition to the road surface, barrier and Category C mitigation proposed by the Project, Kāinga Ora considers that further mitigation is necessary to some non-Category C PPFs in order to address potential adverse health effects. It is sought that this applies where habitable spaces are likely to receive in excess of 40 dB $L_{Aeq(24h)}$ (equivalent to an external noise level of 55 dB $L_{Aeq(24h)}$) from operational noise with windows closed in the Design Year.
30. Kāinga Ora seeks that where mitigation is applicable for PPFs, that the offer for mitigation shall be extended, as per its recommended conditions in Appendix A. This is in the interests of natural justice in terms of giving sufficient time to consider and respond to the offer and mitigating adverse health effects for future occupiers.
31. Furthermore, the Requiring Authority's proposed conditions, which freeze the receiving environment to what exists currently, ignores the future receiving environment. Kāinga Ora considers that the Requiring Authority's proposed conditions should be revised to require a BPO assessment prior to construction in the future that recognises the receiving environment as it exists at the time.

Relief Sought

32. Kāinga Ora seeks the following further actions regarding the NoRs:
 - (a) That the Requiring Authority considers other more direct and accessible walking and cycling crossing options for Spartan Road and Manuroa Road that maintain the existing directness and ease of use of the existing level crossings. Kāinga Ora

preference is for well designed safe and direct walking and cycling underpasses. In the event that overpasses are continued to be preferred by the Requiring Authority Kāinga Ora requests that accessible elevators and steps are added to the design to improve the connections.

- (b) That the Requiring Authority provides justification as to why ramped walking and cycling bridges have been proposed in preference to options which would deliver a higher level of service for people cycling or on foot.
- (c) That the Requiring Authority investigates with KiwiRail: more direct, well-designed and safe walking and cycling opportunities in and around the Takaanini rail station that maximise pedestrian and cycle level of service particularly, at Manuroa Road and Taka Street.

33. Kāinga Ora seeks the following decisions from Auckland Council regarding the NoRs:

- (a) That Condition 12 (Urban and Landscape Design Management Plan) be amended as per Paragraph 17 above to provide safer, more direct and more attractive connections for walking and cycling.
- (b) That Condition 5 (Lapse) be amended from 15 to 10 years as per Paragraph 21 above to provide greater certainty and for the project to benefit communities sooner.
- (c) That the operational noise levels for this project shall not exceed 55 dB $L_{Aeq(24h)}$ beyond the boundaries of the designation or, where exceeded at a sensitive receiver, mitigation is provided.
- (d) That Conditions 28-37 (Operational Conditions) be amended as per Appendix A to address:
 - (i) Best Practical Option (BPO) for PPFs identification and assessment;
 - (ii) Low noise road surface to be implemented on all roads within the designation; and
 - (iii) Building modification mitigation for all PPFs to 40dB $L_{Aeq(24h)}$ inside buildings.

- (e) Such further or other relief, or other consequential or other amendments, as are considered appropriate and necessary to address the concerns set out herein.
 - (f) Any other alternative or consequential relief to give effect to this submission.
34. In the absence of the relief sought, Kāinga Ora considers that the NoRs:
- (a) is contrary to the sustainable management of natural and physical resources and is otherwise inconsistent with Part 2 of the Act;
 - (b) will impact on the ability of people and communities to provide for their social, economic and cultural wellbeing.
35. Kāinga Ora does not consider it can gain an advantage in trade competition through this submission.
36. Kāinga Ora wishes to be heard in support of its submission.
37. If others make a similar submission, Kāinga Ora would be willing to consider presenting a joint case with them at hearing.

Dated this 14th day of December 2023



Brendon Liggett
Manager – Development Planning
Kāinga Ora - Homes and Communities

ADDRESS FOR SERVICE:

Kāinga Ora – Homes and Communities
PO Box 74598
Greenlane, Auckland
Attention: Jennifer Chivers
Email: developmentplanning@kaingaora.govt.nz

Appendix 1

Requiring Authority Notified Conditions in Black
Kāinga Ora Recommended Changes in ~~Red strikethrough~~ and underline.

NoR No.	No.	Condition
Operational Conditions		
NoRs 1 and 2	25.	<p>Low Noise Road Surface</p> <p>(a) Asphaltic concrete surfacing (or equivalent low noise road surface) shall be implemented within 12 months of Completion of Construction of the project.</p> <p>(b) Any future resurfacing works of the Project shall be undertaken in accordance with <u>the BPO as determined in accordance with these conditions and</u> Auckland Transport Reseal Guidelines, Asset Management and Systems 2013 and asphaltic concrete surfacing (or equivalent low noise road surface) shall be implemented where:</p> <p>(i) The volume of traffic exceeds 10,000 vehicles per day; or</p> <p><u>(i) The road is located within the designation; or</u></p> <p>(ii) The road is subject to high wear and tear (such as cul de sac heads, roundabouts and main road intersections); or</p> <p>(iii) It is in an industrial or commercial area where there is a high concentration of truck traffic; or</p> <p>(iv) It is subject to high usage by pedestrians, such as town centres, hospitals, shopping centres and schools.</p> <p>(c) Prior to commencing any future resurfacing works, the Requiring Authority shall advise the Manager if any of the triggers in Condition 25(c)(i) – (iv) are not met by the road or a section of it and therefore where the application of asphaltic concrete surfacing (or equivalent low noise road surface) is no longer required on the road or a section of it. Such advice shall also indicate when any resealing is to occur.</p>

NoRs 1 and 2	<p>Traffic Noise</p> <ul style="list-style-type: none"> (a) For the purposes of Conditions 26 to 37: (b) Building-Modification Mitigation – has the same meaning as in NZS 6806; (c) Design year has the same meaning as in NZS 6806; (d) Detailed Mitigation Options – means the fully detailed design of the Selected Mitigation Options, with all practical issues addressed; (e) Habitable Space – has the same meaning as in NZS 6806; (f) Identified Noise Criteria Category – means the Noise Criteria Category for a PPF identified in <i>Schedule 2: Identified PPFs Noise Criteria Categories</i>; (g) Mitigation – has the same meaning as in NZS 6806:2010 Acoustics – Road-traffic noise – New and altered roads; (h) Noise Criteria Categories – means the groups of preference for sound levels established in accordance with NZS 6806 when determining the Best Practicable Option for noise mitigation (i.e. Categories A, B and C); (i) NZS 6806 – means New Zealand Standard NZS 6806:2010 Acoustics – Road-traffic noise – New and altered roads; (j) Protected Premises and Facilities (PPFs) – means only the premises and facilities identified in green, orange or red in <i>Schedule 2: PPFs Noise Criteria Categories</i>; (k) Selected Mitigation Options – means the preferred mitigation option resulting from a Best Practicable Option assessment undertaken in accordance with NZS 6806 taking into account any low noise road surface to be implemented in accordance with Condition 25; and (l) Structural Mitigation – has the same meaning as in NZS 6806.
---------------------	--

NoR No.	No.	Condition
NoRs 1 and 2	26.	<p>The Noise Criteria Categories identified in <i>Schedule 2: PPFs Noise Criteria Categories</i> at each of the PPFs shall be achieved where practicable and subject to Conditions 26 to 37 (all traffic noise conditions).</p> <p>The Noise Criteria Categories do not need to be complied with at a PPF where:</p> <p>(a) The PPF no longer exists; or</p> <p>(b) Agreement of the landowner has been obtained confirming that the Noise Criteria Category does not need to be met.</p> <p>Achievement of the Noise Criteria Categories for PPFs shall be by reference to a traffic forecast for a high growth scenario in a design year at least 10 years after the programmed opening of the Project.</p>
NoRs 1 and 2	27.	<p>As part of the detailed design of the Project, a Suitably Qualified Person shall determine the Selected Mitigation Options for the PPFs identified on <i>Schedule 2 PPFs Noise Criteria Categories</i>.</p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p> <p>For the avoidance of doubt, the low noise road surface implemented in accordance with Condition 25 may be (or be part of) the Selected Mitigation Options(s).</p>
NoRs 1 and 2	28.	<p>Prior to construction of the Project, a Suitably Qualified Person shall develop the Detailed Mitigation Options for the PPFs identified in <i>Schedule 2 PPFs Noise Criteria Categories</i>, taking into account the Selected Mitigation Options.</p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p>
NoRs 1 and 2	29.	<p>If the Detailed Mitigation Options would result in the Identified Noise Criteria Category changing to a less stringent Category, e.g. from Category A to B or Category B to C, at any relevant PPF, a Suitably Qualified Person shall provide confirmation to the Manager that the Detailed Mitigation Option would be consistent with adopting the Best Practicable Option <u>in accordance with NZS 6806 prior to implementation.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p>
NoRs 1 and 2	30.	<p>The Detailed Mitigation Options shall be implemented prior to completion of construction of the Project, with the exception of any low-noise road surfaces, which shall be implemented within twelve months of completion of construction.</p>
NoRs 1 and 2	31.	<p>Prior to the Start of Construction, a Suitably Qualified Person shall identify those PPFs which, following implementation of all the Detailed Mitigation Options, will not be Noise Criteria Categories A or B and where <u>would still require</u> Building-Modification Mitigation might be required to achieve 40 dB L_{Aeq(24h)} inside Habitable Spaces (Category C Buildings).</p> <p><u>For those PPFs, following the process set out in Conditions 32 to 37, it shall be determined which Building Modification Mitigation is required to achieve 40 dB</u></p>

		<p><u>L_{Aeq}(24h) inside habitable spaces.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs. Any future residential or other PPFs provided for in the AUP:OP zonings shall have a BPO assessment undertaken, including mitigation measures within the Project design where practicable.</u></p>
NoRs 1 and 2	32.	<p>Prior to the Start of Construction in the vicinity of each Category C Building <u>requiring building-modification mitigation</u>, the Requiring Authority shall write to the owner of the Category C Building requesting entry to assess the noise reduction performance of the existing building envelope. If the building owner agrees to entry within six three months of the date of the Requiring Authority's letter, the Requiring Authority shall instruct a Suitably Qualified Person to visit the building and assess the noise reduction performance of the existing building envelope.</p> <p><u>If after 6 months following the date of the requiring authority's letter, no response has been received, the Requiring Authority shall again write to the owner of the Building requesting entry to assess the noise reduction performance of the existing building envelope. If the building owner agrees to entry within six months of the date of the Requiring Authority's letter, the Requiring Authority shall instruct a Suitably Qualified and Experienced Person to visit the building and assess the noise reduction performance of the existing building envelope.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs. Any future residential or other PPFs provided for in the AUP:OP zonings shall have a BPO assessment undertaken, including mitigation measures within the Project design where practicable.</u></p>
NoRs 1 and 2	33.	<p>For each Category C Building <u>requiring Building-Modification Mitigation</u> identified, the Requiring Authority is deemed to have complied with Condition 32 above if:</p> <ul style="list-style-type: none"> (a) The Requiring Authority's Suitably Qualified Person has visited the building and assessed the noise reduction performance of the building envelope; or (b) The building owner agreed to entry, but the Requiring Authority could not gain entry for some reason (such as entry denied by a tenant); or (c) The building owner did not agree to entry within six three months of the date of the Requiring Authority's <u>last</u> letter sent in accordance with Condition 32 above (including where the owner did not respond within that period); or

NoR No.	No.	Condition
		<p>(d) The building owner cannot, after reasonable enquiry, be found prior to completion of construction of the Project.</p> <p>If any of (b) to (d) above apply to a Category C Building, the Requiring Authority is not required to implement Building-Modification Mitigation to that building.</p>
NoRs 1 and 2	34.	<p>Subject to Condition 33 above, within six months of the assessment undertaken in accordance with Conditions 32 and 33, the Requiring Authority shall write to the owner of each Category C Building <u>requiring Building-Modification Mitigation</u> advising:</p> <p>(a) If Building-Modification Mitigation is required to achieve 40 dB LAeq(24h) inside habitable spaces; and</p> <p>(b) The options available, <u>at the cost of the Requiring Authority</u>, for Building-Modification Mitigation to the building, if required; and</p> <p>(c) That the owner has <u>24 three months from completion of construction of the relevant section of the project</u> to decide whether to accept Building-Modification Mitigation to the building and to advise which option for Building-Modification Mitigation the owner prefers, if the Requiring Authority has advised that more than one option is available.</p>
NoRs 1 and 2	35.	<p>Once an agreement on Building-Modification Mitigation is reached between the Requiring Authority and the owner of a Category C Building <u>requiring Building-Modification Mitigation</u>, the mitigation shall be <u>implemented at the Requiring Authority's expense</u>, including any third party authorisations required, in a reasonable and practical timeframe agreed between the Requiring Authority and the owner.</p>
NoRs 1 and 2	36.	<p>Subject to Condition 33, where Building-Modification Mitigation is required, the Requiring Authority is deemed to have complied with Condition 35 if:</p> <p>(a) The Requiring Authority has completed Building Modification Mitigation to the building; or</p> <p>(b) An alternative agreement for mitigation is reached between the Requiring Authority and the building owner; or</p> <p>(c) The building owner did not accept the Requiring Authority's offer to implement Building-Modification Mitigation <u>within 24 months of completion of construction of the relevant section of the Project three months of the date of the Requiring Authority's letter sent in accordance with Condition 37 (including where the owner did not respond within that period)</u>; or</p> <p>(d) The building owner cannot, after reasonable enquiry, be found <u>within 24 months of the prior to</u> completion of construction of the Project.</p>
NoR 1 and 2	37.	<p>The Detailed Mitigation Options shall be maintained <u>by the Requiring Authority</u> so they retain their noise reduction performance as far as practicable.</p>

SUBMISSION ON NOTICE OF REQUIREMENT Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1) Auckland Transport (AT)

To: Auckland Council
unitaryplan@aucklandcouncil.govt.nz

Name of Submitter: DR Levene & JAG Kearns & MA Levene & DW Tibby and EA Levene (The Levene Foundation)

DR Levene & JAG Kearns & MA Levene & DW Tibby and EA Levene (The Levene Foundation) provides this submission on the Notice of Requirement – Requirement by Auckland Transport to project transport routes in Takanini (Takanini Level Crossing (TLC) project). This seeks to propose a new designation.

The purpose of the NOR is described in the public notice as being for the construction, operation and maintenance of an arterial transport corridor and associated facilities.

The activities to be enabled by the designation include environmental mitigation, temporary construction areas, ancillary structures and other activities required for the Project.

The Takaanini Level Crossings (TLC) Project involves two new notices of requirement for designations given by Auckland Transport for the construction, operation, maintenance and upgrade of the following grade-separated crossings of the North Island Main Trunk (NIMT) railway in Takaanini:

- A new multi-modal bridge over the railway at Manuia Road;
- New multi-modal bridges to replace existing level crossings at Taka Street and Walters Road; and
- New active mode bridges to replace existing level crossings at Spartan Road and Manuroa Road.

The submitter is the landowner of 5 Marphona Crescent, Takanini, which is located near the proposed improvement works.

The Submitter could not gain an advantage in trade competition through this submission and the submission does not raise matters that relate to trade competition or the effects of trade competition.

The submission relates to the designation corridor, extent of physical works, and conditions.

The Submitter **supports** the application for the NOR subject to the following relief sought.

The reasons for the submitter's support in part are:

1. The submitter supports the principle of the NoR '1'. The proposed works are well overdue and will support connectivity and traffic safety.
2. The submitter considers that these works should be undertaken with urgency.

Relief sought

The Submitter seeks the following decision from Auckland Council in respect of the NOR's:

- Confirmation of the NoR 1 designation, and a timeframe for the works to be undertaken with priority.
- Such other consequential amendments to the provisions of the NoR 1 as may be necessary to give effect to the relief sought in this submission including conditions on the designation; and
- The submitted would like to request a meeting with the Requiring Authority prior to any hearing.

The Submitter wishes to be heard in support of this submission.



Michael Campbell

Campbell Brown Planning Limited

For and on behalf of DR Levene & JAG Kearns & MA Levene & DW Tibby and EA Levene (The Levene Foundation) as its duly authorised agent.

14th December 2023

Address for service of submitter:

C/- Campbell Brown Planning Limited

PO Box 147001

Ponsonby

AUCKLAND 1144

Attention: Michael Campbell

Mobile: 021 278 9018

Email: michael@campbellbrown.co.nz

Dated : 14/11/2023
To : Auckland Council
Subject: Response & submission regarding Notice of Requirement from Auckland Council
Affected: Properties: 2-17 Portrush Lane, Takanini, 2112
Project: Takanini Level Crossing (TLC):
Walters Road level crossing closure and new multi-modal bridge (NoR 2)
Auckland Transport

Dear Auckland Council,

Thank you for the earlier notice of requirement (NoR) dated **14th November 2023** advising the TLC project. We appreciate your concern towards advising us on the project and that we have an opportunity to submit a submission.

Thanks to the council staff and Denise for setting up the meeting on **4/12/23** to explain further, however, as residents of Portrush Lane, we are deeply concerned about several aspects of the projects and its short term and long term impacts on our community and whanau.

After the council meeting we understand that as we dont fall directly under the designated area, that we have NO RIGHTS under the Public Works Act. We understand that it will go ahead regardless and we were very grateful to find out that the bridge will not be a high fly-over bridge or extension from the motorway so just part of the infrastructure to ease traffic congestion which is very much needed.

Our collective concerns are detailed below and hope you give it your kind consideration.

Short term effects (due to Project)


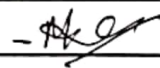
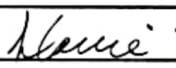
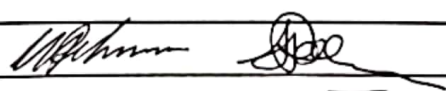
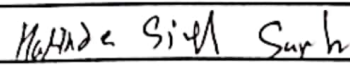
1. Noise & dust pollution - we are aware that we have no way of eliminating these as it's part of construction
2. Heavy machinery, Piling and its effects on structural integrity of the land and the dwellings.- We are concerned as our properties already feel the tremors with the cargo trains passing and wonder if we will experience another with this project?
3. Pollution will affect our properties so water blasting etc will be needed after. We would be very grateful if you would compensate us with the cost of water blasting our houses and roof-tops as we will be affected in this way
4. There is also a blue triangle pool/ or designation on the map that we assume is for water but not sure. During the hot summer days and nights we suffer with mosquitoes due to water standing in the Industrial land that is filled with pipes and other stationery machinery so after the rain - our homes suffer with a mosquito issue. Please can we be advised on this and if the pipes etc are not going to be removed that it be sprayed with a mosquito repellent or the sort.

Long term effects:

1. **Devaluing of properties** and land in the marketplace as it will be a noisy road
2. Concerns around **Increase in traffic** and reduce **safety on Oakleigh Avenue**
3. We believe the traffic circle will accommodate, for another exit option of traffic off the motorway, however there will still be **congestion at peak hours**. We kindly suggest **a set of lights at the end of Oakleigh Ave** so entry onto either side of Manuroa Road will be more controlled as it is a nightmare every morning and evening as not everyone has road courtesy with letting oncoming traffic in especially if we have emergency runs. This would be a big one for us.

Thank you again and we look forward to hearing from you about the next stages and any updates. Thank you.

Kind Regards,
Owners of Portrush Lane

Name of the Owner/ Owners	Property Address PORTRUSH LANE	Signature
1.		
2.		
3. VARINDER KATARIA	3 Portrush Lane	VK
4. Basil Kuriakose	4 Portrush Lane	
5.		
6. Mandeep	6 Portrush lane	
7.		
8. Leigh	8 Portrush lane	
9.		
10.		
11.		
12. Feroza & Wajid Rahman	12 Portrush Lane	
13.		
14. HARINDER SANDHU	14 PORTRUSH	
15.		
16.		
17.		

10/12

Dated : 14/11/2023
To : Auckland Council
Subject: Response & submission regarding Notice of Requirement from Auckland Council
Affected: Properties: 2-17 Portrush Lane, Takanini, 2112
Project: Takanini Level Crossing (TLC):
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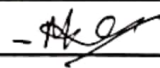
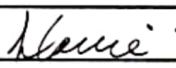
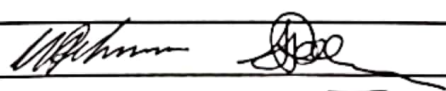
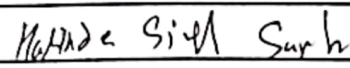
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12. Feroza & Wajid Rahman	12 Portrush Lane	
13.		
14. HARINDER SANDHU	14 PORTRUSH	
15.		
16.		
17.		

10/12

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification

Sections 168A, 169, 181, 189A, 190, and 195A of the Resource Management Act 1991



FORM 21

Send your submission to unitaryplan@aucklandcouncil.govt.nz or post to :

Attn: Planning Technician
Auckland Council
Level 16, 135 Albert Street
Private Bag 92300
Auckland 1142

For office use only
Submission No:
Receipt Date:

Submitter details

Full Name or Name of Agent (if applicable)

Mr/Mrs/Miss/Ms(Full Name)

TAKANINI RENTORS LTD

Organisation Name (if submission is made on behalf of Organisation)

Address for service of Submitter

979 River Rd Hamilton 3210

Telephone:

0274 788243

Email:

Sharyn@takrent.co.nz

Contact Person: (Name and designation if applicable)

This is a submission on a notice of requirement:

By: Name of Requiring Authority

Auckland Transport

For: A new designation or alteration to an existing designation

Takaanini Level Crossing (NoR 1) Spartan Road, Manuia Road and Taka Street

The specific parts of the above notice of requirement that my submission relates to are: (give details including property address):

20 Spartan Rd, Takaanini

My submission is:

I or we support of the Notice of Requirement

I or we oppose to the Notice of Requirement

I or we are neutral to the Notice of Requirement

The reasons for my views are:

Do not want acquisition of land
Blocking access w/o Spartan Road

(continue on a separate sheet if necessary)

I seek the following recommendation or decision from the Council *(give precise details including the general nature of any conditions sought).*

I wish to be heard in support of my submission

I do not wish to be heard in support of my submission

If others make a similar submission, I will consider presenting a joint case with them at a hearing

SPrankish

Signature of Submitter

(or person authorised to sign on behalf of submitter)

31/1/24

Date

Notes to person making submission:

If you are making a submission to the Environmental Protection Authority, you should use Form 16B.

You must serve a copy of your submission on the person who gave the notice of requirement as soon as reasonably practicable after you have served your submission on the Council (unless the Council itself, as requiring authority, gave the notice of requirement)

If your submission relates to a notice of requirement for a designation or alteration to a designation and you are a trade competitor of the requiring authority, you may make a submission only if you are directly affected by an effect of the activity to which the requirement relates that:

- (a) Adversely affects the environment, and
- (b) Does not relate to trade competition or the effects of trade competition.

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Bruce Crompton

Organisation name: Mead Trusts Holdings Ltd - Carters Takanini

Full name of your agent: Bruce Crompton

Email address: cromcar@xtra.co.nz

Contact phone number: 0274231077

Postal address:
440 Papakura Clevedon road
RD 2 Papakura
Auckland 2582

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:
12 Walters road Takanini

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
Effects of the proposed notice of requirements would render this property not fit for purpose, the company 'Carters' established their business and have occupied the site for more than 50 years.

I or we seek the following recommendation or decision from Auckland Council:
Adequate compensation

Submission date: 17 November 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

CAUTION: This email message and any attachments contain information that may be confidential and may be LEGALLY PRIVILEGED. If you are not the intended recipient, any use, disclosure or copying of this message or attachments is strictly prohibited. If you have received this email message in error please notify us immediately and erase all copies of the message and attachments. We do not accept responsibility for any viruses or similar carried with our email, or any effects our email may have on the recipient computer system or network. Any views expressed in this email may be those of the individual sender and may not necessarily reflect the views of Council.

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: TING-CHUN CHO

Organisation name:

Full name of your agent:

Email address: cire1988@gmail.com

Contact phone number: 0276281988

Postal address:
77 JOHN GRAY STREET
PAPAKURA
AUCKLAND 2110

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:
Local Residents accessing this crossing on daily basis

Do you support or oppose the Notice of Requirement? I or we support the Notice of Requirement.

The reason for my or our views are:
The current existing crossings are dangerous, My misses and son almost got hit by the train once while using it, and for me the congestion is just a nightmare in this area. and it is not even in Auckland CBD

I or we seek the following recommendation or decision from Auckland Council:
New multi-modal bridges

Submission date: 20 November 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,

- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Gary Holmes

Organisation name: Takanini Business Association Inc

Full name of your agent:

Email address: admin@takaninibusiness.co.nz

Contact phone number: 0274966283

Postal address:

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The reasons for the TBA being in opposition regarding the Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR2) Link are set out below and include the following: • alternatives • general impacts on businesses • construction effects on transport • operational effects on transport • on street parking • on-site parking • property access • corridor-specific operational effects • construction noise and vibration • lapse period See attached submission

I or we seek the following recommendation or decision from Auckland Council:

The decision the TBA seeks from the Council is to decline the Notice of Requirement. See attached submission for further details regarding relief sought including an underpass as the form of the proposed crossing instead of a bridge.

Submission date: 10 December 2023

Supporting documents

Submission on NOR2 - Takanini Level Crossing (Walters Rd final).pdf

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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**Submission on Notice of Requirement (In Opposition):
Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal
bridge (NoR2) Auckland Transport**

Takanini Business Association

Summary

Although the Takanini Business Association ('TBA') accepts that solutions need to be found to replace the four level crossings that bisect the Takanini community, the TBA opposes the bridge solution proposed in the Notice of Requirement: Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR2) Auckland ('NoR2') [Link](#)

The TBA holds concerns about the following likely negative impacts on businesses or communities accessing businesses from the Walters Road bridge proposal:

- As properties are acquired for the Project, people will likely move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the wider area a number of businesses important to the community will likely be lost, including: • a number of early childhood education ('ECE') centres; a skills training and education centre; and a service station (providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity).
- Other businesses will likely be lost, including vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services (including loss of local employment / livelihood).
- There will be construction effects on transport and long term operational effects on transport, especially for freight movements.
- On street parking and on-site parking will be reduced and property access negatively affected.
- Disruption will be caused by construction noise and vibration (along with a reduction in parking availability due to changed road conditions and demand for parking from the construction workforce).
- Loss in revenue for local businesses directly affected by construction as road blockages or disruptive construction redirect regular businesses customers.

The TBA notes the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). The TBA is of the view that the assessment of alternatives is deficient.

However, should the option as proposed of raising the road (i.e. road-over-rail) be preferred, then to avoid, remedy or mitigate the effects of that option, the TBA agrees with the proposals by the applicant to include conditions and detailed plans, including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by the TBA.

Introduction

The Takanini Business Association ('TBA') is an incorporated society (2666951) which was established by local business and commercial property owners and managers in partnership with local Police in 2017 in order to support local businesses with all matters relating to business development and growth, security, networking, collective communication with local authorities and to establish a unique culture within the Takanini business community. [Link](#)

The TBA welcomes the opportunity to make submissions on the Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR2) Auckland ('NoR2') [Link](#)

NoR2 is part of the Takaanini Level Crossings ('TLC') Project, which involves two new notices of requirement for designations given by Auckland Transport for the construction, operation, maintenance and upgrade of the following grade-separated crossings of the North Island Main Trunk ('NIMT') railway in Takaanini:

- A new multi-modal bridge over the railway at Manuia Road
- New multi-modal bridges to replace existing level crossings at Taka Street and Walters Road; and
- New active mode bridges to replace existing level crossings at Spartan Road and Manuroa Road.

These notices of requirement authorise works to:

- Provide a new bridge over the rail line at Manuia Road, accommodating all transport modes, with a particular focus on providing for heavy vehicles accessing the industrial areas
- Replace current level crossings at Taka Street and Walters Road with new bridges accommodating all transport modes
- Replace existing Spartan Road and Manuroa Road level crossings with new bridges for active modes (ie walking and cycling facilities).

The existing NIMT level crossings at Spartan Road, Manuroa Road, Taka Street and Walters Road will be closed and replaced by these crossings once constructed and operational.

Submissions

The Notice of Requirement being submitted on by the TBA is the Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR2) Auckland ('NoR2').

The submission relates to the entire Notice of Requirement.

The TBA is in opposition regarding the Notice of Requirement.

The TBA will not gain an advantage in trade competition through the submission.

The decision the TBA seeks from the Council is to decline the Notice of Requirement.

Reasons for being in opposition regarding the Notice of Requirement

The reasons for the TBA being in opposition regarding the Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR2) [Link](#) are set out below and include the following:

- alternatives
- general impacts on businesses
- construction effects on transport
- operational effects on transport
- on street parking
- on-site parking

- property access
- corridor-specific operational effects
- construction noise and vibration
- lapse period

(1) Alternatives

The TBA notes the TLC Project Assessment of Alternatives Link which considers a range of alternatives, including broadly options of raising the railway (i.e. rail-over-road), lowering the railway (i.e. rail-under-road), raising the road (i.e. road-over-rail) or lowering the road (i.e. road-under-rail). The TBA says the assessment of alternatives is deficient.

However, should the option as proposed in the NoR2 of raising the road (i.e. road-over-rail) be preferred, then the TBA makes the following submissions regarding that option.

(2) General impacts on businesses

Section 6 of the Social Impact Assessment of the identifies the following likely negative impacts on businesses or for communities accessing businesses from the proposal: [Link](#)

- There will be changes to people's way of life, for those living and working in the area and those who access services and businesses.
- As properties are acquired for the Project, people may move away from the area, and businesses will close and potentially be lost to the area if alternative sites cannot be found.
- Within the wider area, a number of businesses important to the community will potentially be lost, including:
 - a number of early childhood education ('ECE') centres across the Project area;
 - the Skills Update Training and Education Centre on Walters Road; and
 - the BP Service Station on Great South road providing fuel, including heavy vehicle refuelling, small vehicle cleaning and retail activity.¹
- Other businesses which will potentially be lost of the community include vehicle and tyre servicing outlets, house moving, car dealerships, marine retail and servicing, and food retail services.

To avoid, remedy or mitigate these effects, the TBA agrees with the proposals to include detailed plans identified in the TLC - Social Impact Assessment (to be developed in consultation with the Papakura Local Board and the Takanini Business Association) (or equivalents at the time), including:

- a Stakeholder and Community Engagement Plan;
- a Development Response Plan ('DRP');
- a Community Health and Wellbeing Strategy;
- a Property Management Strategy; and
- detailed design and construction planning.

Proposed conditions regarding an Urban and Landscape Design Management Plan, a Construction Traffic Management Plan and a Construction Noise and Vibration Management Plan are also welcomed by the TBA.

(3) Construction effects on transport

Section 5 of the Assessment of Transport Effects [Link](#) notes that construction of the Project includes:

- closing existing level crossings at Spartan Road, Manuroa Road, Taka Street and Walters Road (not necessarily simultaneously);
- constructing grade-separated multi-modal bridges at Manuia Road, Taka Street and Walters Road;
- constructing grade-separated active mode bridges at Spartan Road and

¹ These businesses are considered important to the community as they are included in the access indicator for social deprivation in the NZDep dataset.

Manuroa Road and constructing cul-de-sac turning heads either side of the rail line at these locations; • constructing a cul-de-sac turning head on the northern end of Takanini Road; • constructing accessways along Taka Street and Walters Road; and • constructing the roundabout and signalised intersection either side of the Manuia Road bridge and the signalised intersection at Arion Road.

Section 5 also says that a number of scenarios reflecting construction sequencing of the bridges have been assessed to determine construction effects and especially to identify which construction staging would have significant adverse effects and would therefore need to be avoided or mitigated. These have been grouped under three geographic areas:

- Area 1: • Construction of Manuia Road multi-modal bridge • Testing two scenarios where Manuia Road bridge has not been built yet: o Closure of only Spartan Road level crossing o Closure of only Manuroa Road level crossing
- Area 2: • Closure of Taka Street level crossing and construction of Taka Street multi-modal bridge under the following two scenarios: o Manuia Road bridge built, Spartan Road and Manuroa Road level crossings closed o Spartan Road and Manuroa Road level crossings remain open, Manuia Road bridge not yet built
- Area 3: • Closure of Walters Road level crossing and construction of Walters Road multi-modal bridge

Of particular concern to the TBA are the construction effects on freight movements in Area 3.

Area 3: Construction Effects on freight movements

Section 5.4 of the Assessment of Transport Effects notes that the model indicates that this construction scenario is not expected to have a significant effect on diversions to freight as freight can continue to primarily use Spartan Road and Manuroa Road in the 2038 network. The TBA supports scenarios with reduced effects on freight movements.

Section 5.5 of the Assessment of Transport Effects recommends development of a Construction Traffic Management Plan (CTMP) to mitigate the potential construction traffic effects to be prepared by the nominated contractor prior to works commencing. The TBA agrees and asks that the TBA be involved in development of the CTMP and that businesses be kept informed of construction times and progress.

(4) Operational effects on transport

Section 5.4.1.1 of the Assessment of Transport Effects concludes that Walters Road is a key connection in the Takaanini network, providing access to the Takanini Town Centre. Not providing a connection at this location will result in a significant gap in east-west connectivity in the network. In addition, offline construction is recommended to retain movements in this corridor. The Assessment notes that a suitable alternative to facilitate traffic movement will need to be provided for the closure of Taka Street and / or Walters Road during construction. This could entail offline construction or partial closure.

(5) On Street Parking

Section 6.9 of the Assessment of Transport Effects [Link](#) notes that all TLC corridors have existing on-street parking and the Manuia Road local street also has existing on-street parking.

The Project will remove all existing on-street parking spaces along the TLC corridors and there will be no on-street parking on the grade-separated bridges. The purpose of the existing on-street parking that will likely be impacted by the Project are highlighted in Tables 37 and 38 of the Assessment of Transport Effects and includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(6) On site Parking

Section 6.9 of the Assessment of Transport Effects [Link](#) also notes that in some locations of the Project areas, the Project intends to widen the existing designation and alter the cross section of the corridor to incorporate separated walking and cycling facilities, provide space for the bridge or to incorporate space for the accessways. As a result, existing car parking provision for properties adjacent to the Project corridor will be affected. This includes numerous industrial and commercial businesses, who will be significantly affected.

This is of concern to the TBA.

(7) Property Access

Section 6.10 of the Assessment of Transport Effects [Link](#) notes that in some locations of the Project areas, access arrangements to existing properties will be affected. This includes numerous industrial and commercial businesses (Table 40), and particularly the key entry access point to the Takaanini Town Centre development (30 Walters Road).

This is of concern to the TBA.

(8) Corridor-Specific operational effects

Section 7 of the Assessment of Transport Effects [Link](#) assesses specific transport matters relating to individual corridors as well as measures to avoid, remedy, or mitigate actual or potential adverse effects.

Walters Road: The proposed design of the existing Walters Road layout involves an overpass from Great South Road to the four-legged roundabout with Porchester Road with vehicle and active mode provisions. There are concerns about the gradient of the proposed Walters Road bridge and the effect on freight. The TBA agrees with the recommendation that at the detailed design stage, the Walters Road bridge should be designed to accommodate heavy vehicles and should be aligned with future standards.

(9) Construction Noise and Vibration

Although the TLC - Assessment of Construction Noise and Vibration Effects [Link](#) suggests that predicted noise levels for the majority of works will be able to comply with the relevant daytime limits, which means that effects are generally acceptable inside neighbouring buildings, the TBA has concerns these will, to the contrary, be significant. Where high noise activities would occur (e.g., demolition of close by buildings, piling of bridges or retaining walls, and earthworks), the TBA agrees that these activities should be completed within limited periods (eg weeks) with highest noise levels for only some hours during the workdays. Overall construction of the bridges will take between 1 to 2 years for active mode bridges and 2.5 to 3 years for road bridges. The Assessment of Construction Noise and Vibration Effects notes that effects can be managed through the application of management and mitigation measures through a Construction Noise Vibration Management Plan ('CNVMP').

The Assessment of Construction Noise and Vibration Effects observes that depending on the final construction methodology and receivers in the vicinity, mitigation and management measures may also include the offer of temporary relocation. The appropriate mitigation measures will be determined on a case-by-case basis throughout construction using the CNVMP and/or site-specific schedules as the implementation tool.

Proposed conditions regarding a Construction Noise and Vibration Management Plan are welcomed by the TBA.

(10) Lapse period

Under section 184 of the RMA, the default lapse date for designations is five years unless the designation provides a different lapse period. The Assessment of Effects on the Environment states that a key objective of the Project is to identify and protect land now for future transport networks and that an extended lapse period of 15 years is reasonably necessary to achieve this, as it provides statutory protection of the transport corridors.

The TBA is concerned that the 15-year lapse period is excessive for this Project and creates far longer uncertainty for the business community.

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Krittibas Dasgupta

Organisation name:

Full name of your agent:

Email address: kdabira@gmail.com

Contact phone number: 0274733061

Postal address:
3 Phar Lap Crescent
Takanini
Auckland 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:
My property is at 3 Phar Lap Crescent and will be directly impacted by the proposed changes.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

1. Traffic buildup on Walters Road 2. Risk of Overpass being longer than planned 3. Heightened Risk of Anti-social behaviour 4. Visual Landscape changes 5. Noise and Dust during Construction. 6. Risk of Flooding I have provided detailed arguments regarding the above in my attached word document.

I or we seek the following recommendation or decision from Auckland Council:

I am opposed to this project going ahead based on the information currently provided to me. Before any further consultation is arranged with the community, more detailed drawings of the overpass should be shared with everyone, with full elevation view showing length and width. There also needs to be much more detail on how the visual landscape would be managed, as well as how risk of criminal activity under the overbridge would be managed.

Submission date: 11 December 2023

Supporting documents
Submission on Walters Road.pdf

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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Traffic buildup on Walters Road: The logic behind these overpasses is to improve traffic flow through the Walters Road and Taka Street level crossings once the train frequency increases. However, I consider this logic fundamentally flawed. Due to the closure of Manuroa and Spartan Roads, a lot more traffic, including heavy trucks, will be forced to utilize the two new overpasses to access Great Sth Road. Currently Great South Road stays gridlocked from Walters Road travelling North during peak hours. The situation will likely worsen considerably as a result of the additional traffic. This will progressively lead to traffic backing up on the overpass on the Eastern side of the train tracks, eventually clogging up Walters Road well beyond the Porchester Road roundabout. We need to see clear evidence including modelling data to show that the traffic situation on the roads will not worsen as a result of the proposed changes.

Risk of Overpass being longer than planned: The concept diagram provided in the 14_TLC Appendix A as below of the Walters Road designation shows a really high apex point of the overpass. How confident are the planners that the length of the overpass will be contained within the limits that were suggested? It is likely that to keep the gradient down to a manageable level, the overpass will need to be extended in both directions, and will start from a point close to the Porchester Rd and Walters Rd intersection. This will have significant negative impacts on residents living on Arion Road, Sth of Walters Road, and Phar Lap Crescent as they will lose easy access to their properties. Do you have an elevation drawing of the overpass which shows the height and length clearly?



Risk of Anti-Social Behaviour: For our community, the proposed overpass's design poses serious safety issues. These kinds of structures frequently result in unintentional gathering spots beneath the bridge

that are hard to see and keep an eye on. These isolated areas may attract antisocial behaviour and turn into hubs for crimes like graffiti, vandalism, and other public nuisances. This may cause people to feel less comfortable and discourage them from using the neighbouring common areas. In addition, poorly maintained and lit underpasses run the risk of turning into unsafe zones that discourage foot traffic and isolate particular regions of the neighbourhood after dark.

The impact of these design features on community safety cannot be overstated. It is not enough to erect a structure that facilitates traffic flow without considering its broader implications on community security and cohesion. As residents, we need detailed plans on how these spaces will be managed, monitored, and maintained to prevent them from detracting from the quality of life in our neighborhood. Strategies such as environmental design to promote natural surveillance, adequate lighting, and regular patrolling by security or community patrols should be integral to the project design to mitigate these risks.

Incorporating community safety into the design of the overpass cannot be a mere afterthought; it is a critical component that ensures infrastructure serves its intended purpose without compromising the well-being of the community it is meant to support. Without strong preventive measures in place, the overpass could become a liability rather than an asset.

Visual Landscape Changes: The planned overpass building will drastically change our area's current scenic character. Due to extensive construction activities and the removal of important components of the current landscape, including buildings, vegetation, and iconic street trees like the Sweet Gum trees along Arion Road reserve, the assessment details provided in the main document and the environmental impact summary suggest moderate to high adverse effects on visual amenity during the construction phase. Since these components are fundamental to the community's identity and aesthetic appeal, the permanence of these changes cannot be overstated.

The proposed mitigation measures, including new tree planting and landscaping to soften the impact of the bridge and its abutments, are noted. However, the scale of the new bridge, while designed to reflect anticipated growth, will present a stark contrast to the current urban landscape, potentially becoming an eyesore that dominates our visual field. Any new construction must be both visually and functionally consistent with the surrounding area, especially if it is a large-scale building like the overpass.

Noise and Dust During Construction: The Walters Road project's construction phase is anticipated to bring in a variety of noise and dust sources, which will probably have an effect on nearby homes. Houses within the designation boundary will be demolished, alignment and service relocation earthworks will be completed, bridge pilings will be installed, and final road surface will be completed. It is worth mentioning that certain tasks, like building bridges, are scheduled to take place on long weekends or at night in order to accommodate the line block needed by the rail network in New Zealand. While the construction-related effects may be transient, the long term health effects may linger for a while. I have personally been a witness to my father's long but losing battle against ILD (Interstitial Lung Disease) due to dust generated from grinding.

The construction work's breadth may also mean the removal of vegetation outside the permanent corridors, impacting the landscape character and possibly intruding upon the protected root zones of

existing trees. While the report suggests that open spaces and reserves affected by construction will remain accessible, their character and usability may be compromised during this time.

In light of these findings, my concerns are twofold: the immediate impact of construction-related noise and dust on the quality of life and the potential for long-term effects on the community's health and well-being. How do the planners of this project intend to deal with these?

Risk of Flooding: Being a Phar Lap Crescent resident, I have serious concerns regarding the Walters Road project's flood risk management. Though sound in theory, the design parameters and flood neutrality aims provide little comfort to those of us whose homes and everyday lives could be jeopardised by even minor errors in judgement or implementation. The claim of flood neutrality is predicated on a number of anticipated results and mitigating techniques that, once construction gets underway, won't allow for much leeway in case they turn out to be insufficient.

The construction operations that are being proposed, such as the construction of new culvert crossings and the realignment of overland flow pathways, raise concerns about the possibility of unanticipated effects on flood behaviour, particularly during the increasingly frequent extreme weather events. Particularly concerning are the displacement brought about by fill earthworks and the usage of laydown areas inside floodplains, which imply an irreversibly changed landscape with risks that could not become fully apparent until after a flooding event.

Although it has been agreed that certain areas, such as Manuia Road and Taka Street, are vulnerable, the suggested solutions appear to be a gamble against the unpredictable character of nature. The likelihood that these structures will be overwhelmed or that development would unintentionally divert floodwaters towards residential homes is underestimated when culverts and broadened bridges are relied upon as mitigating measures.

In essence, the flood risk strategies outlined seem to rest on a foundation of best-case scenarios. For those of us living in the potential path of these flood effects, the margin for error is non-existent. We are not comforted by the prospect of "minimal" flood depths or "negligible" displacement effects, as any increase in flood risk—no matter how statistically small—is too great a risk for our community to bear.

As such, I strongly urge a re-evaluation of the flood risk management strategies for this project, with a priority given to the safety and concerns of the residents. It is not enough to respond to flooding after it occurs; we must prevent any additional risk introduced by the project from the outset.

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Glenn Dyer

Organisation name: Carter Building Supplies

Full name of your agent:

Email address: glenn.dyer@carters.co.nz

Contact phone number: 0273499137

Postal address:
Private Bag 94027
Auckland
Auckland

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:
12 Walters Road, Papakura, Takanini 2112

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The proposal reduces the size of the site that we occupy, it takes off a substantial section off the entry and exit from our premises, meaning that the operations would be further constrained making it almost impossible for Carters to be able to operate our business from the site. Walters Road can already be a very busy road during our operating hours, we believe that this will further constrain and slow traffic making it harder for us to be able to operate from the site if these proposed plans are implemented.

I or we seek the following recommendation or decision from Auckland Council:
To exclude any requirement to take any area from the site that we occupy.

Submission date: 11 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.

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Form 21

Submission on requirements for designations

To: Auckland Council
Private Bag 92300
Auckland 1142
unitaryplan@aucklandcouncil.govt.nz

Name of submitter: Aotearoa Towers Group (ATG)
Trading as FortySouth
Private Bag 92161
Auckland, 1142

Chorus New Zealand Limited (Chorus)
PO Box 632
Wellington

Connexa Limited (Connexa)
PO Box 91362
Victoria Street West
Auckland, 1142

One New Zealand (One NZ) (formally Vodafone New Zealand Ltd)
Private Bag 92161
Auckland, 1142

Spark New Zealand Trading Limited (Spark)
Private Bag 92028
Auckland, 1010

These parties are making a joint submission and for the purposes of this submission are referred to collectively as the **Telecommunications Submitters**.

The Proposal:

This is a submission on the following notices of requirement by Auckland Transport for transport projects at the North Island Main Trunk rail line in the Takanini area. These are referred to as the Takanini Level Crossing (TLC) projects:

- Notice of Requirement 1: Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street (Auckland Transport)
- Notice of Requirement 2: Takanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge (Auckland Transport)

The Telecommunications Submitters are not trade competitors for the purposes of section 308B of the Resource Management Act 1991.

The specific parts of the notice of requirement that this submission relates to are:

The conditions of the designations that relate to Network Utility Operators.

The Telecommunications Submitters' submission is that:

The Telecommunications Submitters have no position on the overall Takanini Level Crossing package of transport projects but seek to ensure that existing and potential future telecommunications infrastructure in the project corridors are adequately addressed.

The Telecommunications Submitters **oppose** the proposed designations unless the matters outlined in this submission are satisfactorily addressed.

The organisations collectively deliver and manage the majority of New Zealand's fixed line/fibre and wireless phone and broadband services in New Zealand. The network utility operators in the telecommunications sector deliver critical lifeline utility services (as per Schedule 1 to the Civil Defence Emergency Management Act 2002) including infrastructure to support emergency services calls. It is also crucial for supporting social and economic wellbeing and measures to reduce travel demand. The services provide opportunities for work from home/remote work solutions through fast internet connections by fibre and/or wireless means which promotes a lower carbon economy.

The equipment used to deliver this is often located in road corridors which act as infrastructure corridors as well as just transport corridors. The works enabled by the proposed designations will affect existing and potential future infrastructure that will need to be protected and/or relocated as part of the proposed works. The design and construction of the works should take into account any opportunities for new

infrastructure to be installed which is preferable than trying to retrofit necessary telecommunications/ broadband infrastructure later due to disruptions and/ or incompatibility with project design.

Existing Infrastructure

A summary of existing infrastructure located in the project footprints is as follows and is outlined in more details viewable in **Appendix A**:

- **Connexa Facility:** Light pole located at Taka Street and Great South Road Intersection in NoR 1 [project 4: Taka Street project area] (supporting 2degrees network). This facility is also impacted by the South Frequent Transit Network Project (NoR 1) as both designations overlap each other.
- **FortySouth Facility:** Pole on 20 Walters Street in NoR 2 (supporting One NZ network)
- Chorus has extensive fibre and copper lines networks throughout the project area.
- Mobile operators are progressively rolling out roadside equipment and fibre routes in Auckland roads which may be within project corridors when works proceed.

Future Infrastructure Requirements

Network utility operators need to integrate necessary services into infrastructure projects such as transport projects. This is especially significant for future development with the introduction of advanced technology such as 5G infrastructure, which will be crucial to transport infrastructure. It is most efficient to coordinate any such services with the design and construction of a project, rather than trying to retrofit them at a later date. This process does not always run smoothly. To provide a previous example, Spark, 2degrees and Vodafone (now One NZ) had substantial issues trying to negotiate with the Public Private Partnership (PPP) operator of the Transmission Gully project in the Wellington Region to install services to provide telecommunications coverage. This process proved to be very difficult as there was no requirement to consult and work with relevant network utility operators in the designation conditions, and post completion of the project design and PPP contracting, it proved to be very challenging to try to incorporate necessary telecommunications infrastructure into the design of this project.

Spark achieved a more satisfactory outcome through participation as a submitter in the Auckland East West Link and Warkworth to Wellsford (W2W) project designation conditions where there was a specific obligation for the Requiring Authority to consult with network utility operators as part of the detailed design phase of the project to identify opportunities to enable the development of new network utility

including telecommunications infrastructure where practicable to do so¹. While the Telecommunication Submitters are not asking for the exact same outcomes of these examples, it demonstrates mutual benefits with ease of collaboration, communication and cohesive infrastructure development.

This is reflected in more recent times in two separate occasions earlier this year where Auckland Transport and Waka Kotahi agreed to amend their proposed Network Utility Management Plan (NUMP) conditions to involve network utility operators during the design phase, as well as the inclusion of Land Integration Process (LIP) conditions in Auckland Transport designations. Satisfactory conditions in this regard have been agreed with the requiring authorities in the Airport to Botany and Northwest Transport Projects. Those agreed amendments have been applied to the LIP conditions, however, the NUMP condition have not been carried through to the Takanini Level Crossing projects.

Both NoRs include a NUMP condition in the general conditions (listed as 24), which is not the same as the previously and recently agreed upon NUMP condition wording for the other abovementioned projects. The NUMP conditions used in these NoRs do not include the updated clause “(d) *the development of the NUMP shall consider opportunities to coordinate future work programmes with other network utility operator(s) **during detailed design** where practicable.*”

Further, Spark on behalf of the telecommunication companies has had more recent discussions with SGA representatives on how to have more effective conditions for the various NoRs packages. An SGA representative suggested that design stage is not an actual stage but is instead progressive. Accordingly, further changes to the amended NUMP are sought for:

“(d) *the development of the NUMP shall consider opportunities to coordinate future work programmes with other network utility operator(s) **during the further project stages including detailed design** where practicable.*”

This revised wording is proposed to assure the telecommunication companies has the opportunity to be continued to be involved for future project stages.

Whilst there is no direct obligation on the requiring authority to accommodate such works/opportunities, it is reasonable for there to be provisions to ensure the matter is properly considered during the design phase through consultation with network utility operators as it sets appropriate expectations and ensures these opportunities are properly explored. This enables proper consideration of making provision for communications infrastructure that support the function of the roads and/or serves adjacent growth. This

¹ East West Link Condition NU2, W2W Condition 24A

should be a consideration distinct from protecting or relocating existing network utilities affected by the project which has previously been the focus of conditions to manage network utilities.

Consultation with Telecommunications Network Utility Operators

Key to the outcomes the Telecommunications Submitters are seeking is to ensure they are adequately consulted by the requiring authorities over effects on their existing infrastructure, as well as being provided the opportunity to discuss any future requirements so this can be considered in the project design.

The Assessment of Environmental Effects (AEE) for each notice sets out the relevant utility providers who have assets within and around the proposed designations and is listed in the Network Utility Effects section. However, none of the Telecommunication Submitters except Chorus are listed within the affected Utility Providers despite having existing infrastructure within and around the proposed designated boundaries.

Spark and One NZ operate mobile phone/wireless broadband networks that are often located on facilities located in or adjacent to roads, while Chorus operates fixed line assets in roads including fibre. In addition, Spark has sold its fixed mobile asset infrastructure (e.g., their poles) to Connexa who are also acquiring the fixed assets of 2degrees, and similarly One NZ has sold its fixed mobile assets to Aotearoa Towers Group (trading as FortySouth). Accordingly, the operating landscape for telecommunications companies and who may be affected by these projects has become quite complex.

Land Use Integration Process (LIP)

The Auckland Transport NoRs included a satisfactory LIP condition (Condition 3), reflective of previous agreed upon amendments for the Airport to Botany and Northwest Projects NoRs. Specifically, these were changes to clause (f) and (f)(ii), which enable ongoing communication and opportunities for future infrastructure requirements to be integrated into the NoR projects. The Telecommunication Submitters are supportive of these changes.

The Telecommunications Submitters seeks the following decision from the Requiring Authorities:

Amend the NUMP condition for each notice of requirement, as follows:

Network Utility Management Plan (NUMP)

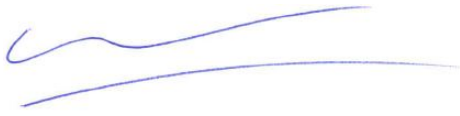
(a) A NUMP shall be prepared prior to the Start of Construction for a Stage of Work.

- (b) The objective of the NUMP is to set out a framework for protecting, relocating and working in proximity to existing network utilities. The NUMP shall include methods to:**
- (i) provide access for maintenance at all reasonable times, or emergency works at all times during construction activities;**
 - (ii) protect and where necessary, relocate existing network utilities;**
 - (iii) manage the effects of dust and any other material potentially resulting from construction activities and able to cause material damage, beyond normal wear and tear to overhead transmission lines in the Project area; and**
 - (iv) demonstrate compliance with relevant standards and Codes of Practice including, where relevant, the NZECP 34:2001 New Zealand Electrical Code of Practice for Electrical Safe Distances 2001; AS/NZS 4853:2012 Electrical Hazards on Metallic Pipelines; and AS/NZS 2885 Pipelines – Gas and Liquid Petroleum.**
- (c) The NUMP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project.**
- (d) The development of the NUMP shall consider opportunities to coordinate future work programmes with other Network Utility Operator(s) during the further project stages including detailed design where practicable.**
- (e) The NUMP shall describe how any comments from the Network Utility Operator in relation to its assets have been addressed.**
- (f) Any comments received from the Network Utility Operator shall be considered when finalising the NUMP.**
- (g) Any amendments to the NUMP related to the assets of a Network Utility Operator shall be prepared in consultation with that asset owner**

Retain the LIP condition.

The Telecommunications Submitters do wish to be heard in support of its submission.

If others make a similar submission, the Telecommunications Submitters will consider making a joint case with them at the hearing.



Signature of submitter
(Chris Horne, authorised agent for the Telecommunications Submitters)

Date: 12 December 2023

Address for service of submitter:

Chris Horne

Incite

PO Box 3082

Auckland

Telephone: 0274 794 980

E-mail: chris@incite.co.nz

Appendix A

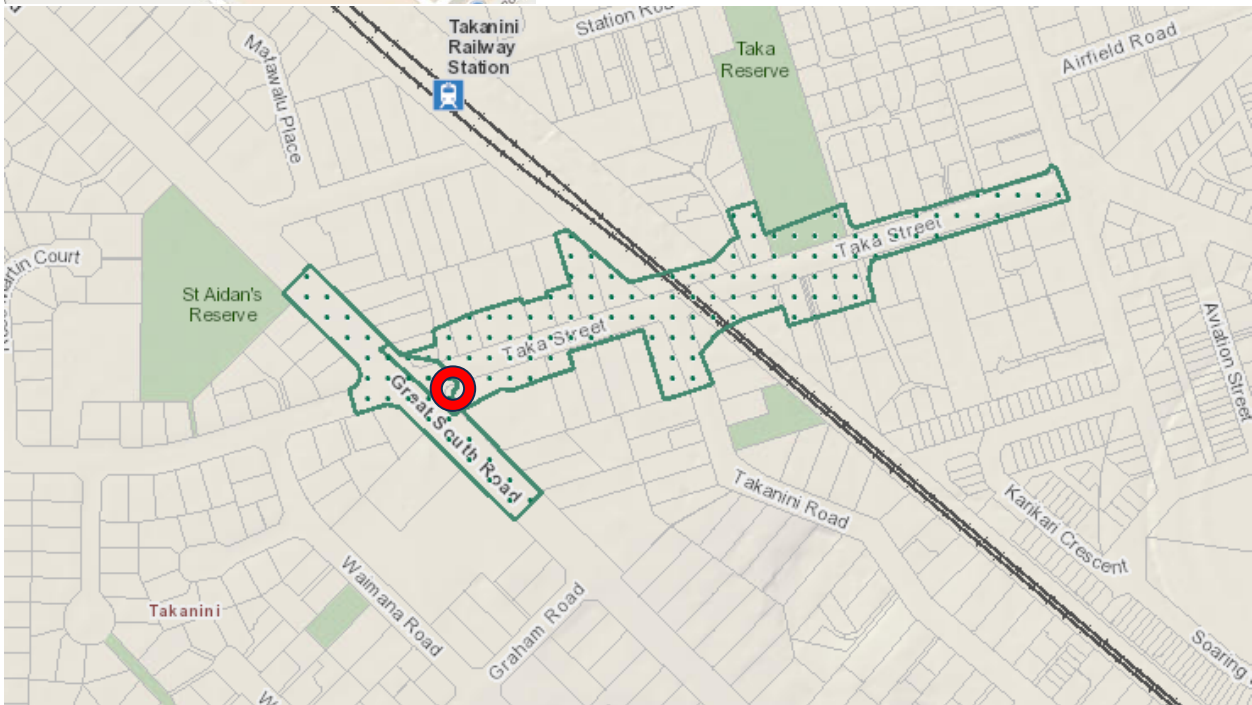
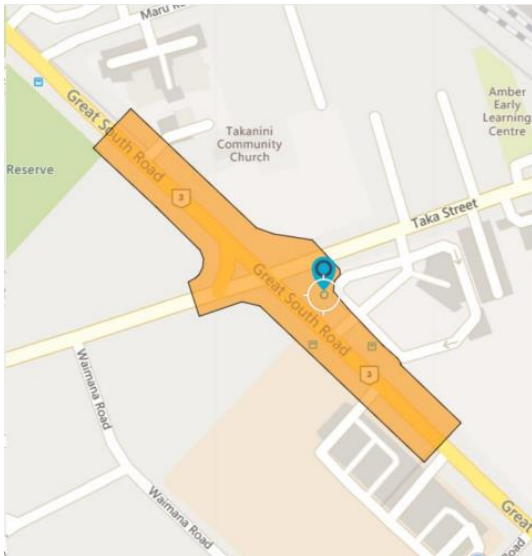
Impacted Telecommunication Facilities

Telecommunication Sites Impacted

Connexa

NoR 1 – Takanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Spartan Road (Auckland Transport)

- Pole located at Taka Street and Great South Road Intersection in NoR 1 [project 4: Taka Street project area] (supporting 2degrees network)



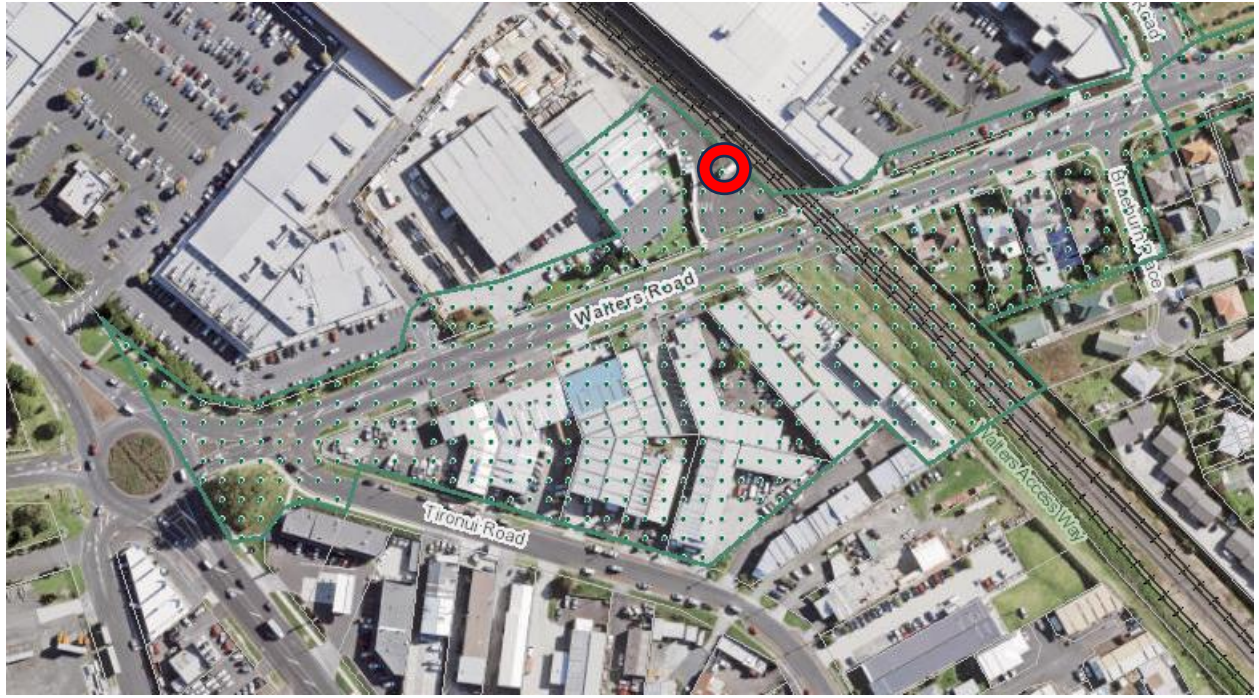


FortySouth

NoR 2: Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (Auckland Transport)

- Pole on 20 Walters Street (supporting One NZ network)





From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1148] Notice of Requirement online submission - Michael Sheridan
Date: Wednesday, 13 December 2023 10:46:27 am
Attachments: [Takinini Level Crossing NoR Submission - Van Den Brink 254 Limited.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Michael Sheridan

Organisation name: Van Den Brink 254 Limited

Full name of your agent: Mathew Husband

Email address: mat@civilplan.co.nz

Contact phone number: 0211828029

Postal address:
PO Box 97796
Manukau
Auckland 2241

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:

The following addresses owned by the submitter which are adjacent to the proposed designation boundary: - 1 Tironui Road, Takanini (Lot 1 DP 44494) - 5 Tironui Road, Takanini (Lot 2 DP 44494) - 254 Great South Road, Takanini (Lot 3 DP 44494) The submitter is generally supportive of the proposal as long as adverse effects of construction are sufficiently mitigated and avoided.

Do you support or oppose the Notice of Requirement? I or we support the Notice of Requirement.

The reason for my or our views are:

The works around the Takanini Level Crossing on Walters Road should result in positive effects on traffic flows in and around the area which should benefit the land owned by the submitter. The submitter just wishes to ensure that sufficient conditions are put in place with the designation to ensure that construction effects on their properties are minimized.

I or we seek the following recommendation or decision from Auckland Council:

As detailed in the attached submission letter, proposed conditions 9, 15, 16 and 18-22 are supported by the submitter and are requested to be confirmed as conditions for the designation.

Submission date: 13 December 2023

Supporting documents
[Takinini Level Crossing NoR Submission - Van Den Brink 254 Limited.pdf](#)

Attend a hearing

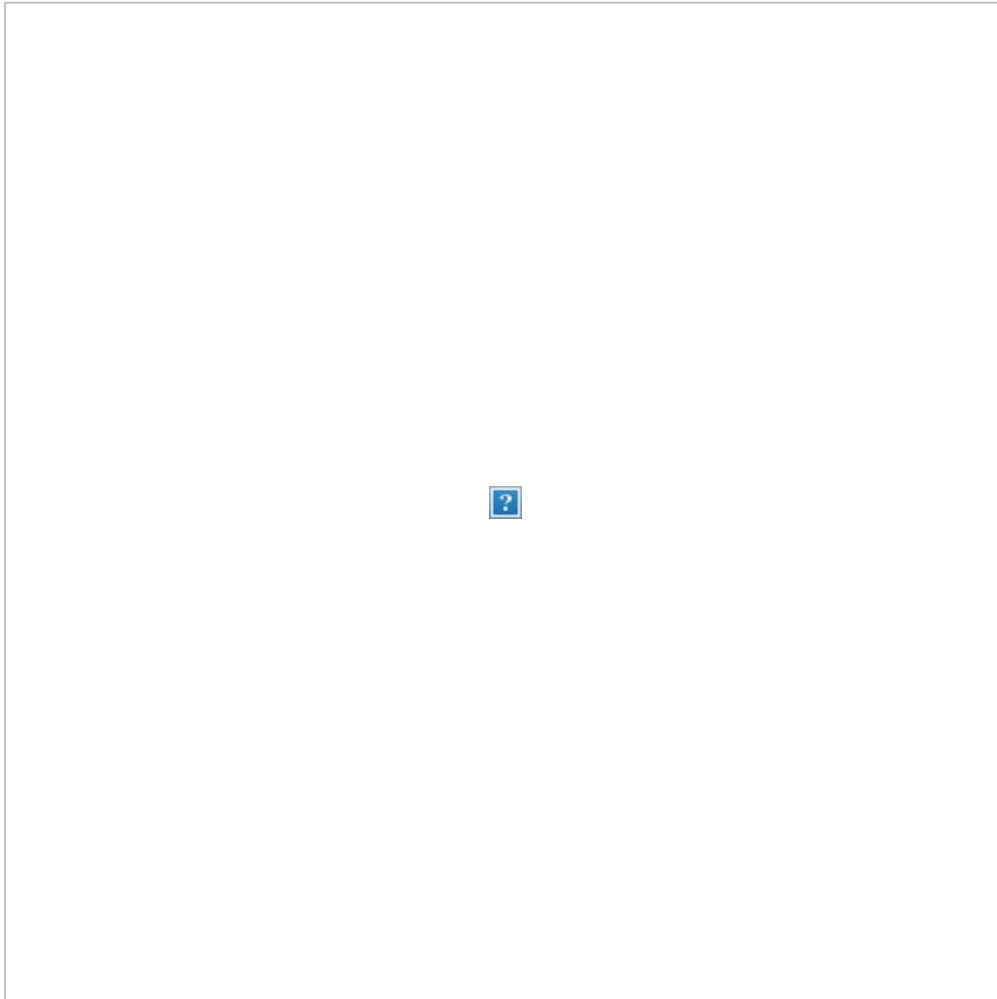
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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Submission on NoR 2 – Takanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge (Auckland Transport)

To: Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Attention: Planning Technician

Name of Submitter: Van Den Brink 254 Limited

Address of Properties: 1 Tironui Road, Takanini (Lot 1 DP 44494)
5 Tironui Road, Takanini (Lot 2 DP 44494)
254 Great South Road, Takanini (Lot 3 DP 44494)

Address for Service: C/- CivilPlan Consultants Limited
PO Box 97796
Manukau City
Auckland 2241

Attn: Mathew Husband

Telephone: (09) 222 2445

Email: mathew@civilplan.co.nz

This is a submission on a notice of requirement from Auckland Transport for a designation ('the notice of requirement'). The notice of requirement is for the 'Takanini Level Crossings Project', specifically the NoR covering the Walters Road level crossing closure and new multi-modal bridge.

The submitter is not a trade competitor for the purposes of section 308B of the Resource Management Act 1991 ('RMA').

This submission relates to the Takanini Level Crossing Designation Notice of Requirement 2 and specifically the portion of the designation that borders 1 & 5 Tironui Road and 254 Great South Road.

This is a submission in **support** subject to conditions.

The submission is as follows:

1. The Submitter and Subject Sites

Van Den Brink 254 Limited (VDB) is the owner of the land at 1 & 5 Tironui Road and 254 Great South Road, Takanini, also referred to as Oak Junction, which is Light Industry zoned land and contains multiple retail and commercial activities within two buildings on the site including food retailers and a petrol station and a carpark with roughly 35 parking spaces.

Currently the site has a two-way vehicle crossing near the southern end of its south-western boundary with Great South Road with one one-way entrance just to the north-west of the other crossing.

A location plan showing the three sites owned by VDB is shown in Figure 1, below.



Figure 1: Location Plan showing VDB owned land on the corner of Tironui Road and Great South Road

2. Submission

It is expected that the proposed designation adjacent to the subject sites (as shown in Figure 2, below), will benefit traffic flows and improve safety in and around the proposed level crossing upgrades, including for staff and customers accessing the commercial activities on the site at 1 & 5 Tironui Road and 254 Great South Road, Takanini. However, the construction of the Takanini level crossing upgrades may have adverse impacts on these sites if construction effects are not sufficiently managed.

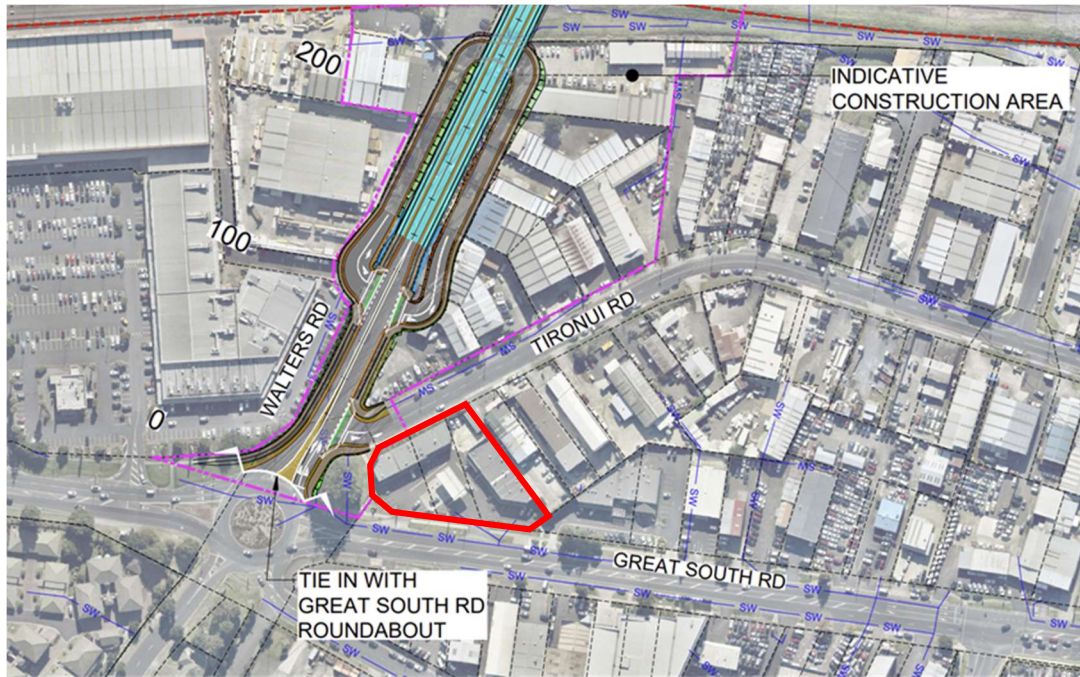


Figure 2: Plan showing the boundary and features of the NoR in relation to the subject sites

The submitter has the following concerns regarding the potential effects of the construction of the Takanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge:

- If improperly managed, vibration effects during construction of the works proposed under NoR 2 could disrupt the operation and compromise the safety of the activities occurring on the submitter’s land.
- If improperly managed, noise effects during construction of the works proposed under NoR 2 could adversely impact the activities occurring on the submitter’s land.
- There are two pedestrian accesses to the subject site within the designation extent; one from Great South Road and one from Tironui Road. Whilst the current plans do not show any proposed changes to these paths, it is recognised that the General Arrangement plans are indicative plans only. Maintaining these two pedestrian path connections to the site is critical to avoid adverse effects on pedestrian access to the commercial activities on the site.

3. Relief Sought

For the reasons set out above, VDB requests the following relief:

- a) That the designation is confirmed subject to conditions identified below.
- b) That the pedestrian footpaths to the site from Great South Road and Tironui Road are maintained during and following works.
- c) The following proposed conditions are supported and are requested to be confirmed as conditions for the designation:

- i) Condition 9: Stakeholder Communication and Engagement Plan (SCEMP). This condition will ensure that stakeholders, including directly adjacent landowners like the submitter, will be engaged and consulted throughout the designation process.
 - ii) Condition 15: Construction Environment Management Plan (CEMP). This plan should set out the methods and procedures for managing adverse construction work effects on the environment, including on the submitter's land, to ensure they are sufficiently mitigated or avoided.
 - iii) Condition 16: Complaints Register. This condition requires a complaints register be established throughout construction works, which will ensure any complaints about the construction works are recorded and actioned.
 - iv) Condition 18: Construction Traffic Management Plan (CTMP). This plan is considered necessary to manage the effects of heavy vehicles moving in and around the site during construction, as well as managing public traffic through and around the site during works, which may affect the operation of the Walters Road / Great South Road roundabout and access to the submitter's land by staff and customers.
 - v) Conditions 19, 20, 21 and 22 which set out the Construction Noise and Vibration Standards, respectively, and require the preparation and implementation of a Construction Noise and Vibration Management Plan (CNVMP). These conditions establish the maximum noise and vibration levels allowed during construction and measures to ensure compliance with these standards, or manage any discrete interruptions. These conditions are critical to ensure that noise and vibration effects are sufficiently mitigated on adjoining properties, including the submitter's land, to avoid damage to property and minimise disturbance to business operation during works.
- d) Any other consequential relief to address the matters raised by the submitter;
- e) Any other alternative relief to address the matters raised by the submitter.

4. Conclusion

The submitter, Van Den Brink 254 Limited, is generally supportive of the proposed project as set out in the NoR 2 – Takanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge documents.

With regard to the land owned by the submitter at 1 & 5 Tironui Road and 254 Great South Road, Takanini, upon completion of construction, the works around the Takanini Level Crossing on Walters Road should result in positive effects on traffic flows in and around the area which should benefit this land. As long as adverse construction effects, such as noise and vibration, are appropriately managed during the construction phase through the mitigation and management measures outlined in section 3 above, then the submitter does not have any objections to the proposed works.

Van Den Brink 254 Limited wishes to be heard in support of this submission.

Signature: .....
Mathew Husband, MPlan, Int.NZPI
Planner, CivilPlan Consultants
On behalf of Van Den Brink 254 Limited

Date: 13th December 2023

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1208] Notice of Requirement online submission - Brian Hogan
Date: Thursday, 14 December 2023 2:00:40 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Brian Hogan

Organisation name:

Full name of your agent: The Blacksmith Restuarant & Bar

Email address: brian_hogan@eml.cc

Contact phone number: 0274936405

Postal address:

26 Loxton Ln

Paerata

Pukekohe 2677

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:

Takanini Town Centre. Walters Rd. Grade Separation Walters Rd, Spartan Rd, Manuroa Rd, andTaka St

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

The option of an over pass vs and under pass has not been properly or fairly assessed. The option of an over pass has been pre-determined, notification has been secretive until highlighted by local Takanini people. ie little consultation, and the decision made for convenience of the paid AT staff. An under pass is less disruptive, will be less and better visually, will take up a smaller foot print, and much easier for mobility access. The proposed plan has no regard for the dissabled. As the father of a dissabled child I know this. It is a disgrace that AT employees even propose this.

I or we seek the following recommendation or decision from Auckland Council:

I seek a proper professional plan with regard to the above shortfalls that the proposed plan lacks in spades.

Submission date: 14 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

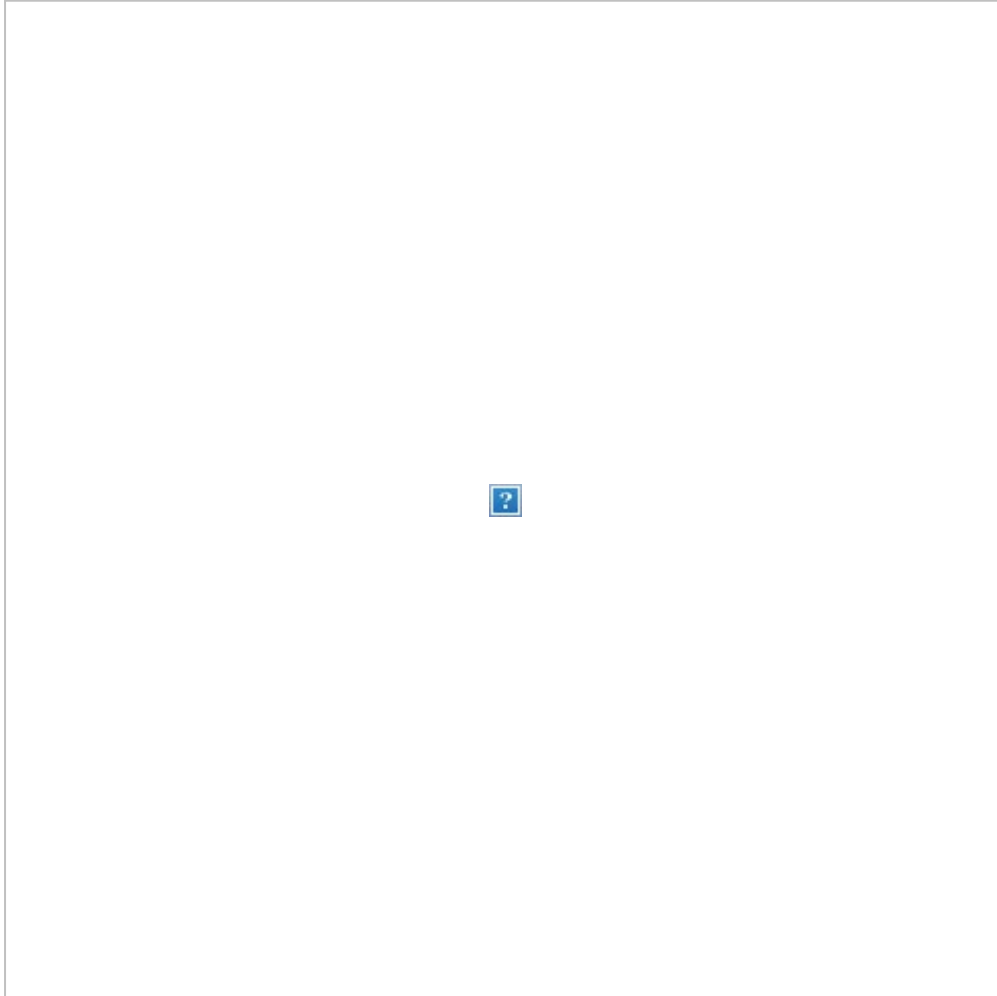
Would you consider presenting a joint case at a hearing if others have made a similar submission?

Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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Dear Sirs,

I hereby lodge my objection to this proposal by Auckland Transport to the changes they propose. Auckland Transport have been predetermined, secretive, and casual about this proposal. Over bridges for these crossings will be a blight on the visual aspects of our Takanini community. A T have not given fair regard to the option of under passes which take up far less land, are cost beneficial, less disruptive in both construction and visually. Staff, who are paid employees of Council and thereby us Rate Payers, have not been fair, professional, or willing to listen to independent professional advice. They have been dismissal of alternative options totally, and not given these any credence or consideration.

I as major stakeholder and an inaugural tenant of the new Takanini Town Centre, and I consider this proposal and the casual approach and dismissal attitude of A T is an insult to the investment and effort we at the Centre have put in over the last 12 + years.

Yours Faithfully,

Brian Hogan

Owner of The Blacksmith Restuarant & Bar

--

Brian Hogan

brian_hogan@eml.cc

**SUBMISSION BY ALDA INVESTMENTS LIMITED ON NOTICE OF REQUIREMENT TO
DESIGNATE LAND**

To: Auckland Council

Name of submitter: Alda Investments Limited ("**Alda**")

Address for service: c/- Aidan Cameron
Barrister
Bankside Chambers
Level 22, 88 Shortland St
AUCKLAND 1140
T + 64 9 307 9955
E aidan@bankside.co.nz

This is a submission on Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR 2) Auckland Transport for a new multi-modal bridge crossing of the North Island Main Trunk (NIMT) railway at Walters Road. (the "**NoR**").

Alda is not a trade competitor for the purposes of section 308B of the Resource Management Act 1991 ("**RMA**").

Alda wishes to be heard in support of our submission.

If others make a similar submission, Alda would consider presenting a joint case with them at any hearing.

Scope of submission

1. This submission relates to the NoR in its entirety.

Nature of submission

2. Alda opposes the NoR in its entirety.

Reasons for submission

3. The primary reasons for this submission are that the NoR:
 - a. fails to promote the sustainable management of natural and physical resources, and therefore fail to meet the purpose and principles of the Resource Management Act 1991 (“**RMA**”);
 - b. fails to meet the reasonably foreseeable needs of future generations;
 - c. fails to enable the social, economic and cultural well-being of the Auckland community to be met;
 - d. is inconsistent with the purposes and provisions of the relevant planning documents, including the Unitary Plan and the Regional Policy Statement (“**RPS**”);
 - e. is inconsistent with Part 2 and other provisions of the RMA, including sections 74 and 75, including the functions of Auckland Council (“**Council**”) under s 31; and
 - f. will have significant adverse effects on the environment;
 - g. is not an efficient use and development of natural and physical resources;
 - h. fails to comply with s 171(1)(b) of the RMA, as adequate consideration has not been given to alternative sites, routes or methods of undertaking the proposed works in circumstances where Waka Kotahi – The New Zealand Transport Agency (“**NZTA**”) does not have an interest in the land sufficient for undertaking the work; and in light of (f) above;
 - i. is not reasonably necessary to achieve the objectives of the requiring authority for which the designations are sought.
4. Without limiting the generality of the above, additional specific reasons for opposing the NoR are set out below.

Background and introduction

5. Alda holds a resource consent to develop the sites at 164-166 Porchester Road, Takanini.
6. The sites are zoned Mixed Housing – Urban under the Auckland Unitary Plan (Operative in Part) (“**AUP**”).
7. The sites are currently bare land, following demolition of the existing dwellings in late 2022. The land has a resource consent for two four level apartment blocks, carrying 42 residential

units, including consents for all necessary land modification, infrastructure provisions, landscaping, vehicle access, car parking and contamination remediation.

8. A copy of the resource consent and approved plans are **attached** to this submission as Appendix A.
9. The units are a mixture of one and two-bedroom residences, which are ideal for social or community housing purposes.
10. Works have commenced to implement the consents, with pre-loading now complete and detailed geotechnical investigation underway.
11. The proposed designation covers the front yard area on the corners of Walters Road and Porchester Road, as shown in the image below:



12. The approved resource consent has allowed the building and its associated decks to be located within the front yard setback, and requires the remaining front yard to be planted to mitigate the effects of the encroachment.
13. The advice Alda has received is that the current development works are also affected by the proposed designation, as the apartment building cannot be constructed without using the area that is proposed to be designated.

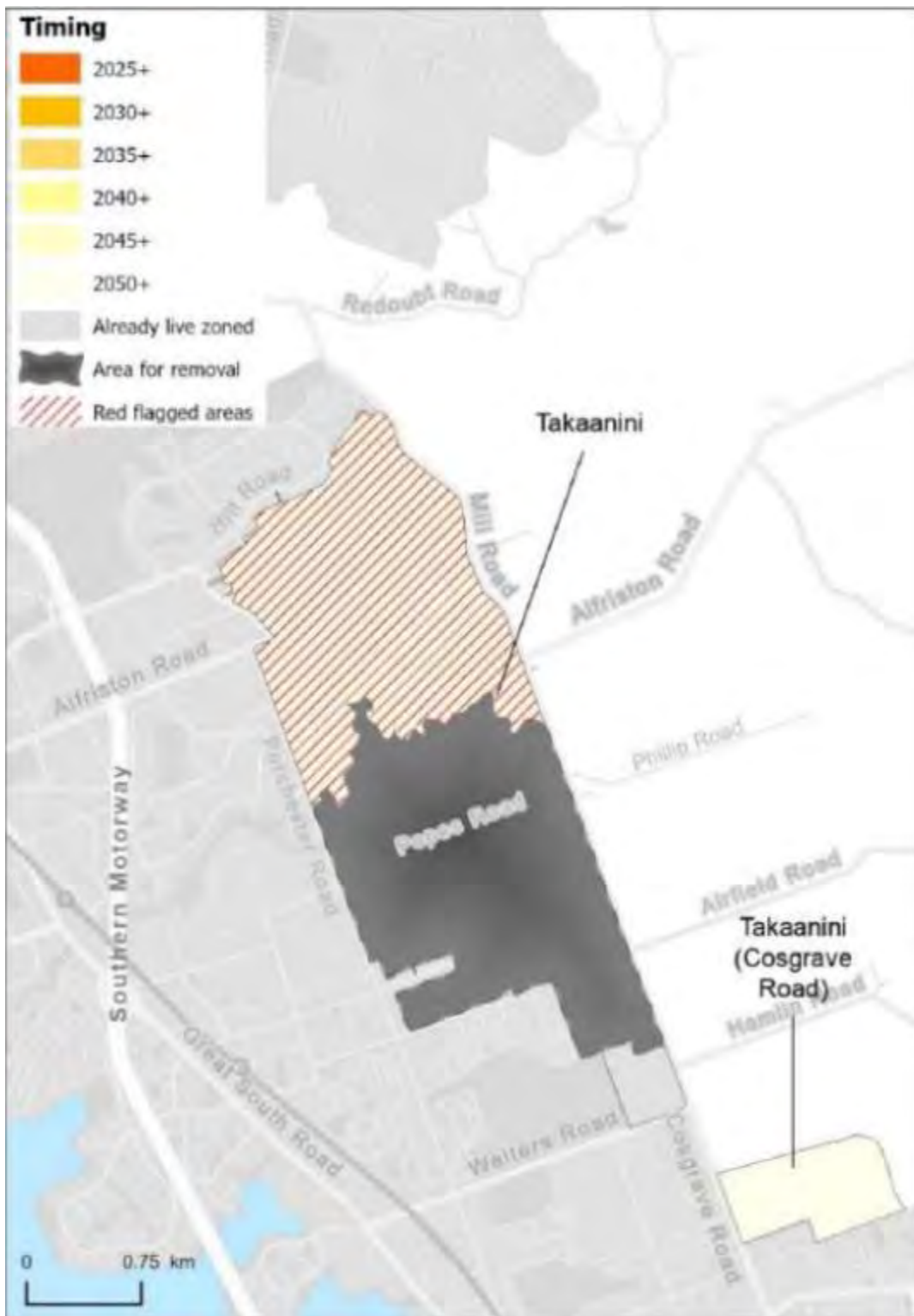
Specific reasons for opposing the NoR

Does not take into account recent planning decisions

14. On 2 November 2023 the Planning, Environment and Parks Committee resolved to adopt a new Future Development Strategy. Specifically resolution clause c) states:

c) tuhi ā-taipitopito / note that once published, the Future Development Strategy replaces the current Development Strategy (2018) and the Future Urban Land Supply Strategy (2017) and will be considered part of the Auckland Plan 2050.

15. Relevant to the Takanini area, the adopted Future Development Strategy has removed a large amount of land that had previously been earmarked for development and has a significant portion of land “red flagged” alongside the Porchester Road corridor. The below image depicted the newly adopted FDS areas.
16. It is not clear whether the necessity for the project, or at a more detailed level the traffic modelling volumes take these anticipated zoning changes into account.



Inconsistent with approved resource consents

17. The resource consent that has been granted for the site may now require approval from the requiring authority under s 178. This is an unwarranted impost upon an approved development. The NOR should be amended to eliminate this source of uncertainty.
18. The resource consent requires ongoing and continued compliance with the conditions of its approved land use consents, which include specific landscaping along both road frontages in accordance with approved plans.

19. The designation and associated works will remove features of the approved land use consents.
20. There is no mechanism proposed to address the resultant non-compliance with approved land use consents.

Construction effects

21. There is insufficient information and conditions to ensure that the construction effects will not damage the proposed apartment building (particularly given the close proximity of the building footings etc to the proposed designation boundary).
22. There is insufficient information to ensure that noise received at the outdoor living spaces of the proposed units will maintain the useability of these spaces during works and following completion of the road (based on the new road volumes). This is particularly acute given that NOR 2 and 4 provide different future traffic volumes as set out below.
23. No specific provision has been made to ensure residents are able to be accessed by emergency services at all times. While such access is important for all residents, it is particularly so for the vulnerable members of the community likely to be housed here.
24. There is insufficient information to manage privacy and screening for units from the works occurring directly adjacent to outdoor living spaces.
25. Insufficient information is provided to demonstrate that the designated works can proceed without undermining the foundations of the units.

Flooding

26. Although it appears that flooding of structures will be avoided, there is no such assurance that flooding or ponding of the apartments' carpark area will be avoided. It is essential that the health and safety of vulnerable residents is assured and not imperilled by the works.

Noise impacts

27. As the proposed apartment buildings are under construction now, any required mitigation for future road noise should be installed at the same time to avoid unnecessary wastage of resources and extensive costs associated with retrofitting.
28. As identified in the matter below, as there is no consistency between the NoR's for volumes of traffic and design of this portion of road, the adequacy of any noise assessment is also questionable as to its accuracy in respect of effects

Certainty of Design

29. NOR2 (Walters Road) and NOR4 (Porchester Road) provide inconsistent design responses fronting 166 Porchester Road. NOR2 proposes a two-lane cross section with central median that ties into the existing roundabout, whereas NOR 4 proposes the signalisation of the Porchester Road/Walters Road intersection. The Resource Consent obtained for 164-166 Porchester Road requires mitigation to be provided on Walters Road based on a road cross section consistent with NOR2.

30. More concerningly, the two NORs proceed on the basis of different factual starting points, with predicted traffic volumes on Walters Road varying by up to almost 50%.

	NOR 2 (Walters Road between Arion Road and Porchester Road)	NOR 4 (Walters Road between Arion Road and Porchester Road)
AM Peak	396 vph northbound 811 vph southbound	366 vph northbound 735 vph southbound + Walters Road (north) southbound through traffic
Interpeak	591 vph northbound 693 vph southbound	Not provided
PM Peak	576 vph northbound 511 vph southbound	835 vph northbound 438 vph southbound + Walters Road (north) southbound through traffic

31. There can be no certainty that the proposed solutions will adequately manage predicted future traffic when there is no certainty about what volumes of traffic are to be.
32. Adjacent landowners need certainty as to what is to be constructed. The two NORs must be reconciled. Maintenance of pedestrian accessibility is essential for the future residents of 164-166 Porchester Road.

Conditions do not provide effective mitigation

33. The conditions do not address the concerns of the submitter and they do not provide for effective mitigation, including for the following reasons:
- (a) There is no requirement for management plans to be certified – they are effectively provided to the Council on a “for information only” basis. It is bad enough that the affected landowners are left having to rely upon Council to protect their interests, worse when the even the Council’s input is dispensed with. Each management plan should be required to achieve a clear objective and Council should retain the role of certifying that the objective has been achieved, in accordance with the approach long-approved by the Environment Court.
 - (b) The LUIP:
 - (i) Only addresses the timeframe between confirmation of the designation and the start of construction. As identified above the proposed apartment building is under construction now.

- (ii) Gives no certainty that working with the Requiring Authority through this process will result in s 176 or s 178 approval.
- (c) Condition 7 relating to Outline Plans enables a Requiring Authority to pick and choose which management plans are relevant to each “stage”, and allows the Requiring Authority to determine what is to form each “stage”.
- (d) The SCEMP does not include any resolution process for where the concerns of the landowner are not being adequately addressed by the outline plan of works/management plans.
- (e) The ULDMP should be utilised as a tool for refinement and implementation of a design which is already of a standard that will achieve quality urban design and landscape outcomes, as opposed to a tool to fix the current concept plan.
- (f) The ULDMP requires stakeholders to be invited to participate in the detailed design 6 months prior to the start of detailed design for “a stage of work”. There is no obligation for this participation to continue through the detailed design, nor to participate in earlier designs (e.g enabling works) which ultimately affect decisions and outcomes in the ULDMP.
- (g) The ULDMP should also include an independent process for resolution of any disagreement in the design outcomes (as listed in clause (f) of the ULDMP condition) or achievement of the ULDMP objective outcomes (as listed in clause (b) of the ULDMP condition).
- (h) The project should not enable any increase in flood hazard on any sites. The consented apartment building and its associated carriage and access has been carefully designed to manage flood effects and velocities of flood to ensure the safety of people and vehicles within the site (as well as maintaining an acceptable minimum floor level of the building to protect the habitable spaces from flood hazard);
- (i) The Management Plans should be required to maintain access (vehicle and pedestrian) at all times during work.

Recommendation sought

- 34. Alda seeks that the NoR is recommended to be **withdrawn**.
- 35. In the alternative, Alda seeks conditions to ensure AT addresses each of the issues raised in this submission including conditions that:
 - a. Require an appropriate roading design that caters for realistic future demand and recognises the value and importance of existing investment, minimises intrusions upon private land and eliminates the designation from 164-166 Porchester Road;
 - b. Require the requiring authority to provide approval under ss 176 or 178 if required to allow the construction of the 42 consented dwelling units on 164-166 Porchester Road;
 - c. Ensure that noise levels received at the units to be constructed at 164-166 Porchester Road are reasonable and that mitigation is installed as the units are built;

- d. Ensure that there is no damage to the buildings to be constructed at 164-166 Porchester Road, including to their foundations;
- e. Secure access for residents (including by emergency services) during construction;
- f. Provide a safe pedestrian environment on the upgraded roads adjacent to 164-166 Porchester Road;
- g. Avoid flooding impacts on the buildings and carparking at 164-166 Porchester Road;
- h. Require management plans to be certified by the Council against a sensible purpose.

Signature:



Janette Campbell

Counsel for the DE Nakhle
Investment Trust

Date: 14 December 2023

Address for Service: c/- Aidan Cameron

Barrister

Bankside Chambers

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Decision on an application for resource consent under the Resource Management Act 1991



Restricted discretionary activity for a residential activity

- Application number:** BUN60343865
LUC60343866 – Land use (s9)
DIS603473867 – Discharge permit (s14)
- Applicant:** Alda Investments Limited
- Site addresses:** 164 Porchester Road, Takanini
166 Porchester Road, Takanini
- Legal descriptions:** Lot 2 DP 21172
Lot 1 DP 21172
- Proposal:** The Applicant proposes to undertake all necessary land modification, infrastructure provisions, landscaping, vehicle access, car parking and contamination remediation associated with the constructing of two four level apartment blocks on the subject sites containing 42 residential units.

Resource consent is required for the following reasons:

Land use consent (s9) – LUC60343866

Auckland Unitary Plan (Operative in part)

District land use (operative plan provisions)

Chapter E12: Land Disturbance – District

- General earthworks over an area greater than 1000m² and less than 2500m³ in a residential zone is a restricted discretionary activity under rule E12.4.1 (A5). The proposal involves general earthworks over an area of 2000m².
- General earthworks of involving volumes greater than 2500m³ in a residential zone is a restricted discretionary activity under rule E12.4.1 (A10). The proposal involves 1220m³ of cut and 3m³ of fill over followed by 2000m³ of fill for pre-loading requirements.
- General earthworks that are restricted discretionary activities under rules E12.4.1 (A5) and (A10) that fail to meet the following standard is a restricted discretionary activity under rule C.1.9(2):
 - Standard E12.6.2(13), as earthworks within the 1% AEP flood plain will exceed maximum specified calendar period of 28 days.

Chapter E36: Natural Hazards and Flooding

- Surface parking areas in the 1% AEP floodplain, that do not comply with Standard E36.6.1.7 are a controlled activity pursuant to rule E36.4.1 (A25)
- To construct stormwater management devices within an identified 1% AEP flood plain and is a restricted discretionary activity under rule E36.4.1 (A33).
- To construct new buildings within the 1% annual exceedance probability (AEP) floodplain is a restricted discretionary activity under rule E36.4.1 (A37).
- To use new buildings within the 1% annual exceedance probability (AEP) floodplain for more vulnerable activities (residential) is a restricted discretionary activity under rule E36.4.1 (A38).

Chapter H5: Residential – Mixed Housing Urban Zone

- To construct and use 42 residential dwellings is a restricted discretionary activity under rule H5.4.1 (A4).
- To construct new buildings which do not comply with standard H5.6.7 Height in Relation to Boundary but comply with standard H5.6.6 Alternative Height in Relation to Boundary is a restricted discretionary activity under rule H5.4.1(A33).
- To develop new buildings (and/or any additions to existing), as the development of new buildings or any additions has the same activity status as the land use activity, is a restricted discretionary activity under rule H4.4.1 (A34).
- Use and development that is a restricted discretionary activity under rules H5.4.1(A4) and (A34) that fails to meet the following core standard and is a restricted discretionary activity under rule C.1.9(2):
 - H5.6.8(1): The proposal involves the following front yard infringements:
 - The North Block enclosed decks are setback 2.08m from the Walters Road boundary.
 - One enclosed deck at the corner of Walters Road and Porchester Road is setback 1.081m from the boundary.
 - The northern eastern corner of North Block is setback 0.802m from the front boundary at the corner of Walters Road and Porchester Road.
 - The West block enclosed decks are setback between 2.036 and 2.098m from the Porchester Road boundary.

National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health (NESCS)

- The proposal involves and subdivision on a piece of land described in Regulation 5(7). As a DSI exists for the site, which states that the soil contamination of the piece of land exceeds the applicable standard, the proposal is a restricted discretionary activity pursuant to regulation 10(2).

Stormwater discharge permit (s14) – DIS60343867

Auckland Unitary Plan (Operative in part)

Chapter E8: Stormwater – Discharge and Diversion

- To discharge stormwater runoff from impervious areas greater than 1000m² and up to 5000m² within an urban area (1831m² of impervious area proposed) complying with standards E8.6.1 and E8.6.3.1 is a controlled activity under rule E8.4.1 (A9).

Decision

I have read the application, supporting documents, and the report and recommendations on the application for resource consent. I am satisfied that I have adequate information to consider the matters required by the Resource Management Act 1991 (RMA) and make a decision under delegated authority on the application.

Acting under delegated authority, under sections 104, 104C, 105, 107 and Part 2 of the RMA, the resource consent is **GRANTED**.

Reasons

The reasons for this decision are:

1. The applications are for controlled and restricted discretionary activity resource consents, as such under s104A and s104C only those matters over which council has restricted its discretion or control have been considered. Those matters are:
 - a. E8.7.1(1) – for stormwater discharge from impervious areas greater than 1000m² and up to 5000m² within an urban area;
 - b. E36.7.1(1) – for surface parking areas in the 1% AEP (AEP) floodplain;
 - c. E12.8.1(1) – for all restricted discretionary district earthworks;
 - d. E36.8.1(8) – for the construction of stormwater management devices in the 1% AEP flood plain;
 - e. E36.8.1(9) – for new structures and buildings within the 1% AEP flood plain;
 - f. E36.8.1(10) – for use of new buildings to accommodate more vulnerable activities located within the 1% AEP floodplain;
 - g. H5.8.1(2) – for four or more dwellings on a site;
 - h. H5.8.1(4) – for buildings that do not comply with:
 - i. H5.6.8 Yards;
 - ii. H5.6.9 Maximum impervious area;
 - iii. H5.6.11 Landscaped area;
 - iv. H5.6.12 Outlook space;

- v. H5.6.13 Daylight;
- vi. H5.6.14 Outdoor living space;
- vii. H5.6.15 Front, side and rear fences and walls;
- viii. H5.6.16 Minimum dwelling size;
- i. H5.8.1(5) – for buildings that do not comply with standard H5.6.5 Height in relation to boundary but comply with standard H5.6.6 Alternative height in relation to boundary; and
- j. Regulation 10(3) – for restricted discretionary activities under the NES:CS

No other effects have been taken into account in this assessment.

2. In accordance with an assessment under ss104(1)(a) and (ab) of the RMA the actual and potential effects from the proposal will be acceptable as:
 - a. The proposal provides for apartment typologies in two blocks that are three storeys in height, which achieves an overall built form; building scale; and density that will be compatible in the context of the existing and anticipated character of the surrounding residential area.
 - b. From an urban design perspective, the proposal is appropriate to its context; will result in a development that responds positively to the unique characteristics of the site and the surrounding neighbourhood; and is consistent with the type of activity and design quality anticipated within the zone.
 - c. The stormwater management regime comprising treatment, detention, retention and soakage devices (incorporating recharge) is appropriate in the context of the site's ground conditions (peat soils) and absence of available public stormwater network.
 - d. The timing, duration and scale of the proposed earthworks programme is commensurate with residential development. In particular, the building platform pre-loading is necessary due to ground conditions (peat soils). Overall, the earthworks can be appropriately managed and mitigated through compliance with the AUP(OP) noise and vibration standards; and suitable consent conditions to ensure that the proposed earthworks are acceptable, including the preparation and approval of a construction noise and vibration management plan prior to earthworks commencing.
 - e. All geotechnical and land stability matters will be addressed through the robust suite of geotechnical related conditions agreed to by the applicant. Overall, the site is suitable for the residential activities subject to adherence to the geotechnical conditions.
 - f. No significant OLFP spills into the property from the road as the levels in the road reserve limit flow into the site and existing levels in the site prevent the small flow that does enter the site from the east from crossing the site. As such, the development will not impact on overland flow paths. The potential for flood hazards affecting habitable dwellings will be addressed by providing a minimum floor level of 21.1m.
 - g. The existing road network will adequately cater for the additional traffic generated by the proposed development and the vehicle crossing on Walters Road will adequately cater for the anticipated vehicle movements to and from the site. As such, the traffic generation

associated with the proposal will not result in any discernible adverse effects on the local or regional roading networks.

- h. Adequate onsite circulation and manoeuvring space will be achieved for all parking spaces including the onsite refuse collection point; therefore, vehicles will exit the site in a forward gear.
- i. The pedestrian refuge island within Walters Road will be relocated 2m to the east to enable sufficient tracking space for an 8m medium rigid truck exiting the subject site from the vehicle crossing and turning right.
- j. The driveway and pedestrian access points are the only impervious surfaces that will be visible from the streetscape, as the car parking area is located behind the apartment blocks. This design response, in combination with the front yard achieving greater than 50% of the total area as landscape and planting ensures that the increased paved surface does not detract from the amenity values of the streetscape.
- k. The quality and quantity of landscaping elements in the car parking area and front yard complements the built form and incorporates a range of strategically placed specimen trees, screening shrubs, low level shrubs, ground covers and planted buffers between various thresholds, all of which assist in providing privacy, visual interest and residential amenity.
- l. The apartment layouts achieve outlook and outdoor living spaces of sufficient width and depth to ensure an appropriate level of onsite amenity for future residents is achieved.
- m. The effects of the combined retaining and fence will not affect privacy or passive surveillance over the street as it occurs in the south western corner of the site, in the car parking area which adjoins neighbouring properties.
- n. The length of fence over 2m is 3m along the southern boundary and 5.2m along the western boundary and is a maximum of 0.4m over the 2m height. The neighbouring land to the west is part of a right of way serving 43 and 45 Walters Road; therefore, the over height will not affect the amenity of persons associated with these properties. The neighbouring land to the south is the rear corner of 2/162 Porchester Road and is primarily used for car parking; therefore, the over height will not affect the amenity of persons associated with these properties.
- o. The one-bedroom units at 43m² and 44m² have been demonstrated to be functional and of sufficient size to cater for future resident's liveability through provision of appropriately dimensioned living, dining and kitchen areas and generous double bedrooms.
- p. The use of the AHIRB standard is not considered to detract from the streetscape or the amenity of adjoining neighbours and is reflective of an appropriate built form for the site. The units have been provided with balconies which overlook the streetscape enabling passive surveillance, front yard landscaping has been optimised as described above, car parking has been located to the rear of the buildings avoiding visual effects of car parking on the streetscape and pedestrian access have been provided to the development from three separate entry points. While there are some windows which are located along the southern boundary, these are located in excess of 4m from the adjoining boundary. Furthermore, the main outlook is via the balcony which overlooks Porchester Road.

- q. Following site remediation, (accordance with the SMP with site validation sampling), the risks to human health and the environment as a result of soil contamination will be adequately addressed.
 - r. With reference to s104(1)(ab), there are no specific offsetting or environmental compensation measures proposed or agreed to by the applicant to ensure positive effects on the environment.
3. In accordance with an assessment under s104(1)(b) of the RMA the proposal is consistent with the relevant statutory documents for the reasons outlined in section 2 of this decision. In this instance, the proposal specifically accords with the following:
- a. Objectives: E1.2(1)-(3) – Chapter E1 (Water quality and integrated management)
 - b. Policies: E1.3(1)-(2), (4)-(6), (9)-(16) – Chapter E1 (Water quality and integrated management)
 - c. Objectives: E12(2)(1) – Chapter E12 (Land Disturbance – District)
 - d. Policies: E12.3.(1) to E12.3(6) – Chapter E12 (Land Disturbance – District)
 - e. Objectives: E36.2(2) and (5) – Chapter E36 (Natural hazards and flooding)
 - f. Policies: E36.2.(3) (4), (13)-(15), (21), (25), and (26) – Chapter E36 (Natural hazards and flooding)
 - g. Objectives: H5.2 (1)-(3) – Chapter H5 (Residential – Mixed Housing Urban Zone)
 - h. Policies: H5.2(1)-(7) – Chapter H5 (Residential – Mixed Housing Urban Zone)
4. In accordance with an assessment under s104(1)(c) no other matters are considered relevant.
5. In terms of s 105 and s107 of the RMA, the proposal is acceptable for the following reasons:
- a. The discharge proposed is for onsite disposal and soakage into the peat soils – this allows for recharge of peat soils but also is required as there is no public connection for stormwater available to the site.
 - b. Contaminant removal has been provided for the whole site, thereby ensuring that effects on the receiving environment (being the soils and groundwater as applicable to the site) are managed in accordance with best practice.
 - c. The proposed system is required as there is no available stormwater connection. Specific devices have been chosen based on their known consistency to reduce contaminants in a manner which accords with best practice guidelines.
 - d. The alternative is to provide a new piped system which can connect to an existing system (which must be first tested to have capacity) or a new outfall to the Manukau Harbour for discharge. These options are not considered practicable in the case of this application.
 - e. Overall, the applicant has demonstrated that stormwater management regime will minimise the risk of discharges giving rise to:

- i. The production of any conspicuous oil / grease films, scums / foams, floatable or suspended materials;
 - ii. any conspicuous change in colour or visual clarity;
 - iii. any emission of objectionable odours;
 - iv. the rendering of fresh water unsuitable for consumption by farm animals; or
 - v. any significant adverse effects on aquatic life.
6. In the context of this restricted discretionary activity, where the objectives and policies of the relevant statutory documents were prepared having regard to Part 2 of the RMA, they capture all relevant planning considerations and contain a coherent set of policies designed to achieve clear environmental outcomes. They also provide a clear framework for assessing all relevant potential effects and there is no need to go beyond these provisions and look to Part 2 in making this decision as an assessment against Part 2 would not add anything to the evaluative exercise.
7. Overall, the proposal is acceptable and achieves the sustainable management purpose of Part 2 of the RMA avoiding, remedying, or mitigating adverse environmental effects as appropriate.

Conditions

Under sections 108 and 108AA of the RMA, this consent is subject to the following conditions:

Activity in accordance with application

1. The construction, and use, of the residential development and associated site preparatory work (i.e. the activity) shall be carried out in accordance with the documents and drawings and all supporting additional information submitted with the application, detailed below, and all referenced by the council as resource consent number BUN60343865.
 - Application Form, and Assessment of Effects prepared by Renee Fraser-Smith of Tollemache Consultants Limited, titled: "Resource consents for 42 Residential Units and associated works; 164-166 Porchester Road, Takanini" and dated August 2019.

Report title and reference	Author	Rev	Dated
Infrastructure Report 164-166 Porchester Road, Takanini Project No. 2094	CivilPlan Consultants	R001v3	30 July 2019
Transportation Assessment 164 to 166 Porchester Road, Takanini	Flow Transportation Consultants	B	25 July 2019

Urban Design Assessment Residential Development at 164 and 166 Porchester Road, Takanini	Transurban	-	26 July 2019
Detailed Site Investigation – Contamination 164A, 164B and 166 Porchester Road, Papakura Project No. 32709	Fraser Thomas	1	26 March 2019
Remedial Action Plan / Site Management Plan 164A, 164B and 166 Porchester Road, Papakura Project No. 32709	Fraser Thomas	1	1 October 2019
Geotechnical Memorandum 164-166 Porchester Road – Preload Design and Settlement Monitoring	Lander Geotechnical	-	8 May 2019
Geotechnical Report Proposed Three Storeys Apartment Development at 164 to 166 Porchester Road, Papakura Project No. J01138	Lander Geotechnical	A	16 April 2020

Plan title and reference	Author	Rev	Dated
2094-01-000 COVER SHEET	CivilPlan Consultants	C2	June 2019
2094-01-130 PROPOSED SITE LAYOUT	CivilPlan Consultants	C2	26 June 2019
2094-01-150 TOPOGRAPHICAL PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-200 PROPOSED CONTOUR PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-210 PRE-LOAD PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-220 ISOPACH PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-230 SEDIMENT AND EROSION CONTROL PLAN	CivilPlan Consultants	C2	26 June 2019

2094-01-235 SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 1	CivilPlan Consultants	C2	26 June 2019
2094-01-236 SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 2	CivilPlan Consultants	C2	26 June 2019
2094-01-370 VEHICLE CROSSING - PLAN AND DETAIL	CivilPlan Consultants	C2	26 June 2019
2094-01-400 PROPOSED DRAINAGE- OVERALL LAYOUT	CivilPlan Consultants	C3	29 July 2019
2094-01-401 PROPOSED DRAINAGE- DETAIL PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-410 EXISTING FLOODPLAIN - 100YR WITH CLIMATE CHANGE	CivilPlan Consultants	C2	26 June 2019
2094-01-411 PROPOSED FLOODPLAIN - 100YR WITH CLIMATE CHANGE	CivilPlan Consultants	C3	29 July 2019
2094-01-450 WASTEWATER LONGITUDINAL SECTIONS	CivilPlan Consultants	C2	26 June 2019
2094-01-475 PROPOSED DRAINAGE - STORMWATER360 - STORMFILTER DETAIL	CivilPlan Consultants	C2	26 June 2019
2094-01-476 PROPOSED DRAINAGE - STORMWATER360 - STORMFILTER CALCULATION	CivilPlan Consultants	C2	26 June 2019
2094-01-477 PROPOSED DRAINAGE - RECHARGE PIT DETAILS	CivilPlan Consultants	C2	26 June 2019
2094-01-490 PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 1	CivilPlan Consultants	C2	26 June 2019
2094-01-491 PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 2	CivilPlan Consultants	C2	26 June 2019

2094-01-495 WASTEWATER STANDARD DETAILS	CivilPlan Consultants	C2	26 June 2019
2094-01-500 PROPOSED WATERMAIN - LAYOUT PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-590 WATERMAIN STANDARD DETAILS	CivilPlan Consultants	C2	26 June 2019
A000 COVER SHEET	RCG	-	July 2019
A100 SITE & GROUND FLOOR PLAN	RCG	G	23 July 2019
A101 FIRST FLOOR	RCG	D	23 July 2019
A102 SECOND FLOOR	RCG	D	23 July 2019
A103 THIRD FLOOR	RCG	D	23 July 2019
A104 ELEVATIONS (WALTERS, PORCHESTER & SOUTH)	RCG	E	8 November 2019
A105 ELEVATIONS (WEST, SOUTH & CROSS SECTIONS)	RCG	E	8 November 2019
A106 OUTLOOK SPACE	RCG	B	23 July 2019
A107 LETTERBOXES	RCG	B	23 July 2019
A200 3D VIEWS (WALTERS & PORCHESTER CORNER)	RCG	A	23 July 2019
A201 3D VIEWS (PORCHESTER)	RCG	A	23 July 2019
A202 3D VIEWS (CAR PARK & BUILDING REAR)	RCG	A	23 July 2019
LA00 LANDSCAPE NOTES AND PLANTING SCHEDULES	Transurban	-	26 July 2019
LA01 SITE PLAN	Transurban	-	26 July 2019

LA02 SITE PLAN	Transurban	-	26 July 2019
LA03 DETAILS	Transurban	-	26 July 2019
LA04 PLANT INFORMATION	Transurban	-	26 July 2019
SHEET 1 OF 2 164 TO 166 PORCHESTER ROAD (RESIDENTIAL) VEHICLE TRACKING	Flow Transportation Consultants	F	24 September 2019
SHEET 2 OF 2 164 TO 166 PORCHESTER ROAD (RESIDENTIAL) VEHICLE TRACKING	Flow Transportation Consultants	F	24 September 2019
2094-00-SK04 VEHICLE TRACKING	CivilPlan Consultants	1	26 May 2020

Other additional information	Author	Rev	Dated
S92 response	Mike Smith of CivilPlan Consultants	-	21 November 2019
s92 response	Terry Church of Flow Transportation Consultants	-	24 September 2019
Email s92 response Fwd_ BUN60343865_ 164-166 Porchester Road, Takanini - Urban Design s92 response	Renee Fraser-Smith of Tollemache Consultants Limited	-	14 January 2020
s92 response	Transurban	-	14 January 2020 (received via email)
s92 response	John Lenihan of RCG	-	14 January 2020
s92 response	Alister Hood of CivilPlan Consultants	-	3 March 2020
s92 response	Shane Lander of Lander Geotechnical	-	12 March 2020
Email s92 response Re_ FW_ BUN60343865_ 164- 166 Porchester Road, Takanini - Update	Renee Fraser-Smith of Tollemache Consultants Limited	-	21 May 2020

Lapsing of consent

2. Under section 125 of the RMA, this consent lapses five years after the date it is granted unless:
 - a. The consent is given effect to; or

- b. The council extends the period after which the consent lapses.

Duration – DIS60343867

3. Stormwater diversion and discharge permit DIS60343867 shall expire 35 years from the decision date of this consent unless it has lapsed, been surrendered or been cancelled at an earlier date pursuant to the RMA.

Monitoring fee

4. The consent holder shall pay the council an initial consent compliance monitoring charge of \$660 (inclusive of GST), plus any further monitoring charge or charges to recover the actual and reasonable costs incurred to ensure compliance with the conditions attached to these consents.

Advice Note:

The initial monitoring deposit is to cover the cost of inspecting the site, carrying out tests, reviewing conditions, updating files, etc., all being work to ensure compliance with the resource consent. In order to recover actual and reasonable costs, monitoring of conditions, in excess of those covered by the deposit, shall be charged at the relevant hourly rate applicable at the time. The consent holder will be advised of the further monitoring charge. Only after all conditions of the resource consent have been met, will the council issue a letter confirming compliance on request of the consent holder.

Pre-commencement meeting

5. Prior to the commencement of the earthworks activity, the consent holder shall hold a pre-start meeting that:
 - a. is located on the subject site
 - b. is scheduled not less than 5 days before the anticipated commencement of earthworks
 - c. includes monitoring officers
 - d. includes representation from the contractors who will undertake the works and any suitably qualified professionals if required by other conditions

The following matters shall be discussed at the meeting:

- e. the erosion and sediment control measures
- f. the earthworks methodology
- g. shall ensure all relevant parties are aware of and familiar with the necessary conditions of this consent

The following information shall be made available at the pre-start meeting:

- h. Timeframes for key stages of the works authorised under this consent
- i. Resource consent conditions
- j. Geotechnical detail design report
- k. Settlement monitoring report
- l. *Remedial Action Plan/Site Management Plan* – 164a,164B and 166 Porchester Road, Papakura, prepared by Fraser Thomas Limited, dated 1 October 2019 ('RAP/SMP').

Advice Note:

To arrange the pre-start meeting required by condition 4 please contact the Team Leader Monitoring (South) to arrange this meeting or email monitoring@aucklandcouncil.govt.nz . The conditions of consent should be discussed at this meeting. All information required by the council and listed in that condition should be provided 2 days prior to the meeting.

Architectural design plans

- 6. Prior to the approval of Building Consent, a finalised set of architectural detail drawings and materials specifications shall be submitted to Council's Team Leader Monitoring (South) for written certification. The information shall include the following:
 - a. details of the building's façade treatment / architectural features;
 - b. materials schedule and specification, sample palette of materials, surface finishes, and colour schemes (including colour swatches) referenced on the architectural elevations; and
 - c. external / rooftop services / plant, and visual.

The finalised set of drawings shall ensure that the building's proposed architectural treatment and finished appearance is consistent with the plans and information referenced at condition 1. All works shall then be carried out with the details certified by council, and thereafter retained and maintained, to the satisfaction of Team Leader Monitoring (South).

Advice Note:

As part of the condition monitoring process, Council's monitoring inspectors will liaise with members of the Council's Auckland Design Office to ensure that the submitted details are consistent with the approved plans and information.

Finalised landscape design drawings, specifications and maintenance requirements

7. Prior to the commencement of apartment building construction on site, the consent holder shall provide to the Team Leader Monitoring (South) for approval, a finalised set of detailed landscape design drawings and supporting written documentation which have been prepared by a landscape architect or suitably qualified professional. The submitted information shall be consistent with the consented landscape concept plans prepared by TransUrban dated 26.07.2019 and, at a minimum, shall include landscape design drawings, specifications and maintenance requirements including:
 - a. An annotated planting plans which communicate the proposed location and extent of all areas of planting
 - b. Annotated cross-sections and/or design details with key dimensions to illustrate that adequate widths and depths are provided for any planter boxes / garden beds
 - c. A plant schedule based on the submitted planting plans which details specific plant species, plant sourcing, the number of plants, height and/or grade (litre) / Pb size at time of planting, and estimated height / canopy spread at maturity
 - d. Details of draft specification documentation for any specific drainage, soil preparation, tree pits, staking, irrigation and mulching requirements
 - e. An annotated pavement plan and related specifications, detailing proposed site levels and the materiality and colour of all proposed hard surfacing
 - f. An annotated plan and related specifications which confirm the location and type of any seats, bins, lights, fences, walls and other structural landscape design elements
 - g. A landscape maintenance plan (report) and related drawings and specifications for all aspects of the finalised landscape design, including in relation to the following requirements:
 - i. Irrigation
 - ii. Weed and pest control
 - iii. Plant replacement
 - iv. Inspection timeframes
 - v. Contractor responsibilities

The finalised landscape design shall be consistent with the landscape design intent / objectives identified in the conceptual plans and information referenced at condition 1 and confirm responsibilities for ongoing maintenance requirements.

Implementation and maintenance of approved landscape design

8. Prior to the occupation of the apartment buildings or within the first planting season following occupation, the consent holder shall implement the landscape design which has been approved by the council under condition 6 and thereafter retain and maintain this landscape (planting and pavement) in perpetuity to the satisfaction of the Team Leader Monitoring (South) in accordance with the maintenance plan which has been approved under condition 7.
9. All planting located within the front yard of this development (excluding specimen trees) should be maintained to a maximum height of 1.5m in order to ensure a high degree of activation and surveillance is maintained over the public realm.

Advice Note:

It is recommended that the consent holder consider a minimum three-year management / maintenance programme for plant establishment and provide, in particular, details of maintenance methodology and frequency, allowance for fertilising, weed removal / spraying, replacement of plants, including specimen trees in case plants are severely damaged / die over the first five years of the planting being established and watering to maintain soil moisture. As part of the approval process, the council's monitoring team will liaise with landscape architects from the council's Auckland Design Office to ensure that the submitted drawings and related information are consistent with the originally consented landscape concept plan(s).

Lighting plans

10. Prior to the lodgement of Building Consent, the consent holder shall provide a Lighting Plan and Certification/ Specifications prepared by a qualified Lighting Engineer, to Team Leader Monitoring (South). The purpose of this condition is to provide adequate lighting for the safety of people residing, working or visiting the premises and its immediate environs outside of daylight hours. The Lighting Plan shall:
 - a. include all accessible areas of the premises where movement of people are expected. Such locations include, but are not limited to the shared accessways, building entrances, communal carpark and footpaths.
 - b. include proposed locations, lux levels and types of lighting (i.e. manufacturer's specifications once a lighting style has been determined) and any light support structures required to control timing, level of lighting, or to minimise light spill, glare, and loss of night time viewing.
 - c. Demonstrate compliance with the relevant standards in E24.6.1 Lighting of the Auckland Unitary Plan (Operative in Part).

- d. Demonstrate compliance with the AS/NZS 1158 P requirements and clearly specify what P Category the lighting design will achieve. The selection criteria for the chosen lighting category should also be presented (i.e. pedestrian/cycle activity, risk of crime etc.)
- e. Demonstrate the vertical illuminance by means of lux contours or a similar method to assess light spill on neighbouring properties (where relevant). The limits of the vertical illuminance should comply with Auckland Unitary Plan (Operative in Part) Standard E24.6.1.3.
- f. Include an executive summary (i.e. non-technical summary) of the above information that outlines the relevant requirements to their application and their design response to them.

The finalised design details certified by the qualified Lighting Engineer shall be established prior to the development hereby consented being first occupied, and thereafter retained and maintained, to the satisfaction of the Team Leader Monitoring (South).

Advice Note:

The purpose of this condition is to ensure that adequate lighting is provided to frequently used areas within the proposed development for the safety of users. Adequate lighting is the amount of lighting at eye level for a person with average eyesight so they can identify any potential threat approaching them from at least a 15-metre distance. Council's monitoring officers will liaise with members of the Council's Auckland Design Office to ensure that the submitted details are consistent with the above condition.

Contamination

- 11. Earthworks shall be undertaken in accordance with *Remedial Action Plan/Site Management Plan* – 164a, 164B and 166 Porchester Road, Papakura, prepared by Fraser Thomas Limited, dated 1 October 2019 ('RAP/SMP'). Any variations to the RAP/SMP shall be submitted to the Team Leader Monitoring (South) for approval.

Advice Note:

The Council acknowledges that the Remedial Action Plan/ Site Management Plan is intended to provide flexibility of the management of the works. Accordingly, the plan may need to be updated. Any updates should be limited to the scope of this consent and be consistent with the conditions of this consent. If you would like to confirm that any proposed updates are within scope, please contact the Team Leader Monitoring (South).

- 12. During earthworks all necessary action shall be taken to prevent dust generation and sufficient water shall be available to dampen exposed soil, and/or other dust suppressing measures shall be available to avoid dust formation. The consent holder shall ensure that dust management during the excavation works generally complies with the *Good Practice Guide for Assessing and Managing Dust* (Ministry for the Environment, 2016).

13. In the event of the unexpected discovery of contamination during earthworks which has not been previously identified, the consent holder shall immediately cease the works in the vicinity of the contamination, notify the Team Leader Monitoring (South), and engage a Suitably Qualified and Experienced Practitioner (SQEP) to assess the situation (including possible sampling and testing) and decide in conjunction with a council compliance officer on the best option for managing the material.
14. Any excavated material that is not re-used on site shall be disposed of at an appropriate facility licensed to accept the levels of contamination identified.
15. The consent holder shall ensure that the imported soil complies with condition 30 of this consent.
16. Within three (3) months of the completion of earthworks on the site, a Site Validation Report (SVR) shall be provided to the Team Leader Monitoring (South). The SVR shall be prepared by a Suitably Qualified and Experienced Professional (SQEP) in accordance with the Contaminated Land Management Guidelines No. 1: Reporting on Contaminated Sites in New Zealand (Ministry for the Environment, 2011) and contain sufficient detail to address the following matters:
 - a. A summary of the works undertaken, including the location and dimensions of the excavations carried out and the volume of soil excavated;
 - b. Details and results of any testing, including validation testing, undertaken and interpretation of the results in the context of the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health and the AUP(OP);
 - c. Copies of the disposal dockets for any material removed from the site;
 - d. Records of any unexpected contamination encountered during the works and response actions, if applicable;
 - e. Conditions of the final site ground surface and details of any validation sampling undertaken on materials re-used on site or imported to site; and
 - f. A statement certifying that all works have been carried out in accordance with the requirements of the consent.

Advice Notes:

If you are demolishing any building or structure that may have asbestos containing materials (ACM) in it:

- *You have obligations under the relevant regulations for the management and removal of asbestos, including the need to engage a Competent Asbestos Surveyor to confirm the presence or absence of any ACM.*
- *Work may have to be carried out under the control of person holding a WorkSafe NZ Certificate of Competence (CoC) for restricted works.*
- *If any ACM is found, removal or demolition will have to meet the Health and Safety at Work (Asbestos) Regulations 2016.*

Information on asbestos containing materials and your obligations can be found at www.worksafe.govt.nz.

If ACM is found on site following the demolition or removal of the existing buildings, you may be required to further remediate the site and carry out validation sampling. Dependent on the amount of soil disturbance, a further consent application may be required.

Geotechnical

17. All earthworks on the site are to be carried out in accordance with the requirements of the Geotechnical Investigation report titled 'Geotechnical Investigation for Proposed Three Storey Apartment Development at 164 to 166 Porchester Road, Papakura' ref J01138- rev A, dated 16/04/2020.
18. An Engineer's certificate and Geotechnical Completion Report (GCR) prepared by an appropriately qualified engineering professional responsible for supervising the works must be provided for written certification by the Team Leader Regulatory Engineering South, confirming that the works have been completed in accordance with the approved plans prior to the occupation of the apartment buildings. The GCR is to cover the following (as a minimum):
 - a. That the works were undertaken in accordance with NZS4431:1989 Code of Practice for Earth Fill for Residential Subdivisions and the site-specific designs outlined in the Geotechnical Investigation Report; and
 - b. Recommendations, confirming adequate factors of safety, and including as-built records of earthworks, groundwater levels and drainage; and
 - c. Confirmation that settlement criteria and/or ground improvement as defined in the Geotechnical Investigation Report and Detail Geotechnical Design Report have been met prior to commencement of house construction; and
 - d. The extent to which settlement of the site is expected and its impact on future house/structure construction; and

- e. Include a statement of professional opinion for the suitability of the site for residential development; and
- f. Details of all earthworks and as-built plans including, the depth, extent of fill and drainage, subsoil drains, shear keys and soil reinforcement. As-built plans should be certified by a licensed Cadastral Surveyor; and
- g. Specific requirements for future earthworks, building foundations, retaining walls and other works which may occur in each lot. This shall include requirements for works which may take place within the vicinity of subsoil drains; and
- h. The ongoing maintenance requirements of landowners to ensure efficient functioning of the privately-owned subsoil drains and subsoil drain outlets. This shall include a requirement to provide CCTV monitoring of subsoil drain outlets to Council; and
- i. If subsoil drains are installed to a 'zero maintenance' standard, the Completion Report shall include requirements to avoid damage to the subsoil drains including the extent to which modifications can be made to the capping (covering) at ground level; and
- j. Any related matters identified in other conditions of this consent.

Advice Note:

Auckland Council may appoint an independent geotechnical engineer to observe critical aspects of the construction works and/ or peer review the GCR, at the Consent Holder's expense. Critical observations are to be agreed between the independent geotechnical engineer and the Supervising Engineer prior to construction commencing.

- 19. Prior the commencement of the construction of the apartment buildings on site, a detailed Geotechnical Design Report including specific foundation design shall be prepared and submitted to the Team Leader Monitoring (South) for written certification.
- 20. Detailed assessment of the expansive site class by specific laboratory tests in accordance with published information (AS2870:2011 or BRANZ 120A) shall be undertaken as a part of Geotechnical Completion Report or Geotechnical Design Report.

Advice Note:

The approved Detail Geotechnical Design Report shall be included as part of the building consent support documents.

Supervision and certification of geotechnical works

- 21. The construction of permanent building platform, settlement mitigation measures, building foundations, pavements and floor slabs and the placement and compaction of fill material shall be supervised by a suitably qualified engineering professional. In supervising the works, the suitably qualified engineering professional shall ensure that they are constructed and

otherwise completed in accordance with the engineering plans and geotechnical recommendations, relevant engineering codes of practice and detailed plans forming part of the application.

22. Certification from a suitably qualified engineering professional responsible for supervising the works shall be provided to the Team Leader Monitoring (South), confirming that the works have been completed in accordance with Geotechnical Investigation report and Detail Geotechnical Design report, within ten (10) working days following completion. Written certification shall be in the form of a geotechnical completion report, or any other form acceptable to the council.

Groundwater

23. An on-site monitoring system is to be established by the consent holder to:
- a. determine if the groundwater levels post-construction are similar to the pre-construction levels.
 - b. demonstrate that groundwater equilibrium has been maintained such that no detrimental effect to soil structures/ strength or stability has occurred.
 - c. the required groundwater monitoring systems, and devices are to be established across the site prior occupation of the apartment buildings.
 - d. an on-site groundwater monitoring plan and methodology is to be submitted to the Council for approval prior to commencement of the works and prior to occupation of the apartment buildings.

The monitoring is to be carried out on a regular basis throughout the development phases starting a minimum of two months prior to commencement of works on the site, through the development construction phases, post-construction and continuing for sufficient time to confirm that the natural ground water levels have stabilised. The monitoring plan must take account of seasonal changes so that levels are compared over the same calendar period to avoid natural variations influencing the interpretation of the results.

24. Geotechnical/geo-hydrologist certification is to be provided to the Team Leader Monitoring (South) prior to occupation of the apartment buildings. This must include confirmation that:
- a. the groundwater levels have been maintained or re-established; and
 - b. there were no significant groundwater fluctuations that may have resulted in irreversible shrinkage of the organic component in the peaty soils, and that no detrimental effect to soil structure/strength or stability has occurred and will not occur in the future.

Ground settlement

25. Prior to placement of any fill on-site the consent holder shall:

- a. Submit to Council for written certification a settlement monitoring and contingency plan which shall detail settlement monitoring to be undertaken during and following construction and measures to be taken should settlements exceed expected values.
 - b. Install specific settlement monitoring points and, if applicable, undertake structural survey of surrounding buildings to ensure settlement is as anticipated and there are no detrimental effects on neighbouring land & buildings
26. Settlement shall be monitored at the site for a period commencing from the initial baseline measurements prior to construction, until the time when all the specific design criteria has been met as per the Geotechnical Consultant's report and subject to Council's approval.
27. Geotechnical certification that the settlement criteria have been met is to be provided to Council prior submitting the Geotechnical Completion Report.
28. No building works are to commence until the above requirements of conditions 24, 25 and 26 have been certified by Auckland Council's Geotechnical Engineer.

Ongoing monitoring

29. After the CCC has been granted for the two apartment blocks, a post housing construction geotechnical groundwater and settlement monitoring plan is to be prepared and submitted to Team Leader Regulatory Engineering South for approval. The post housing construction on-site monitoring system is to be established to:
- a. provide evidence that ground settlement has attenuated; and
 - b. demonstrate that groundwater level fluctuations resulting from works carried out for the development have been less than significant and are such that no detrimental effect to soil structures/ strength or stability has occurred.
 - c. All monitoring sites are to be identified on a plan clearly showing their location and set out positions in relation to property boundaries for ease of locating in the future. Attached to the plan is to be a copy of the monitoring device design, an as-built detail drawing and the maintenance plan for each device. Each of the monitoring sites is to be provided with adequate protection to ensure they are not damaged during construction works and/or over their intended life span.
 - d. If the monitoring site is on private property a consent notice is to be registered on the Certificate(s) of Title detailing the location, the need for protection of the systems and the need for ongoing monitoring by the consent holder.
 - e. Each monitoring site is to be monitored and readings recorded at regular intervals throughout the development period and for a minimum five-year period commencing from the date CCC being issued. The records are to be provided to the Council to the satisfaction of Team Leader Regulatory Engineering South on completion of each year (or if on request then monthly). If at any stage the results show any indication of settlement or the possibility of detrimental changes in water levels the consent holder must immediately advise the Council, the geotechnical engineer and the consent holder's engineer.

- f. If after a three-year monitoring period it is demonstrated that no further monitoring is required, to the satisfaction of the Council's Manager Regulatory Engineering South monitoring activities may cease.
30. The Council may appoint an independent geotechnical engineer to observe critical aspects of the construction works (e.g. geotechnical drainage installations, shear key excavations, retaining walls, capping requirements for rock if encountered in deep cuts, etc.), and/ or peer review the Geotechnical Completion Report, at the Consent Holder's expense. Critical observations are to be agreed between the independent geotechnical engineer and the Supervising Engineer prior to construction commencing.

Quality of fill

31. All imported fill used shall:
- a. comply with the definition for 'cleanfill' in the Ministry for the Environment publication 'A Guide to the Management of Cleanfills' (2002)
 - b. be solid material of a stable, inert nature; and
 - c. not contain hazardous substances or contaminants above recorded natural background levels of the receiving site.

General sediment control

32. All earthworks shall be managed to minimise any discharge of debris, soil, silt, sediment or sediment-laden water beyond the subject site to either land, stormwater drainage systems, watercourses or receiving waters. In the event that a discharge occurs, works shall cease immediately, and the discharge shall be mitigated and/or rectified to the satisfaction of the Team Leader Monitoring (South).

Advice Note:

In accordance with condition 32 all earthworks shall be undertaken to ensure that all potential sediment discharges are appropriately managed. Such means and measures may include:

- *Catchpit protection*
- *run-off diversions*
- *sediment retention ponds*
- *silt and sediment traps*
- *decanting earth bunds*
- *silt fences*

During excavation, the ingress and accumulation of surface run-off water and/or perched groundwater can be minimised by:

- *maintaining a waterproof cover over any excavation trenches and pits outside of working hours,*
- *diversion of surface water flow around the works area, and*
- *regular disposal of the water into an appropriate sediment control device, if ponding occurs within the excavation.*

Please note that the diversion of stormwater and/or groundwater may require a consent in accordance with Chapters 5 and 6 of the Auckland Council Regional Plan (Air, Land and Water) or the Proposed Auckland Unitary Plan.

It is recommended that you discuss any potential measures with the council's monitoring officer who may be able to provide further guidance on the most appropriate approach to take. Please contact the Council's Team Leader Monitoring (South) on monitoring@aucklandcouncil.govt.nz for more details. Alternatively, please refer to "Auckland Regional Council, Technical Publication No. 90, Erosion & Sediment Control Guidelines for Land Disturbing Activities in the Auckland Region".

Construction noise and vibration management plan

33. At least five days prior to the commencement of construction and / or any earthworks activity, a Construction Noise and Vibration Management Plan ('CNVMP') shall be prepared by a suitably qualified person and submitted to the Team Leader Monitoring (South) for review and written certification. At a minimum, the CNVMP shall address the measures in Annex E of NZS 6803:1999 "Acoustics – Construction Noise". The approved CNVMP shall be implemented throughout the construction phase of the project.

The plan may be updated as necessary to the satisfaction of the Team Leader Monitoring (South). The objectives of the CNVMP shall be:

- a. Applicable site noise and vibration criteria.
- b. Programme of works and hours of operation.
- c. Identification of surrounding noise and/or vibration sensitive receivers.
- d. An assessment of vibration from the proposed construction activities.
- e. The proposed neighbour liaison approach, including communication with occupants of all buildings within 50 m of the site of the works in writing at least ten (10) days prior to the commencement of construction and / or any earthworks activity activities on site. The written advice shall set out:
 - (i) a brief overview of the construction works
 - (ii) the working hours and expected duration

- (iii) all mitigation measures to be implemented
 - (iv) the procedure for recording concerns/complaints regarding noise and vibration
 - (v) the procedure for noise and vibration monitoring where concerns are raised by receivers
 - (vi) contact details for site personnel for any concerns regarding noise and vibration
- f. Details of the management and mitigation measures required to comply with the relevant noise and vibration criteria.
 - g. A requirement to undertake pre- and post-building condition surveys of neighbouring buildings/structures with the potential to receive vibration levels in excess of the limits in AUP rule E25.6.30, assuming access is granted by the owner/occupier.
 - h. The requirement to measure construction vibration at the most exposed surrounding property(ies), as identified in Item (g). Where the vibration levels exceed or are likely to exceed the DIN 4150-3 guideline values then the construction activity shall only proceed if there is appropriate monitoring of vibration levels and effects on those buildings at risk of exceeding the DIN 4150-3 guideline values, by suitably qualified experts.

Minimum floor level

34. The approved minimum floor level of RL 21.1m shall apply to both apartment buildings unless a lower floor level is approved by the Auckland Council that has been subject to specific engineering design.

All levels are in terms of the Lands & Survey Auckland Datum, 1946 and in accordance with the approved plans referenced in condition 1.

Certification by Registered Professional Surveyor shall be submitted to the Team Leader Monitoring (South) for compliance of the above condition.

Engineering plans (major)

35. Prior to commencement of any public works related on site, the consent holder shall provide design plans and specifications detailing the following works required in respect to this Land use, to the satisfaction of the Team Leader Regulatory Engineering South.

The engineering plans submitted for approval shall detail all works associated with, and be in accordance with current Council Engineering Standards, including but not limited to;

- a. Roothing Works
- b. Stormwater Reticulation
- c. Wastewater Reticulation

Advice Notes:

The engineering plan application forms including fees can be found at the following Auckland Council website:

<https://www.aucklandcouncil.govt.nz/building-and-consents/engineering-approvals/Pages/default.aspx>

In the former Papakura District water and wastewater services are provided by Veolia under a Franchise Agreement. The consent holder deals directly with Veolia in relation to all water and wastewater servicing matters throughout the development process. Compliance with Veolia's water and wastewater requirements (as evidenced by the issuing by Veolia of a Compliance Certificate) is necessary prior to the occupation of the apartment buildings. (papakura.developments@veolia.com)

Public road

36. The consent holder shall apply for Engineering Plan Approval for relocating the traffic island 2m away from the current location to ensure that vehicle exiting the site will not encroach onto the existing traffic island. The works to relocate and complete the new traffic island shall be completed prior to occupation of the apartment buildings.

Advice Notes:

The line marking changes shall require resolution process approval.

In the case that the proposed modification of Auckland Transport's assets as part of the applicant's proposal require further resolutions, this will be completed by the applicant following Auckland Transport's Resolution & Approval Reports Guidebook (Transport Controls Team, June 2015).

37. The traffic Island works shall be constructed in accordance with the approved Engineering Plans to the satisfaction of the Team Leader Regulatory Engineering South prior to occupation of the apartment buildings.
38. An Engineering Completion Certificate certifying that all the ancillary structures on the roads have been constructed in accordance with the approved Engineering Plans prior to occupation of the apartment buildings.

Vehicle crossings and driveways

39. The driveway shall be formed, paved and drained to the Council's current Local Engineering Standards, including the provision of stormwater catch pits and/or slot drains, within the boundaries of the common area and when necessary the provision of kerbing or other mechanism to prevent water flowing on to other property including footpaths.

40. Certification shall be provided by a Chartered Professional Engineer confirming the construction and stormwater runoff from the driveway is in accordance with the Council's current Local Engineering Standards prior to occupation of the apartment buildings.
41. The vehicle crossing shall be constructed in accordance with Auckland Transport Code of Practice Standards GD017A-Residential Vehicle Crossing for more than 4 dwellings. The berm shall be re-instated to Council's "Code Of Practice For Working In The Road." (<https://at.govt.nz/about-us/working-on-the-road/>).

Advice Note:

A vehicle crossing permit is required to be obtained from Auckland Transport prior to the construction of the vehicle crossing on existing public roads. See Auckland Transport's website <https://at.govt.nz/about-us/working-on-the-road/vehicle-crossing-application/> for more information.

Private stormwater system

42. The stormwater detention system shall have a minimum of 136m³ storage capacity to mitigate the stormwater run-off generated from all impervious area prior discharge to the recharge pits on site. Detail design of the tank and calculations shall be provided at building consent stage.

Advice Notes:

Due to high ground water in the Peat soil during winter, the rainsmart module shall be designed to prevent ground water seepage which will reduce the detention capacity of this detention system. Buoyancy also a major consideration for this design

Recharge pits

43. A residential recharge design and pit maintenance guide is to be created by the consent holder. This guide is to also include standard detail drawings for recharge pit with recommendations in terms of impervious area discharge before overflow to the public stormwater reticulation system.

A producer statement 1 (PS1) and geotechnical recommendation memo are to be included as part of the building consent application due to specific design of recharge pit.

44. This condition shall be complied with to the satisfaction of the Team Leader Monitoring (South) prior to occupation of the apartment buildings.

Water and wastewater certification

45. A Completion Certificate certifying that all public water and wastewater and individual including connections have been constructed in accordance with the approved Engineering Plan and shall be provided to Team Leader Monitoring (South) prior to occupation of the apartment buildings.

Advice Note:

In the former Papakura District, water and wastewater services are provided by Veolia under a Franchise Agreement. The consent holder shall deal directly with Veolia for approval in relation to all water and wastewater servicing matters throughout the development. Compliance certification shall be obtained from Veolia prior to connection to any Veolia network. (papakura.developments@veolia.com)

Wastewater and water supply capacity

46. The consent holder shall demonstrate that there is adequate wastewater and water supply capacity to services the proposal development at Engineering Plan approval state to the satisfactory of the Team Leader Regulatory Engineering South.

Any required upgrade for the existing public wastewater and water supply to services this proposed development shall be fully funded by the consent holder.

Electricity supply

47. The consent holder shall provide and install an underground electricity supply system to service the apartment buildings as shown on the approved plans. The system shall be installed in accordance with the requirements of relevant network utility operator. The consent holder shall provide certification from the network utility operator to Team Leader Monitoring (South), that the system has been installed in accordance with their requirements prior to occupation of the apartment buildings.

Telecommunications services

48. The consent holder shall provide and install an underground telecommunications system to service the apartment buildings generally as shown on the approved plans. The system shall be installed in accordance with the relevant network utility operator. The consent holder shall provide certification from the network utility operator to Team Leader Monitoring (South), that the system has been installed in accordance with their requirements prior to occupation of the apartment buildings.

Advice notes

1. Any reference to number of days within this decision refers to working days as defined in s2 of the RMA.
2. For the purpose of compliance with the conditions of consent, “the council” refers to the council’s monitoring inspector unless otherwise specified. Please email monitoring@aucklandcouncil.govt.nz to identify your allocated officer.
3. For more information on the resource consent process with Auckland Council see the council’s website: www.aucklandcouncil.govt.nz. General information on resource consents, including making an application to vary or cancel consent conditions can be found on the Ministry for the Environment’s website: www.mfe.govt.nz.

4. *If you disagree with any of the above conditions, and/or disagree with the additional charges relating to the processing of the application(s), you have a right of objection pursuant to sections 357A and/or 357B of the Resource Management Act 1991. Any objection must be made in writing to the council within 15 working days of your receipt of this decision (for s357A) or receipt of the council invoice (for s357B).*
5. *The consent holder is responsible for obtaining all other necessary consents, permits, and licences, including those under the Building Act 2004, and the Heritage New Zealand Pouhere Taonga Act 2014. This consent does not remove the need to comply with all other applicable Acts (including the Property Law Act 2007 and the Health and Safety at Work Act 2015), regulations, relevant Bylaws, and rules of law. This consent does not constitute building consent approval. Please check whether a building consent is required under the Building Act 2004.*
6. *This consent approval does not authorise the construction of the necessary engineering works shown on the plans. A separate Engineering Plan Approval is required to undertake any engineering works related to the proposal.*
7. *A Corridor Access Request, (CAR), is required for all works undertaken within the 'road corridor'. See Auckland Transport's website <https://at.govt.nz/about-us/working-on-the-road/corridor-access-requests/#applycar> for more information.*

Delegated decision maker:

Name: Colin Hopkins
Title: Principal Project Lead
Premium, Resource Consents

Signed:



Date: 06 July 2020

BUN60343865

Approved Resource Consent Plan

06/07/2020



DRAWLING LIST	
Sheet Number	Sheet Name
A000	COVER SHEET
A100	SITE & GROUND FLOOR PLAN
A101	FIRST FLOOR
A102	SECOND FLOOR
A103	THIRD FLOOR
A104	ELEVATIONS
A105	ELEVATIONS
A106	OUTLOOK SPACE
A107	LETTERBOXES
A200	3D VIEWS
A201	3D VIEWS
A202	3D VIEWS



LOCATION PLAN

PORCHESTER ROAD

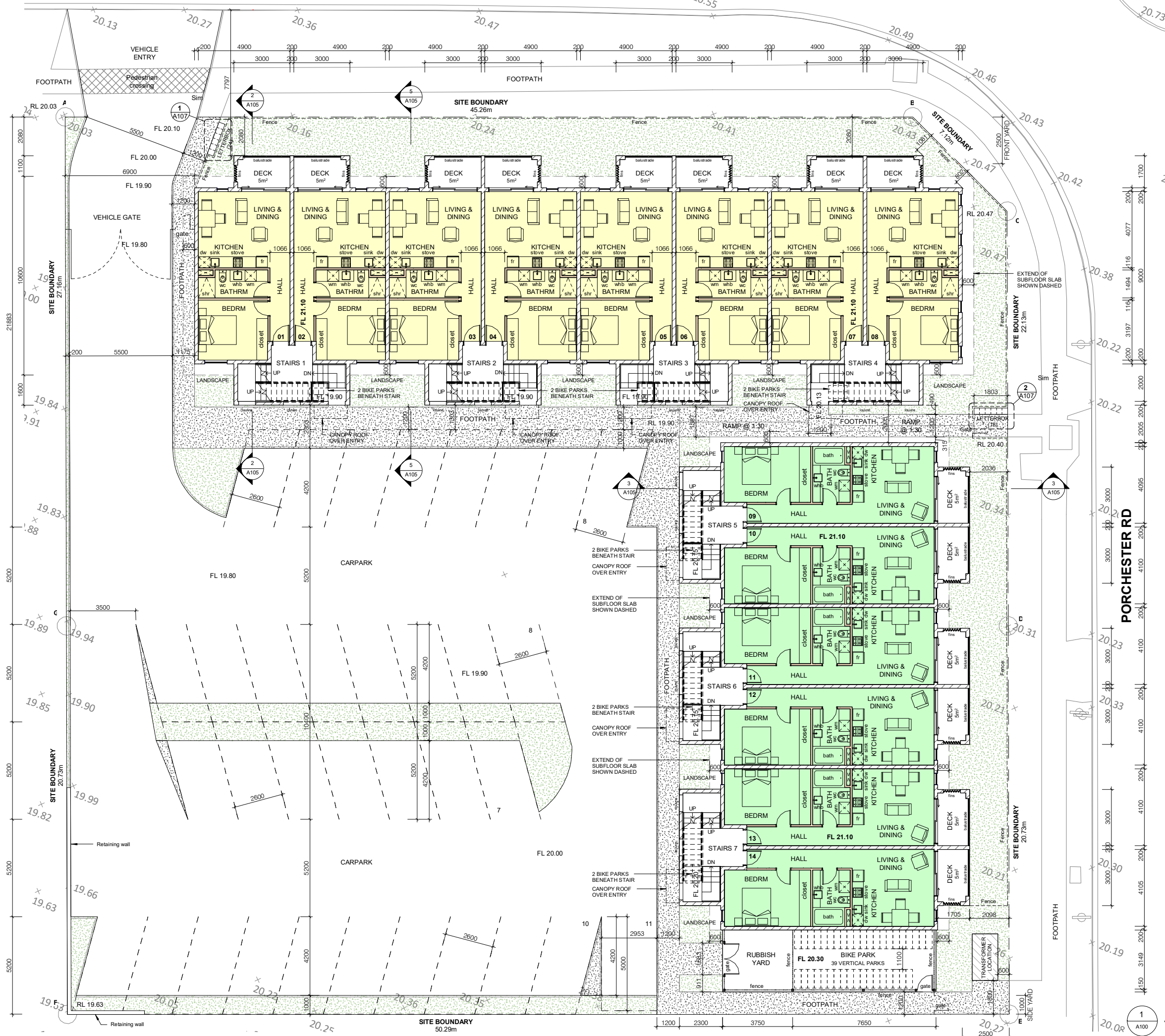
**164-166 PORCHESTER RD
TAKANINI**

**ALDA INVESTMENTS
LTD**



Constructive
thinking,
delivered.

RCG PROJECT NO: 19001
ISSUE DATE: JUL 2019
ISSUED FOR: RESOURCE CONSENT



KEY LEGEND

Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53

AREAS

Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%

NOTES:

All work shown or implied to be carried out in strict accordance with NZS3604, the NZ Building Code and Local Body requirements.

Refer to consultant drawings for specific details on systems flashing, other such services where not specified in these documents.

All drawings to be read in conjunction with the architectural specification and structural engineers drawings and specification.

Refer to Fire Safety Design Report for fire safety requirements and precautions.

The plumbing and drainage installation must comply with AS/NZ 3500.2003, the NZ Building Code and requirements of the local Territorial Authority.

Timber framing treatments to DBH, NZS3602/NZS3604 and NZBC.

Refer to consultant drawings for specific details on systems flashing and other such services where not specified in these documents.

DISCLAIMER:

This document has been prepared for the sole use of RCG's client in accordance with RCG's specific engagement. It is not to be used, or relied upon for any other purpose, or by any other person without RCG's written approval.

NOT FOR CONSTRUCTION

CONSULTANTS REFERENCE NOTE

Refer to specific consultant documentation for relevant information on:

- Civil Engineering
- Structural Engineering
- Plumbing & Drainage
- Fire
- Landscape
- Traffic

LETTERBOX NOTES

- Allow for two Letterbox units, one per Building Block: North Block with 24 units, West Block with 18 units.
- Letterbox numbers to correspond to unit numbers.
- Allow for 190(h)x250(w)x400(d) Contemporary Apartment Box Mailboxes to be built into block wall.

Revisions

Ref	Description	Date
D	FOR CONSENT	03.07.19
E	FOR CONSENT	08.07.19
F	FOR CONSENT	19.07.19
G	RESOURCE CONSENT	23.07.19

rcg Constructive thinking, delivered.

RCG, 11 Cheshire Street, Parnell, Auckland 1052
PO Box 137313, Auckland 1151, New Zealand
T: +64 9 303 1501 W: rcg.co.nz

PORCHESTER ROAD

ADDRESS:
**164-166 PORCHESTER RD
TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

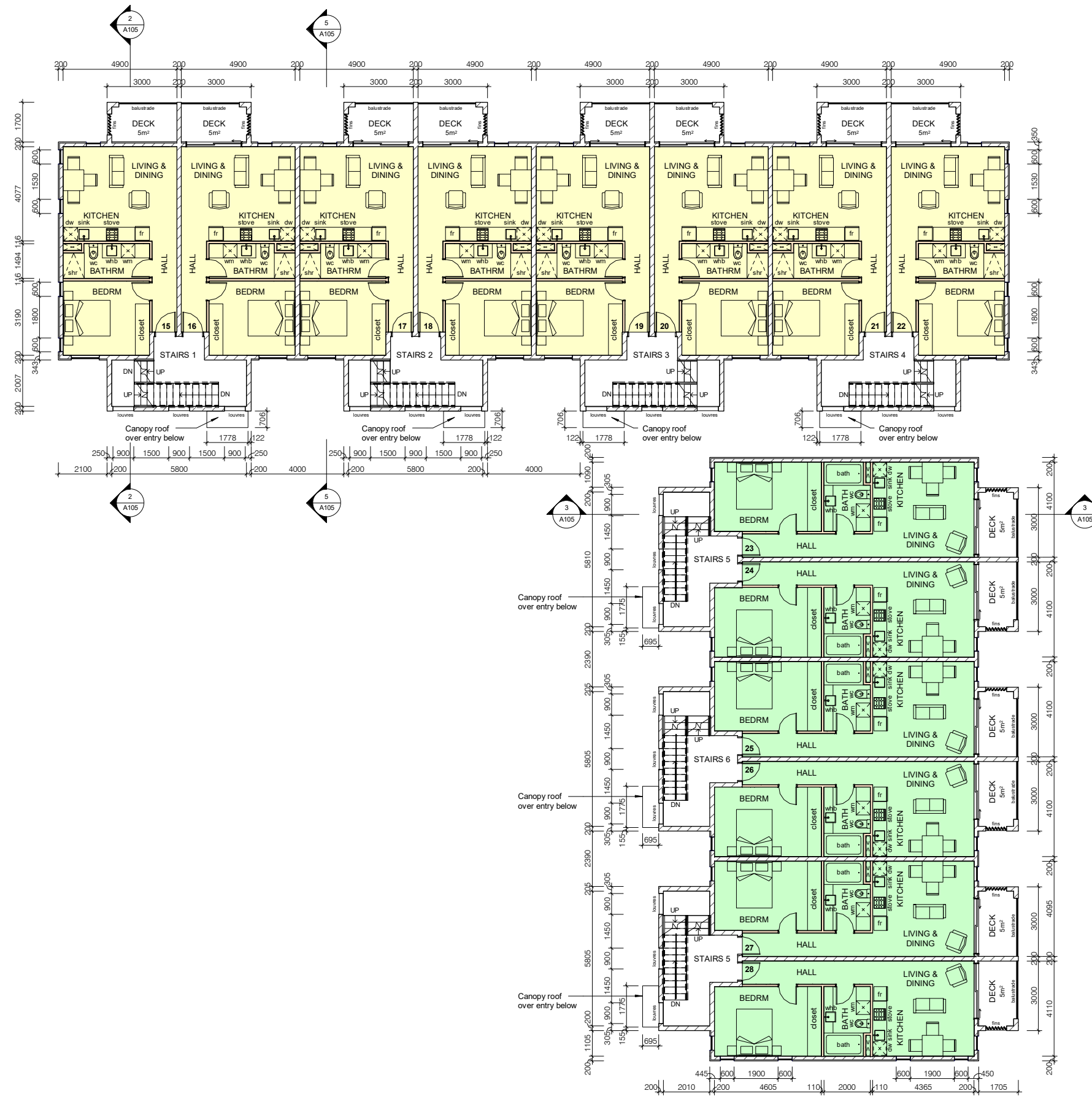
SHEET NAME:
SITE & GROUND FLOOR PLAN

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A100** REV: **G**

SITE & GROUND FLOOR
1 : 100

VERIFY ALL DATA ON SITE BEFORE COMMENCING WORK
COPYRIGHT © 2019 RCG. ALL RIGHTS RESERVED
23/07/2019 11:53 AM



1 FIRST FLOOR
A101 1:100

KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
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Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53
AREAS		
Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%
CONSULTANTS REFERENCE NOTE		
Refer to specific consultant documentation for relevant information on:		
<ul style="list-style-type: none"> • Civil Engineering • Structural Engineering • Plumbing & Drainage • Fire • Landscape • Traffic 		

NOTES:

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Revisions		
Ref	Description	Date
A	UPDATED DRAWINGS	29.04.19
B	FOR CONSENT	17.06.19
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19



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PROJECT:

PORCHESTER ROAD

ADDRESS:
**164-166 PORCHESTER RD
TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

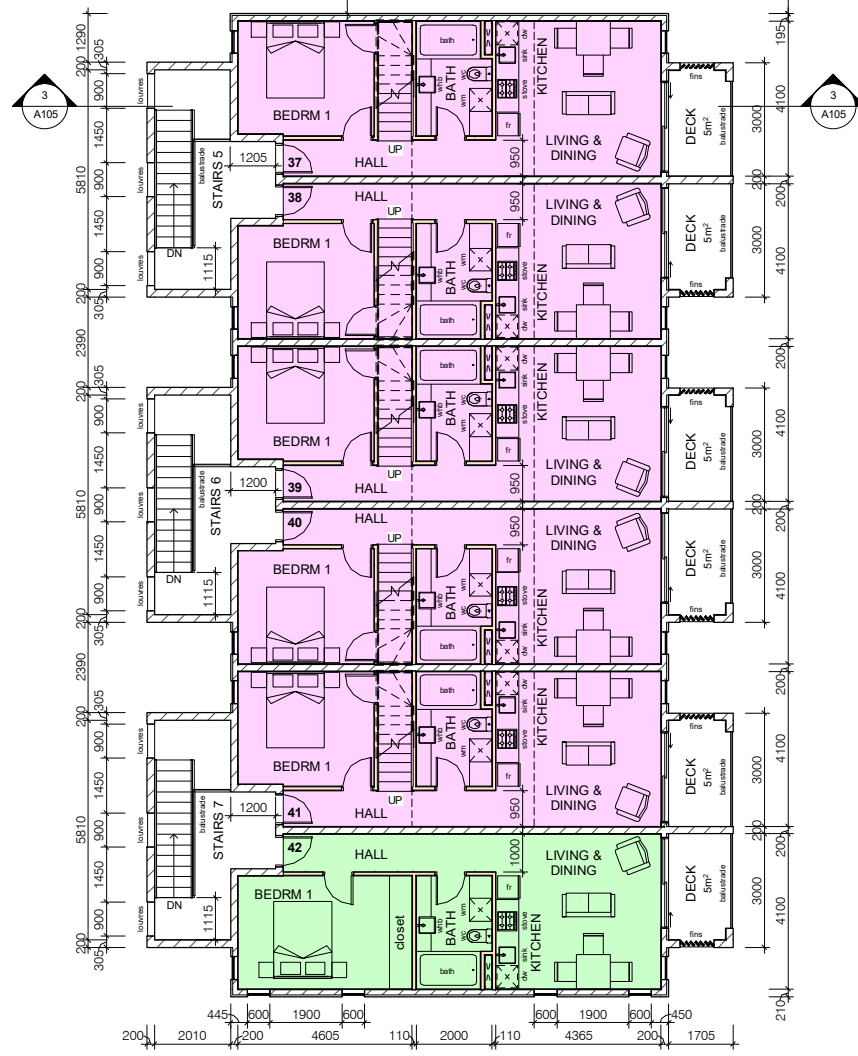
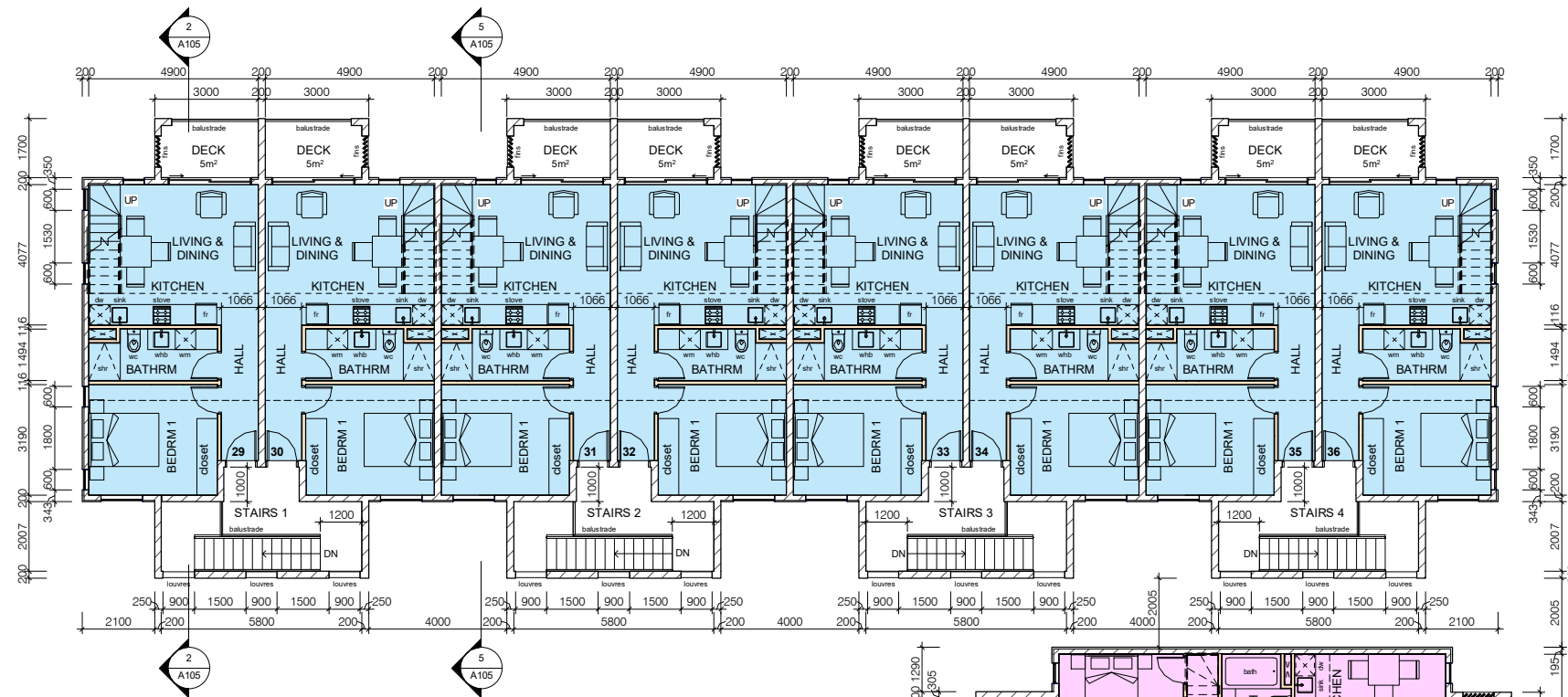
ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
FIRST FLOOR

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As indicated@A1

SHEET NO: **A101** REV: **D**

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1
A102
SECOND FLOOR
1 : 100

KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53
AREAS		
Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%

CONSULTANTS REFERENCE NOTE	
Refer to specific consultant documentation for relevant information on:	
<ul style="list-style-type: none"> • Civil Engineering • Structural Engineering • Plumbing & Drainage • Fire • Landscape • Traffic 	

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PORCHESTER ROAD

ADDRESS:
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CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

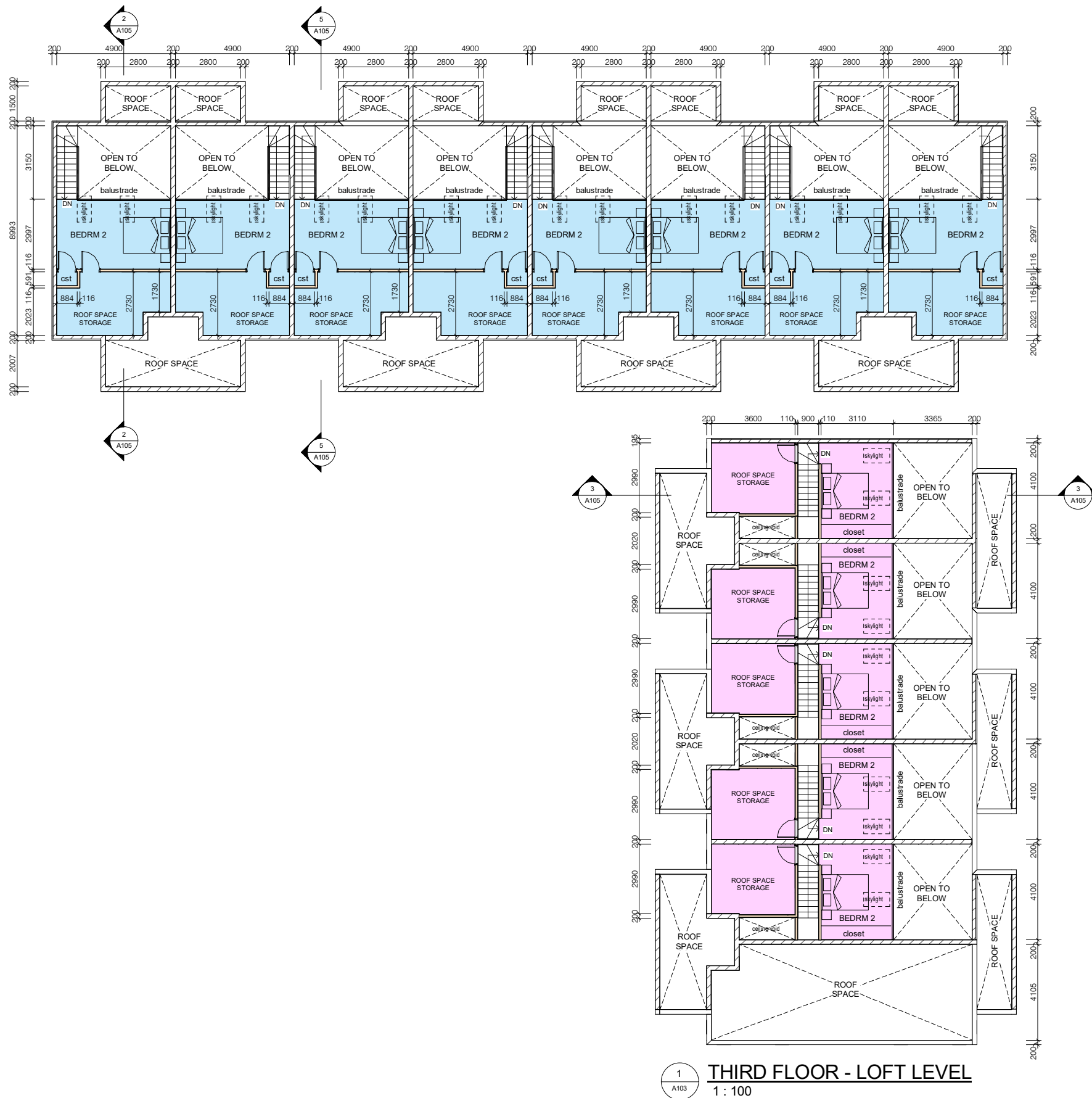
ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
SECOND FLOOR

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As indicated@A1

SHEET NO: **A102** REV: **D**

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KEY LEGEND		
Description:	Internal Area:	Qty:
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North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53
AREAS		
Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%
CONSULTANTS REFERENCE NOTE		
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ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
THIRD FLOOR

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As Indicated@A1

SHEET NO: **A103** REV: **D**

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1 NORTH ELEVATION - WALTERS ST
1 : 100



2 EAST ELEVATION - PORCHESTER RD
1 : 100



3 NORTH BLOCK - SOUTH ELEVATION
1 : 100

- FINISHES: EXTERIOR**
- FLOORS:**
1.1 Timber floor construction.
- WALLS:**
2.1 Timber frame wall with N2 Brick Designer series cladding, colour: dark grey.
2.2 Timber frame wall with Metal Cladding, colour: Ironsand.
2.3 20-series concrete block with Cemintel.
2.4 Aluminium balustrade.
2.5 Timber frame wall with N2 Brick Designer series cladding, colour: light grey.
- ROOFS:**
3.1 Metal profile roofing, colour: Ironsand.
- WINDOWS / DOORS:**
4.1 Aluminium frame doors & windows, colour: Ironsand.
4.2 Aluminium frame roof window, colour: Ironsand.
4.3 Frosted glass to bottom pane of all slot windows.
- FENCES & GATES:**
5.1 Steel fence & gate, paint finish.
- PAVEMENT:**
6.1 Exposed aggregate concrete paving.
- LOUVRES / FINIS:**
7.1 Timber horizontal louvres.
7.2 Timber vertical fins.

- CONSULTANTS REFERENCE NOTE**
- Refer to specific consultant documentation for relevant information on:
- Civil Engineering
 - Structural Engineering
 - Plumbing & Drainage
 - Fire
 - Landscape
 - Traffic

NOTES:

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Revisions		
Ref	Description	Date
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19
E	RESOURCE CONSENT	08.11.19

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PORCHESTER ROAD

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TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
ELEVATIONS

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A104** REV: **E**

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WEST ELEVATION - FROM CARPARK
1 : 100

FINISHES: EXTERIOR

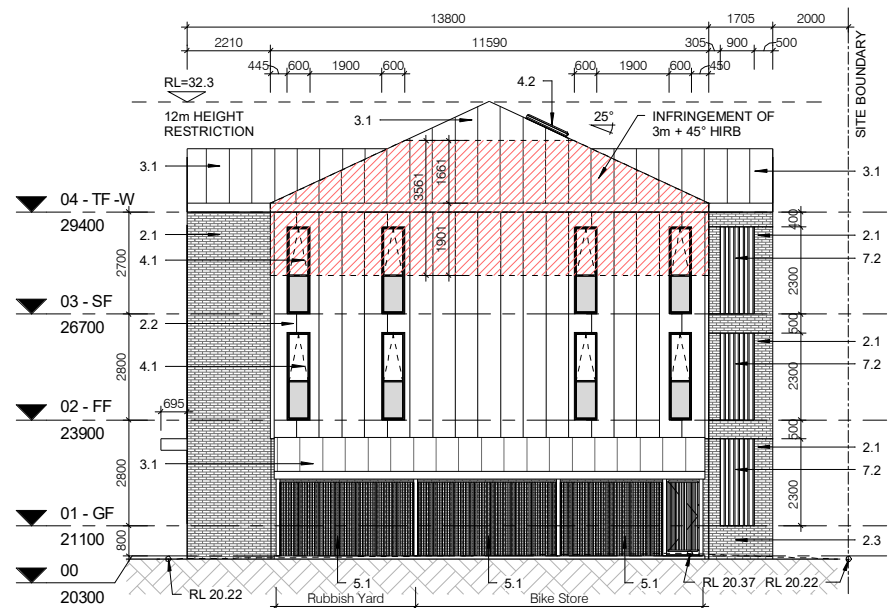
- FLOORS:**
1.1 Timber floor construction.
- WALLS:**
2.1 Timber frame wall with N2 Brick Designer series cladding, colour: dark grey.
2.2 Timber frame wall with Metal Cladding, colour: Ironsand.
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2.4 Aluminium balustrade.
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3.1 Metal profile roofing, colour: Ironsand.
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4.1 Aluminium frame doors & windows, colour: Ironsand.
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- PAVEMENT:**
6.1 Exposed aggregate concrete paving.
- LOUVRES / FINS:**
7.1 Timber horizontal louvres.
7.2 Timber vertical fins.

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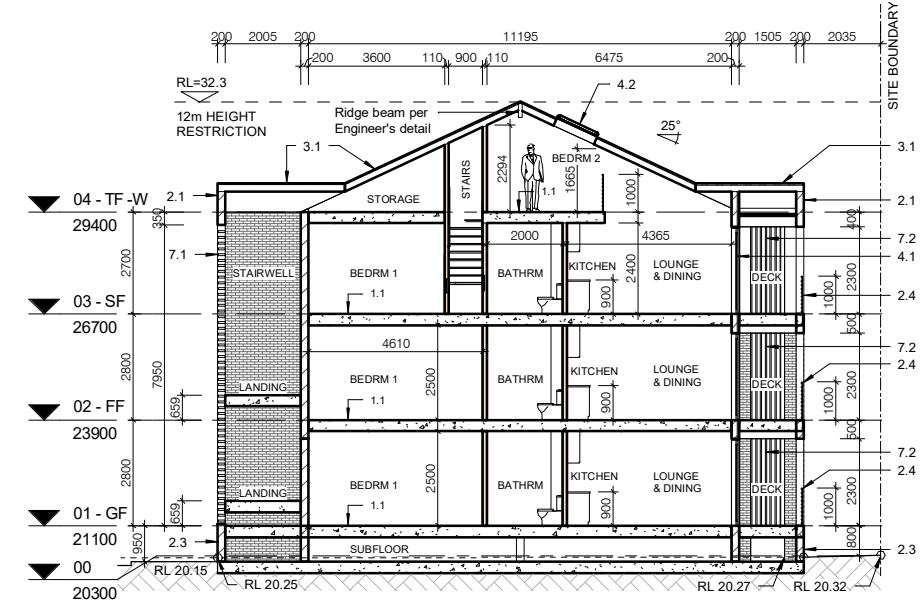
CONSULTANTS REFERENCE NOTE

- Refer to specific consultant documentation for relevant information on:
- Civil Engineering
 - Structural Engineering
 - Plumbing & Drainage
 - Fire
 - Landscape
 - Traffic

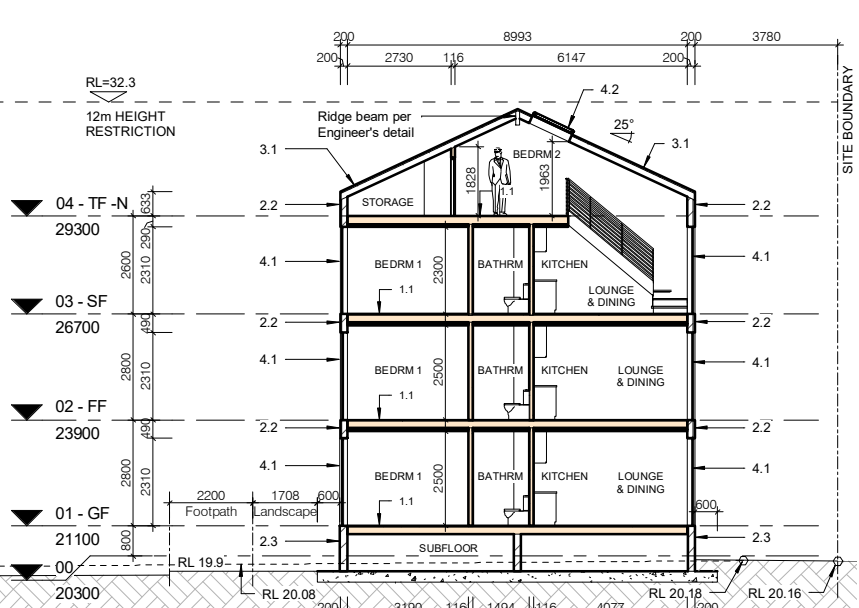
Revisions		
Ref	Description	Date
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19
E	RESOURCE CONSENT	08.11.19



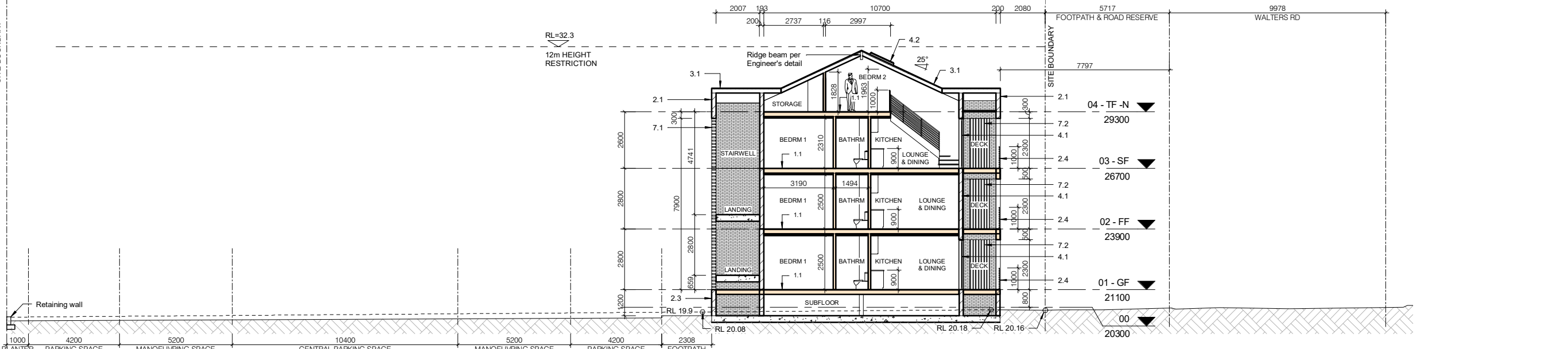
WEST BLOCK - SOUTH ELEVATION
1 : 100



TYPICAL WEST BLOCK CROSS SECTION
1 : 100



TYPICAL NORTH BLOCK CROSS SECTION 2
1 : 100



TYPICAL NORTH BLOCK CROSS SECTION
1 : 100



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PORCHESTER ROAD

ADDRESS:
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TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
ELEVATIONS

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A105** REV: **E**

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A	FOR CONSENT	08.07.19
B	RESOURCE CONSENT	23.07.19



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ADDRESS:

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CLIENT:

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NO: 19001 ISSUE DATE: JUL 2019

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

OUTLOOK SPACE

DESIGN **Designer**

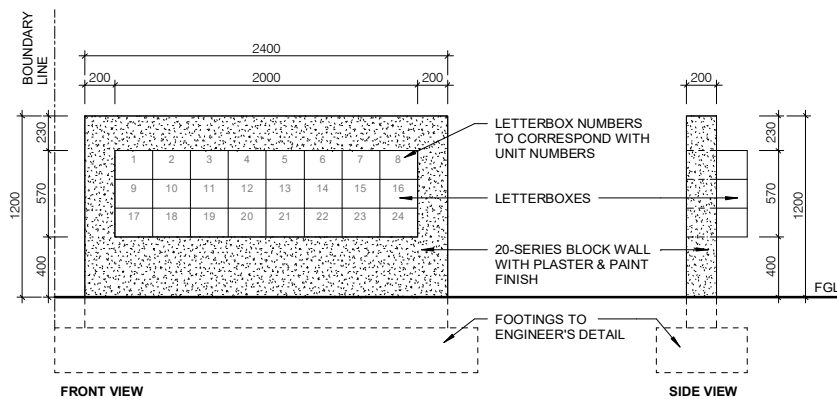
DRAWN: **Author**

CHECKED: **Checker** SCALE: 1 : 100@A1

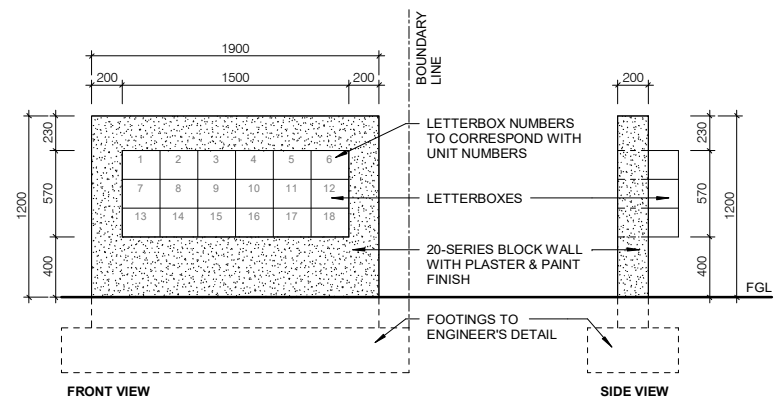
SHEET NO: REV:

A106 B

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1 **LETTERBOX - 24 UNITS**
1:25



2 **LETTERBOX - 18 UNITS**
1:25

LETTERBOX NOTES

- Allow for two Letterbox units, one per Building Block; North Block with 24 units, West Block with 18 units.
- Letterbox numbers to correspond to unit numbers.
- Allow for 190(h)x250(w)x400(d) Contemporary Apartment Box Mailboxes to be built into block wall.

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NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
LETTERBOXES

DESIGN: **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As indicated@A1

SHEET NO: **A107** REV: **B**

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NO: **19001**

ISSUE DATE: **JUL 2019**

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SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: 1 : 1@A1

SHEET NO:

A200

REV:

A

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1 VIEW OF CORNER - WALTERS AND PORCHESTER
A200 1 : 1



1 VIEW OF PORCHESTER ELEVATION
A201 1:1

NOTES:

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SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: 1:1@A1

SHEET NO:

A201

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A

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ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: 1 : 1@A1

SHEET NO: REV:

A202 A

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1 VIEW OF CARPARK AND REAR OF BUILDING
A202 1 : 1

PLAN SET

- LA00 Landscape Notes and Plant Schedules
- LA01 Landscape Northern Site
- LA02 Landscape Southern Site
- LA03 Details
- LA04 Plant Information

TYPICAL SOIL DEPTH

Shrubs: 300mm min.
 Trees: 3m³ min. per tree, 1m depth

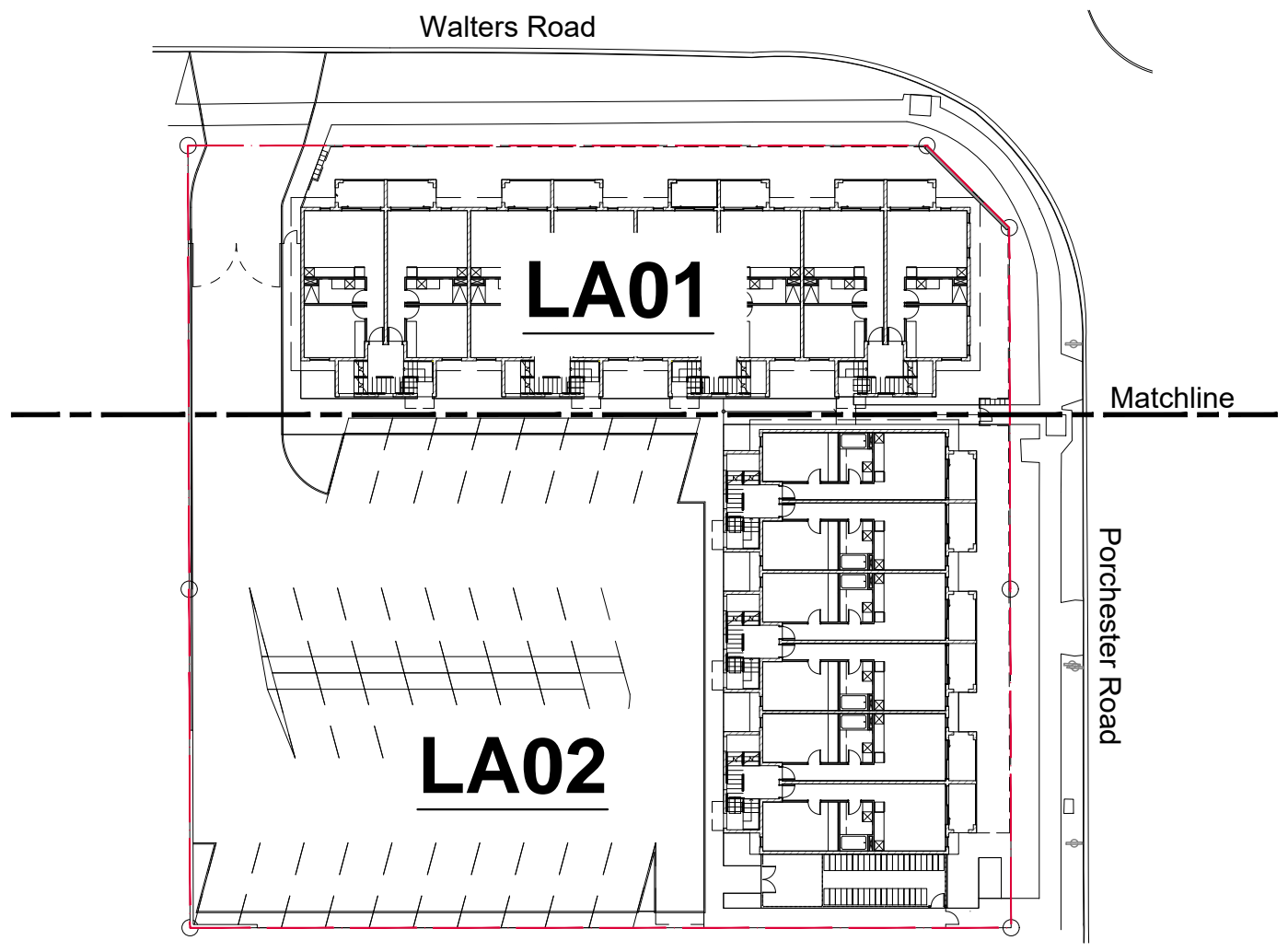
PLANT LIST

TREES

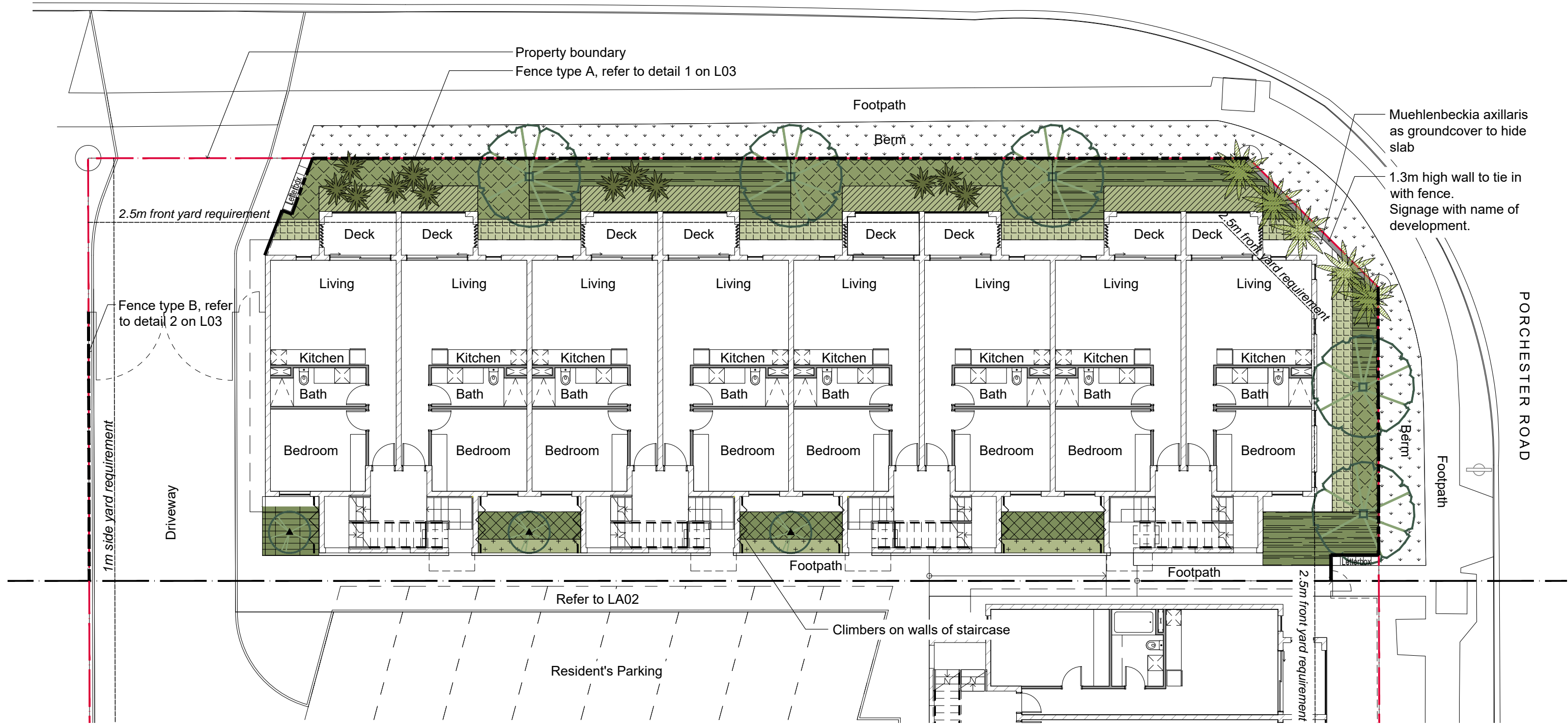
Latin Name	Common Name	Size	Qty
Cordyline australis	Cabbage Tree	25l	26
Liriodendron tulipifera fastigiata	Tulip Tree	160l	9
Pseudopanax crassifolius	Horoeka	45l	8
Rhopalostylis sapida	Nikau	45l	5
Strebulus banksii	Ewekuri	45l	4

SHRUBS, CLIMBERS & GRASSES

Latin Name	Common Name	Size	Spacing	Qty
Carex testaceae	Orange sedge	2l	5/m ²	77
Clivia miniata	Flame Lily	2l	4/m ²	51
Corokia Yellow Wonder	Korokia	10l	1/m	43
Dietes bicolor	Fortnight Lily	2l	2/m ²	50
Griselinia littoralis 'Ardmore Green'	Kapuka	10l	1/m	36
Hebe flame	Hebe	3l	3/m ²	44
Hebe stricta	Hebe	3l	3/m ²	12
Hemerocallis Stella Bella	Daylily	2l	2/m ²	43
Lobelia angulata	Panakenake	2l	1/m ²	71
Lomandra 'Little Con'		2l	4/m ²	124
Lomandra hystrix major		2L	1/m ²	28
Loropetalum chinense rubrum	Chinese fringe flower	3.5l	1/m ²	16
Muehlenbeckia axillaris		2l	2/m ²	4
Parthenocissus tricuspidata	Boston Ivy	2.5l	1/2m	20
Phormium 'Purple Haze'	Flax	2l	2/m ²	32
Trachelospermum jasminoides	Star Jasmine	8l	1/1m	36



Keyplan



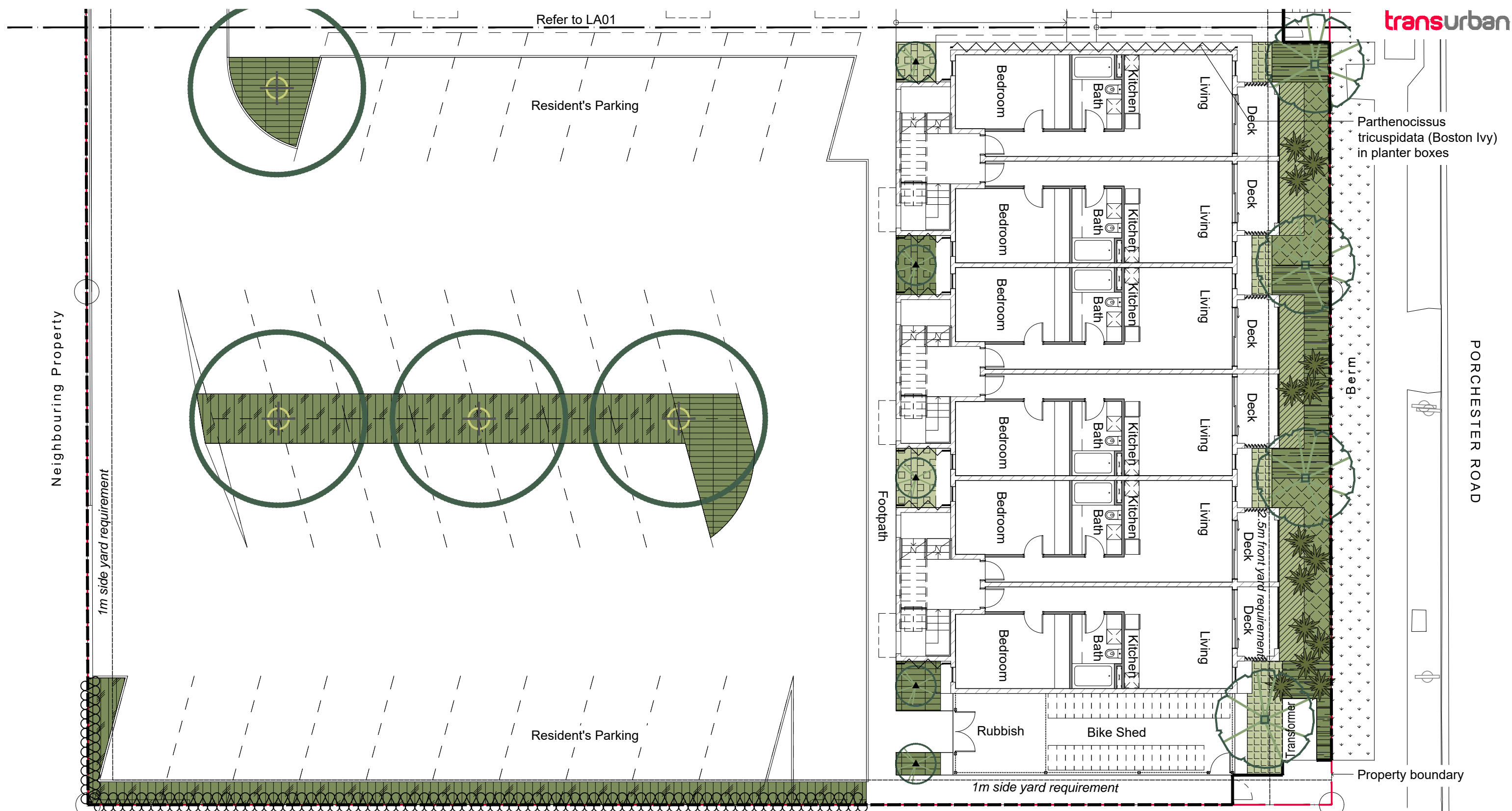
Legend

- Cordyline australis (Cabbage Tree)
- Liriodendron tulipifera fastigiata (45L)
- Pseudopanax crassifolius (45L)
- Rhopalostylis sapida (45L)
- Streblus banksii (45L)
- Carex testacea
- Lomandra hystrix major
- Clivia miniata
- Corokia Yellow Wonder (as hedge)
- Diets bicolor
- Hebe flame
- Hebe stricta
- Hemerocallis 'Stella Bella'
- Loropetalum chinense Rubrum
- Phormium cookianum 'Black Magic'
- Griselinia littoralis 'Ardmore Green' (as hedge)
- Hebe flame
- Hebe stricta
- Lobelia angulata
- Lomandra hystrix major
- Parthenocissus tricuspidata
- Trachelospermum jasminoides
- Fence Type A - 1.3m (Refer to LA03, Detail 1)
- Fence Type B - 1.8m (Refer to LA03, Detail 2)
- Wall - 1.3m with Development Name



-Resource Consent-
-Not for construction-

Siteplan LA01
166 & 164 Porchester Road - Residential Development
Scale 1:150 @A3 Date : 26 July 2012



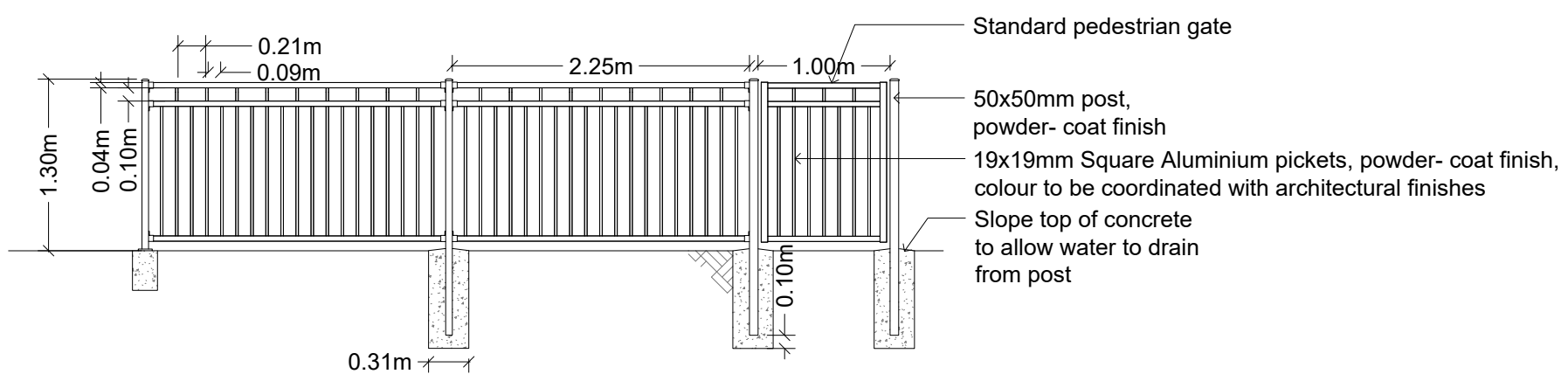
Legend

- | | | | | | | | | | |
|--|--|--|----------------------------------|--|--|--|-----------------------------|--|---|
| | Cordyline australis (Cabbage Tree) | | Carex testacea | | Griselinia littoralis 'Ardmore Green' (as hedge) | | Lobelia angulata | | Fence Type A - 1.3m (Refer to LA03, Detail 1) |
| | Liriodendron tulipifera fastigiata (45L) | | Lomandra hystrix major | | Hebe flame | | Lomandra hystrix major | | Fence Type B - 1.8m (Refer to LA03, Detail 2) |
| | Pseudopanax crassifolius (45L) | | Clivia miniata | | Hebe stricta | | Parthenocissus tricuspidata | | Wall - 1.3m with Development Name |
| | Rhopalostylis sapida (45L) | | Corokia Yellow Wonder (as hedge) | | Hemerocallis 'Stella Bella' | | Trachelospermum jasminoides | | |
| | Streblus banksii (45L) | | Dietes bicolor | | Loropetalum chinense Rubrum | | | | |
| | | | Lomandra 'Little Con' | | Phormium cookianum 'Black Magic' | | | | |

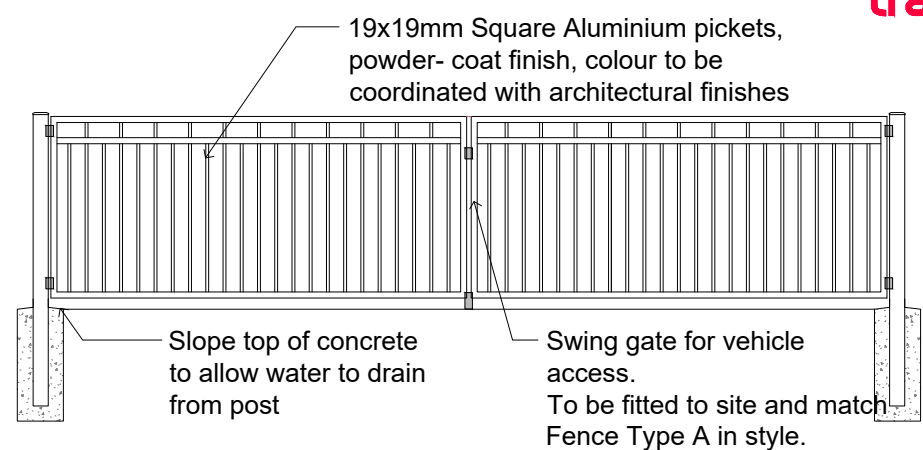


-Resource Consent-
-Not for construction-

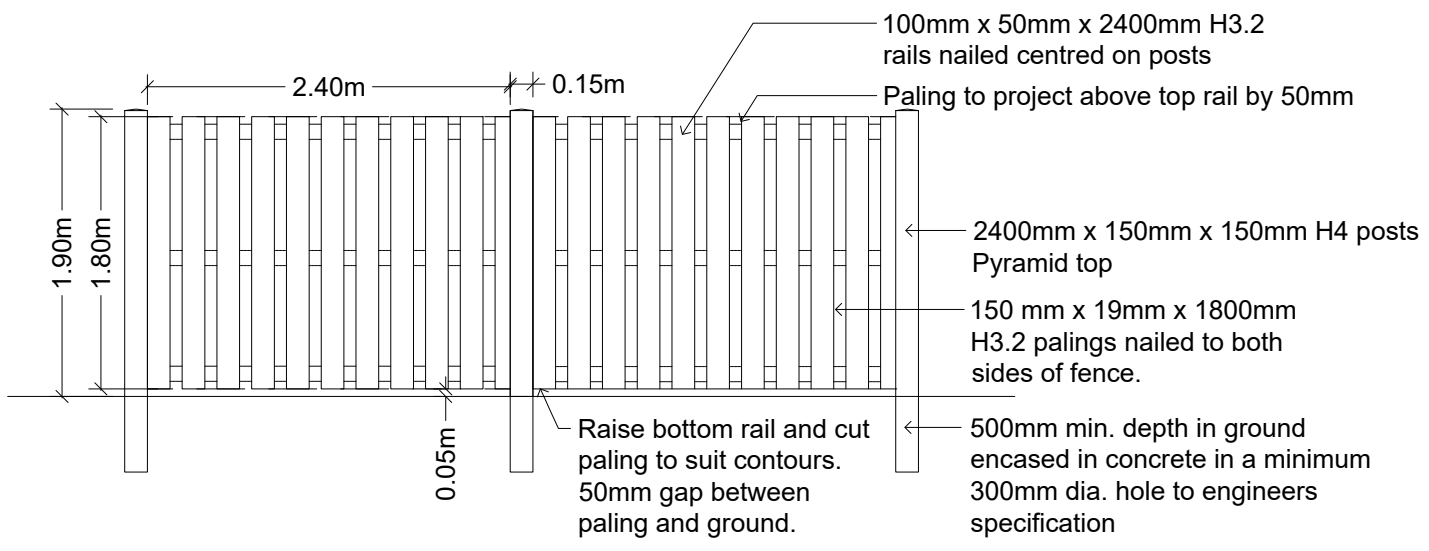
Siteplan LA02
166 & 164 Porchester Road - Residential Development
Scale 1:150 @A3 Date : 26 July 2013



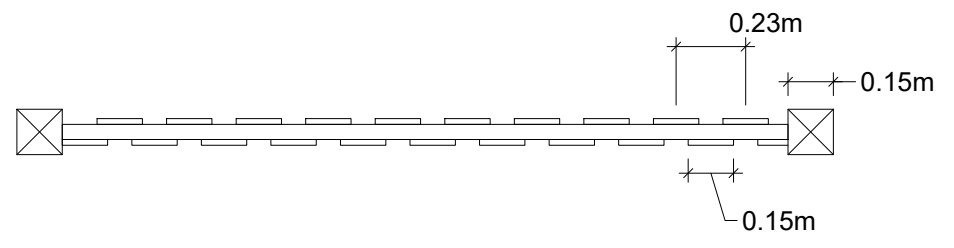
Detail 1 - Standard Fence and Pedestrian Gate Type A Scale 1:50@A3



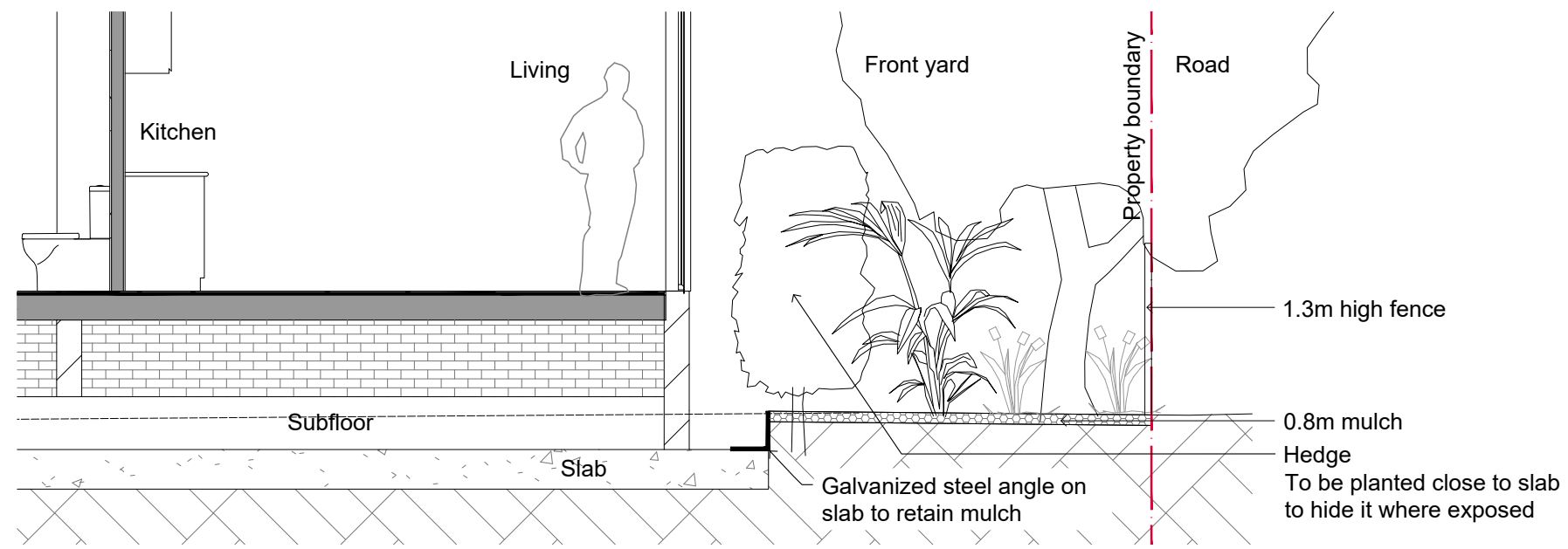
Detail 1 - Vehicle Gate Type A Scale 1:50@A3



Detail 2 - Standard Fence Type B Scale 1:50@A3



Detail 2a - Standard Fence Type B - planview Scale 1:25@A3



Detail 3 - Planting detail at slab Scale 1:50@A3



-Resource Consent-
-Not for construction-

Shrubs



Carex testacea (NZ sedge)
A native grass with a distinctive rich orange and green colour and a weeping form, very popular for amenity planting. Best planted in mass for effect.
Full sun, intolerant of wet conditions, hardy
H x W: 0.4 x 0.4m



Clivia miniata (Flame lily)
Excellent plant for the shade, especially under trees. In spring (& sometimes autumn) clusters of orange tubular flowers are borne above the dark green, broad, strap-like foliage.
Full to semi-shade. Good drainage.
H x W: 0.8m x 1m



Corokia Yellow Wonder (Korokio)
Upright densely branched. Green small foliage. Starry yellow flower followed by masses of golden-yellow berries. Ideal for hedging.
Full sun, free draining soil
H x W: 2 x 1.2m.



Dietes bicolor (Fortnight Lily)
Easy growing perennial with erect, sword shaped green leaves. Iris like pale yellow flowers. Flowers abundantly throughout the warmer weather.
Full sun to part shade, frost tolerant, hardy.
H x W: 60 x 40cm



Griselinia littoralis 'Ardmore Green' (Kapuka)
Dark green, smaller leaf Griselinia with a compact tight habit. Easier to keep as a low formal hedge as slower growth than other griselinias.
Hardy, trim once a year to form a formal hedge
H: 0.5m to 2m hedge



Hebe 'Flame'
Shiny leaves which are green during summer and turning a bronze purple colour during winter. The leaves have a purple midrib and purple stems.
H x W: 0.6 x 0.75m



Hebe stricta (Koromiko)
Bushy shrub bearing pairs of long narrow pointed thin leaves. Flowers white or pinkish. It will not grow under kauris.
Full sun only, tolerates wet, hardy
H x W: 1 x 1m



Hemerocallis 'Stella Bella' (Dayily)
Extremely tolerant and hardy, can bloom for up to 6 months. Yellow flowers. Perfect for mass planting. Evergreen.
Full sun, but will tolerate some shade.
H x W: 0.5-0.8 x .4-1.0m



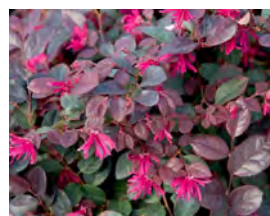
Lobelia angulata (Panakenake)
A wide spreading groundcover, fast growing and easy to establish.
Prefers a damp situation, but does well in most places.
H x W: 50 x 50cm



Lomandra 'Little Con'
Grasslike plant, forming a compact spiky ball.
Very tough, can grow on dry soils.
H x W: 30 x 30cm



Lomandra hystrix major
Use this giant grass as a specimen feature in gardens or as a large back drop effect.
H x W: 1.5 x 1.5m



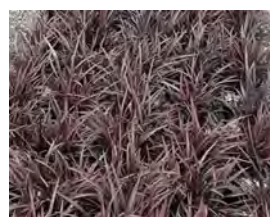
Loropetalum chinense Rubrum (Chinese fringe flower)
Evergreen shrub, burgundy wine foliage, china pink flowers in spring. Its dense and spreading habit, requires little if any maintenance.
Hardy, tolerates most climatic situations throughout NZ.
H x W: 1.5m x 2m



Muehlenbeckia axillaris (Creeping Pohuehue)
Hardy groundcover with wiry stems and small dark green leaves.
Hardy, full sun, tolerant of dry situations.
H x W: 0.15 x 1m



Parthenocissus tricuspidata (Boston Ivy)
Vigorous, self-clinging large deciduous climber. Full sun or shade, fertile, well-drained soil.



Phormium Cookianum Black Magic
Deep purple, strap-shaped leaf. Great in low maintenance gardens and pots. Ideal in gardens, border or for mass planting. Frost tolerant and drought hardy once established.
H x W: .45 x .45m



Trachelospermum jasminoides (Star Jasmine)
Deep green, glossy foliage with highly scented, white flowers. Suitable in warm, sunny position and a cool climate, most soils and can withstand mild frosts.
H x W: up to 3 x 3m

Trees



Cordyline australis (Cabbage Tree)
Creamy, fragrant white flowers in late spring, followed by white-blue berries. Can be grown in a variety of conditions. Bird distributed seed, provides food for native birds.
Full sun and semi-shade, tolerates wet and dry conditions, hardy.
H x W: 8 x 3m



Liriodendron tulipifera Fastigiata (Tulip Tree)
Deciduous tree, crown has narrow pyramid shape. Prefers deep, fertile, free- draining soil and full sun
H : up to 8 m



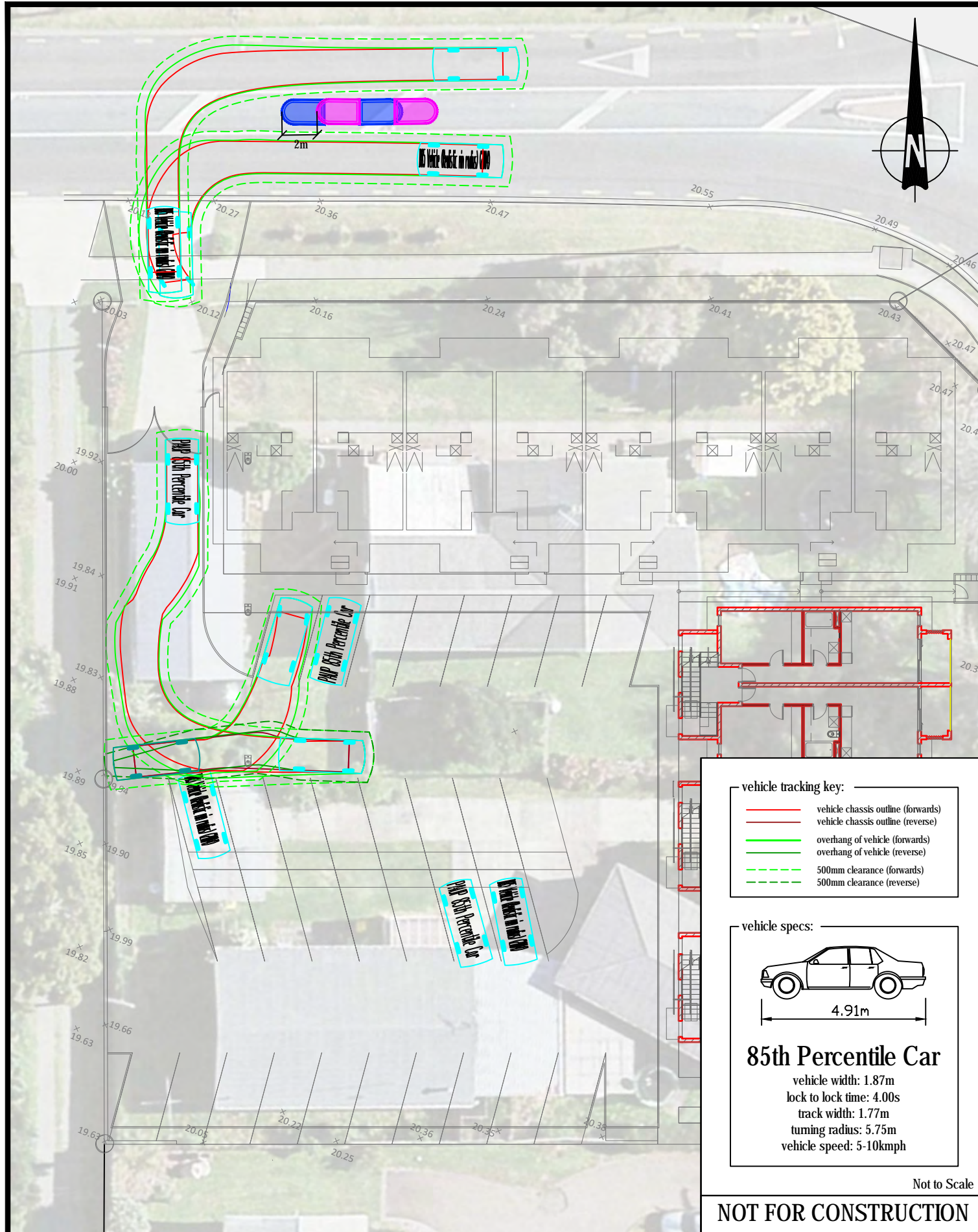
Pseudopanax crassifolius (Lancewood)
Round-headed tree with straight, clean trunk when mature. Long upright juvenile phase with long descending dark-green leaves gives an architectural look to even the smallest of spaces. Attractive to birds and bees. Evergreen.
Shade tolerant and wind resistant, frost tender when young.
H x W: 12m x 3m



Rhopalostylis sapida (Pitt Island Nikau)
Flowering doesn't occur until it is at least 30 years old, followed by red fruits which take a year to ripen. Provides food for native birds.
Semi shade or shade, tolerant of light frosts, prefers a sheltered position
H : 10m+



Streblus banksii (Ewekuri)
Tree with grey spotted bark and dark green elliptical leaves. Flowers small, in clusters of long spikes.
Prefers deep, fertile soils
H: up to 12m



vehicle tracking key:

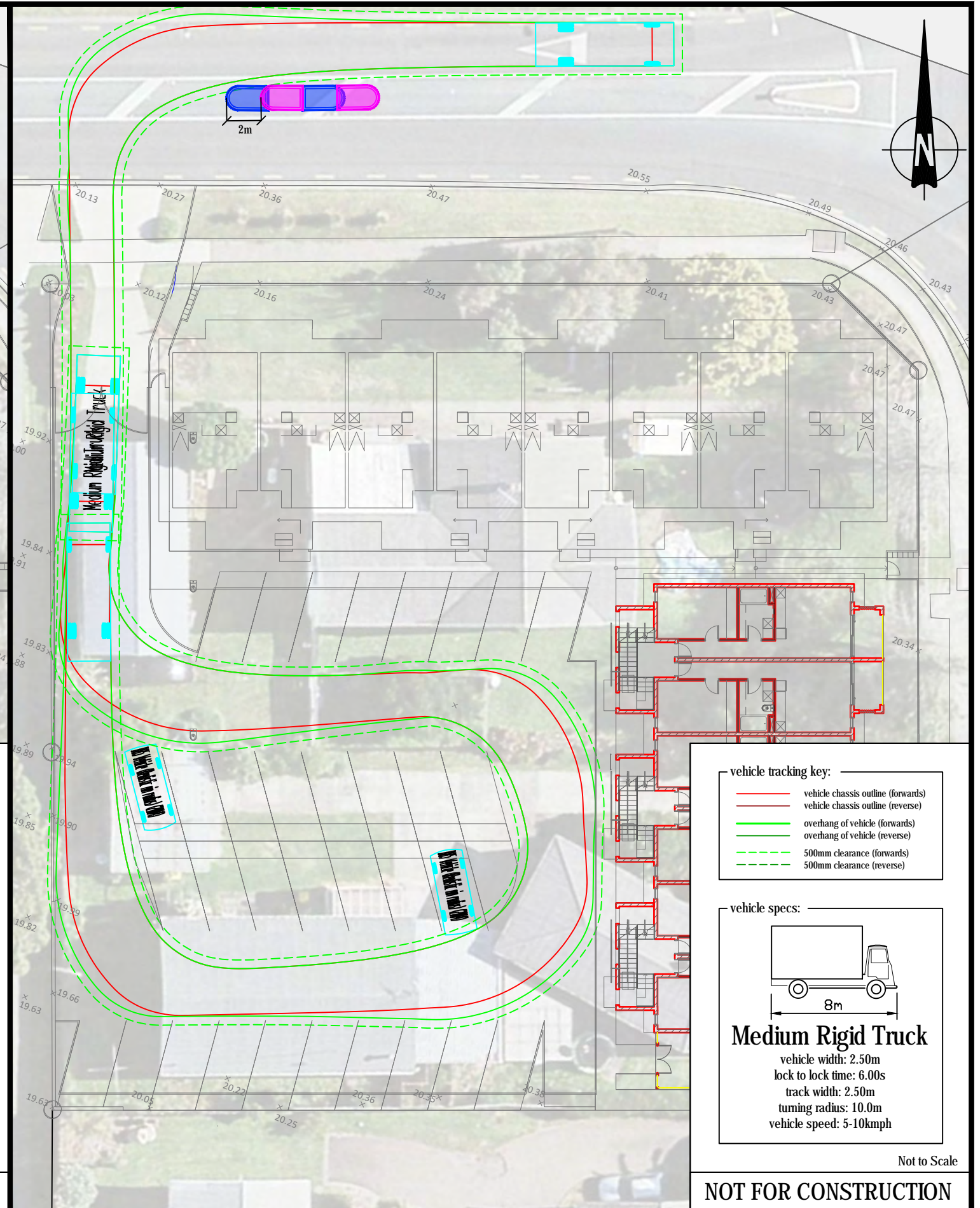
- vehicle chassis outline (forwards)
- - - vehicle chassis outline (reverse)
- overhang of vehicle (forwards)
- - - overhang of vehicle (reverse)
- - - 500mm clearance (forwards)
- - - 500mm clearance (reverse)

vehicle specs:

85th Percentile Car
 vehicle width: 1.87m
 lock to lock time: 4.00s
 track width: 1.77m
 turning radius: 5.75m
 vehicle speed: 5-10kmph

Not to Scale

NOT FOR CONSTRUCTION



vehicle tracking key:

- vehicle chassis outline (forwards)
- - - vehicle chassis outline (reverse)
- overhang of vehicle (forwards)
- - - overhang of vehicle (reverse)
- - - 500mm clearance (forwards)
- - - 500mm clearance (reverse)

vehicle specs:

Medium Rigid Truck
 vehicle width: 2.50m
 lock to lock time: 6.00s
 track width: 2.50m
 turning radius: 10.0m
 vehicle speed: 5-10kmph

Not to Scale

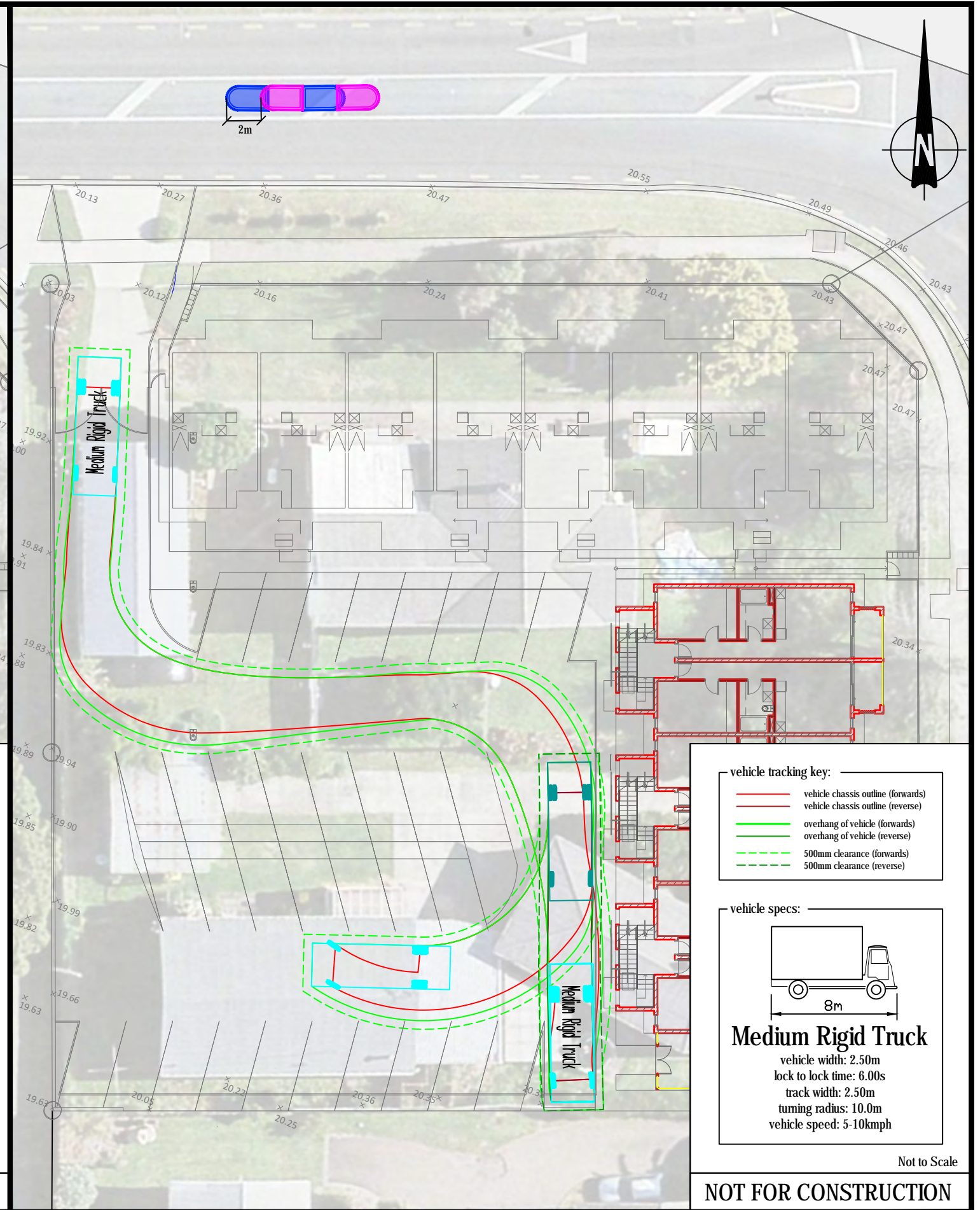
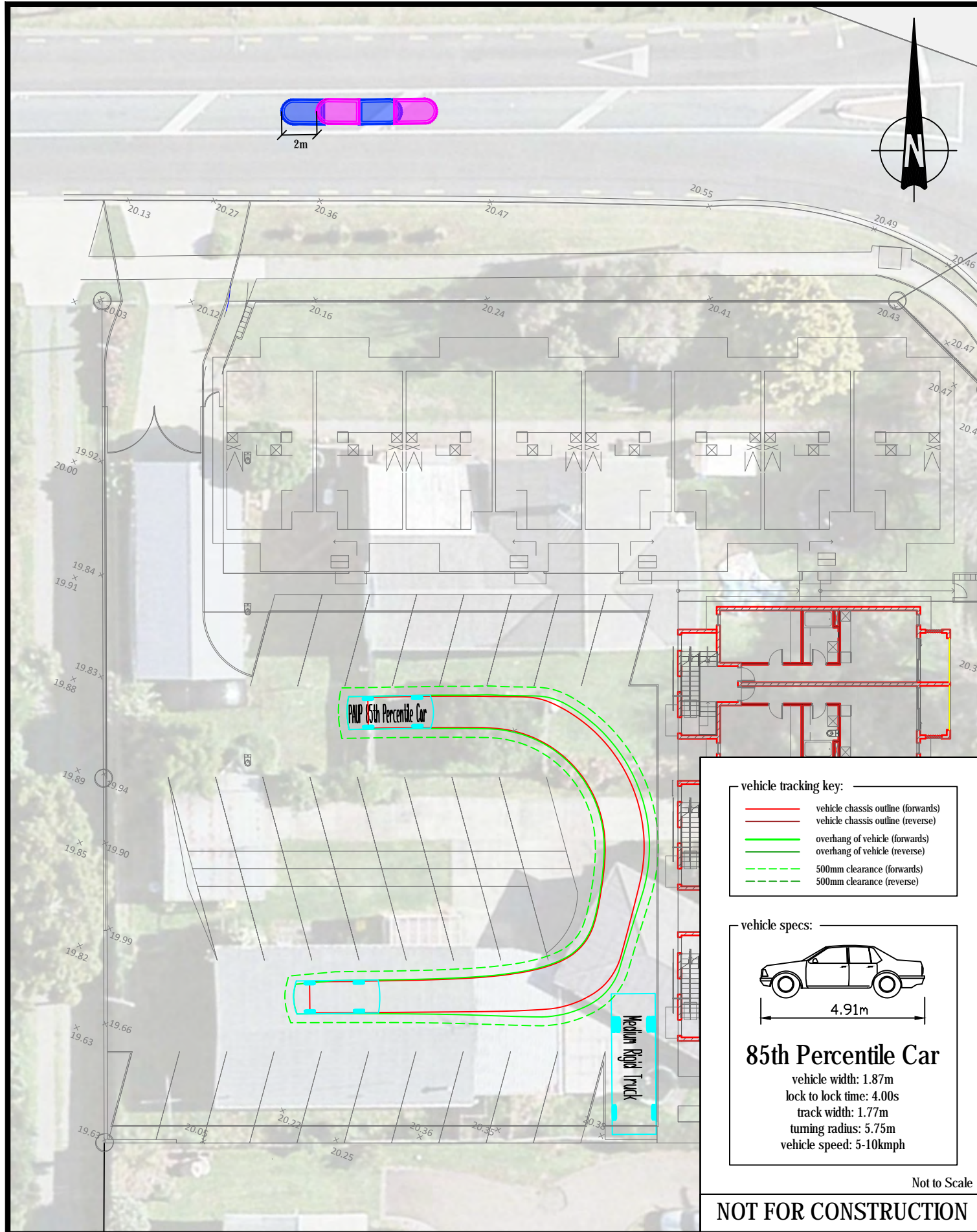
NOT FOR CONSTRUCTION



1 of 2 sheets		status	issued
scale: NTS	design: CR	c	19/02/2019
date: 24/09/2019	drawn: CR	d	12/04/2019
ref: TNGX 012	checked: TC	e	12/07/2019
		f	24/09/2019

164 to 166 Porchester Road (Residential) Vehicle Tracking

flow
 TRANSPORTATION SPECIALISTS
 Level 1, 11 Blake Street, Ponsonby, Auckland 1014
 p 09 970 3820 | f 09 970 3890 | www.flownz.com



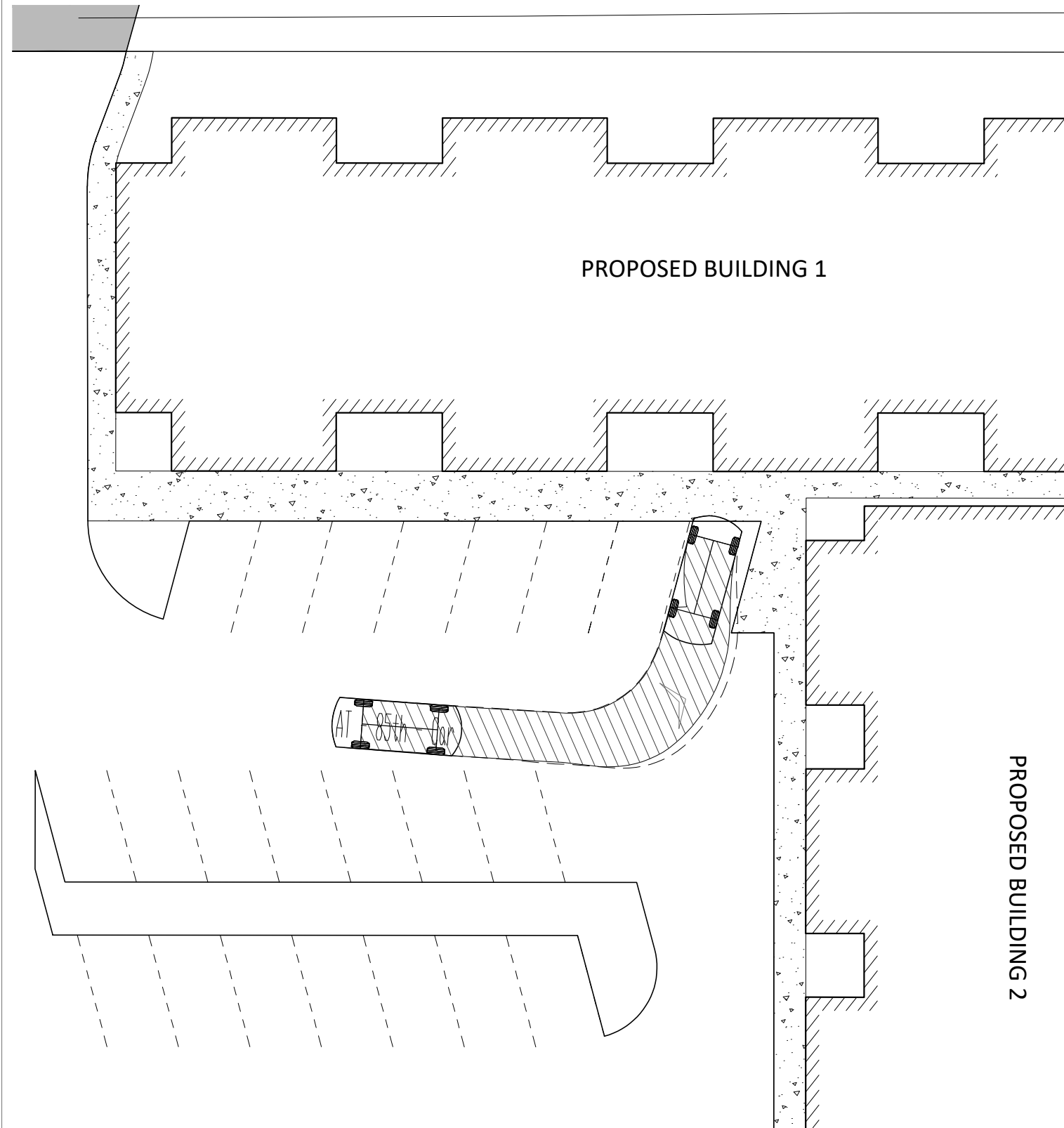
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date:	24/09/2019	drawn:	CR
ref:	TNGX 012	checked:	TC

status	issued
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d	12/04/2019
e	12/07/2019
f	24/09/2019

164 to 166 Porchester Road (Residential) Vehicle Tracking

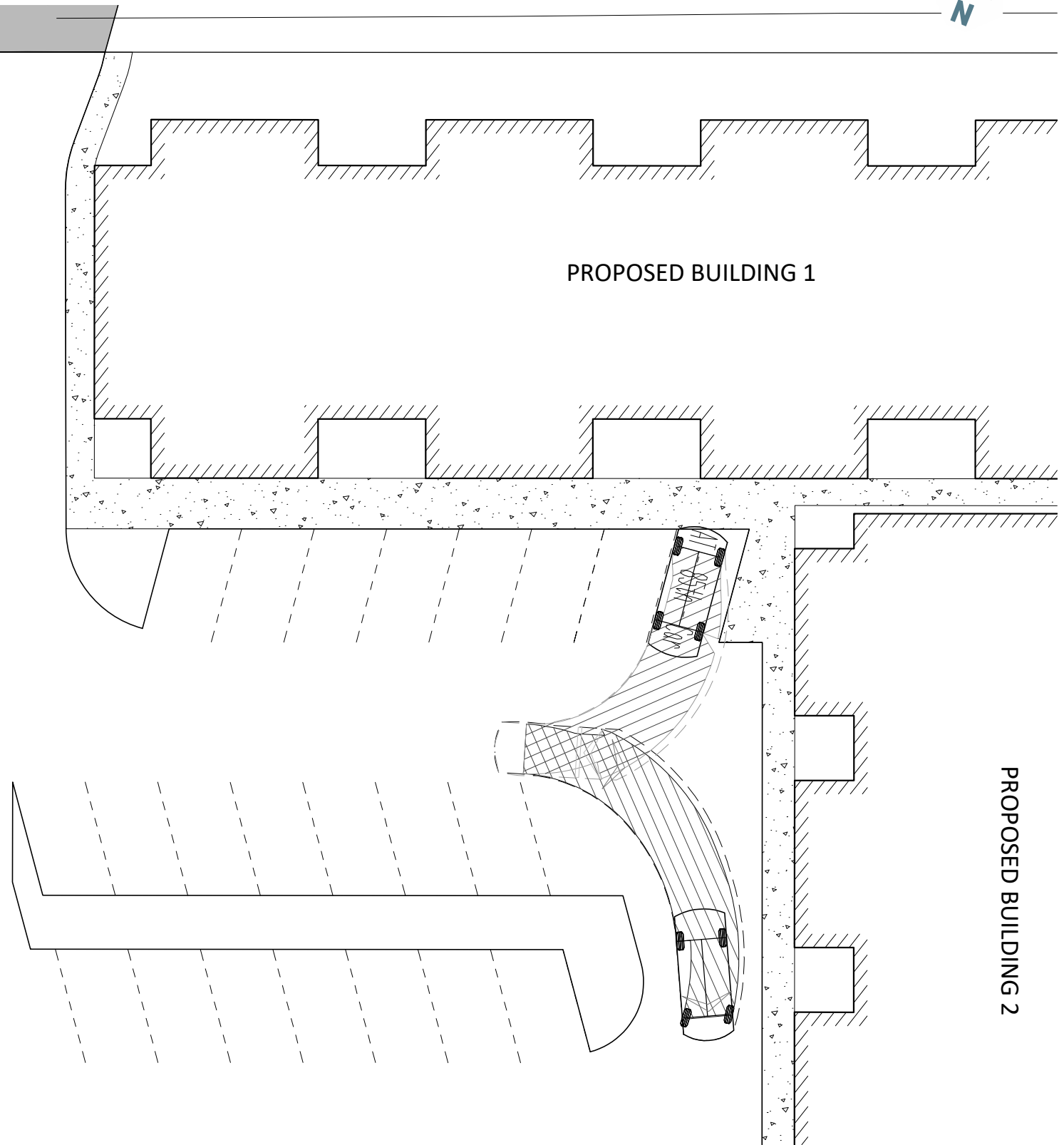
flow
 TRANSPORTATION SPECIALISTS
 Level 1, 11 Blake Street, Ponsonby, Auckland 1065 | P 09 970 3820 | F 09 970 3890 | www.flownz.com



PROPOSED BUILDING 1

PROPOSED BUILDING 2

85TH PERCENTILE CAR - FORWARD
SCALE: (A1/A3) 1:100/1:200



PROPOSED BUILDING 1

PROPOSED BUILDING 2

85TH PERCENTILE CAR - REVERSE
SCALE: (A1/A3) 1:100/1:200

Printed by: RYAN@26.05.20 // Filepath: S:\JOBS\2094 - ALDA INVESTMENTS LIMITED - 164-166 PORCHESTER ROAD\CAD\2094-00-5004.DWG

REV	REVISION DETAILS	BY	DATE
1	TRACKING INTO CARPARK	RJP	26.05.20

DRAWN:	RJP	DATE:	05.2020
CHECKED:	RJP	DATE:	26.05.20
APPROVED:	RJP	DATE:	26.05.20

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PROJECT TITLE:
ALDA INVESTMENTS
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

SHEET TITLE:
VEHICLE TRACKING

ISSUE STATUS: **DRAFT@26.05.20**

SCALE: (A1/A3) 1:100 / 1:200

SCALE BAR 1:200@A3

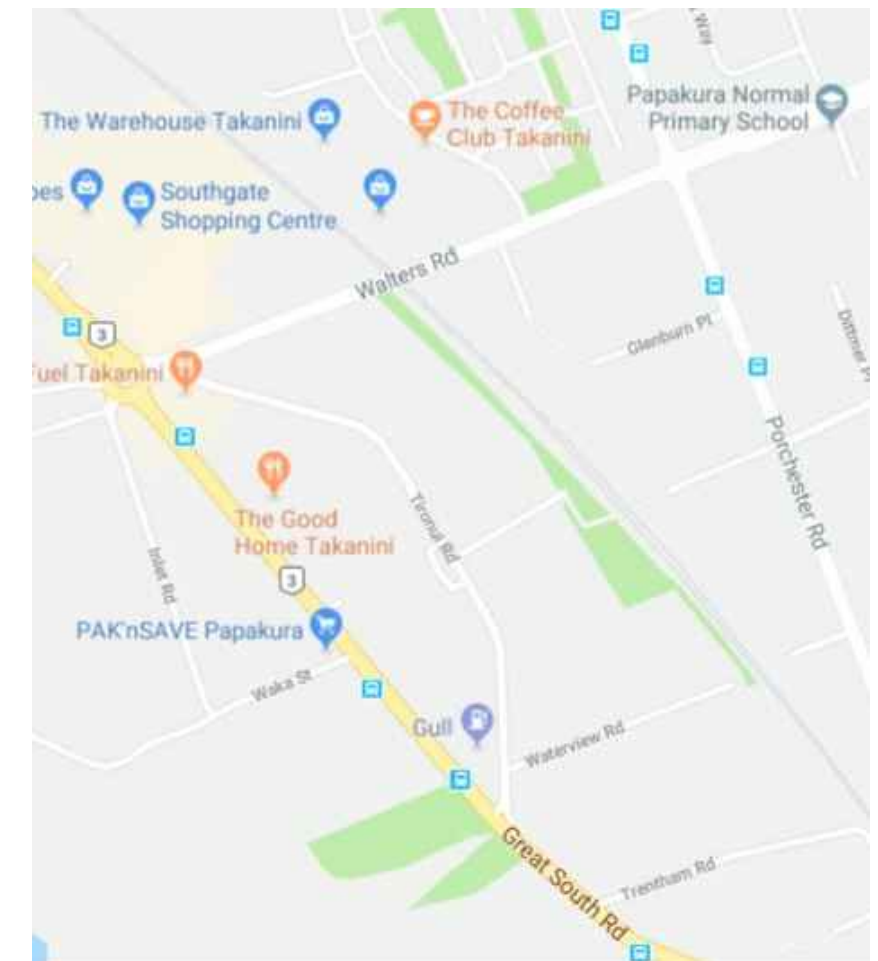
DRAWING NUMBER: **2094-00-5004** REV: **1**

**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND
PROJECT NUMBER: 2094**

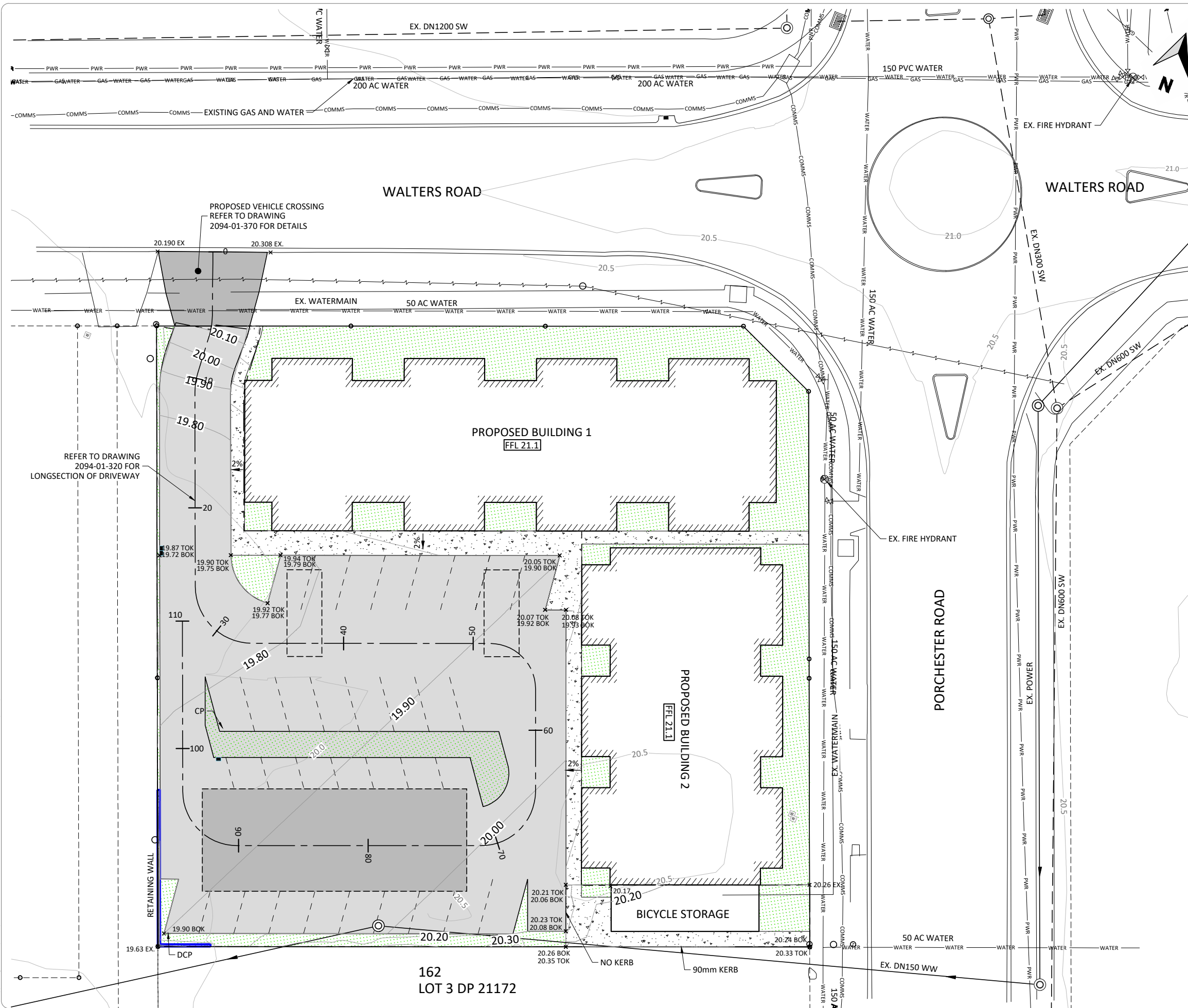
**ISSUED FOR RESOURCE CONSENT
JUNE 2019**

DRAWING INDEX

Sheet Number	Sheet Title
2094-01-000	COVER SHEET
2094-01-130	PROPOSED SITE LAYOUT
2094-01-150	TOPOGRAPHICAL PLAN
2094-01-200	PROPOSED CONTOUR PLAN
2094-01-210	PRE-LOAD PLAN
2094-01-220	ISOPACH PLAN
2094-01-230	SEDIMENT AND EROSION CONTROL PLAN
2094-01-235	SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 1
2094-01-236	SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 2
2094-01-370	VEHICLE CROSSING - PLAN AND DETAIL
2094-01-400	PROPOSED DRAINAGE-OVERALL LAYOUT
2094-01-401	PROPOSED DRAINAGE-DETAIL PLAN
2094-01-410	EXISTING FLOODPLAIN - 100YR WITH CLIMATE CHANGE
2094-01-411	PROPOSED FLOODPLAIN - 100YR WITH CLIMATE CHANGE
2094-01-450	WASTEWATER LONGITUDINAL SECTIONS
2094-01-475	PROPOSED DRAINAGE - STORMWATER360 -STORMFILTER DETAIL
2094-01-476	PROPOSED DRAINAGE - STORMWATER360 -STORMFILTER CALCULATION
2094-01-477	PROPOSED DRAINAGE - RECHARGE PIT DETAILS
2094-01-490	PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 1
2094-01-491	PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 2
2094-01-495	WASTEWATER STANDARD DETAILS
2094-01-500	PROPOSED WATERMAIN - LAYOUT PLAN
2094-01-590	WATERMAIN STANDARD DETAILS



LOCALITY PLAN
NOT TO SCALE



NOTES:

1. ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS AND NZ BUILDING CODE.
2. ALL WORKS ON EXISTING STORMWATER, WASTEWATER LINES AND WATERMANS TO BE CARRIED OUT BY AN APPROVED LICENSED CONTRACTOR AT THE DEVELOPERS EXPENSE.
3. CONTRACTOR TO LOCATE / MARK AND CONFIRM ALL EXISTING SERVICES PRIOR TO COMMENCEMENT OF WORK ON SITE AND PROTECTION OF SAME.

LEGEND
EXISTING

- DN1350 SW STORMWATER LINE AND MANHOLE
- WASTEWATER LINE AND MANHOLE
- CONTOUR
- POWER
- WATER
- COMMS
- GAS
- COMMS PLINTH
- POWER PLINTH
- POWER POLE
- GAS PIT
- CATCHPIT
- WATERMAIN
- FIRE HYDRANT
- WATER METER
- STORMWATER DEVICE REFER TO DRAINAGE DRAWINGS
- CONCRETE CARPARK
175mm THICK 25MPa CONCRETE WITH 1 LAYER OF 663 MESH PLACED CENTRALLY, ON 150mm COMPACTED GAP65 SUBBASE. SAW-CUT EXPANSION JOINTS AT 4m CRS MAX. (SUBGRADE CBR ≥5%)
- LANDSCAPED AREAS
- RETAINING WALL
- KERB BLOCK
SCALE: (A1/A3) N.T.S.

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REV	REVISION DETAILS	BY	DATE
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

DRAWN:	MM	DATE:	03.2019
CHECKED:	MSS	DATE:	05.06.19
APPROVED:	MSS	DATE:	05.06.19

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164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND**

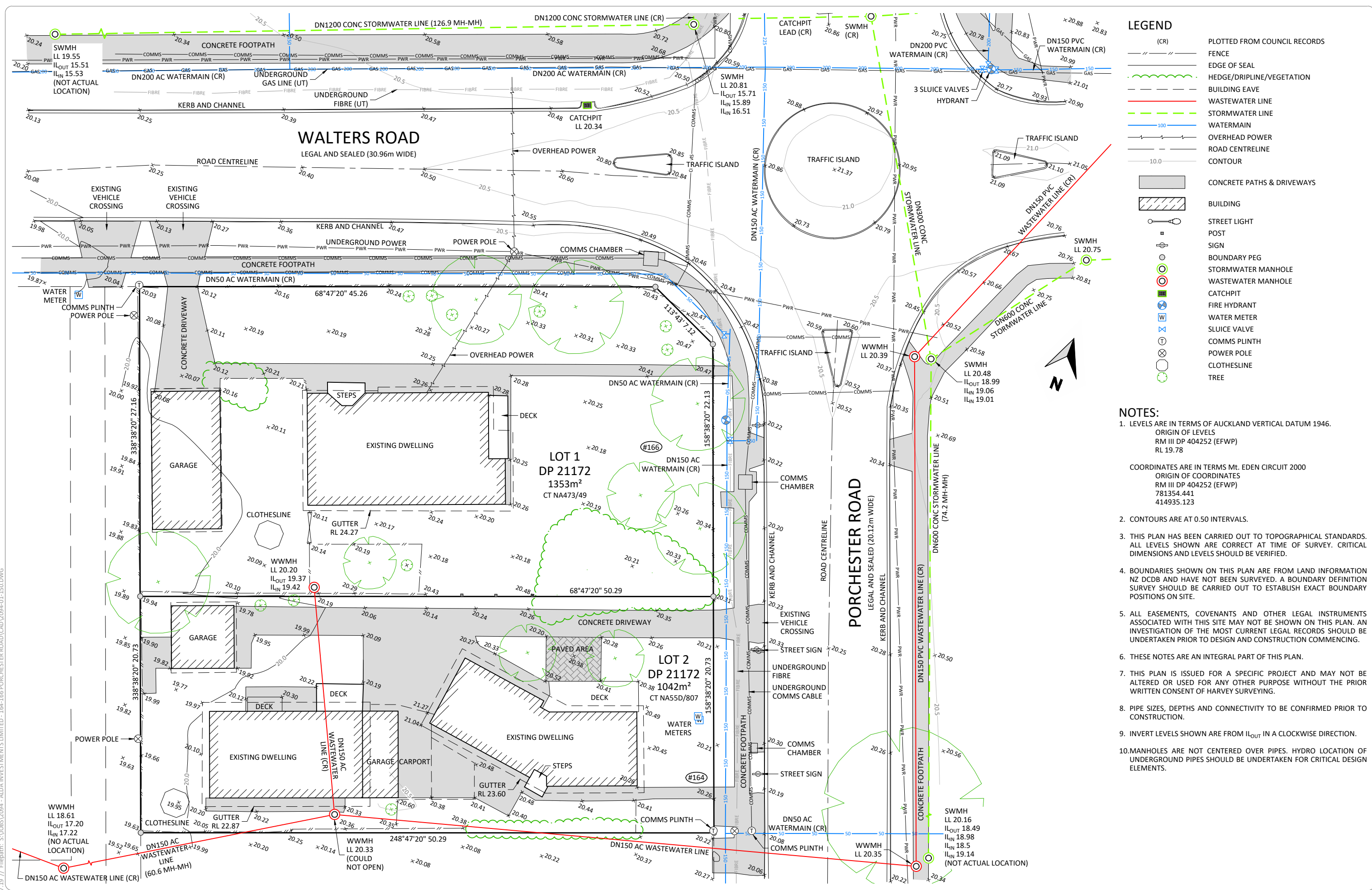
SHEET TITLE:
PROPOSED SITE LAYOUT

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-130** REV: **C2**



LEGEND

- (CR) PLOTTED FROM COUNCIL RECORDS
- FENCE
- EDGE OF SEAL
- HEDGE/DRIPLINE/VEGETATION
- BUILDING EAVE
- WASTEWATER LINE
- STORMWATER LINE
- WATERMAIN
- OVERHEAD POWER
- ROAD CENTRELINE
- CONTOUR
- CONCRETE PATHS & DRIVEWAYS
- BUILDING
- STREET LIGHT
- POST
- SIGN
- BOUNDARY PEG
- STORMWATER MANHOLE
- WASTEWATER MANHOLE
- CATCHPIT
- FIRE HYDRANT
- WATER METER
- SLUICE VALVE
- COMMS PLINTH
- POWER POLE
- CLOTHESLINE
- TREE

- ### NOTES:
- LEVELS ARE IN TERMS OF AUCKLAND VERTICAL DATUM 1946. ORIGIN OF LEVELS RM III DP 404252 (EFWP) RL 19.78

COORDINATES ARE IN TERMS M.L. EDEN CIRCUIT 2000 ORIGIN OF COORDINATES RM III DP 404252 (EFWP) 781354.441 414935.123
 - CONTOURS ARE AT 0.50 INTERVALS.
 - THIS PLAN HAS BEEN CARRIED OUT TO TOPOGRAPHICAL STANDARDS. ALL LEVELS SHOWN ARE CORRECT AT TIME OF SURVEY. CRITICAL DIMENSIONS AND LEVELS SHOULD BE VERIFIED.
 - BOUNDARIES SHOWN ON THIS PLAN ARE FROM LAND INFORMATION NZ DCDB AND HAVE NOT BEEN SURVEYED. A BOUNDARY DEFINITION SURVEY SHOULD BE CARRIED OUT TO ESTABLISH EXACT BOUNDARY POSITIONS ON SITE.
 - ALL EASEMENTS, COVENANTS AND OTHER LEGAL INSTRUMENTS ASSOCIATED WITH THIS SITE MAY NOT BE SHOWN ON THIS PLAN. AN INVESTIGATION OF THE MOST CURRENT LEGAL RECORDS SHOULD BE UNDERTAKEN PRIOR TO DESIGN AND CONSTRUCTION COMMENCING.
 - THESE NOTES ARE AN INTEGRAL PART OF THIS PLAN.
 - THIS PLAN IS ISSUED FOR A SPECIFIC PROJECT AND MAY NOT BE ALTERED OR USED FOR ANY OTHER PURPOSE WITHOUT THE PRIOR WRITTEN CONSENT OF HARVEY SURVEYING.
 - PIPE SIZES, DEPTHS AND CONNECTIVITY TO BE CONFIRMED PRIOR TO CONSTRUCTION.
 - INVERT LEVELS SHOWN ARE FROM I_{OUT} IN A CLOCKWISE DIRECTION.
 - MANHOLES ARE NOT CENTERED OVER PIPES. HYDRO LOCATION OF UNDERGROUND PIPES SHOULD BE UNDERTAKEN FOR CRITICAL DESIGN ELEMENTS.

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C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

DRAWN: DPK 03.2019
 CHECKED: MSS 05.06.19
 APPROVED: MSS 05.06.19

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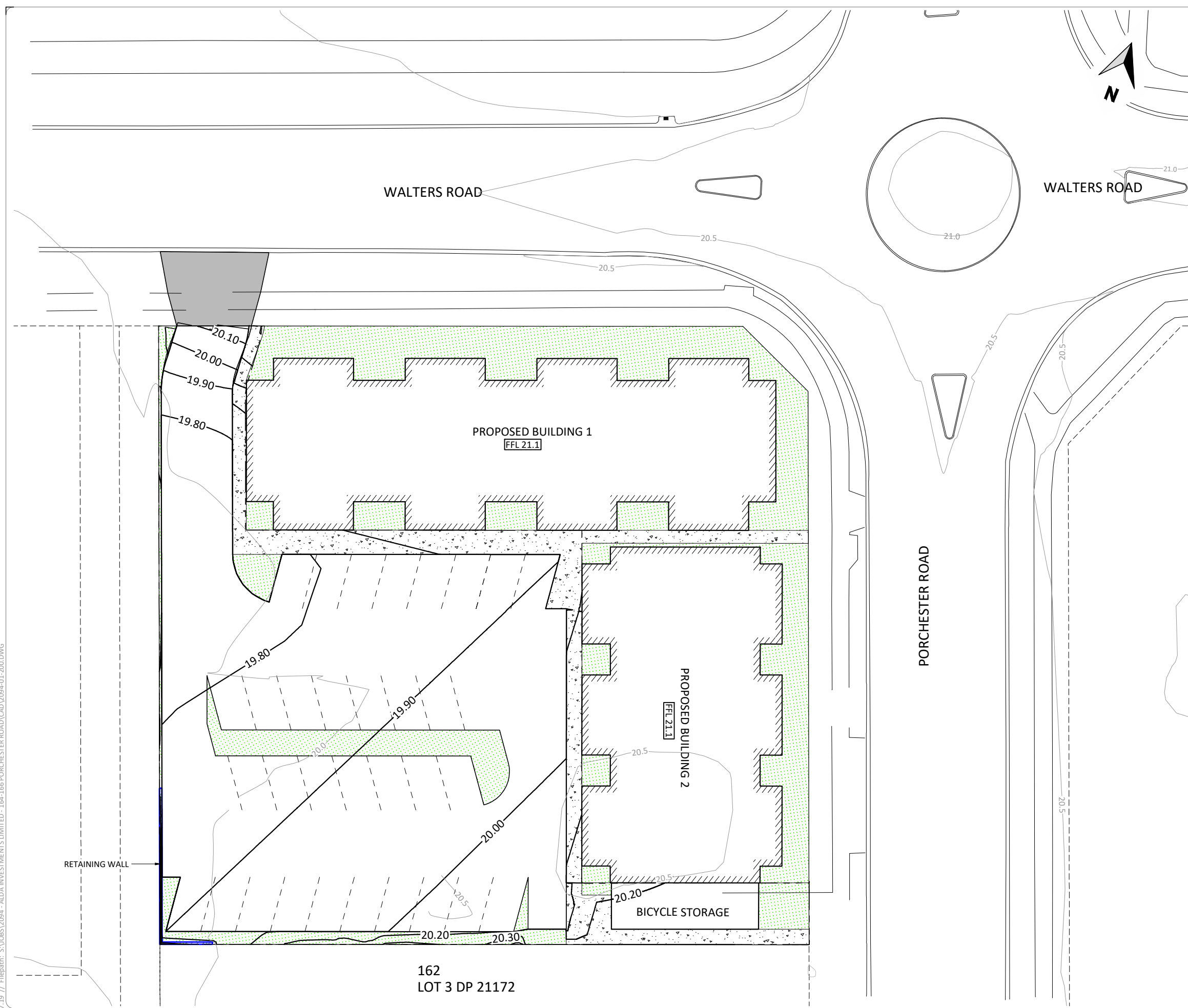
SHEET TITLE:
**TOPOGRAPHICAL PLAN OF
 LOTS 1 AND 2 DP 21172**

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-150** REV: **C2**



- NOTES:**
- EXISTING CONTOURS ARE SHOWN AT 0.5m INTERVALS.
 - DESIGN CONTOURS ARE SHOWN AT 0.1m INTERVALS.
 - CONTOURS SHOWN ARE FINISHED SURFACE LEVELS.
 - SILT AND STORMWATER CONTROL IS TO BE IMPLEMENTED DAILY AND COMPLY WITH THE GENERAL SPECIFICATION.
 - THE CONTRACTOR MUST BE AWARE OF AND COMPLY WITH AUCKLAND COUNCIL REQUIREMENTS FOR EARTHWORKS, AT ALL TIMES.
 - IT IS INTENDED THAT THE CONTRACTOR SHALL GRADE THE EARTHWORKS TO THE FINISHED CONTOURS SHOWN. HOWEVER, THE FINAL MARRYING AND SHAPING OF THE EARTHWORKS AREAS ARE SUBJECT TO THE ENGINEERS APPROVAL.
 - ROAD LONGSECTION SUBGRADE LEVELS, TYPICAL CROSS-SECTION LEVELS AND BOUNDARY LEVELS TAKE PRECEDENCE OVER PLAN CONTOUR LEVELS.

- LEGEND**
- 10.0 ——— EXISTING CONTOUR
 - 10.0 ——— PROPOSED MAJOR CONTOUR

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REV	REVISION DETAILS	BY	DATE
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19

DRAWN	DATE
MM	03.2019
CHECKED	DATE
MSS	05.06.19
APPROVED	DATE
MSS	05.06.19

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

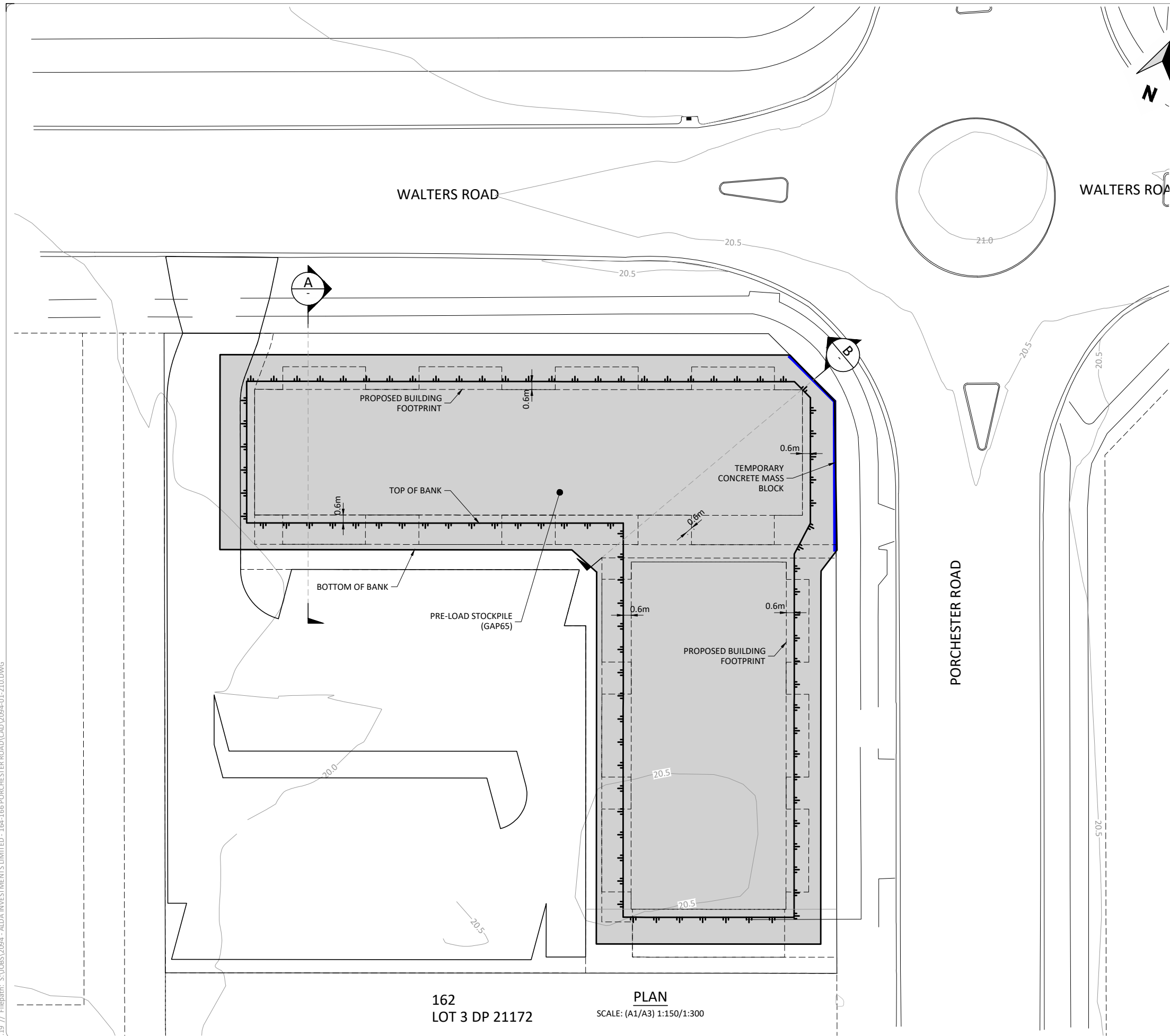
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PROPOSED CONTOUR PLAN

ISSUE STATUS: **CONSENT**

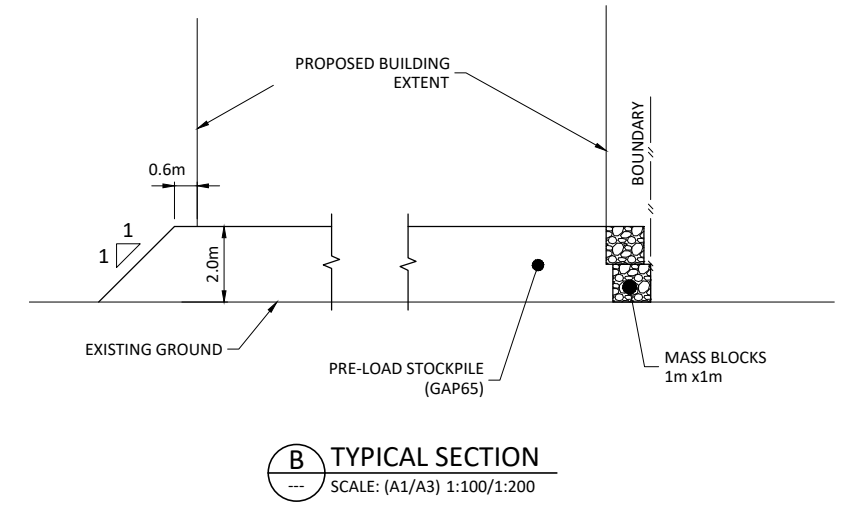
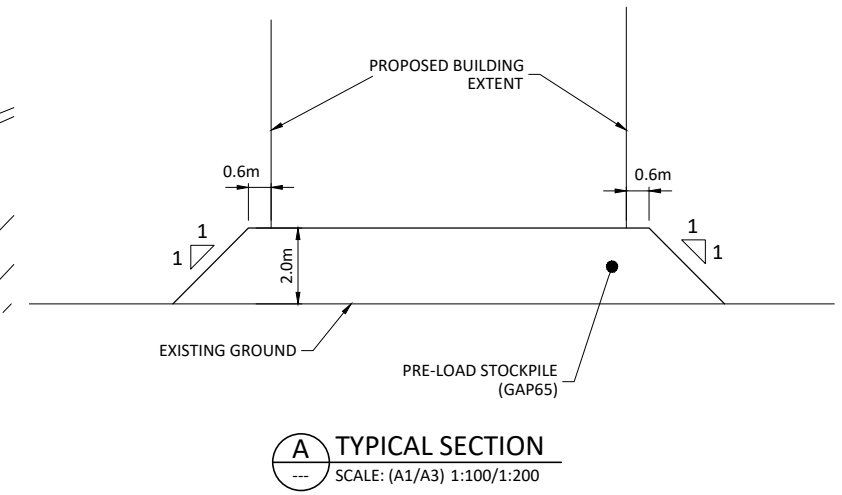
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SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-200** REV: **C2**



NOTES:
 1. REFER TO LANDER GEOTECHNICAL PRELOAD DESIGN REPORT FOR DETAILS.



162
 LOT 3 DP 21172
PLAN
 SCALE: (A1/A3) 1:150/1:300

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MM	03.2019	DRAWN DATE	
MM	19.06.19	CHECKED DATE	
MSS	19.06.19	APPROVED DATE	
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MM	20.06.19
REV	REVISION DETAILS	BY	DATE

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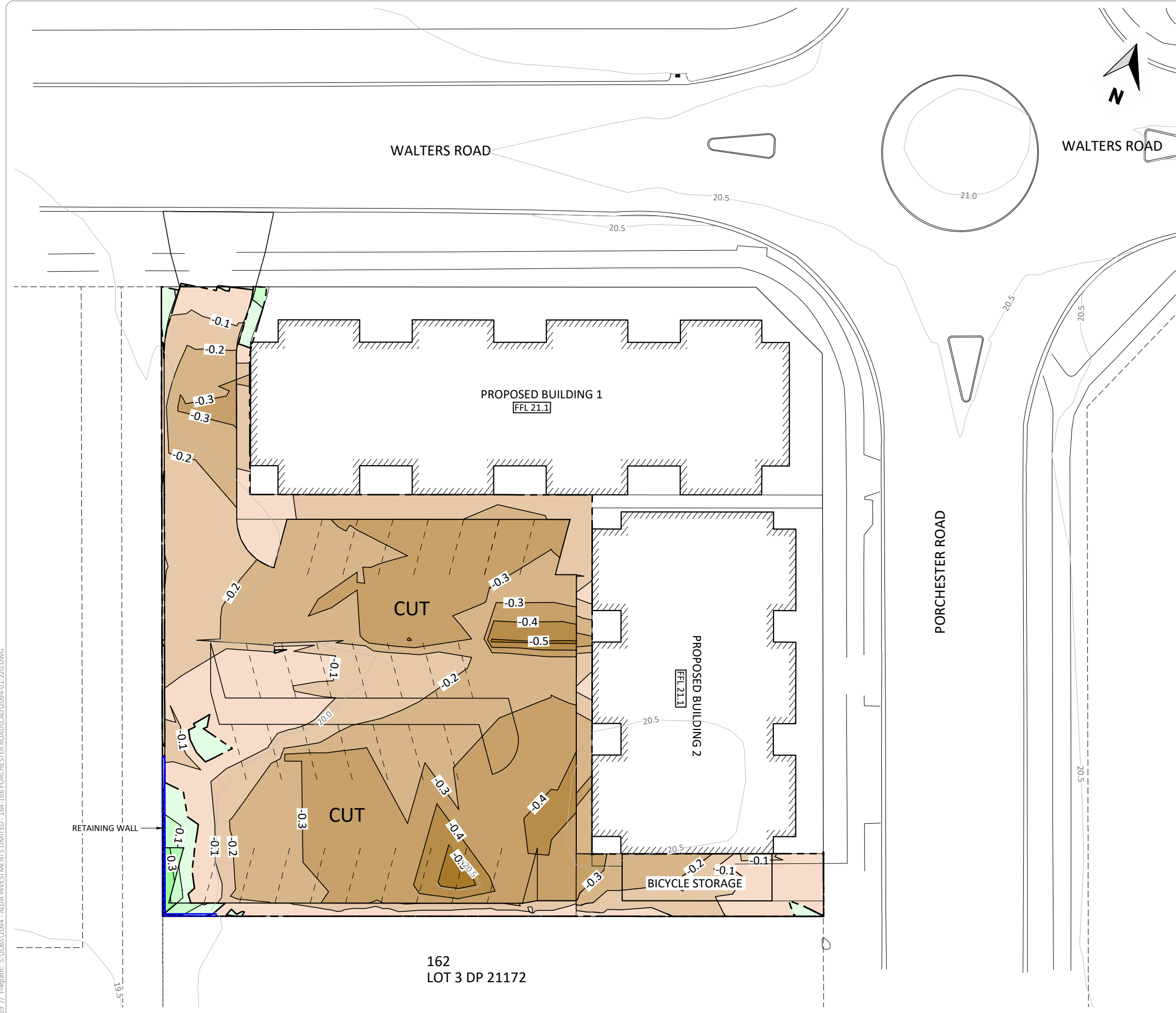
SHEET TITLE:
PRE-LOAD PLAN

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **AS SHOWN**

SCALE BAR
 N.T.S.

DRAWING NUMBER: **2094-01-210** REV: **C2**



NOTES:

1. PROPOSED ISOPACHS ARE SHOWN AT 0.1m INTERVALS.
2. ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS .
3. ALL TOPSOIL TO BE STRIPPED AND STOCKPILED CLEAR OF THE PROPOSED EARTHWORKS IN A LOCATION AGREED WITH THE ENGINEER.
4. ALL EROSION AND SEDIMENT CONTROL DEVICES TO BE INSTALLED, AS-BUILT AND INSPECTED BY THE ENGINEER / COUNCIL REPRESENTATIVE PRIOR TO COMMENCING THE EARTHWORKS ON THE SITE.
5. AREA OF EARTHWORKS = 0.12ha.
6. EARTHWORK VOLUMES ARE FROM EXISTING SURFACE TO FINISHED SURFACE AND ARE:
 CUT VOLUME= 277 m³
 FILL VOLUME= 3 m³

LEGEND

	-10.0	PROPOSED CUT ISOPACH
	-10.0	PROPOSED FILL ISOPACH
		EARTHWORKS ZERO CUT/FILL LINE
		EARTHWORK EXTENT
	CUT	PROPOSED CUT
	FILL	PROPOSED FILL

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MSS	05.06.19	APPROVED DATE
MSS	05.06.19	DATE
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C1	ISSUED FOR CONSENT	MSS 05.06.19
REV	REVISION DETAILS	BY DATE

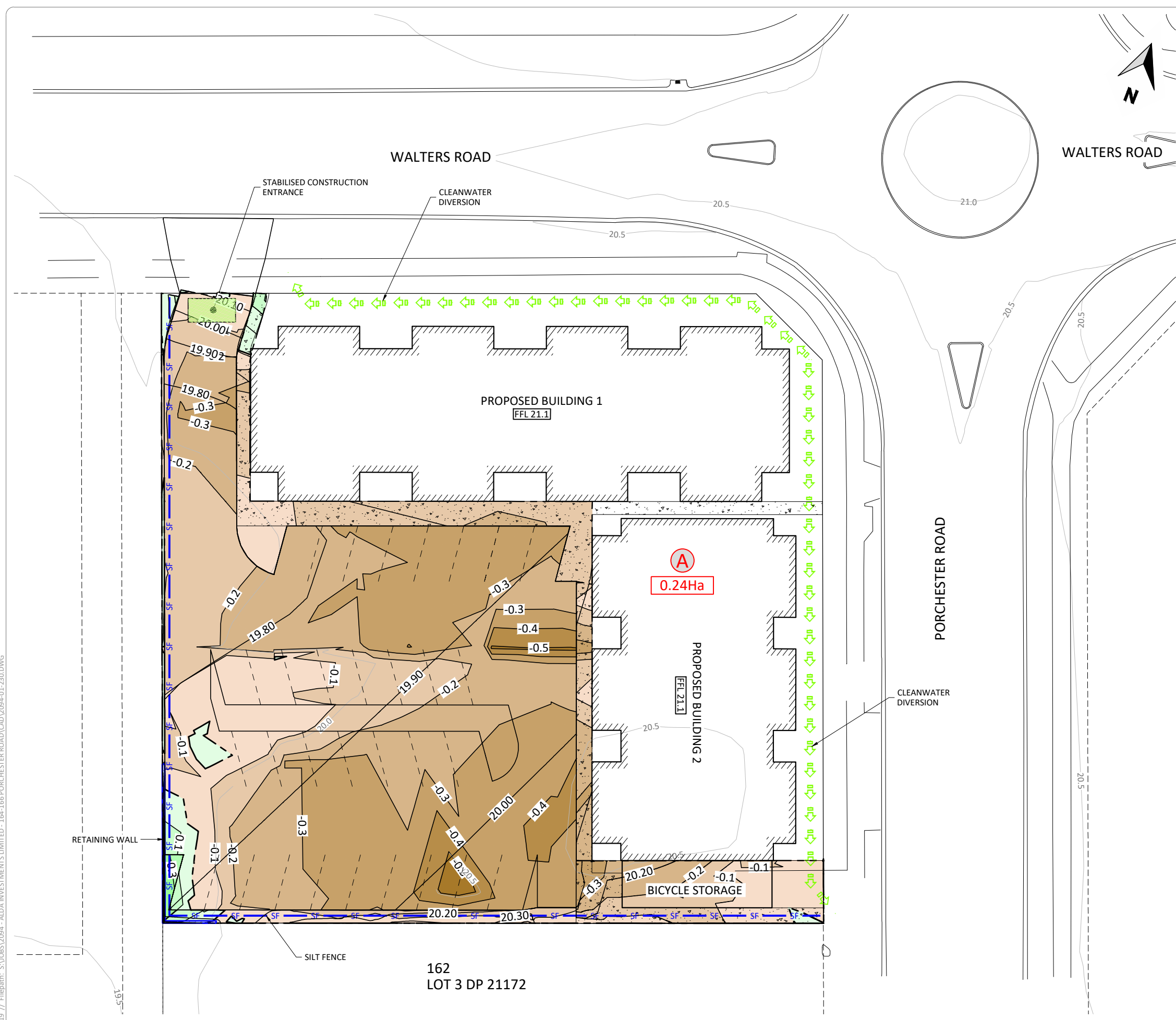
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
 ISOPACH PLAN**

ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) 1:150 / 1:300
 SCALE BAR 0 3 6 9 12 15m
 DRAWING NUMBER: **2094-01-220** REV: **C2**



- NOTES:**
- EXISTING CONTOURS ARE SHOWN AT 0.5m INTERVALS.
 - FINISHED CONTOURS ARE SHOWN AT 0.5m INTERVALS.
 - TOTAL SITE AREA = 0.24ha.
 - ALL WORKS TO COMPLY WITH COUNCIL ENGINEERING QUALITY STANDARDS AND ALL MATERIALS TO COMPLY WITH COUNCIL REQUIREMENTS.
 - ALL EROSION AND SEDIMENT CONTROL MEASURES MUST BE OPERATIONAL PRIOR TO ANY WORKS COMMENCING.
 - SEE SHEET 2094-01-235 TO 236 FOR STANDARD DETAILS
 - A COPY OF THE SEDIMENT CONTROL PLAN SHALL BE AVAILABLE ON THE SITE DURING WORK HOURS AND ALL PERSONNEL INVOLVED IN EARTHWORK ACTIVITIES ON THE SITE (INCLUSIVE OF SUB-CONTRACTORS) SHALL BE FAMILIAR WITH THE PLAN REQUIREMENTS AS THEY RELATE TO EROSION AND SEDIMENT CONTROL.
 - ALL "CLEANWATER" RUNOFF FROM STABILISED SURFACES INCLUDING CATCHMENT AREAS ABOVE THE SITE SHALL BE DIVERTED AWAY FROM EARTHWORK AREA VIA STABILISED SYSTEM, SO AS TO PREVENT SURFACE EROSION.
 - CUTOFF DRAINS TO BE INSTALLED AT THE MAXIMUM INTERVAL OF 50m AT THE END OF EACH DAY UNTIL SLOPES ARE STABILISED.
 - FURTHER SEDIMENT CONTROL WORKS MAY BE REQUIRED BY THE ENGINEER AS THE PROJECT ADVANCES. THESE WILL BE INSTALLED AS AND WHERE DIRECTED BY THE ENGINEER. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ENSURING THAT THE SITE HAS EFFECTIVE SILT DETENTION FACILITIES OPERATING AT ALL TIMES.
 - EITHER DECANTING EARTH BUND (DEB), POND, OR SILT FENCE TO BE UTILISED AS DIRECTED BY THE ENGINEER IN CONSULTATION WITH AUCKLAND COUNCIL.
 - ALL EROSION AND SEDIMENT CONTROL MEASURES TO COMPLY WITH AUCKLAND COUNCIL'S GD05.
 - SEDIMENT AND EROSION CONTROL MEASURES SHOWN ON THIS DRAWING ARE WORST CASE AND ASSUME A FULLY OPEN SITE WITH UNSTABILISED CLAY. CONTRACTOR METHODOLOGY AND MATERIALS USED WILL HAVE A BEARING ON REQUIRED SEDIMENT AND EROSION CONTROL. FOR EXAMPLE, AS FINAL LAYERS OF FILL START TO SLOPE TOWARDS BOUNDARIES, IF METAL IS USED IN TO COVER THESE AREAS, THE AREA IS DEEMED STABILISED AND REQUIRED SEDIMENT AND EROSION CONTROL MEASURES WILL BE REDUCED UPON APPROVAL BY COUNCIL.
 - DEVICES SHALL NOT BE DE-COMMISSIONED WITHOUT THE EXPRESS APPROVAL OF COUNCILS MONITORING OFFICER.

LEGEND

- 10.00- EXISTING CONTOURS
- 10.00- PROPOSED CONTOURS
- [Green hatched box] STABILISED CONSTRUCTION ACCESS
- SF SILT FENCE
- [Green arrows] CLEAN WATER DIVERSION
- [Red X in circle] CATCHMENT NUMBER
- [Red box with X.XXha] CATCHMENT AREA

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
 SEDIMENT AND EROSION CONTROL
 PLAN**

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-236** REV: **C2**

SILT FENCE:

APPLICATION

- ON LOW GRADIENT SITES OR FOR CONFINED AREAS WHERE THE CONTRIBUTING CATCHMENT IS SMALL, SUCH AS SHORT STEEP BATTER FILLS AND AROUND WATERCOURSES.
- TO DELINEATE THE LIMIT OF DISTURBANCE ON AN EARTHWORKS SITE SUCH AS RIPARIAN AREAS OR BUSH RESERVES.
- TO STORE RUNOFF BEHIND THE SILT FENCE WITHOUT DAMAGING THE FENCE OR THE SUBMERGED AREA BEHIND THE FENCE.
- DO NOT INSTALL SILT FENCES ACROSS WATERCOURSES OR IN AREAS OF CONCENTRATED FLOWS.

DESIGN

- ENSURE THE SILT FENCE HEIGHT IS A MINIMUM OF 600mm ABOVE AND 200mm BELOW GROUND LEVEL.
- PLACE SUPPORTING POSTS/WARATAHS FOR SILT FENCES 2m-4m WITH SUPPORT PROVIDED BY TENSIONED WIRE (2.5mm HT) ALONG THE TOP OF THE SILT FENCE. WHERE A STRONG WOVEN FABRIC IS USED IN CONJUNCTION WITH A WIRE SUPPORT, THE DISTANCE BETWEEN POSTS CAN BE EXTENDED UP TO 4m. DOUBLE THE SILT FENCE FABRIC OVER AND FASTEN TO THE WIRE AND POSTS WITH SILT FENCE CLIPS AT 500mm SPACINGS. ENSURE SUPPORTING POSTS ARE EMBEDDED A MINIMUM OF 400mm INTO THE GROUND.
- ALWAYS INSTALL SILT FENCES ALONG THE CONTOUR (AT A BREAK IN SLOPE). WHERE THIS IS NOT POSSIBLE OR WHERE THERE ARE LONG SECTIONS OF SILT FENCE, INSTALL SHORT SILT FENCE RETURNS PROJECTING UP SLOPE TO MINIMISE CONCENTRATION OF FLOWS. SILT FENCE RETURNS ARE A MINIMUM 2m IN LENGTH, CAN INCORPORATE A TIE BACK AND ARE GENERALLY CONSTRUCTED BY CONTINUING THE SILT FENCE AROUND THE RETURN AND DOUBLING BACK, ELIMINATING JOINS.
- JOIN LENGTHS OF SILT FENCE BY DOUBLING OVER FABRIC ENDS AROUND A WARRATAH, WOODEN POST OR BATTEN OR BY STAPLING THE FABRIC ENDS TO A BATTEN AND BUTTING THE TWO BATTENS TOGETHER OR BY OVERLAPPING AT LEAST 2m.
- MAXIMUM SLOPE LENGTHS, SPACING OF RETURNS AND ANGLES FOR SILT FENCES ARE SHOWN IN THE TABLE BELOW.
- INSTALL SILT FENCE RETURNS AT EITHER END OF THE SILT FENCE PROJECTING UPSLOPE TO A SUFFICIENT HEIGHT TO PREVENT OUTFLANKING.
- WHERE IMPOUNDED FLOW MAY OVERTOP THE SILT FENCE, CROSSING NATURAL DEPRESSIONS OR LOW POINTS, MAKE PROVISION FOR A RIPRAP SPLASH PAD OR OTHER OUTLET PROTECTION DEVICE.

MAINTENANCE

- INSPECT SILT FENCES AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL. MAKE ANY NECESSARY REPAIRS WHEN BULGES OCCUR OR WHEN SEDIMENT ACCUMULATION REACHES 50% OF THE FABRIC HEIGHT.
- ANY AREAS OF COLLAPSE, DECOMPOSITION OR INEFFECTIVENESS NEED TO BE IMMEDIATELY REPLACED.
- REMOVE SEDIMENT DEPOSITS AS NECESSARY TO CONTINUE TO ALLOW FOR ADEQUATE SEDIMENT STORAGE AND REDUCE PRESSURE ON THE SILT FENCE. ENSURE THAT THE SEDIMENT IS REMOVED TO A SECURE AREA.
- DO NOT REMOVE SILT FENCE MATERIALS AND SEDIMENT DEPOSITION UNTIL THE CATCHMENT AREA HAS BEEN APPROPRIATELY STABILISED. STABILISE THE AREA OF THE REMOVED SILT FENCE.

SUPER SILT FENCE DESIGN CRITERIA:

SLOPE STEEPNESS %	SLOPE LENGTH (m) (MAXIMUM)	SPACING OF RETURNS (m)	SUPER SILT FENCE LENGTH (m) (MAXIMUM)
0-10%	UNLIMITED	60	UNLIMITED
10-20%	60	50	450
20-33%	30	40	300
33-50%	30	30	150
>50%	15	20	75

SUPER SILT FENCE:

APPLICATION

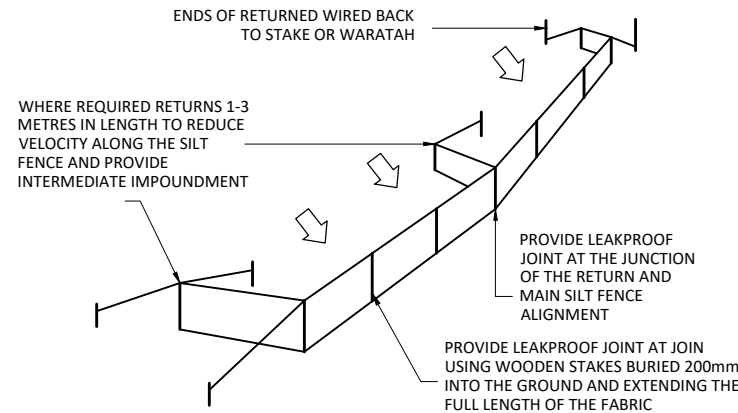
- PROVIDES A BARRIER THAT CAN COLLECT AND HOLD DEBRIS AND SOIL, PREVENTING THE MATERIAL FROM ENTERING CRITICAL AREAS, WATERCOURSES AND STREETS.
- CAN BE USED WHERE THE INSTALLATION OF AN EARTH OR TOPSOIL BUND WOULD DESTROY SENSITIVE AREAS SUCH AS BUSH AND WETLANDS.
- SHOULD BE PLACED AS CLOSE TO THE CONTOUR AS POSSIBLE. NO SECTION OF THE FENCE SHOULD EXCEED A GRADE OF 5% FOR A DISTANCE OF MORE THAN 15m.

DESIGN

- ENSURE THE SILT FENCE HEIGHT IS A MINIMUM OF 600mm ABOVE AND 200mm BELOW GROUND LEVEL.
- WHEN CONSIDERING SUPER SILT FENCE INSTALLATION FOR LARGER CATCHMENTS (GREATER THAN 0.5ha) AS IN THE TABLE BELOW, CAREFULLY CONSIDER THE SPECIFIC SITE CONDITIONS AND OTHER ALTERNATIVE CONTROL MEASURES AVAILABLE. BASE THE LENGTH OF THE SUPER SILT FENCE ON THE LIMITS SHOWN IN THE TABLE LEFT.
- ALWAYS INSTALL SILT FENCES ALONG THE CONTOUR (AT A BREAK IN SLOPE). WHERE THIS IS NOT POSSIBLE OR WHERE THERE ARE LONG SECTIONS OF SILT FENCE, INSTALL SHORT SILT FENCE RETURNS PROJECTING UP SLOPE TO MINIMISE CONCENTRATION OF FLOWS. SILT FENCE RETURNS ARE A MINIMUM 2m IN LENGTH, CAN INCORPORATE A TIE BACK AND ARE GENERALLY CONSTRUCTED BY CONTINUING THE SILT FENCE AROUND THE RETURN AND DOUBLING BACK, ELIMINATING JOINS.
- JOIN LENGTHS OF SILT FENCE BY DOUBLING OVER FABRIC ENDS AROUND A WARRATAH, WOODEN POST OR BATTEN OR BY STAPLING THE FABRIC ENDS TO A BATTEN AND BUTTING THE TWO BATTENS TOGETHER OR BY OVERLAPPING AT LEAST 2m.
- LIMITS IMPOSED BY ULTRA VIOLET LIGHT AFFECT THE STABILITY OF THE FABRIC AND WILL DICTATE THE MAXIMUM PERIOD THAT THE SUPER SILT FENCE MAY BE USED.
- WHERE ENDS OF THE GEOTEXTILE FABRIC COME TOGETHER, OVERLAP, FOLD AND STAPLE THE FABRIC ENDS TO PREVENT SEDIMENT BYPASS.

MAINTENANCE

- INSPECT SILT FENCES AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL. MAKE ANY NECESSARY REPAIRS WHEN BULGES OCCUR OR WHEN SEDIMENT ACCUMULATION REACHES 50% OF THE FABRIC HEIGHT.
- ANY AREAS OF COLLAPSE, DECOMPOSITION OR INEFFECTIVENESS NEED TO BE IMMEDIATELY REPLACED.
- REMOVE SEDIMENT DEPOSITS AS NECESSARY TO CONTINUE TO ALLOW FOR ADEQUATE SEDIMENT STORAGE AND REDUCE PRESSURE ON THE SILT FENCE. ENSURE THAT THE SEDIMENT IS REMOVED TO A SECURE AREA.
- DO NOT REMOVE SILT FENCE MATERIALS AND SEDIMENT DEPOSITION UNTIL THE CATCHMENT AREA HAS BEEN APPROPRIATELY STABILISED. STABILISE THE AREA OF THE REMOVED SILT FENCE.

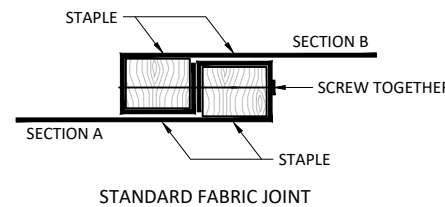


PERSPECTIVE VIEW

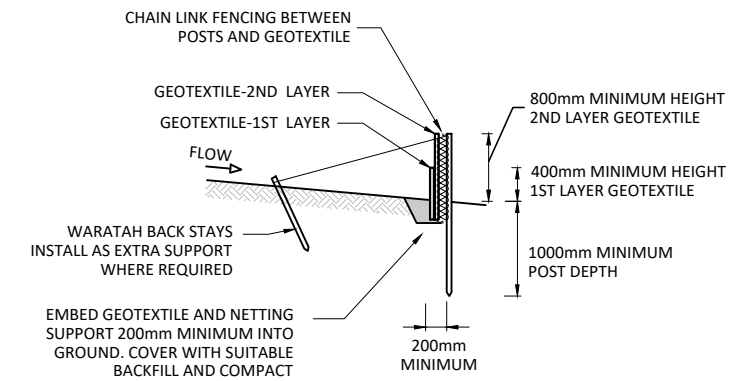
SILT FENCE DESIGN CRITERIA:

SLOPE STEEPNESS %	SLOPE LENGTH (m) (MAXIMUM)	SPACING OF RETURNS (m)	SILT FENCE LENGTH (m) (MAXIMUM)
< 2%	UNLIMITED	N/A	UNLIMITED
2-10%	40	60	300
10-20%	30	50	230
20-33%	20	40	150
33-50%	15	30	75
>50%	6	20	40

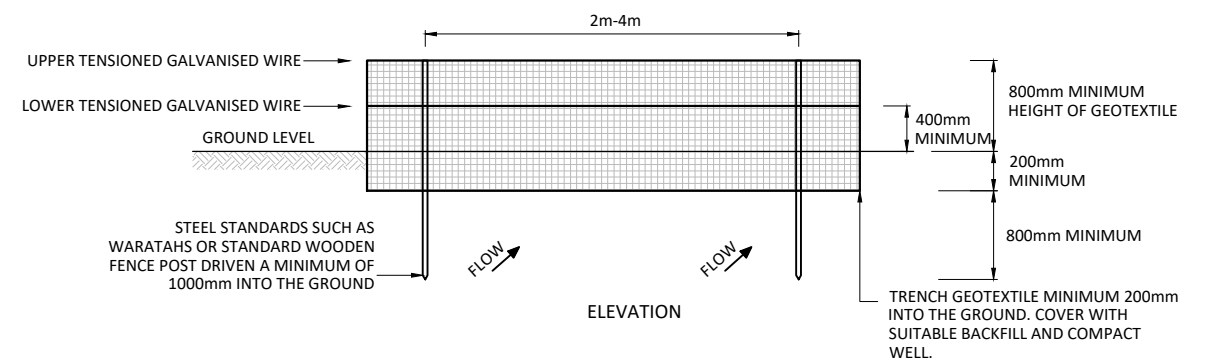
GRAB TENSILE STRENGTH: >440N (ASTM D4632)
 TENSILE MODULUS: 0.140 pa (MINIMUM)
 APPARENT OPENING SIZE: 0.1-0.5mm (ASTM D4751)



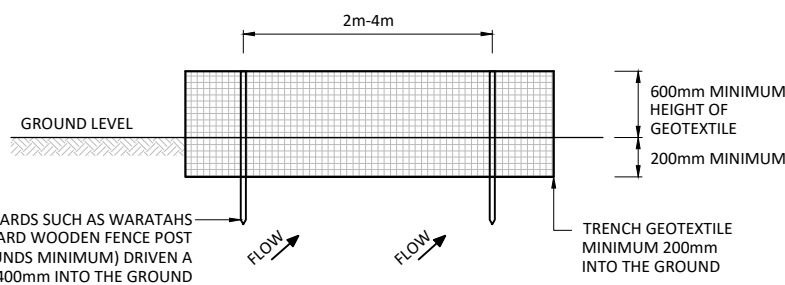
STANDARD FABRIC JOINT



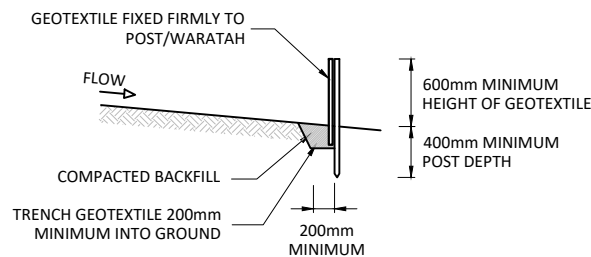
CROSS SECTION



SUPER SILT FENCE CONSTRUCTION



ELEVATION



CROSS SECTION

SILT FENCE CONSTRUCTION

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C1	ISSUED FOR CONSENT	05.06.19	MSS
REV	REVISION DETAILS		

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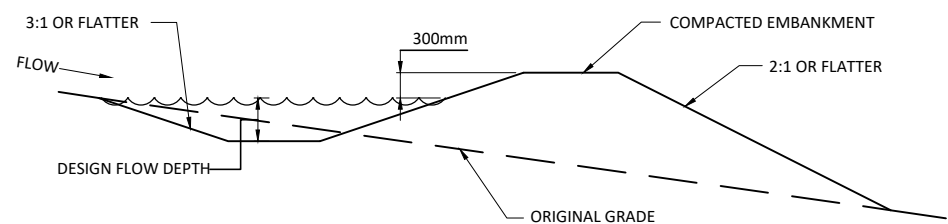
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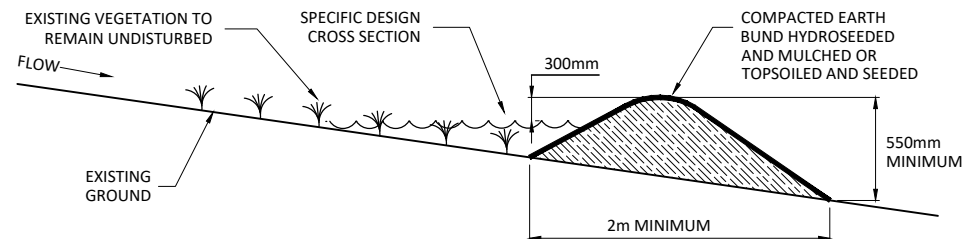
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**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
 SEDIMENT AND EROSION CONTROL
 STANDARD DETAILS-SHEET 1**

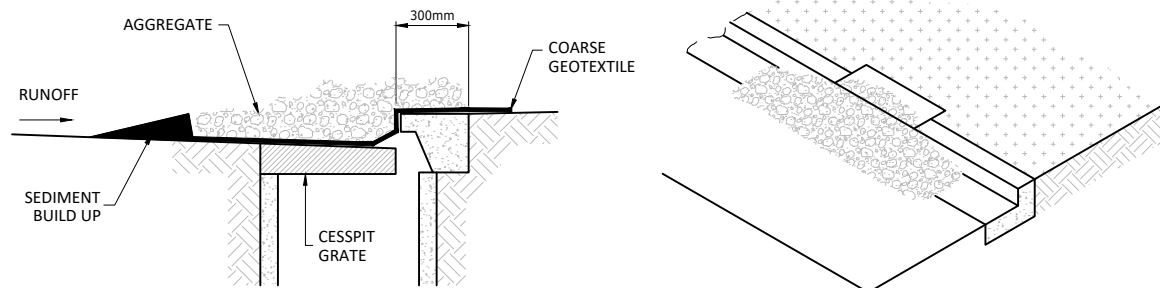
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 SCALE: (A1/A3) **NOT TO SCALE**
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 N.T.S.
 DRAWING NUMBER: **2094-01-235** REV: **C2**



DIRTY WATER DIVERSION - CROSS SECTION



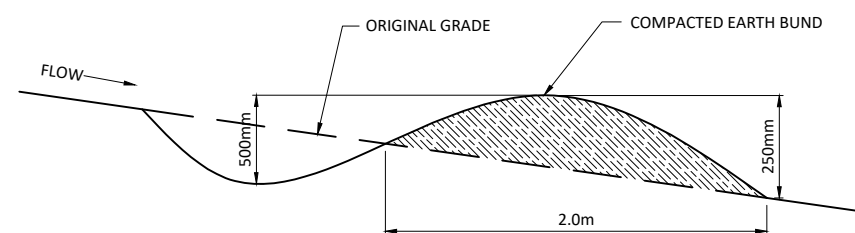
CLEANWATER DIVERSION - CROSS SECTION



CROSS SECTION

PERSPECTIVE VIEW

STORMWATER INLET PROTECTION



CONTOUR DRAIN

STABILISED CONSTRUCTION ENTRANCE:

APPLICATION

- USE A STABILISED CONSTRUCTION ENTRANCE AT ALL POINTS OF CONSTRUCTION SITE INGRESS AND EGRESS WITH A CONSTRUCTION PLAN LIMITING TRAFFIC TO THESE ENTRANCES ONLY.

DESIGN

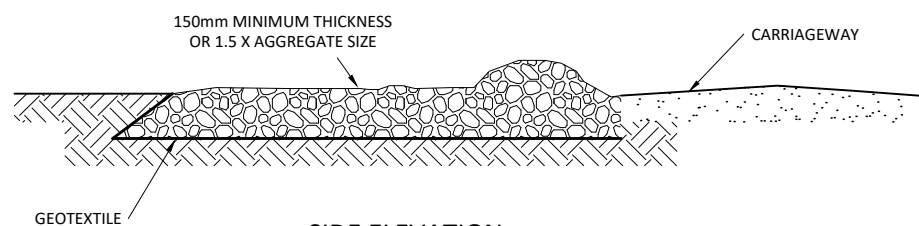
- CLEAR THE ENTRANCE AND EXIT AREA OF ALL VEGETATION, ROOTS AND OTHER UNSUITABLE MATERIAL AND PROPERLY GRADE IT.
- LAY WOVEN GEOTEXTILE; PIN DOWN EDGES AND OVERLAP JOINTS.
- PROVIDE DRAINAGE TO CARRY RUNOFF FROM THE STABILISED CONSTRUCTION ENTRANCE TO A SEDIMENT CONTROL MEASURE.
- PLACE AGGREGATE TO THE SPECIFICATIONS BELOW AND SMOOTH IT.

STABILISED CONSTRUCTION ENTRANCE AGGREGATE SPECIFICATIONS:

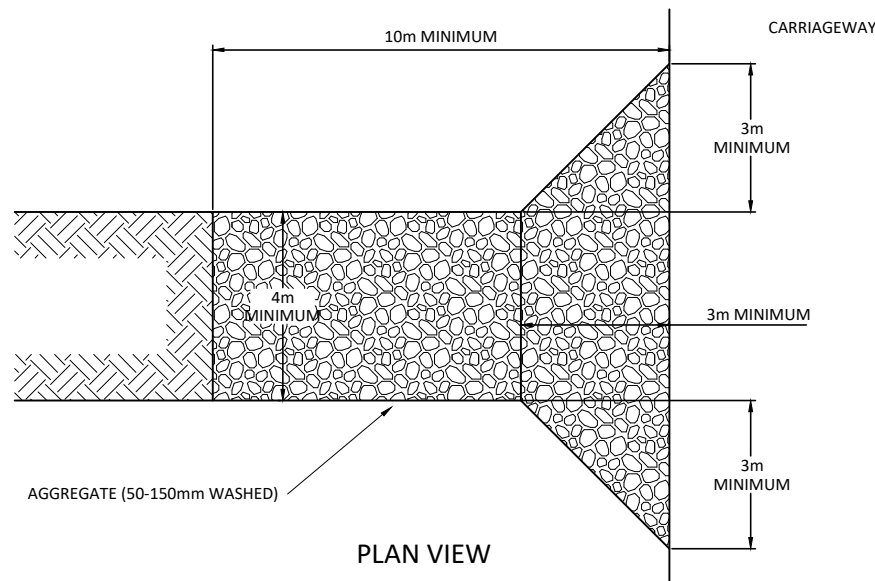
AGGREGATE SIZE	50-150mm WASHED AGGREGATE
THICKNESS	150mm MINIMUM OR 1.5 X AGGREGATE SIZE
LENGTH	10m MINIMUM LENGTH RECOMMENDED
WIDTH	4m MINIMUM WIDTH

MAINTENANCE

- MAINTAIN THE STABILISED CONSTRUCTION ENTRANCE IN A CONDITION TO PREVENT SEDIMENT FROM LEAVING THE CONSTRUCTION SITE. AFTER EACH RAINFALL INSPECT ANY STRUCTURE USED TO TRAP SEDIMENT FROM THE STABILISED CONSTRUCTION ENTRANCE AND CLEAN OUT AS NECESSARY.
- WHEN WHEEL WASHING IS ALSO REQUIRED, ENSURE THIS IS DONE ON AN AREA STABILISED WITH AGGREGATE WHICH DRAINS TO AN APPROVED SEDIMENT RETENTION FACILITY.

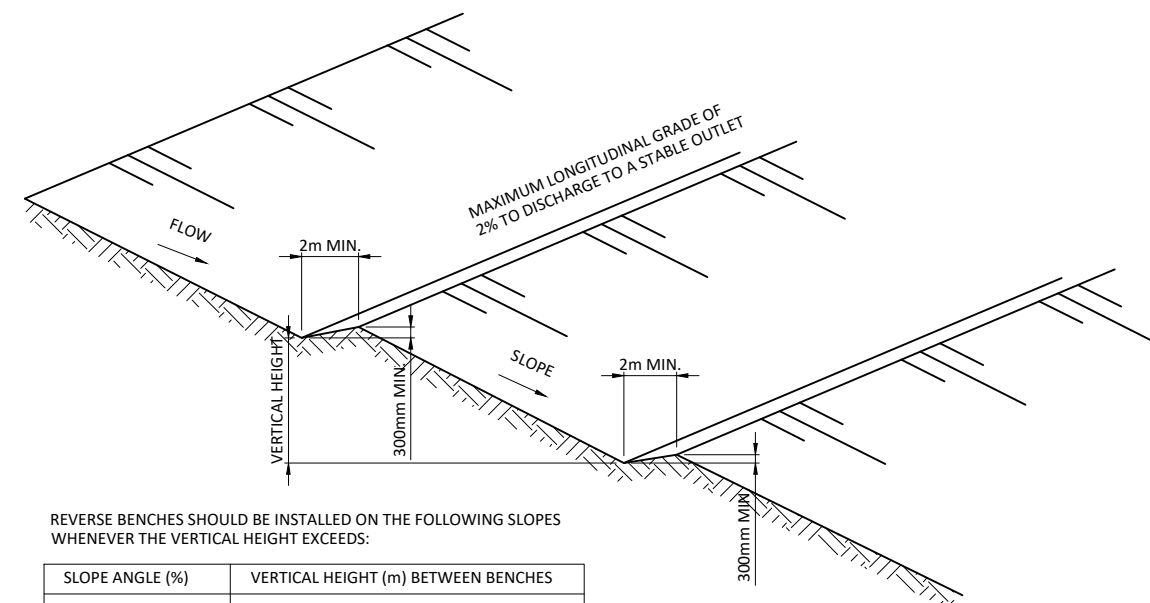


SIDE ELEVATION



PLAN VIEW

STABILISED CONSTRUCTION ENTRANCE



REVERSE BENCHES SHOULD BE INSTALLED ON THE FOLLOWING SLOPES WHENEVER THE VERTICAL HEIGHT EXCEEDS:

SLOPE ANGLE (%)	VERTICAL HEIGHT (m) BETWEEN BENCHES
50	10
33	15
25	20

BENCHED SLOPE

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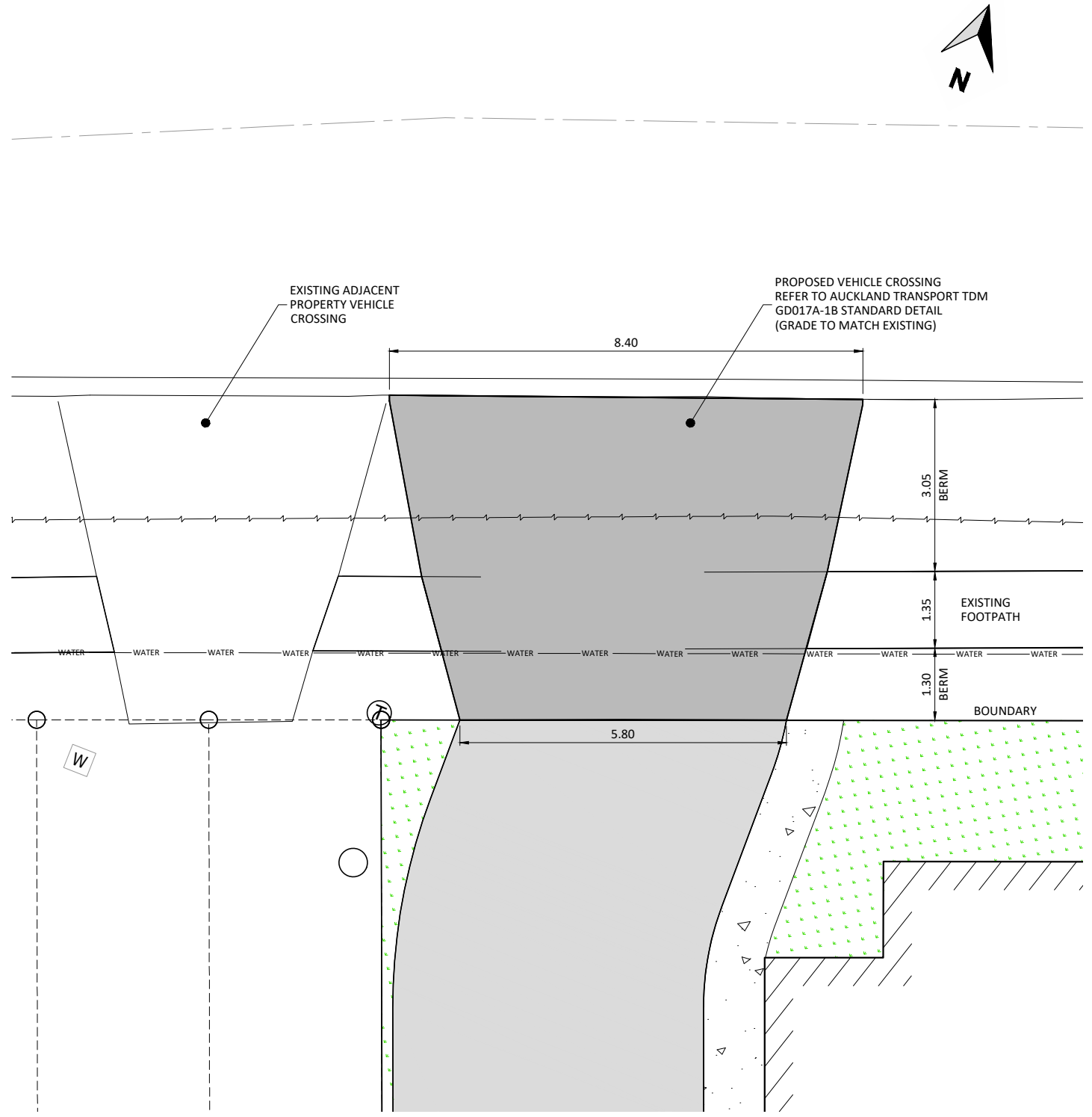
CIVILPLAN CONSULTANTS
 Level 3, 3 Osterley Way, Manukau, Auckland. Phone: 09 222 2445

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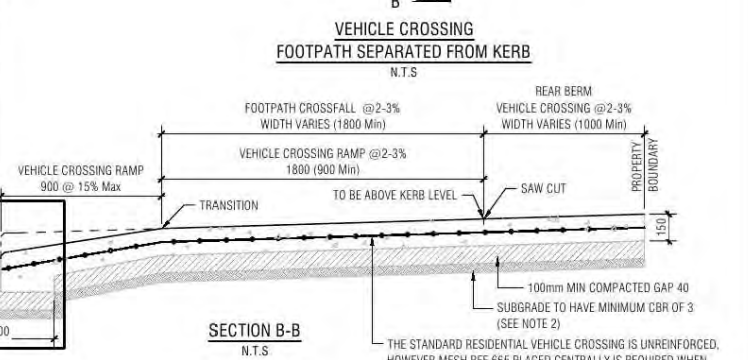
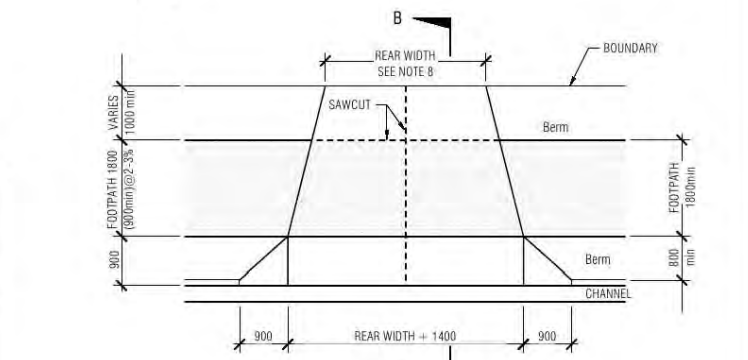
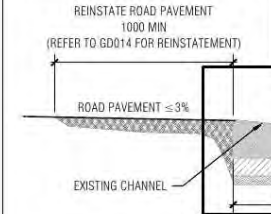
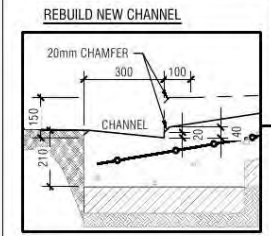
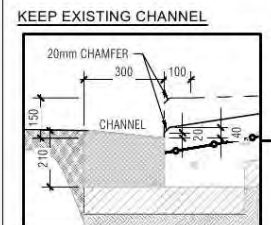
SHEET TITLE:
 PROPOSED EARTHWORKS
 SEDIMENT AND EROSION CONTROL
 STANDARD DETAILS-SHEET 2

ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) NOT TO SCALE
 SCALE BAR
 N.T.S.
 DRAWING NUMBER: 2094-01-235
 REV: C2

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VEHICLE CROSSING PLAN
SCALE: (A1/A3) 1:50/1:100



- Notes:**
- All dimensions are in millimetres unless noted otherwise.
 - If CBR of existing Subgrade is < 3, Pavement Design should be provided and approved by Auckland Transport.
 - All concrete to be 20 Mpa and constructed in accordance with NZS 3109 with a broom finish and may contain upto 4% oxide.
 - Saw cut expansion joints at 4m centres maximum each way in addition to saw cuts shown on dwg.
 - All work must comply with the NZTA's 'CoPTM' (code of practice for temporary traffic management).
 - Construct in same material and finish as surrounding footpath.
 - Existing channel may be retained if:
 - kerb can be removed without disturbing channel
 - road crossfall does not exceed 3%
 - Rear Width to be as permitted under Auckland unitary Plan:
 - 2750-3000 - Single vehicle crossing
 - 5500-6000 - Two-Way Shared Access
 - 3000-3500 - One-Way Shared Access

GO TO COVER SHEET

	Drawn: Ulysses Gabriel	Project:	TDM TECHNICAL STANDARDS Road layout and geometric design Residential Vehicle Crossing (Sheet 3 of 4)	Date:	-
	Checked: Richard Batty			Scales:	N.T.S.
	Approved: Chris Beasley			Drawing No.:	GD017A-1B
	Authorised: Chief Engineer				

C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

DRAWN:	DATE:	
MM	03.2019	
CHECKED:	DATE:	
MSS	05.06.19	
APPROVED:	DATE:	
MSS	05.06.19	

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PROJECT TITLE:
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164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

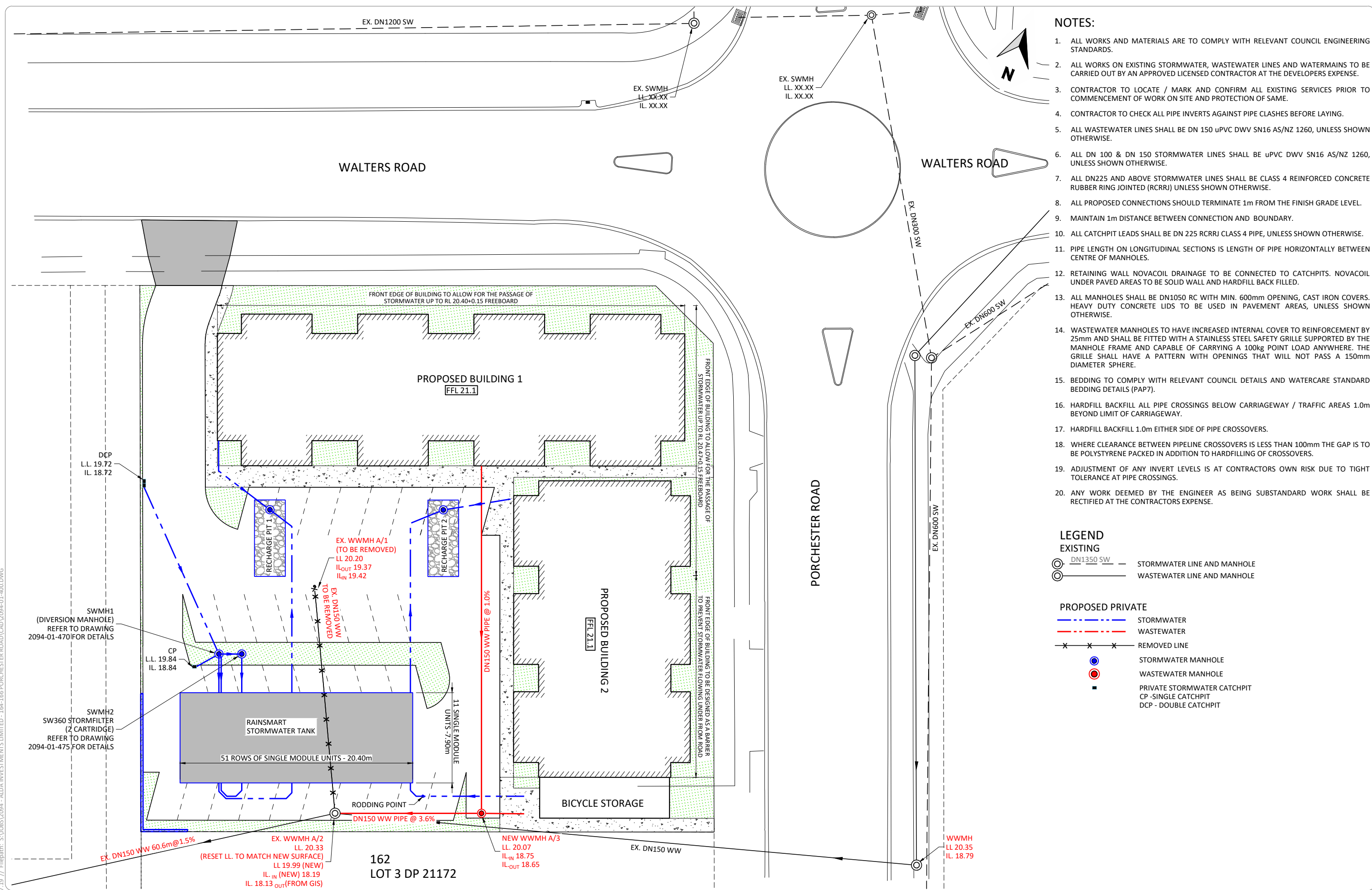
SHEET TITLE:
VEHICLE CROSSING
PLAN AND DETAIL

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:50 / 1:100

SCALE BAR 0 1 2 3 4 5m

DRAWING NUMBER: 2094-01-370
REV: C2



- NOTES:**
- ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS.
 - ALL WORKS ON EXISTING STORMWATER, WASTEWATER LINES AND WATERMANS TO BE CARRIED OUT BY AN APPROVED LICENSED CONTRACTOR AT THE DEVELOPERS EXPENSE.
 - CONTRACTOR TO LOCATE / MARK AND CONFIRM ALL EXISTING SERVICES PRIOR TO COMMENCEMENT OF WORK ON SITE AND PROTECTION OF SAME.
 - CONTRACTOR TO CHECK ALL PIPE INVERTS AGAINST PIPE CLASHES BEFORE LAYING.
 - ALL WASTEWATER LINES SHALL BE DN 150 uPVC DWV SN16 AS/NZ 1260, UNLESS SHOWN OTHERWISE.
 - ALL DN 100 & DN 150 STORMWATER LINES SHALL BE uPVC DWV SN16 AS/NZ 1260, UNLESS SHOWN OTHERWISE.
 - ALL DN225 AND ABOVE STORMWATER LINES SHALL BE CLASS 4 REINFORCED CONCRETE RUBBER RING JOINTED (RCRRJ) UNLESS SHOWN OTHERWISE.
 - ALL PROPOSED CONNECTIONS SHOULD TERMINATE 1m FROM THE FINISH GRADE LEVEL.
 - MAINTAIN 1m DISTANCE BETWEEN CONNECTION AND BOUNDARY.
 - ALL CATCHPIT LEADS SHALL BE DN 225 RCRRJ CLASS 4 PIPE, UNLESS SHOWN OTHERWISE.
 - PIPE LENGTH ON LONGITUDINAL SECTIONS IS LENGTH OF PIPE HORIZONTALLY BETWEEN CENTRE OF MANHOLES.
 - RETAINING WALL NOVA COIL DRAINAGE TO BE CONNECTED TO CATCHPITS. NOVA COIL UNDER PAVED AREAS TO BE SOLID WALL AND HARDFILL BACK FILLED.
 - ALL MANHOLES SHALL BE DN1050 RC WITH MIN. 600mm OPENING, CAST IRON COVERS. HEAVY DUTY CONCRETE LIDS TO BE USED IN PAVEMENT AREAS, UNLESS SHOWN OTHERWISE.
 - WASTEWATER MANHOLES TO HAVE INCREASED INTERNAL COVER TO REINFORCEMENT BY 25mm AND SHALL BE FITTED WITH A STAINLESS STEEL SAFETY GRILLE SUPPORTED BY THE MANHOLE FRAME AND CAPABLE OF CARRYING A 100kg POINT LOAD ANYWHERE. THE GRILLE SHALL HAVE A PATTERN WITH OPENINGS THAT WILL NOT PASS A 150mm DIAMETER SPHERE.
 - BEDDING TO COMPLY WITH RELEVANT COUNCIL DETAILS AND WATERCARE STANDARD BEDDING DETAILS (PAP7).
 - HARDFILL BACKFILL ALL PIPE CROSSINGS BELOW CARRIAGEWAY / TRAFFIC AREAS 1.0m BEYOND LIMIT OF CARRIAGEWAY.
 - HARDFILL BACKFILL 1.0m EITHER SIDE OF PIPE CROSSOVERS.
 - WHERE CLEARANCE BETWEEN PIPELINE CROSSOVERS IS LESS THAN 100mm THE GAP IS TO BE POLYSTYRENE PACKED IN ADDITION TO HARDFILLING OF CROSSOVERS.
 - ADJUSTMENT OF ANY INVERT LEVELS IS AT CONTRACTORS OWN RISK DUE TO TIGHT TOLERANCE AT PIPE CROSSINGS.
 - ANY WORK DEEMED BY THE ENGINEER AS BEING SUBSTANDARD WORK SHALL BE RECTIFIED AT THE CONTRACTORS EXPENSE.

LEGEND

EXISTING

- DN1350 SW — STORMWATER LINE AND MANHOLE
- — WASTEWATER LINE AND MANHOLE

PROPOSED PRIVATE

- STORMWATER
- WASTEWATER
- REMOVED LINE
- STORMWATER MANHOLE
- WASTEWATER MANHOLE
- PRIVATE STORMWATER CATCHPIT
- CP - SINGLE CATCHPIT
- DCP - DOUBLE CATCHPIT

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C3	ISSUED FOR CONSENT	MSS	29.07.19
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19

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MM	03.2019
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MSS	05.06.19
APPROVED	DATE
MSS	05.06.19

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PROJECT TITLE:
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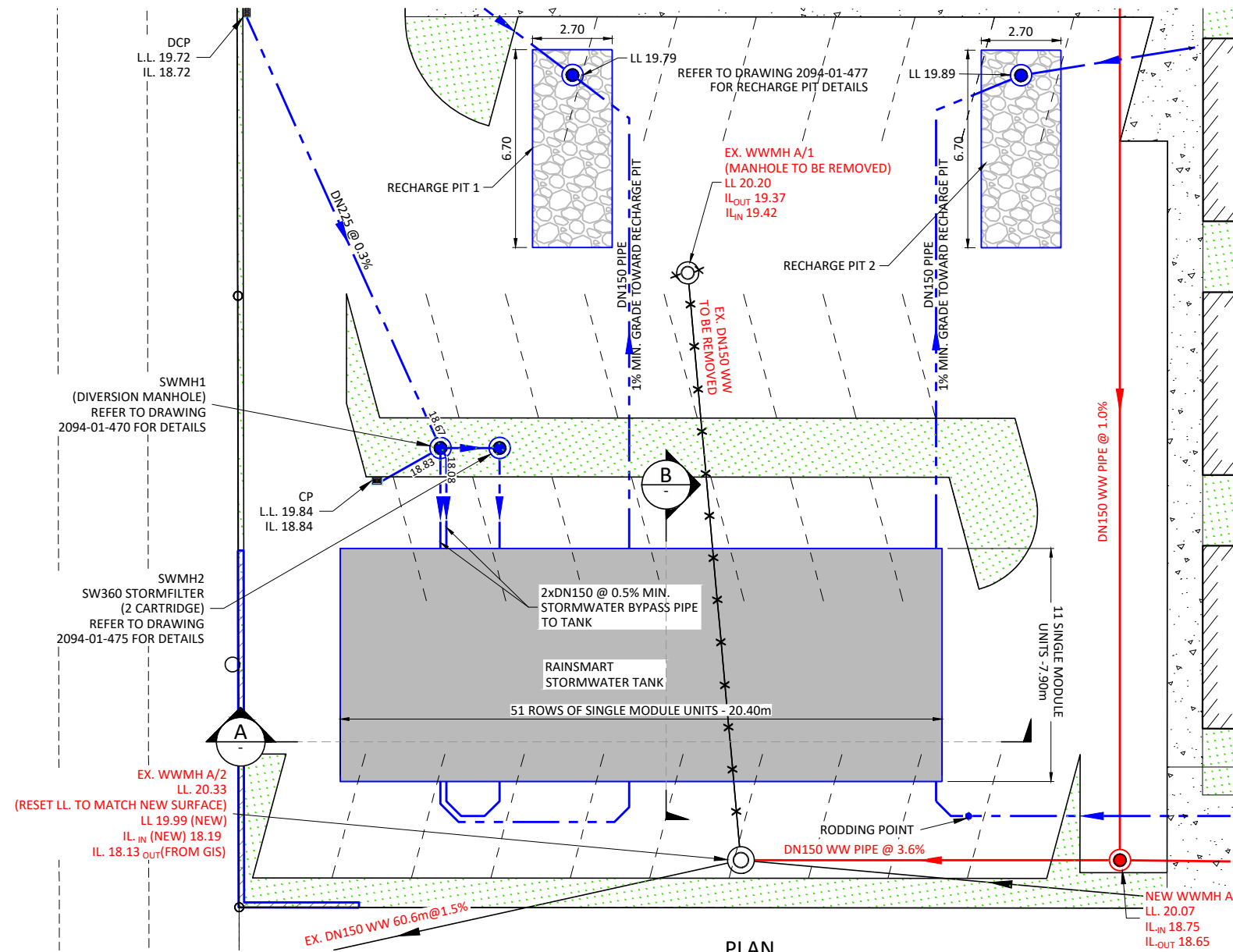
SHEET TITLE:
PROPOSED DRAINAGE
OVERALL LAYOUT PLAN

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

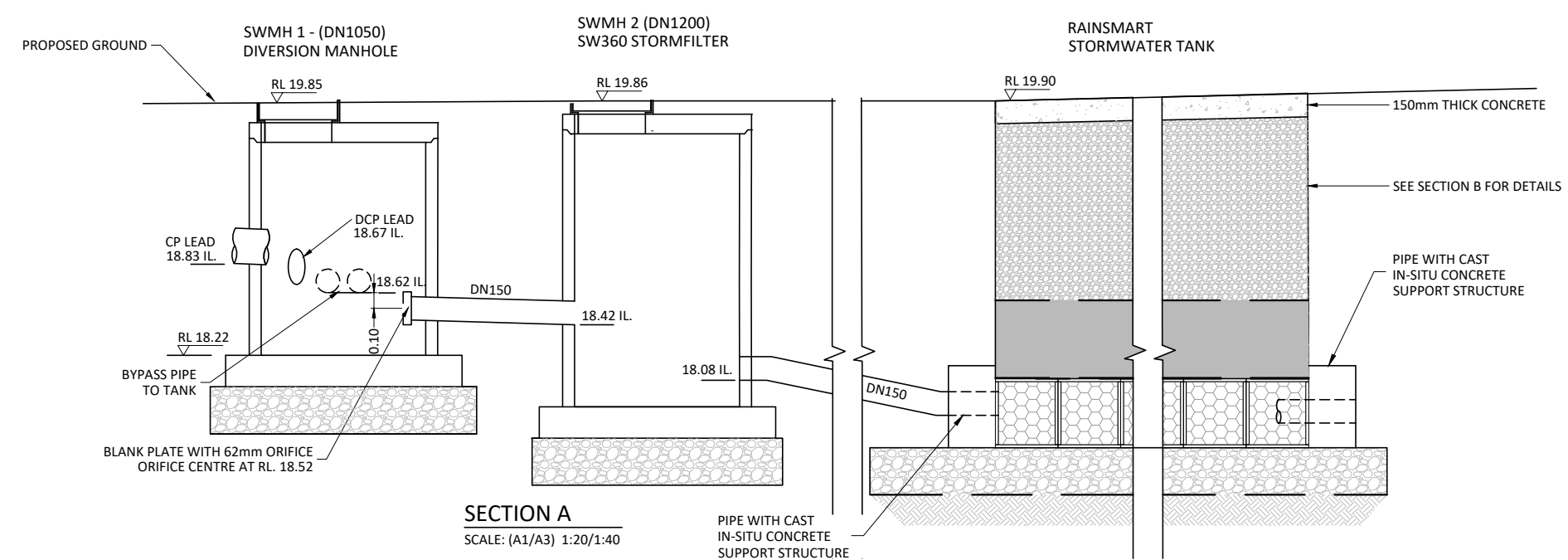
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DRAWING NUMBER: **2094-01-400** REV: **C3**

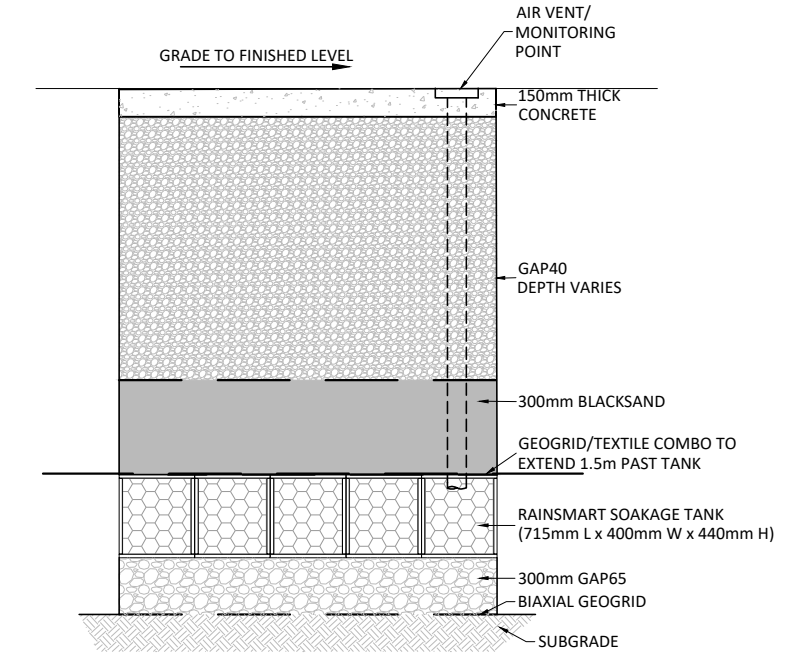


PLAN
SCALE: (A1/A3) 1:100/1:200

- NOTES:**
1. ALL STORAGE TANK DRAINAGE WORKS SHALL BE CARRIED OUT IN ACCORDANCE WITH THE SPECIFICATIONS, METHOD STATEMENTS AND RECOMMENDATIONS OF RAINSMART SOLUTIONS PTY.
 2. ALL LEVELS AND DIMENSIONS SHALL BE VERIFIED ON SITE PRIOR TO THE COMMENCEMENT OF ANY WORKS. ANY DISCREPANCIES SHALL BE IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER.



SECTION A
SCALE: (A1/A3) 1:20/1:40



SECTION B
SCALE: (A1/A3) 1:20/1:40

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C2	ISSUED FOR CONSENT	26.06.19	MSS	26.06.19
C1	ISSUED FOR CONSENT	05.06.19	MSS	05.06.19
REV	REVISION DETAILS			

DRAWN:	MM	DATE:	03.2019
CHECKED:	MSS	DATE:	05.06.19
APPROVED:	MSS	DATE:	05.06.19

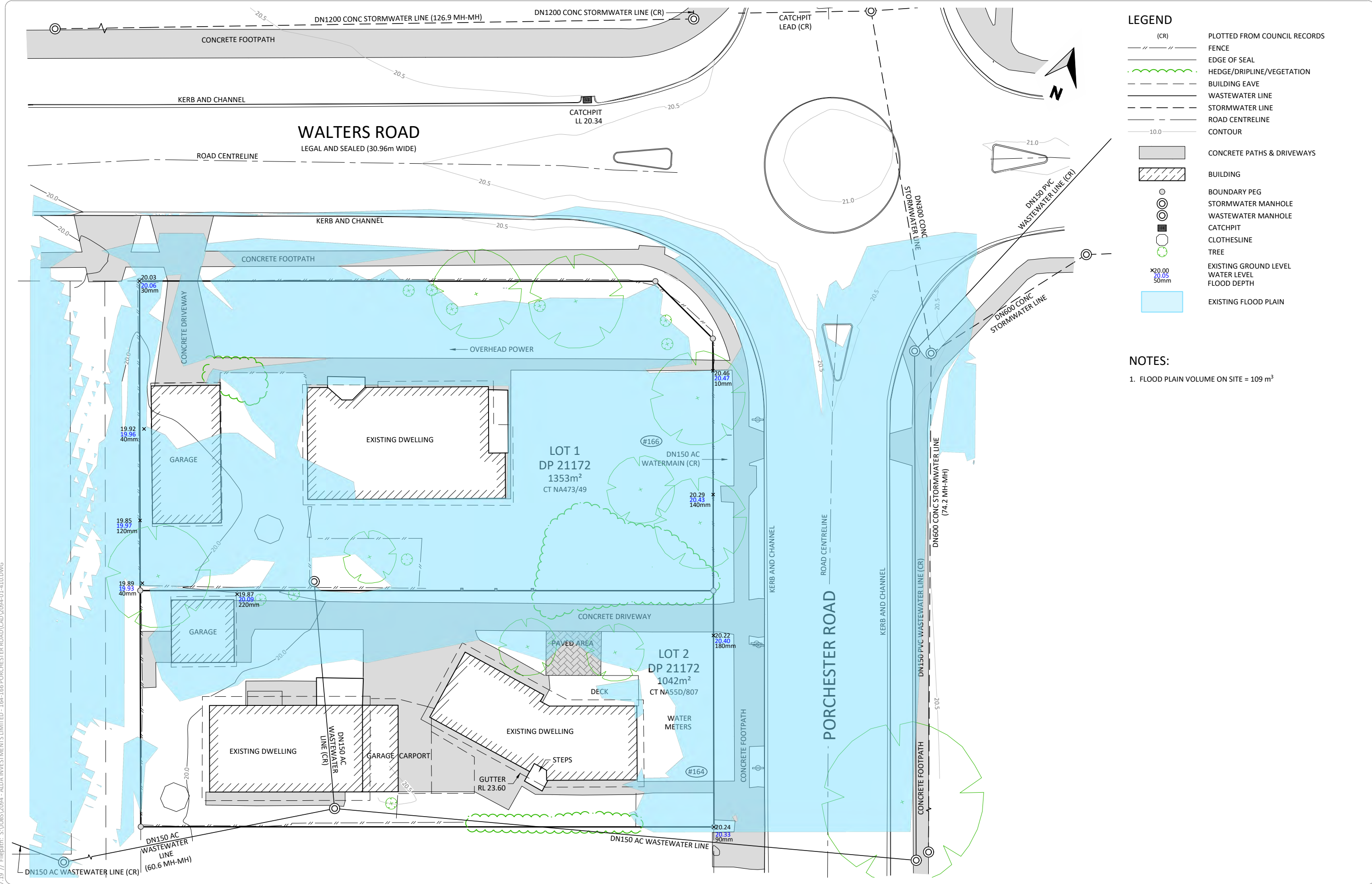
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED DRAINAGE
DETAIL PLAN**

ISSUE STATUS: **CONSENT**
SCALE: (A1/A3) AS SHOWN
SCALE BAR
DRAWING NUMBER: **2094-01-401** REV: **C2**



LEGEND

- (CR) PLOTTED FROM COUNCIL RECORDS
- FENCE
- EDGE OF SEAL
- HEDGE/DRIPLINE/VEGETATION
- BUILDING EAVE
- WASTEWATER LINE
- STORMWATER LINE
- ROAD CENTRELINE
- CONTOUR
- CONCRETE PATHS & DRIVEWAYS
- BUILDING
- BOUNDARY PEG
- STORMWATER MANHOLE
- WASTEWATER MANHOLE
- CATCHPIT
- CLOTHESLINE
- TREE
- EXISTING GROUND LEVEL
- WATER LEVEL
- FLOOD DEPTH
- EXISTING FLOOD PLAIN

NOTES:
 1. FLOOD PLAIN VOLUME ON SITE = 109 m³

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C1	ISSUED FOR CONSENT	MSS	05.06.19
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PROJECT TITLE:
 ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND

SHEET TITLE:
 EXISTING FLOODPLAIN
 100 YR WITH CLIMATE CHANGE

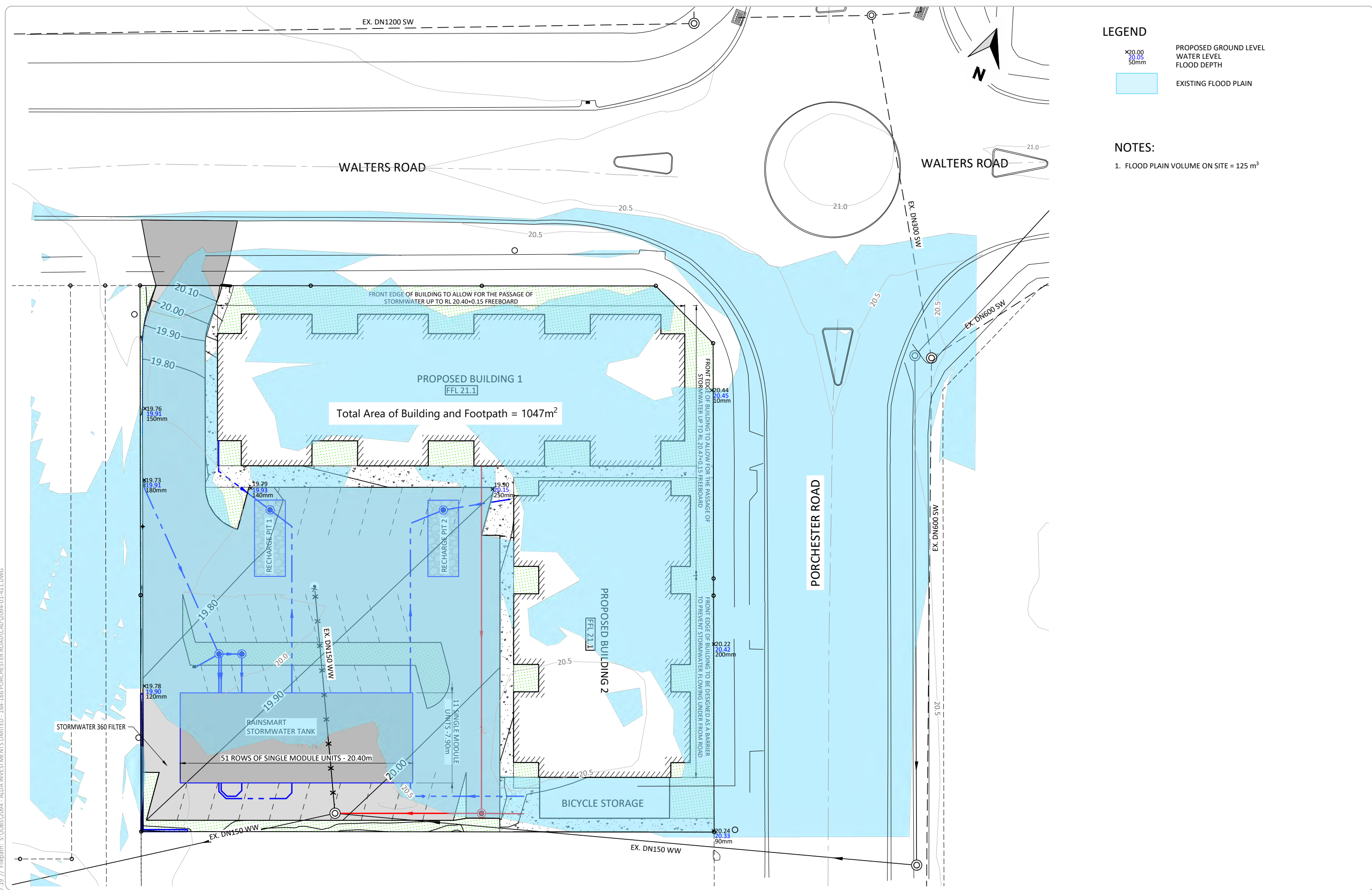
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SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR 1:300@A3

DRAWING NUMBER: 2094-01-410

REV: C2



LEGEND

- PROPOSED GROUND LEVEL
- WATER LEVEL
- FLOOD DEPTH
- EXISTING FLOOD PLAIN

NOTES:

- FLOOD PLAIN VOLUME ON SITE = 125 m³

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C3	ISSUED FOR CONSENT	MSS	29.07.19
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19

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MSS	05.06.19
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED FLOODPLAIN
 100 YR WITH CLIMATE CHANGE**

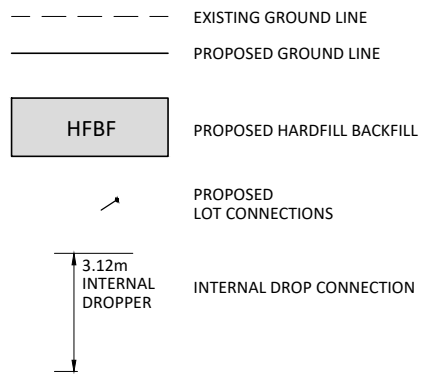
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DRAWING NUMBER: **2094-01-411** REV: **C3**

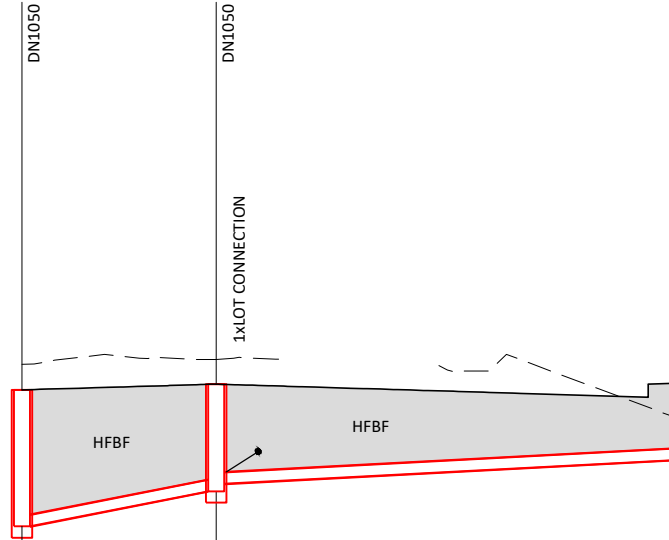
LEGEND



NOTES:

1. REFER TO SHEET 2094-01-400 FOR GENERAL NOTES.
2. LOT CONNECTIONS TO TERMINATE 1m FROM PROPOSED GROUND LINE
3. LENGTHS OF RAMP RISERS ARE STATED IN VERTICAL LENGTH OF PIPE. CONTRACTOR TO ENSURE CORRECT LENGTH OF PIPE IS USED SO THAT THE CONNECTION TERMINATES WITHIN THE APPROPRIATE LOT BOUNDARY.

EX.WWMH. A/2 NEW WWMH. A/3



DATUM	14.0		
EXISTING GROUND LEVEL	20.33	20.39	19.62
LID LEVEL	19.99	20.07	20.08
INVERT LEVEL	18.19	18.65	19.05
DEPTH TO INVERT	1.80	1.42	1.03
GRADIENT & PIPE SIZES	3.60% DN150uPVC SN16		1.00% DN150uPVC SN16 LOT CONNECTION
DISTANCE (m)	12.85	30.46	

WASTEWATER LONGSECTION

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MM	03.2019		
MSS	05.06.19		
MSS	05.06.19		
C2	ISSUED FOR CONSENT	MSS	26.06.19
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PROJECT TITLE:
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TAKANINI, AUCKLAND

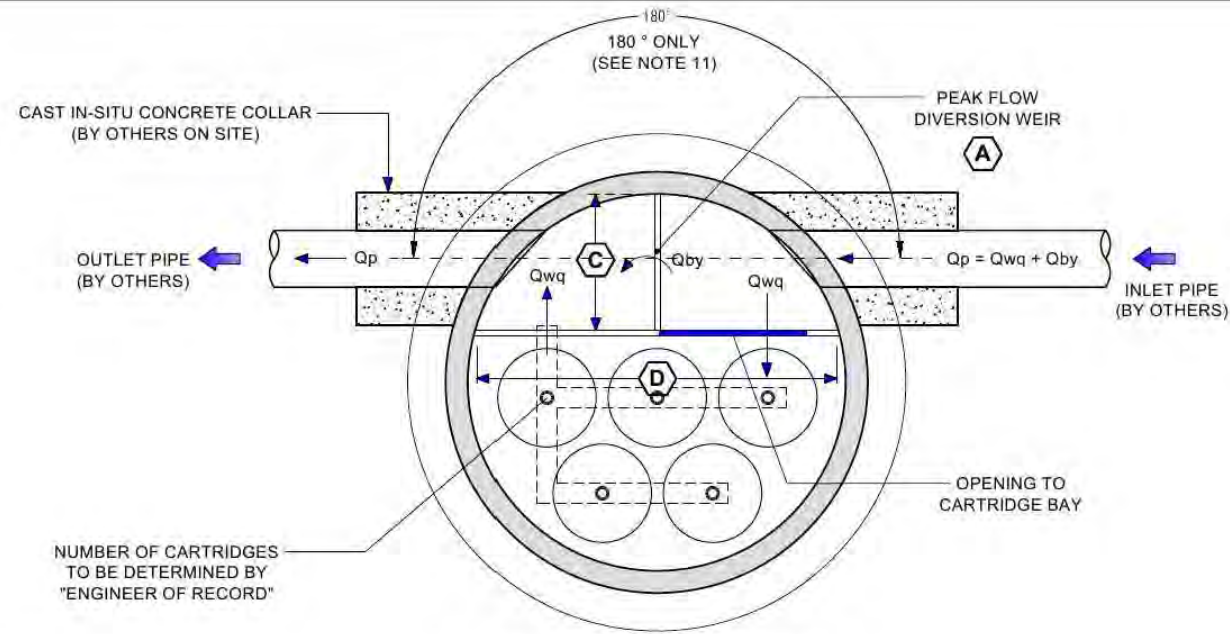
SHEET TITLE:
PROPOSED DRAINAGE
WASTEWATER LONGITUDINAL SECTIONS

ISSUE STATUS: **CONSENT**

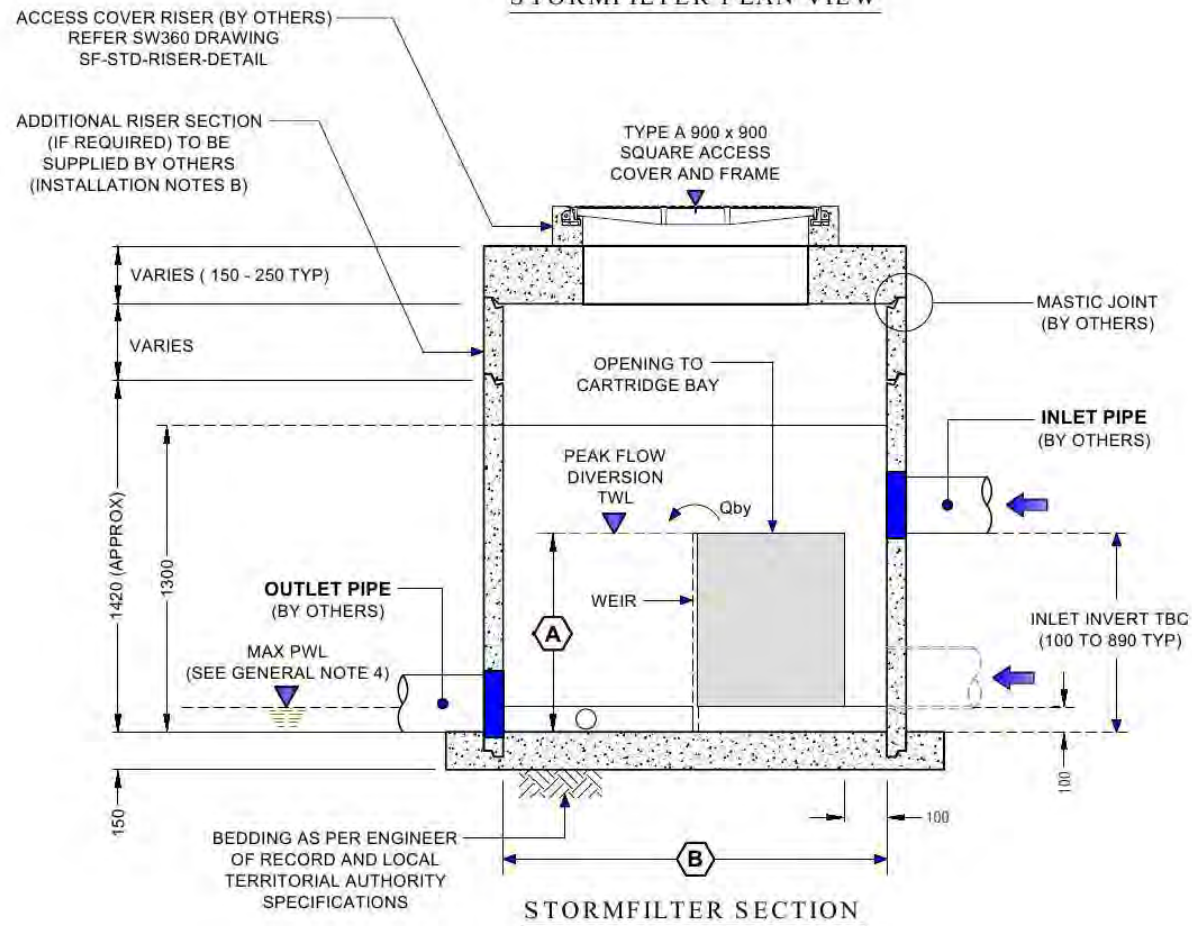
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SCALE BAR
N.T.S.

DRAWING NUMBER: 2094-01-450
REV: C2



STORMFILTER PLAN VIEW



STORMFILTER SECTION

STORMFILTER DESIGN NOTES

STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. THE PEAK DIVERSION MODEL INTEGRATES AN "OFFLINE" BYPASS WEIR WITHIN THE STORMFILTER MANHOLE. THE STORMFILTER RATED MAXIMUM TREATMENT CAPACITY PER CARTRIDGE IS SHOWN IN THE TABLE BELOW. MAXIMUM HYDRAULIC INTERNAL BYPASS @ 200 mm HYDRAULIC HEAD IS SHOWN IN THE TABLE BELOW. SITE SPECIFIC PEAK CONVEYANCE CAPACITY TO BE DETERMINED BY ENGINEER OF RECORD. CONTACT YOUR STORMWATER CONSULTANT FOR ADDITIONAL INFORMATION

CARTRIDGE HEIGHT (cm)	69	46	30 (LOW DROP)
RECOMMENDED HYDRAULIC DROP (mm) (A)	890	660	500
SPECIFIC FLOW RATE (L/s/m ²)	1.40	0.70	1.40 0.70
CARTRIDGE FLOW RATE (L/s)	1.42	0.71	0.95 0.475 0.63 0.315

STORMFILTER STRUCTURE MODEL	SFMHPD1015	SFMHPD1215	SFMHPD1515	SFMHPD1815	SFMHPD2015	SFMHPD2315
MAXIMUM No. OF CARTRIDGES	1	2	3	5	6	9
MAX. TREATMENT FLOW RATE Qwq (L/s)	1.42	2.84	4.26	7.10	8.56	12.78
(B) CHAMBER DIAMETER	Ø 1050	Ø 1200	Ø 1500	Ø 1800	Ø 2050	Ø 2300
(C) BYPASS WEIR LENGTH*	400	430	560	650	700	750
(D) BYPASS BAFFLE LENGTH*	1035	1165	1420	1750	1930	2150
BYPASS FLOW, Qby @ 200mm ABOVE WL*	65 L/S	70 L/S	90 L/S	105 L/S	110 L/S	120 L/S

*CUSTOM PEAK DIVERSION CONFIGURATIONS ARE AVAILABLE WHERE GREATER BYPASS FLOWS ARE REQUIRED. CONTACT A SW360 STORMWATER CONSULTANT FOR MORE INFORMATION.

SITE SPECIFIC DATA REQUIREMENTS

STRUCTURE ID	
CATCHMENT AREA	940m ²
WATER QUALITY FLOW RATE - Qwq (L/s)	2.6 L/s
PEAK FLOW RATE - Qp (L/s)	N.A. (EXTERNAL BYPASS PROVIDED)
RETURN PERIOD OF PEAK FLOW (yrs)	N.A. (EXTERNAL BYPASS PROVIDED)
# OF CARTRIDGES REQUIRED x CARTRIDGE HEIGHT (ie 1 x 69 cm)	2 X 69 cm
CARTRIDGE FLOW RATE	1.3 L/s
MEDIA TYPE (ZEO, PER, ZPG, PHS)	PER

PIPE DATA:	R.L.	MATERIAL	DIAMETER
INLET PIPE	18.42	PVC	150
OUTLET PIPE	18.08	PVC	150
LID LEVEL		N/A	N/A

AS PER ENGINEER OF RECORD

GENERAL NOTES :

1. STORMWATER360 TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
2. FOR SITE SPECIFIC DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, PLEASE CONTACT YOUR SW360 STORMWATER CONSULTANT VIA www.stormwater360.co.nz, OR 0800 STORMWATER, OR sales@stormwater360.co.nz.
3. T.W.L. = TREATMENT WATER LEVEL.
4. THE MAXIMUM DOWNSTREAM PERMANENT WATER LEVEL (PWL) IS TO BE SET AT OR BELOW THE BASE OF THE CARTRIDGES. TYPICALLY 100 mm ABOVE OUTLET INVERT.
5. PEAK DIVERSION WEIR CORRESPONDS TO RECOMMENDED HYDRAULIC DROP (A) ON TABLE ABOVE.
6. STRUCTURE SHALL MEET NZTA'S HN-HO-72 OR PER APPROVING JURISDICTION TRAFFICKED LOAD REQUIREMENTS, WHICHEVER IS MORE STRINGENT. COVER AND FRAME ARE TO BE RATED TO EITHER CLASS B (FOR PEDESTRIAN AREAS) OR CLASS D (TRAFFICKED ROADS) IN ACCORDANCE WITH AS 3996 : 2006.
7. STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO NZS 3109 : 1997, NZS 3114 : 1987 AND AS/NZS 4058 : 2007.
8. FILTER CARTRIDGES SHALL BE MEDIA-FILLED, PASSIVE, SIPHON ACTUATED, RADIAL FLOW, AND SELF CLEANING. RADIAL MEDIA DEPTH SHALL BE 178 mm. FILTER MEDIA CONTACT TIME SHALL BE AT LEAST 39 SECONDS.
9. SPECIFIC FLOW RATE IS EQUAL TO THE FILTER TREATMENT CAPACITY (L/s) DIVIDED BY THE FILTER CONTACT SURFACE AREA (m²).
10. MINIMUM INVERT DIFFERENCE BETWEEN INLET PIPE AND OUTLET PIPE IS 100 mm.
11. DEVICE SUITABLE FOR INSTALLATION ON STRAIGHT (180°) PIPE RUNS. ALTERNATIVE PIPE ANGLES ARE NOT SUITABLE. CONTACT SW360 FOR ALTERNATE OPTIONS.
12. NO PRODUCT SUBSTITUTIONS SHALL BE ACCEPTED UNLESS SUBMITTED 10 DAYS PRIOR TO PROJECT BID DATE, OR AS DIRECTED BY THE ENGINEER OF RECORD.

INSTALLATION NOTES :

- A. SIZE AND CLASS OF PIPE OR SQUARE KNOCKOUT SIZE TO BE SPECIFIED ON DRAWING BY CLIENT / CONTRACTOR.
- B. ADDITIONAL RISERS TO BE FORMED ON SITE BY CONTRACTOR (IF REQUIRED).
- C. ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- D. CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STORMFILTER STRUCTURE (LIFTING CLUTCHES PROVIDED).
- E. CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
- F. CONTRACTOR TO PROVIDE, INSTALL, AND GROUT INLET AND OUTLET PIPES. STRAIGHT PIPES TO BE INSTALLED WITH 180° ANGLES ONLY. A CAST IN-SITU CONCRETE COLLAR IS TO BE POURED ON SITE BY CONTRACTOR AS SHOWN ON DRAWINGS.
- G. CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.



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www.stormwater360.co.nz

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STORMFILTER PEAK DIVERSION MANHOLE CONFIGURATION GENERAL ARRANGEMENT
 DRAWING 1 C
 SCALE : XXX DRG No : SF-STD-MHPD-ALL

DRAWING	JOB NO :	
1	PROJECT :	
C	DEVICE # :	
DRN :	R.P. :	30.04.18
CKD :	T.B. :	30.04.18

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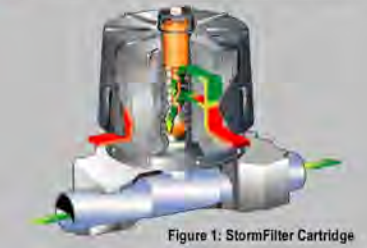
PROJECT TITLE:
 ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND

SHEET TITLE:
 PROPOSED DRAINAGE
 STORMWATER360
 STORMFILTER DETAIL

ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) NOT TO SCALE
 SCALE BAR
 N.T.S.
 DRAWING NUMBER: 2094-01-475
 REV: C2

Project Name **164-166 Porchester Rd, Takanini, Auckland** Location **AKL**
 Job # **5447** Device # **SF1** Option # **A** Revision # **RA**
 Author Date **2 May 2019**

CALCULATIONS - Please Read Instructions First		INSTRUCTIONS	
1.0 Water Quality Design Storm Peak Runoff Flowrate (RATIONAL METHOD)		1.0 Use the rational method to compute the water quality design storm peak runoff flow rate. Values with blue text require user input. Values in red text are automatically calculated. Values with black text remain constant.	
1.1 Runoff Co-efficients		1.1 Input the appropriate runoff co-efficient for each sub-catchment.	
Coefficient of Impervious Roof (Croof)	1.00	Use C=1.0 for impervious roof surfaces runoff co-efficient ¹	
Coefficient of Impervious Road (Croad)	1.00	Use C=1.0 for impervious paved surfaces runoff co-efficient ¹	
Coefficient of Pervious Area (Cper)	0.50	Use C=0.4 for pervious surfaces with clay soils runoff co-efficient & C=0.3 for all other pervious surfaces ¹	
1.2 Catchment Areas		1.2 Input the appropriate catchment area for each sub-catchment.	
Area Impervious Roof (Aroof)	0 m ²	Enter impervious roof surface catchment area	
Area Impervious Road (Aroad)	940 m ²	Enter impervious paved surfaces catchment area	
Area Pervious Area (Aper)	0 m ²	Enter pervious grassed/landscaped surfaces catchment area	
Area Total Catchment (Acatch)	940 m ²	Total catchment area (i.e. $A_{catch} = A_{roof} + A_{road} + A_{per}$)	
Product of Area & Coefficients (CA)	940 m ²	Product of catchment areas & runoff co-efficients (i.e. $CA = (Croof \times Aroof) + (Croad \times Aroad) + (Cper \times Aper)$)	
1.3 Rainfall Intensity		1.3 Input rainfall intensity	
Water Quality Rainfall Intensity (IWQ)	10 mm/hr	Rainfall intensity of 10mm/hr to be used for StormFilter water quality treatment within Auckland Council boundaries ⁴	
1.4 Water Quality Design Storm Peak Runoff Flowrate		1.4 Compute the water quality design storm peak runoff flow rate via Rational Method	
Design Water Quality Treatment Flowrate (Qwq)	2.611 L/s	i.e. $Q = I \times CA$	
2.0 StormFilter Peak Treatment Flowrate		2.0 Use the stormfilter stage-discharge equation to calculate the StormFilter peak treatment flowrate.	
2.1 Preliminary		Enter cartridge filtration media (i.e. Perlite or ZPG)	
Cartridge Media (Media)	Perlite	Enter cartridge height (i.e. 69cm / 46cm / 30cm)	
Cartridge Height (Hcart)	69 cm	Enter restrictor disc size, refer table below for max disc diameter	
Diameter Disc Orifice (d)	27.6 mm		
Internal bypass weir height (Hweir)	0.79 m		
Priming depth (Hprime)	0.66 m		
Area of a Cartridge (Acart)	0.181 m ²		
2.2 StormFilter Cartridge Peak Treatment Flowrate			
StormFilter cartridge stage-discharge equation	$= 0.111 d^{2.06} \Delta h^{0.5}$ L/min		
Design treatment flowrate per cartridge (Qcart)	1.420 L/s/cart		
Number (actual) of StormFilter cartridges required	1.839 cart(s)		
Number (rounded) of StormFilter cartridges required (nCART)	2 cart(s)		
Design StormFilter Treatment Flowrate (QSF)	2.840 L/s		
3.0 Estimate Sediment Mass Loading		3.0 Estimate sediment mass loading (Refer sheet '2. Mass Load Calcs' for more details)	
3.1 StormFilter Manhole/Vault Dimensions		3.1 Use tables 2 & 3 below to fill in StormFilter Dimensions based on number of cartridges as calculated in 2.2 above	
Length Cartridge Bay (Lbay)	1.000 m	3.2 Use table 3 below to fill in estimated TSS concentration. For roads with $\geq 25,000$ vpd, use minimum 600kg/ha/yr	
Width Cartridge Bay (Wbay)	1.670 m	3.3 Use 0% pretreatment for vaults/manholes with no forebay. Use 10-15% pretreatment for vaults/manholes with forebays. Use 50% pretreatment for upstream GPT to EnviroPod. Use 75%-90% system efficiency	
Area Cartridge Bay (Abay)	1.670 m ²		
Total area of Cartridges (Acarts)	0.362 m ²		
Area Lower Volume (Alow)	1.308 m ²		
Volume Lower Volume (Vlow)	903 L		
Area Upper Volume (Aupp)	1.670 m ²		
Volume Upper Volume (Vupp)	167 L		
Live storage volume at internal bypass (Vstor)	1070 L		
3.2 Catchment Sediment Loading			
Estimated TSS Concentration Impervious Roof (TSSroof)	100 kg/ha/year		
Estimated TSS Concentration Impervious Road (TSSroad)	300 kg/ha/year		
Estimated TSS Concentration Pervious Area (TSSper)	200 kg/ha/year		
Estimated Total TSS Load (TSSload)	28 kg/year		
3.3 Treatment Efficiencies			
Pre-treatment Efficiency (EFFpre)	0 %		
System Efficiency (EFFsys)	75 %		
3.4 Maintenance Requirements			
Estimated number of cleans per annum (nCleans)	0.66		
Estimated Maintenance Frequency (Mfreq)	18.182 months		
4.0 Design Summary			
Design Water Quality Treatment Flowrate (Qwq)	2.611 L/s		
StormFilter Design WQ Treatment flowrate (QSF)	2.840 L/s		
StormFilter Design flowrate at internal bypass (Qbypass)	3.057 L/s		
Number of StormFilter Cartridges required (nTOTAL)	2 ea x 69cm Perlite cart(s)		
Treatment Flux per cartridge (FLUX)	1.4 L/s/m ²		
Restrictor Disc Size (d)	27.600 mm		
Maximum Hydraulic Effect (hmax)	0.930 m		
Estimated Maintenance Frequency (Mfreq)	18 months		



$$Q = 0.111 d^{2.06} \Delta h^{0.5}$$

[Q]=L.min⁻¹; [d]=mm; [h]=m

Cart Height (cm)	Actual Height (m)	Priming Depth (m)	Max Disc Diam. (mm)	Max. Design Q (L/s)	Filter Bed Area (m ²)	Flow Rate (L/s/m ²)	Bed Depth (mm)	Media Volume (m ³)	Flow Rate (L/s/m ³)
30	0.305	0.27	22.70	0.63	0.460	1.37	175	0.052	12.0
46	0.457	0.43	25.00	0.95	0.689	1.38	175	0.078	12.1
69	0.686	0.66	27.60	1.42	1.034	1.37	175	0.118	12.1

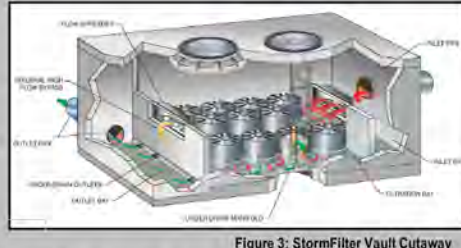
Table 1: StormFilter Cartridge Specifics [3]
 Compute the StormFilter peak treatment flowrate at internal bypass per cartridge via the StormFilter stage-discharge equation
 Compute the number of actual StormFilter cartridges required i.e. $nCART = QWQ / QCART$
 Compute the number (rounded up to whole number) of StormFilter cartridges required
 Compute the StormFilter peak treatment flowrate at internal bypass via the StormFilter stage-discharge equation

Std Manhole Dimensions	Without forebay				With forebay			
	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts
1050	1.00	0.77	0.77	1	1.00	N/A	N/A	N/A
1200	1.00	1.03	1.03	3	1.00	0.76	0.76	2
1500	1.00	1.67	1.67	4	1.00	1.39	1.39	3
1800	1.00	2.44	2.44	7	1.00	1.83	1.83	5
2050	1.00	3.20	3.20	9	1.00	2.80	2.80	7

Std Vault Dimensions	Without forebay				With forebay			
	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts
3.4 L x 1.5 W x 1.8 D	2.85	1.50	4.28	11	2.30	1.80	4.14	8
4.5 L x 1.5 W x 1.8 D	3.95	1.50	5.93	17	3.40	1.50	5.10	14
4.2 L x 2.0 W x 1.8 D	3.95	1.95	7.70	23	3.40	2.10	7.14	18
5.6 L x 2.0 W x 1.8 D	5.05	1.95	9.85	31	4.50	2.10	9.45	26
5.6 L x 2.4 W x 1.8 D	5.05	2.40	12.12	39	4.50	2.10	9.45	27
6.2 L x 2.4 W x 1.8 D	5.60	2.40	13.44	44	4.50	2.40	10.80	33

Land Use	TSS (kg/ha/yr)
Road	281 - 723
Commercial	242 - 1369
Residential (low)	60 - 340
Residential (high)	97 - 547
Terraced	133 - 755
Bush	26 - 146
Grass	80 - 588
Roof	50-110 (T)
Pasture	103 - 583

Table 4: Suggested TSS loads⁴



References
 1. Stormwater360 StormFilter: Interim Proprietary Device Evaluation, Part 1 - Air, Land and Water Plan Evaluation, Auckland Council, 24th August 2015.
 2. Derived from Stormwater Management Inc., Technical Publication PD-04-002.0
 3. Contech Stormwater Solutions, StormFilter Product Design Manual.
 4. Table 4-4, Technical Publication 10, 2nd Edition, May 2003, Auckland Regional Council

0:02 SF jpbh0407: 164-166 Porchester Road, Takanini, Auckland 06/07/2020 StormFilter Flow-Based Sizing - Auckland Council Method v2 | 1: Input and Output Page

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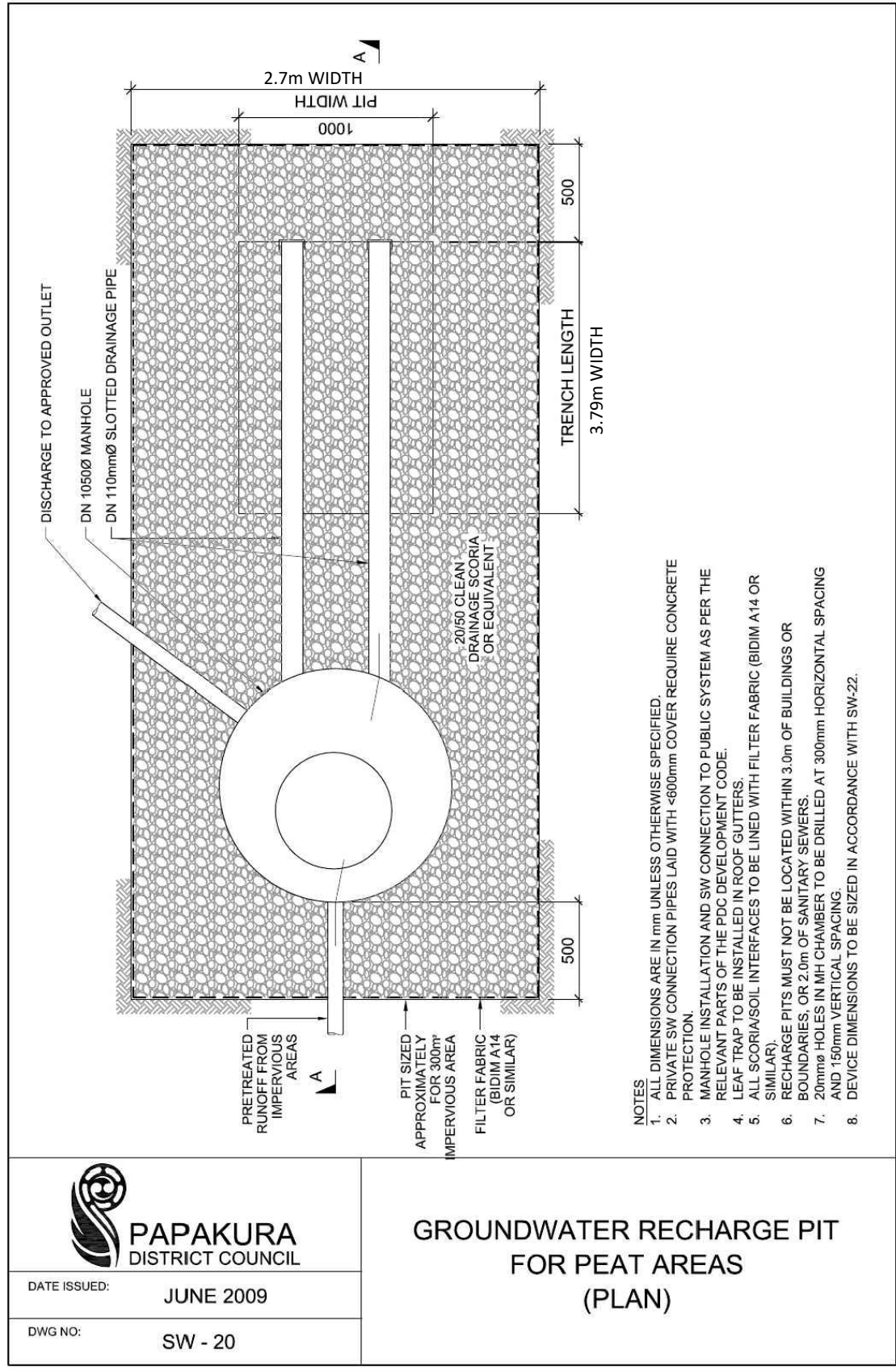
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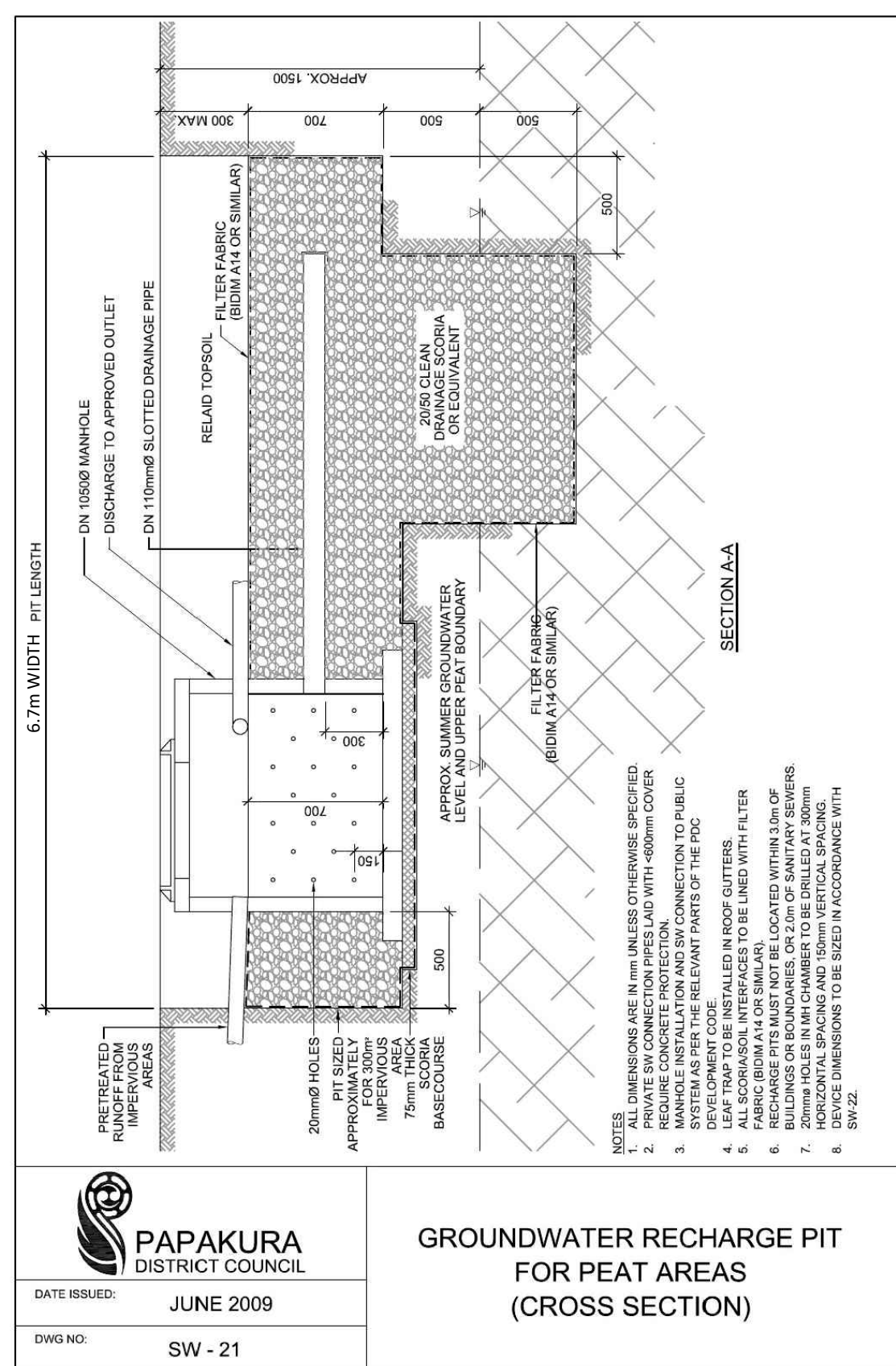
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SHEET TITLE:
**PROPOSED DRAINAGE
 STORMWATER360
 STORMFILTER CALCULATION**

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- NOTES**
1. ALL DIMENSIONS ARE IN mm UNLESS OTHERWISE SPECIFIED.
 2. PRIVATE SW CONNECTION PIPES LAID WITH <math>< 600\text{mm}</math> COVER REQUIRE CONCRETE PROTECTION.
 3. MANHOLE INSTALLATION AND SW CONNECTION TO PUBLIC SYSTEM AS PER THE RELEVANT PARTS OF THE PDC DEVELOPMENT CODE.
 4. LEAF TRAP TO BE INSTALLED IN ROOF GUTTERS.
 5. ALL SCORIA/SOIL INTERFACES TO BE LINED WITH FILTER FABRIC (BIDIM A14 OR SIMILAR).
 6. RECHARGE PITS MUST NOT BE LOCATED WITHIN 3.0m OF BUILDINGS OR BOUNDARIES, OR 2.0m OF SANITARY SEWERS.
 7. 20mm ϕ HOLES IN MH CHAMBER TO BE DRILLED AT 300mm HORIZONTAL SPACING AND 150mm VERTICAL SPACING.
 8. DEVICE DIMENSIONS TO BE SIZED IN ACCORDANCE WITH SW-22.



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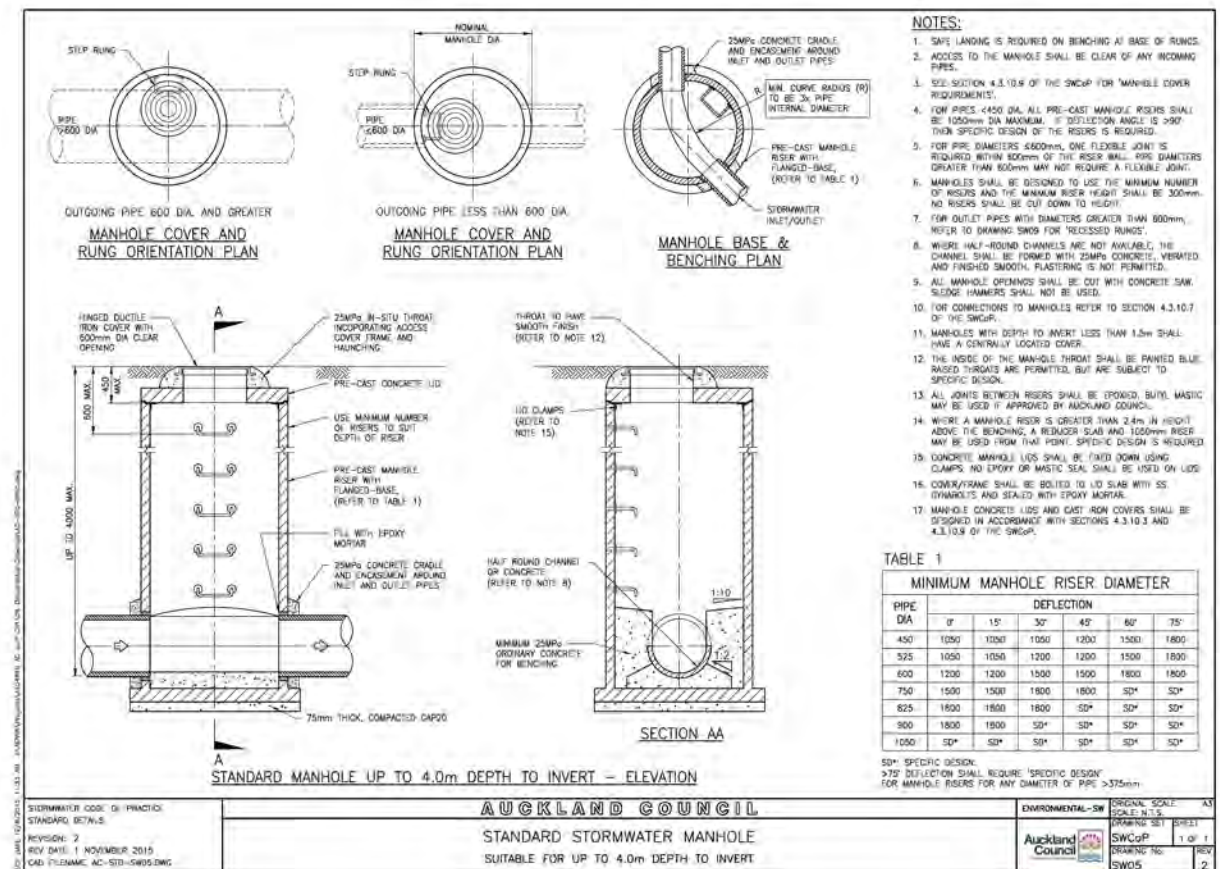
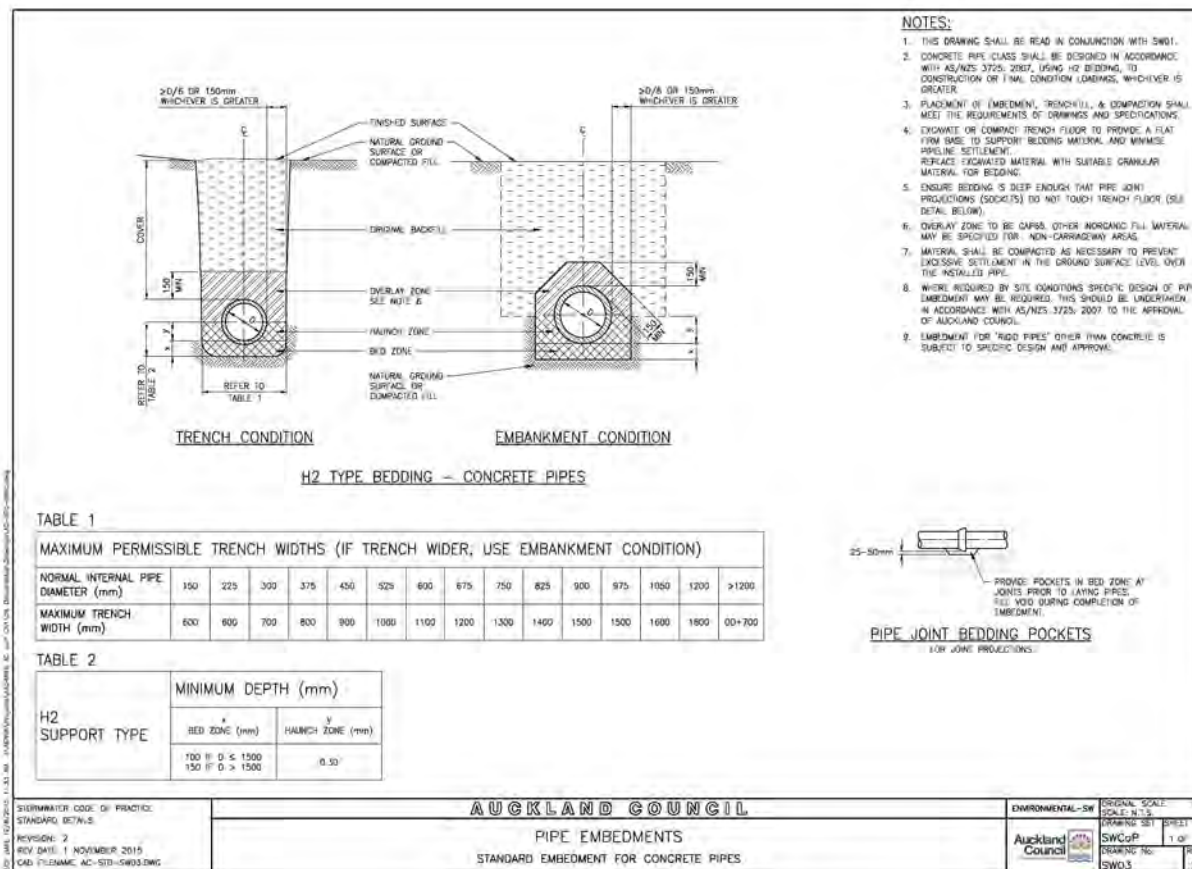
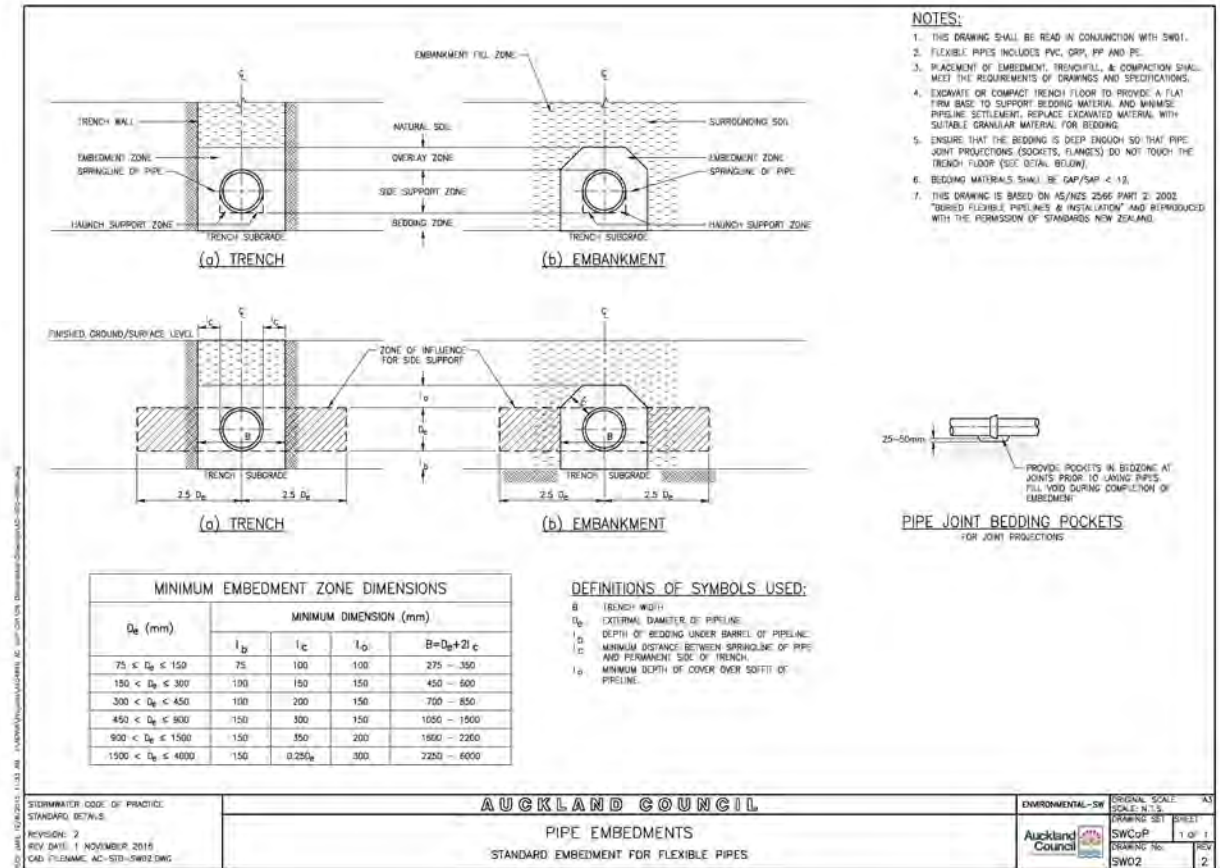
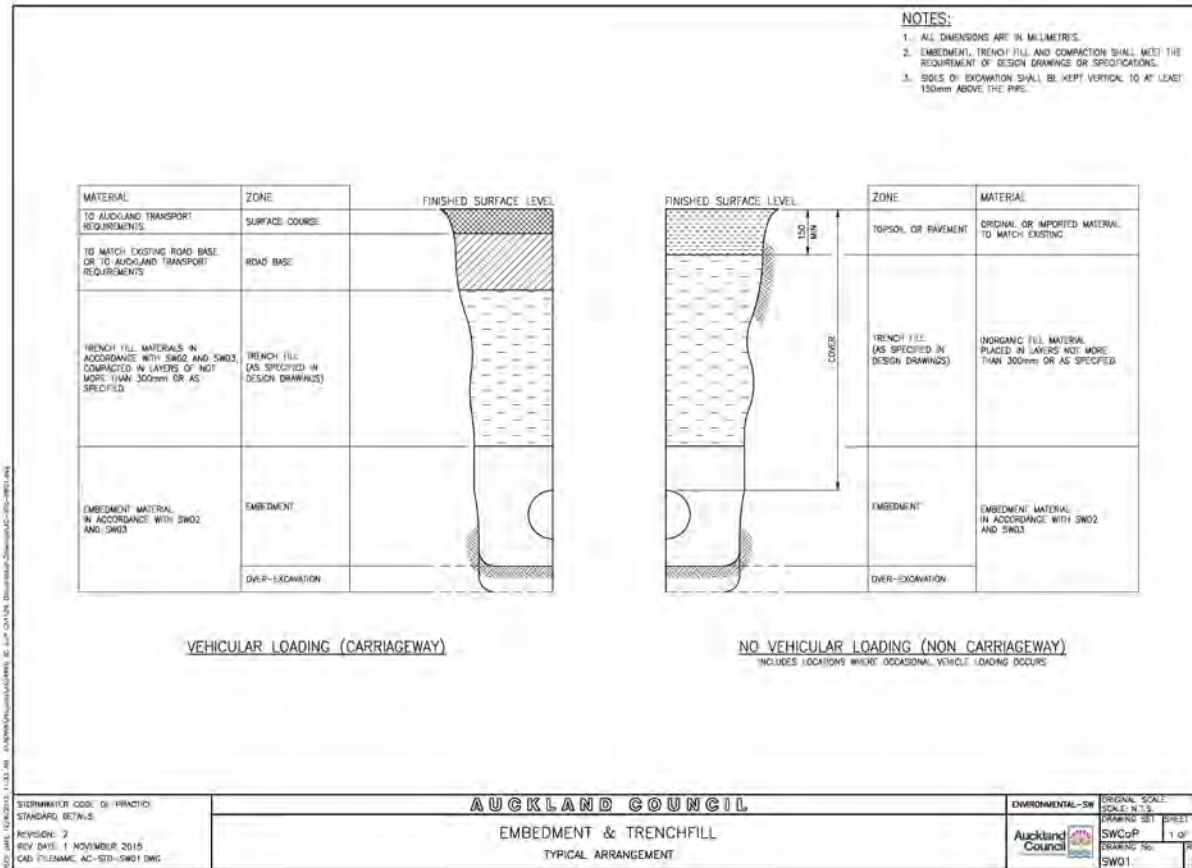
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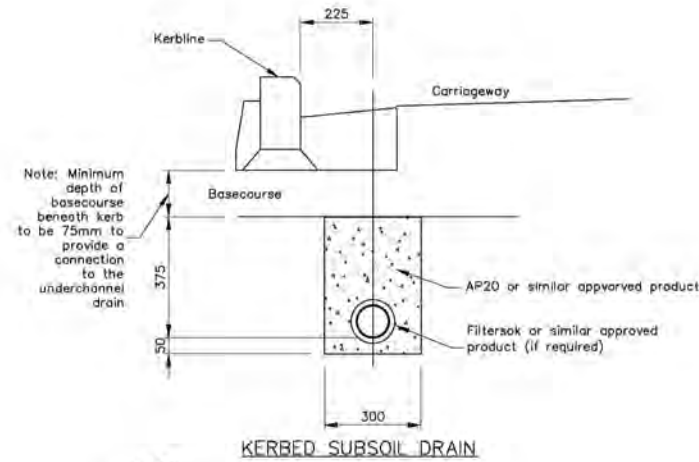
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STORMWATER STANDARD DETAILS
SHEET 1

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DRAWING NUMBER: 2094-01-490 REV: C2

Drawing set for Chapter 17 - Road Drainage



KERBED SUBSOIL DRAIN

NOTE

1. Construct subsoil drain after stabilisation of subgrade.
2. **Underchannel Drains**
Shall be approved perforated drain pipe of 100mm internal diameter unless specified or scheduled otherwise. Subsoil drain pipes shall comply with the requirements of TNZ Specifications F/2. Trench backfill shall be approved AP20 material or similar. Trench backfill shall be approved 30/10 scoria or similar if a filter sock is provided. Depth below subgrade to be 375mm.

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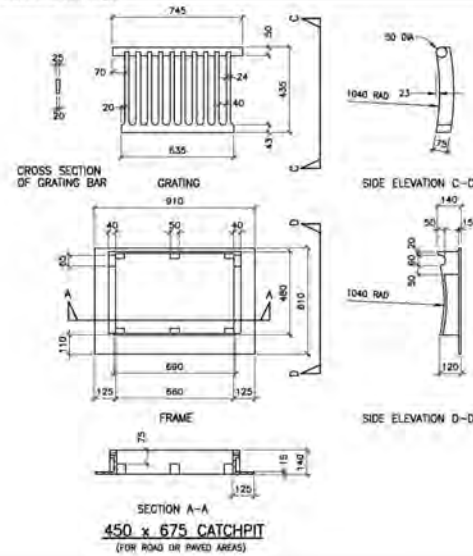
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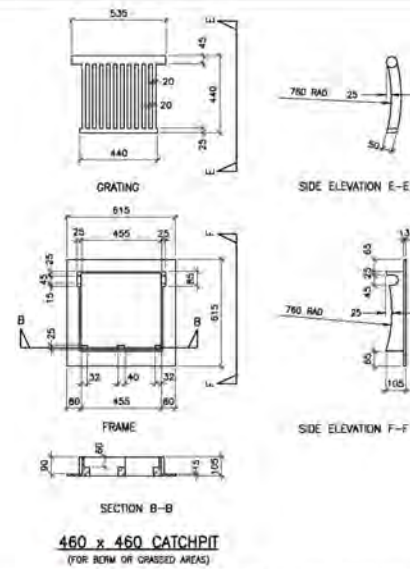
VERSION

TITLE: SUBSOIL DRAIN

Drawing set for Chapter 17 - Road Drainage



SECTION A-A
450 x 675 CATCHPIT
(FOR ROAD USE PAVED AREAS)



SECTION B-B
460 x 460 CATCHPIT
(FOR BORN OR GRASSED AREAS)

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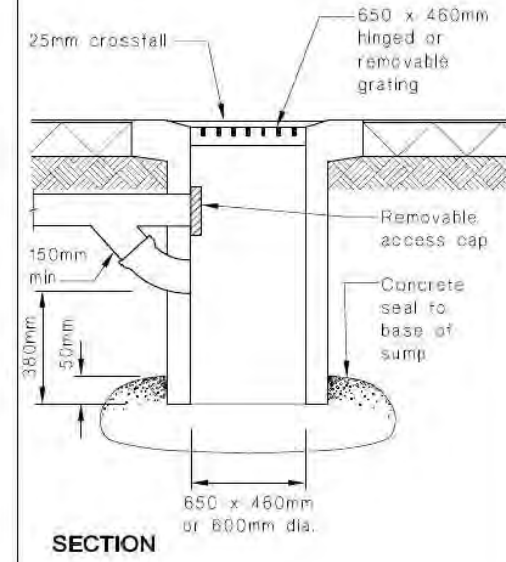
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DRAWING No. RDO30

VERSION

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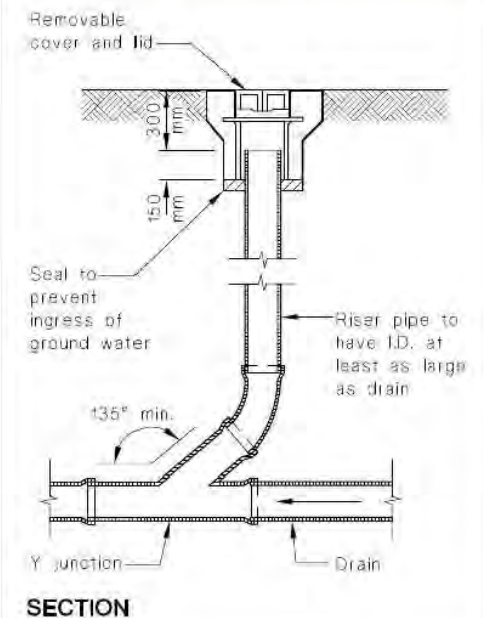
Figure 9: Type-two Surface Water Sump
Paragraph 3.6.2



SECTION

CATCHPIT DETAIL

Figure 10: Typical Rodding Point
Paragraph 3.7.1



SECTION

RODDING POINT DETAIL

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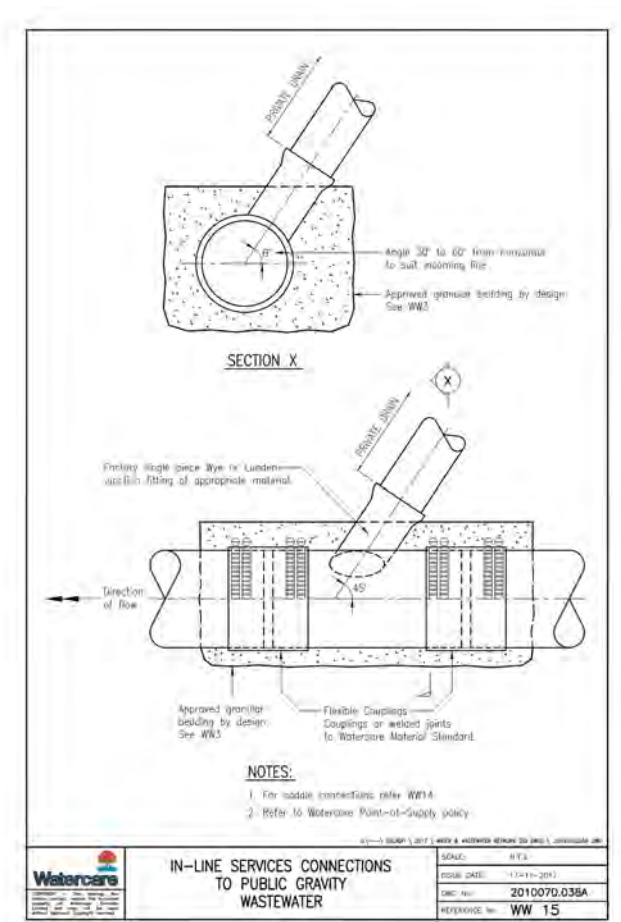
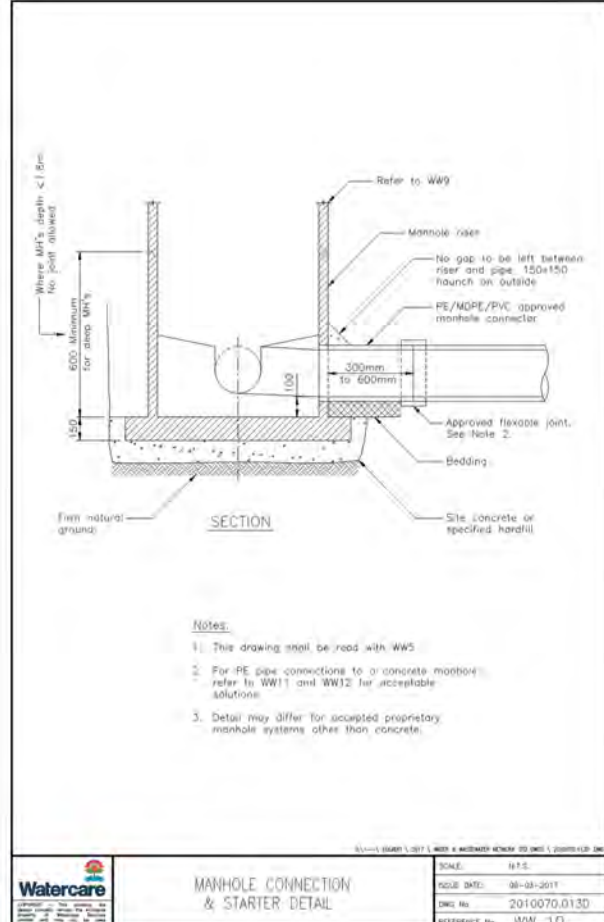
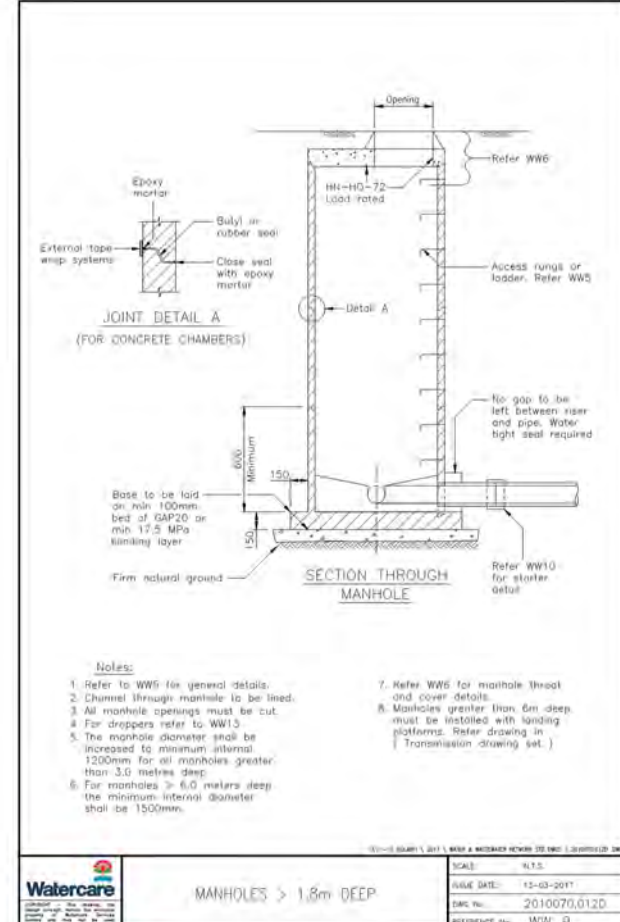
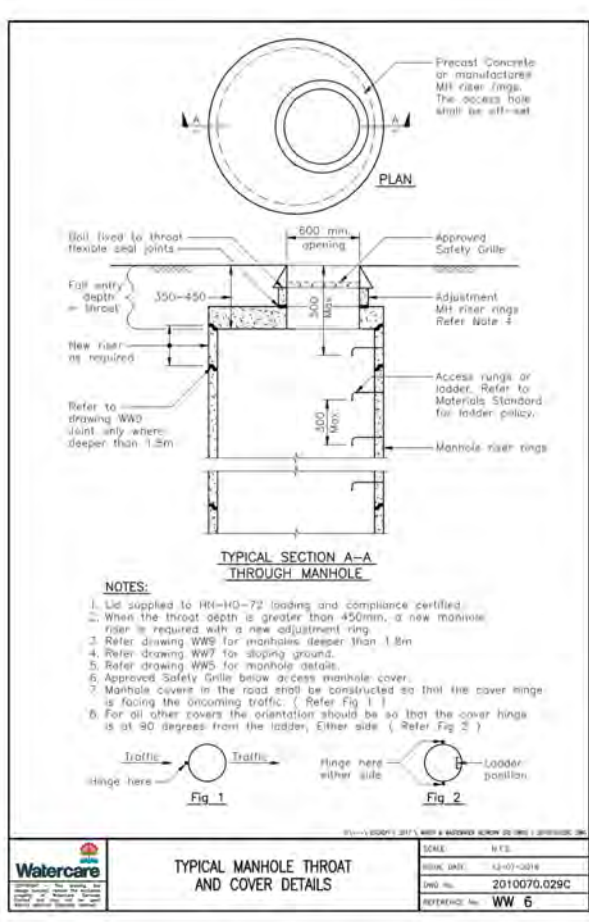
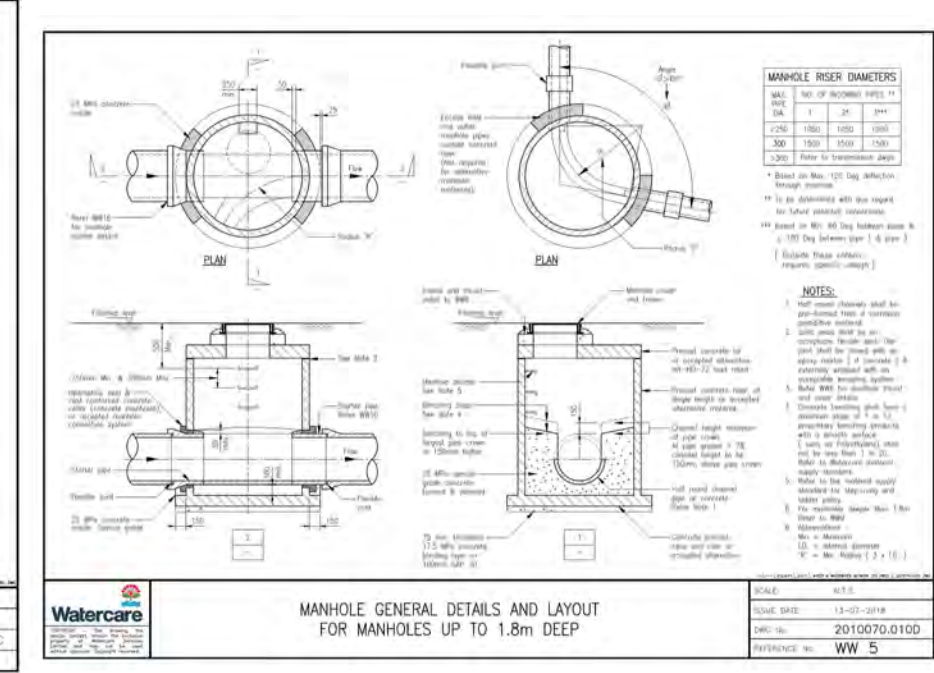
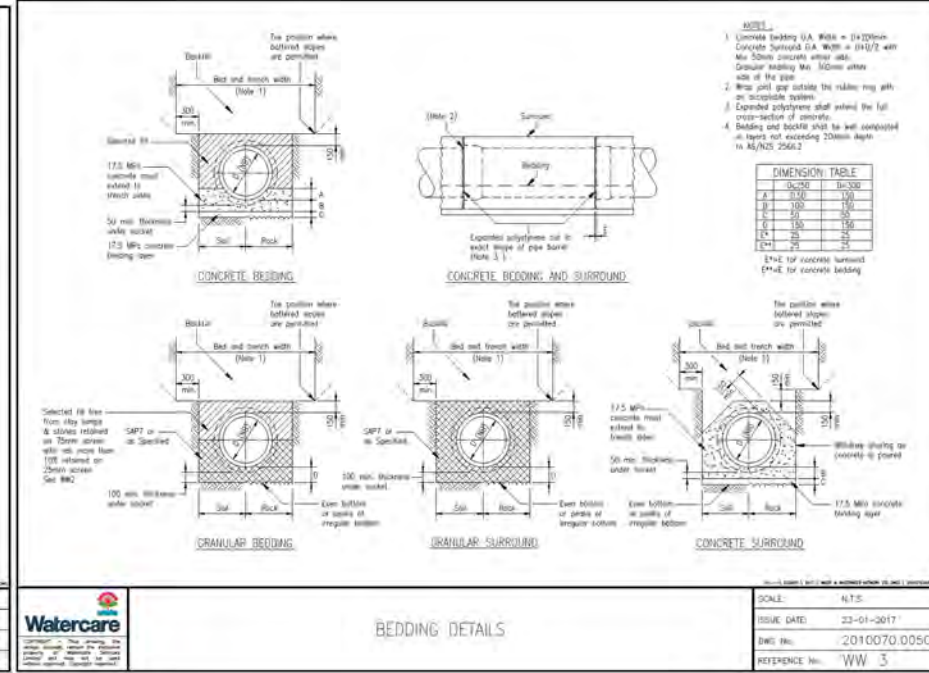
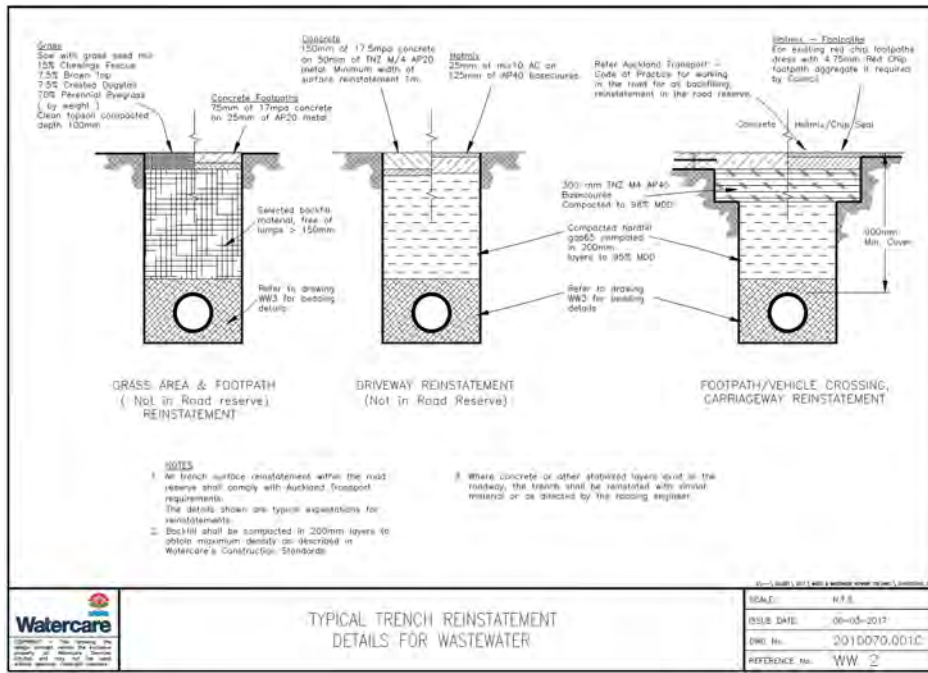
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PROPOSED DRAINAGE
STORMWATER STANDARD DETAILS
SHEET 2

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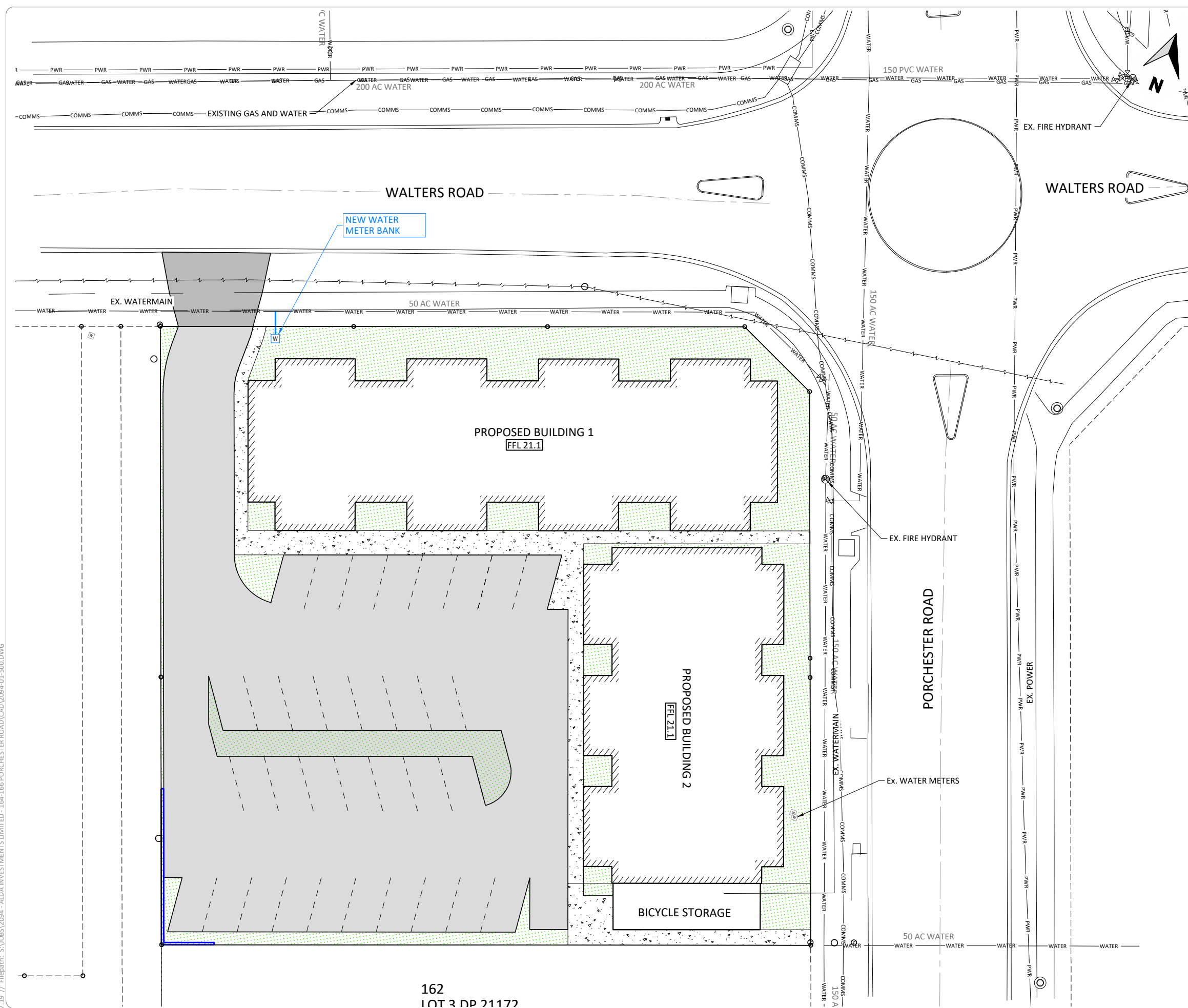
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**PROPOSED DRAINAGE
 WASTEWATER STANDARD DETAILS**

ISSUE STATUS: **CONSENT**

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DRAWING NUMBER: **2094-01-495** REV: **C2**



- NOTES:**
1. ALL WATERMANS TO COMPLY WITH WATERCARE'S 'WATER AND WASTEWATER CODE OF PRACTICE FOR LAND DEVELOPMENT AND SUBDIVISION' AND IN ACCORDANCE WITH THE REQUIREMENTS OF THE BUILDING CODE.
 2. ALL WATERMANS TO BE LAID AND BACKFILLED IN ACCORDANCE WITH COUNCIL STANDARD SPECIFICATIONS.
 3. ALL WATERMANS TO BE LAID 1.1m FROM AND PARALLEL TO THE BOUNDARY UNLESS SHOWN OTHERWISE WITH 600mm MIN. COVER (900mm UNDER CARRIAGEWAYS).
 4. ALL TRENCHES UNDER CARRIAGEWAY / VEHICLE CROSSINGS TO BE BACKFILLED WITH HARDFILL.
 5. ALL WORKS ON EXISTING WATERMANS ARE TO BE CARRIED OUT BY AN APPROVED CONTRACTOR UNDER APPROVAL FROM WATERCARE AT DEVELOPERS EXPENSE.
 6. WHERE WATERMANS ARE TO BE LAID IN AN ARC, THE PIPES SHALL BE LAID IN A CONSTANT RADIUS IN THE APPROPRIATE POSITION.
 7. CONTRACTOR TO SEARCH, LOCATE, PROTECT AND MAINTAIN ALL EXISTING SERVICES, POWER, COMMS ETC.
 8. SEE SHEET 2094-01-590 FOR STANDARD DETAILS.

- LEGEND**
- EXISTING**
- WATER — WATERMAIN
 - ⊗ FIRE HYDRANT
 - ⊗ SLUICE VALVE
 - PEET VALVE
 - ⊗ BLANK CAP
 - PWR — POWER
 - WATER — WATER
 - COMMS — COMMS
 - GAS — GAS
- PROPOSED**
- ⊗ WATER METER BANK

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REV	REVISION DETAILS	BY	DATE
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

DRAWN:	MM	DATE:	03.2019
CHECKED:	MSS	DATE:	05.06.19
APPROVED:	MSS	DATE:	05.06.19

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED WATERMAIN
 LAYOUT PLAN**

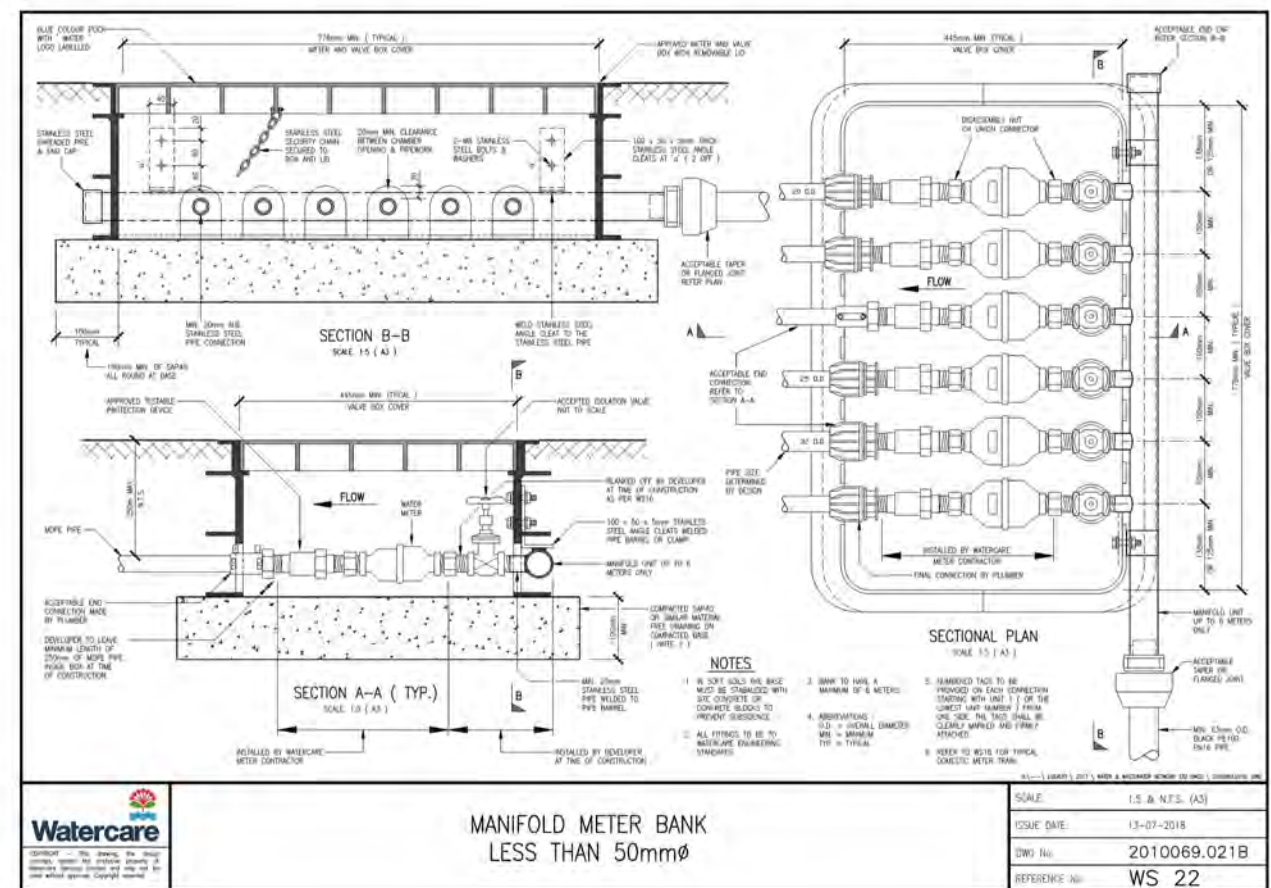
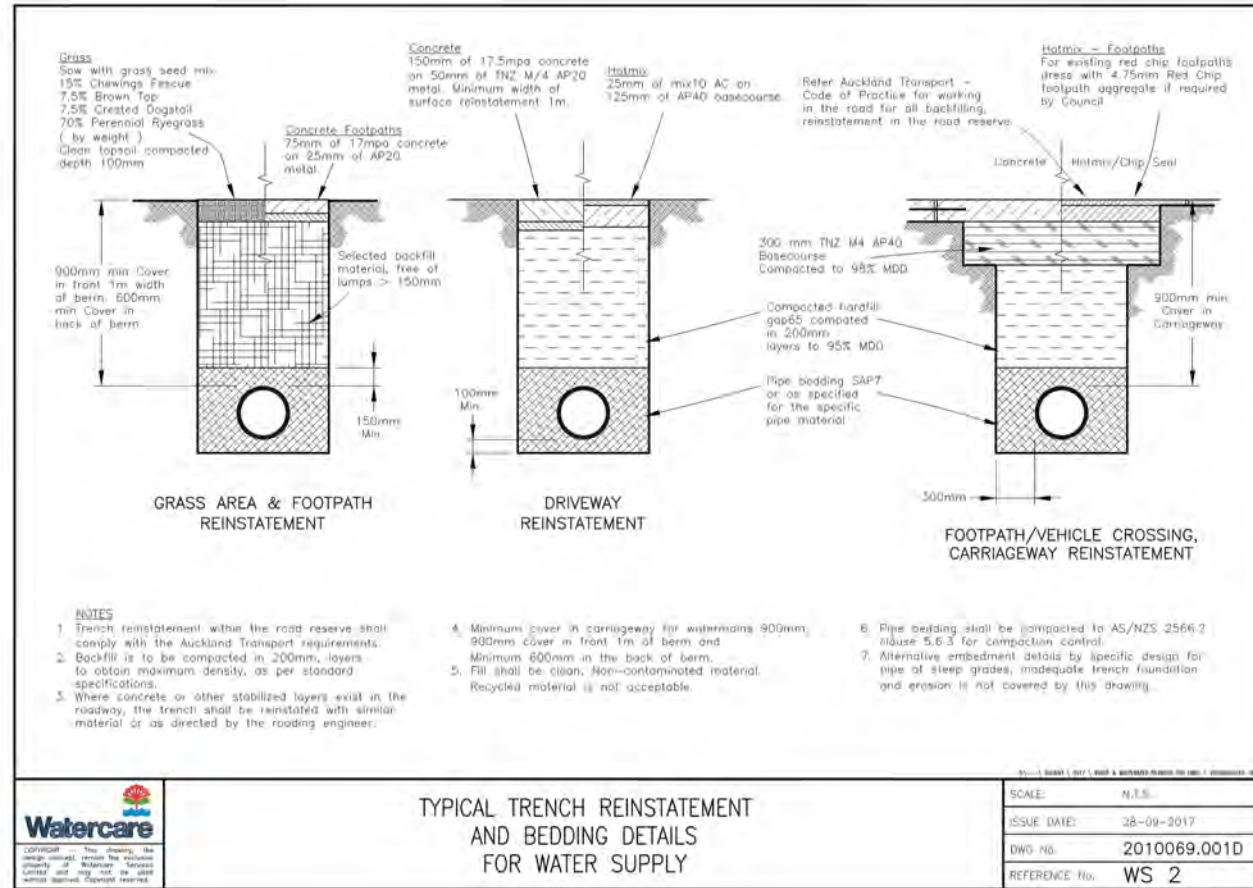
ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-590** REV: **C2**

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MM	03.2019		
MSS	05.06.19		
MSS	05.06.19		
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND**

SHEET TITLE:
WATERMAIN STANDARD DETAILS

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **NOT TO SCALE**

SCALE BAR

DRAWING NUMBER: **2094-01-590** REV: **C2**

**SUBMISSION BY THE D E NAKHLE INVESTMENT TRUST ON NOTICE OF REQUIREMENT
TO DESIGNATE LAND**

To: Auckland Council

Name of submitter: Daniel Elias Nakhle and Feroz Rahat Ali as trustees of the D E Nakhle Investment Trust ("**DNIT**")

Address for service: c/- Aidan Cameron
Barrister
Bankside Chambers
Level 22, 88 Shortland St
AUCKLAND 1140
T + 64 9 307 9955
E aidan@bankside.co.nz

This is a submission on Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR 2) Auckland Transport for a new multi-modal bridge crossing of the North Island Main Trunk (NIMT) railway at Walters Road. (the "**NoR**").

The trustees of DNIT are not a trade competitor for the purposes of section 308B of the Resource Management Act 1991 ("**RMA**").

DNIT wishes to be heard in support of our submission.

If others make a similar submission, DNIT would consider presenting a joint case with them at any hearing.

Scope of submission

1. This submission relates to the NoR in its entirety.

Nature of submission

2. DNIT opposes the NoR in its entirety.

Reasons for submission

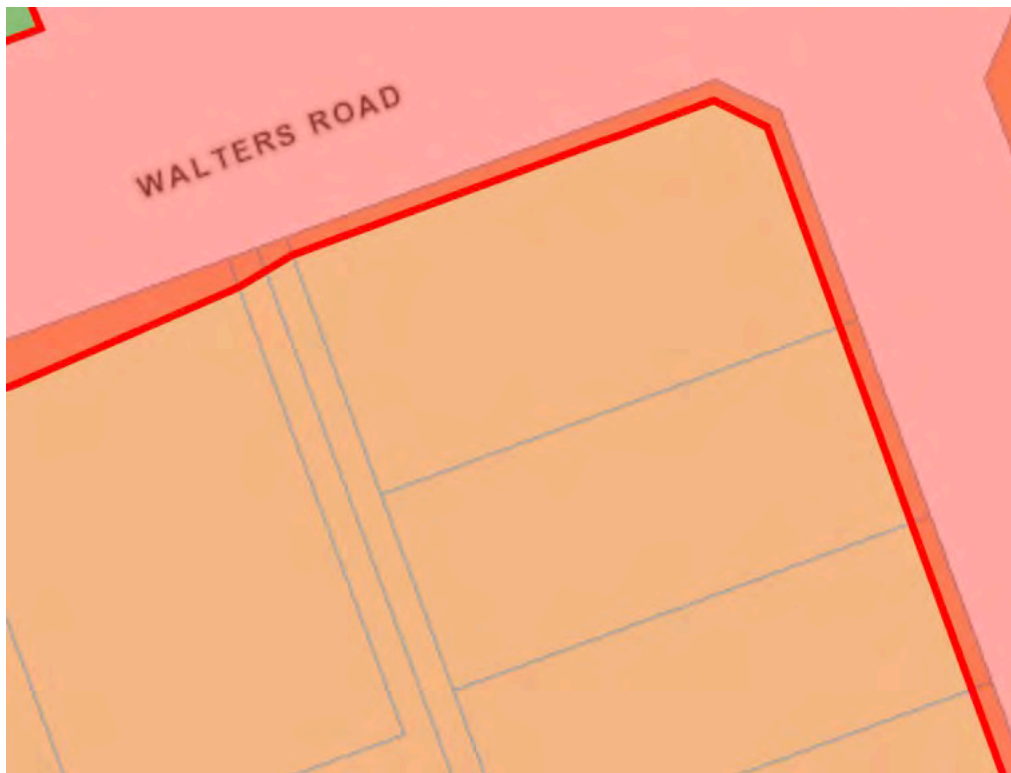
3. The primary reasons for this submission are that the NoR:
 - a. fails to promote the sustainable management of natural and physical resources, and therefore fail to meet the purpose and principles of the Resource Management Act 1991 (“**RMA**”);
 - b. fails to meet the reasonably foreseeable needs of future generations;
 - c. fails to enable the social, economic and cultural well-being of the Auckland community to be met;
 - d. is inconsistent with the purposes and provisions of the relevant planning documents, including the Unitary Plan and the Regional Policy Statement (“**RPS**”);
 - e. is inconsistent with Part 2 and other provisions of the RMA, including sections 74 and 75, including the functions of Auckland Council (“**Council**”) under s 31; and
 - f. will have significant adverse effects on the environment;
 - g. is not an efficient use and development of natural and physical resources;
 - h. fails to comply with s 171(1)(b) of the RMA, as adequate consideration has not been given to alternative sites, routes or methods of undertaking the proposed works in circumstances where Waka Kotahi – The New Zealand Transport Agency (“**NZTA**”) does not have an interest in the land sufficient for undertaking the work; and in light of (f) above;
 - i. is not reasonably necessary to achieve the objectives of the requiring authority for which the designations are sought.
4. Without limiting the generality of the above, additional specific reasons for opposing the NoR are set out below.

Background and introduction

5. DNIT owns the sites at 164-166 Porchester Road, Takanini.
6. The sites are zoned Mixed Housing – Urban under the Auckland Unitary Plan (Operative in Part) (“**AUP**”).
7. The sites are currently bare land, following demolition of the existing dwellings in late 2022. The land has a resource consent for two four level apartment blocks, carrying 42 residential

units, including consents for all necessary land modification, infrastructure provisions, landscaping, vehicle access, car parking and contamination remediation.

8. A copy of the resource consent and approved plans are **attached** to this submission as Appendix A.
9. The units are a mixture of one and two-bedroom residences, which are considered to be ideal for social or community housing purposes. DNIT has recently concluded an agreement to lease with a social housing provider.
10. Works have commenced to implement the consents, with pre-loading now complete and detailed geotechnical investigation underway.
11. The proposed designation covers the front yard area on the corners of Walters Road and Porchester Road, as shown in the image below:



12. The approved resource consent has allowed the building and its associated decks to be located within the front yard setback, and requires the remaining front yard to be planted to mitigate the effects of the encroachment.
13. The advice DNIT has received is that the current development works are also affected by the proposed designation, as the apartment building cannot be constructed without using the area that is proposed to be designated.

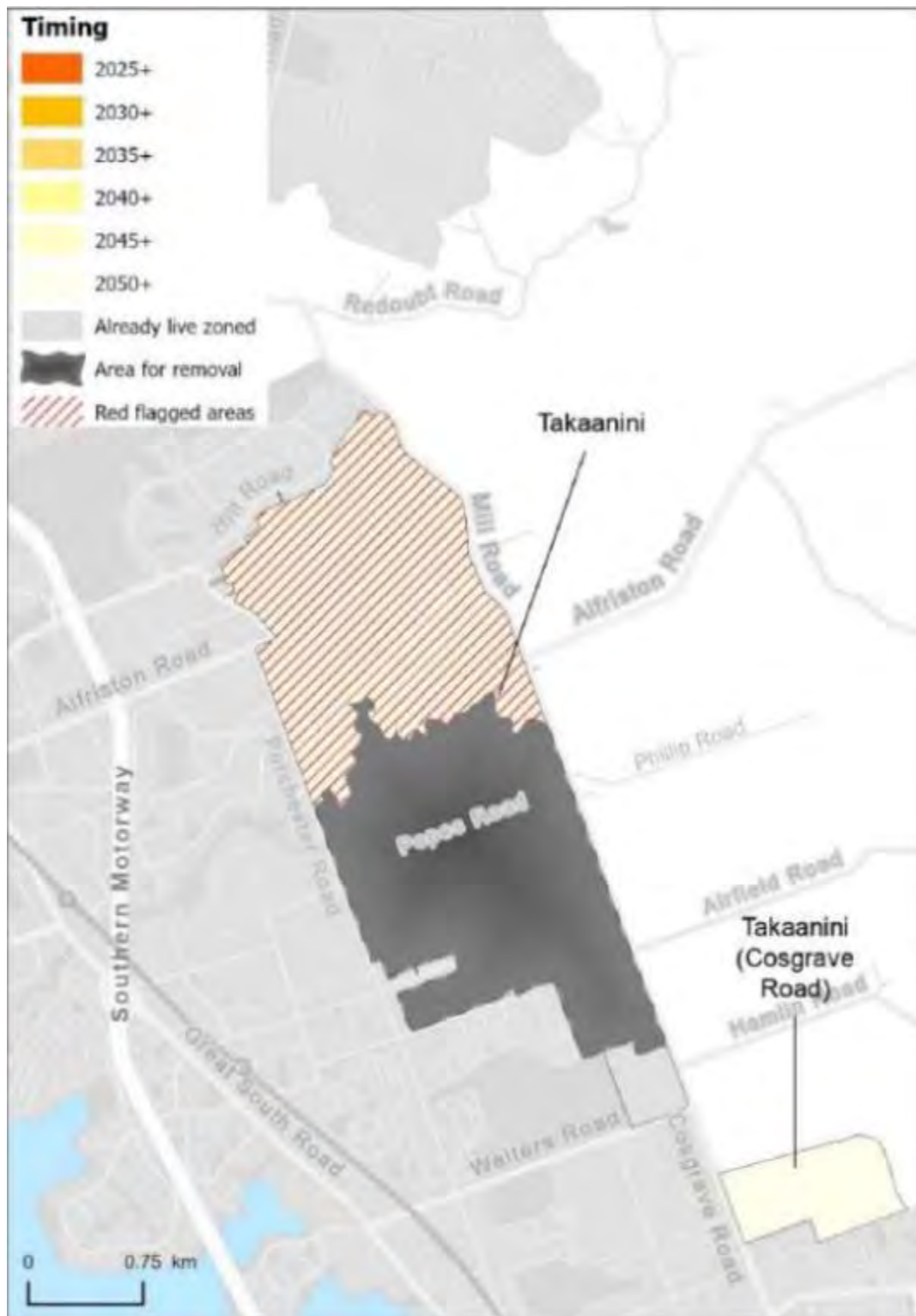
Specific reasons for opposing the NoR

Does not take into account recent planning decisions

14. On 2 November 2023 the Planning, Environment and Parks Committee resolved to adopt a new Future Development Strategy. Specifically resolution clause c) states:

c) tuhi ā-taipitopito / note that once published, the Future Development Strategy replaces the current Development Strategy (2018) and the Future Urban Land Supply Strategy (2017) and will be considered part of the Auckland Plan 2050.

15. Relevant to the Takanini area, the adopted Future Development Strategy has removed a large amount of land that had previously been earmarked for development and has a significant portion of land “red flagged” alongside the Porchester Road corridor. The below image depicted the newly adopted FDS areas.
16. It is not clear whether the necessity for the project, or at a more detailed level the traffic modelling volumes take these anticipated zoning changes into account.



Inconsistent with approved resource consents

17. The resource consent that has been granted for the site may now require approval from the requiring authority under s 178. This is an unwarranted impost upon an approved development. The NOR should be amended to eliminate this source of uncertainty.
18. The resource consent requires ongoing and continued compliance with the conditions of its approved land use consents, which include specific landscaping along both road frontages in accordance with approved plans.

19. The designation and associated works will remove features of the approved land use consents.
20. There is no mechanism proposed to address the resultant non-compliance with approved land use consents.

Construction effects

21. There is insufficient information and conditions to ensure that the construction effects will not damage the proposed apartment building (particularly given the close proximity of the building footings etc to the proposed designation boundary).
22. There is insufficient information to ensure that noise received at the outdoor living spaces of the proposed units will maintain the useability of these spaces during works and following completion of the road (based on the new road volumes). This is particularly acute given that NOR 2 and 4 provide different future traffic volumes as set out below.
23. No specific provision has been made to ensure residents are able to be accessed by emergency services at all times. While such access is important for all residents, it is particularly so for the vulnerable members of the community likely to be housed here.
24. There is insufficient information to manage privacy and screening for units from the works occurring directly adjacent to outdoor living spaces.
25. Insufficient information is provided to demonstrate that the designated works can proceed without undermining the foundations of the units.

Flooding

26. Although it appears that flooding of structures will be avoided, there is no such assurance that flooding or ponding of the apartments' carpark area will be avoided. It is essential that the health and safety of vulnerable residents is assured and not imperilled by the works.

Noise impacts

27. As the proposed apartment buildings are under construction now, any required mitigation for future road noise should be installed at the same time to avoid unnecessary wastage of resources and extensive costs associated with retrofitting.
28. As identified in the matter below, as there is no consistency between the NoR's for volumes of traffic and design of this portion of road, the adequacy of any noise assessment is also questionable as to its accuracy in respect of effects

Certainty of Design

29. NOR2 (Walters Road) and NOR4 (Porchester Road) provide inconsistent design responses fronting 166 Porchester Road. NOR2 proposes a two-lane cross section with central median that ties into the existing roundabout, whereas NOR 4 proposes the signalisation of the Porchester Road/Walters Road intersection. The Resource Consent obtained for 164-166 Porchester Road requires mitigation to be provided on Walters Road based on a road cross section consistent with NOR2.

30. More concerningly, the two NORs proceed on the basis of different factual starting points, with predicted traffic volumes on Walters Road varying by up to almost 50%.

	NOR 2 (Walters Road between Arion Road and Porchester Road)	NOR 4 (Walters Road between Arion Road and Porchester Road)
AM Peak	396 vph northbound 811 vph southbound	366 vph northbound 735 vph southbound + Walters Road (north) southbound through traffic
Interpeak	591 vph northbound 693 vph southbound	Not provided
PM Peak	576 vph northbound 511 vph southbound	835 vph northbound 438 vph southbound + Walters Road (north) southbound through traffic

31. There can be no certainty that the proposed solutions will adequately manage predicted future traffic when there is no certainty about what volumes of traffic are to be.
32. Adjacent landowners need certainty as to what is to be constructed. The two NORs must be reconciled. Maintenance of pedestrian accessibility is essential for the future residents of 164-166 Porchester Road.

Conditions do not provide effective mitigation

33. The conditions do not address the concerns of the submitter and they do not provide for effective mitigation, including for the following reasons:
- (a) There is no requirement for management plans to be certified – they are effectively provided to the Council on a “for information only” basis. It is bad enough that the affected landowners are left having to rely upon Council to protect their interests, worse when the even the Council’s input is dispensed with. Each management plan should be required to achieve a clear objective and Council should retain the role of certifying that the objective has been achieved, in accordance with the approach long-approved by the Environment Court.
 - (b) The LUIP:
 - (i) Only addresses the timeframe between confirmation of the designation and the start of construction. As identified above the proposed apartment building is under construction now.

- (ii) Gives no certainty that working with the Requiring Authority through this process will result in s 176 or s 178 approval.
- (c) Condition 7 relating to Outline Plans enables a Requiring Authority to pick and choose which management plans are relevant to each “stage”, and allows the Requiring Authority to determine what is to form each “stage”.
- (d) The SCEMP does not include any resolution process for where the concerns of the landowner are not being adequately addressed by the outline plan of works/management plans.
- (e) The ULDMP should be utilised as a tool for refinement and implementation of a design which is already of a standard that will achieve quality urban design and landscape outcomes, as opposed to a tool to fix the current concept plan.
- (f) The ULDMP requires stakeholders to be invited to participate in the detailed design 6 months prior to the start of detailed design for “a stage of work”. There is no obligation for this participation to continue through the detailed design, nor to participate in earlier designs (e.g enabling works) which ultimately affect decisions and outcomes in the ULDMP.
- (g) The ULDMP should also include an independent process for resolution of any disagreement in the design outcomes (as listed in clause (f) of the ULDMP condition) or achievement of the ULDMP objective outcomes (as listed in clause (b) of the ULDMP condition).
- (h) The project should not enable any increase in flood hazard on any sites. The consented apartment building and its associated carriage and access has been carefully designed to manage flood effects and velocities of flood to ensure the safety of people and vehicles within the site (as well as maintaining an acceptable minimum floor level of the building to protect the habitable spaces from flood hazard);
- (i) The Management Plans should be required to maintain access (vehicle and pedestrian) at all times during work.

Recommendation sought

- 34. DNIT seeks that the NoR is recommended to be **withdrawn**.
- 35. In the alternative, DNIT seeks conditions to ensure AT addresses each of the issues raised in this submission including conditions that:
 - a. Require an appropriate roading design that caters for realistic future demand and recognises the value and importance of existing investment, minimises intrusions upon private land and eliminates the designation from 164-166 Porchester Road;
 - b. Require the requiring authority to provide approval under ss 176 or 178 if required to allow the construction of the 42 consented dwelling units on 164-166 Porchester Road;
 - c. Ensure that noise levels received at the units to be constructed at 164-166 Porchester Road are reasonable and that mitigation is installed as the units are built;

- d. Ensure that there is no damage to the buildings to be constructed at 164-166 Porchester Road, including to their foundations;
- e. Secure access for residents (including by emergency services) during construction;
- f. Provide a safe pedestrian environment on the upgraded roads adjacent to 164-166 Porchester Road;
- g. Avoid flooding impacts on the buildings and carparking at 164-166 Porchester Road;
- h. Require management plans to be certified by the Council against a sensible purpose.

Signature:



Janette Campbell

Counsel for the DE Nakhle
Investment Trust

Date: 14 December 2023

Address for Service: c/- Aidan Cameron

Barrister

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Decision on an application for resource consent under the Resource Management Act 1991



Restricted discretionary activity for a residential activity

- Application number:** BUN60343865
LUC60343866 – Land use (s9)
DIS603473867 – Discharge permit (s14)
- Applicant:** Alda Investments Limited
- Site addresses:** 164 Porchester Road, Takanini
166 Porchester Road, Takanini
- Legal descriptions:** Lot 2 DP 21172
Lot 1 DP 21172
- Proposal:** The Applicant proposes to undertake all necessary land modification, infrastructure provisions, landscaping, vehicle access, car parking and contamination remediation associated with the constructing of two four level apartment blocks on the subject sites containing 42 residential units.

Resource consent is required for the following reasons:

Land use consent (s9) – LUC60343866

Auckland Unitary Plan (Operative in part)

District land use (operative plan provisions)

Chapter E12: Land Disturbance – District

- General earthworks over an area greater than 1000m² and less than 2500m³ in a residential zone is a restricted discretionary activity under rule E12.4.1 (A5). The proposal involves general earthworks over an area of 2000m².
- General earthworks of involving volumes greater than 2500m³ in a residential zone is a restricted discretionary activity under rule E12.4.1 (A10). The proposal involves 1220m³ of cut and 3m³ of fill over followed by 2000m³ of fill for pre-loading requirements.
- General earthworks that are restricted discretionary activities under rules E12.4.1 (A5) and (A10) that fail to meet the following standard is a restricted discretionary activity under rule C.1.9(2):
 - Standard E12.6.2(13), as earthworks within the 1% AEP flood plain will exceed maximum specified calendar period of 28 days.

Chapter E36: Natural Hazards and Flooding

- Surface parking areas in the 1% AEP floodplain, that do not comply with Standard E36.6.1.7 are a controlled activity pursuant to rule E36.4.1 (A25)
- To construct stormwater management devices within an identified 1% AEP flood plain and is a restricted discretionary activity under rule E36.4.1 (A33).
- To construct new buildings within the 1% annual exceedance probability (AEP) floodplain is a restricted discretionary activity under rule E36.4.1 (A37).
- To use new buildings within the 1% annual exceedance probability (AEP) floodplain for more vulnerable activities (residential) is a restricted discretionary activity under rule E36.4.1 (A38).

Chapter H5: Residential – Mixed Housing Urban Zone

- To construct and use 42 residential dwellings is a restricted discretionary activity under rule H5.4.1 (A4).
- To construct new buildings which do not comply with standard H5.6.7 Height in Relation to Boundary but comply with standard H5.6.6 Alternative Height in Relation to Boundary is a restricted discretionary activity under rule H5.4.1(A33).
- To develop new buildings (and/or any additions to existing), as the development of new buildings or any additions has the same activity status as the land use activity, is a restricted discretionary activity under rule H4.4.1 (A34).
- Use and development that is a restricted discretionary activity under rules H5.4.1(A4) and (A34) that fails to meet the following core standard and is a restricted discretionary activity under rule C.1.9(2):
 - H5.6.8(1): The proposal involves the following front yard infringements:
 - The North Block enclosed decks are setback 2.08m from the Walters Road boundary.
 - One enclosed deck at the corner of Walters Road and Porchester Road is setback 1.081m from the boundary.
 - The northern eastern corner of North Block is setback 0.802m from the front boundary at the corner of Walters Road and Porchester Road.
 - The West block enclosed decks are setback between 2.036 and 2.098m from the Porchester Road boundary.

National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health (NESCS)

- The proposal involves and subdivision on a piece of land described in Regulation 5(7). As a DSI exists for the site, which states that the soil contamination of the piece of land exceeds the applicable standard, the proposal is a restricted discretionary activity pursuant to regulation 10(2).

Stormwater discharge permit (s14) – DIS60343867

Auckland Unitary Plan (Operative in part)

Chapter E8: Stormwater – Discharge and Diversion

- To discharge stormwater runoff from impervious areas greater than 1000m² and up to 5000m² within an urban area (1831m² of impervious area proposed) complying with standards E8.6.1 and E8.6.3.1 is a controlled activity under rule E8.4.1 (A9).

Decision

I have read the application, supporting documents, and the report and recommendations on the application for resource consent. I am satisfied that I have adequate information to consider the matters required by the Resource Management Act 1991 (RMA) and make a decision under delegated authority on the application.

Acting under delegated authority, under sections 104, 104C, 105, 107 and Part 2 of the RMA, the resource consent is **GRANTED**.

Reasons

The reasons for this decision are:

1. The applications are for controlled and restricted discretionary activity resource consents, as such under s104A and s104C only those matters over which council has restricted its discretion or control have been considered. Those matters are:
 - a. E8.7.1(1) – for stormwater discharge from impervious areas greater than 1000m² and up to 5000m² within an urban area;
 - b. E36.7.1(1) – for surface parking areas in the 1% AEP (AEP) floodplain;
 - c. E12.8.1(1) – for all restricted discretionary district earthworks;
 - d. E36.8.1(8) – for the construction of stormwater management devices in the 1% AEP flood plain;
 - e. E36.8.1(9) – for new structures and buildings within the 1% AEP flood plain;
 - f. E36.8.1(10) – for use of new buildings to accommodate more vulnerable activities located within the 1% AEP floodplain;
 - g. H5.8.1(2) – for four or more dwellings on a site;
 - h. H5.8.1(4) – for buildings that do not comply with:
 - i. H5.6.8 Yards;
 - ii. H5.6.9 Maximum impervious area;
 - iii. H5.6.11 Landscaped area;
 - iv. H5.6.12 Outlook space;

- v. H5.6.13 Daylight;
- vi. H5.6.14 Outdoor living space;
- vii. H5.6.15 Front, side and rear fences and walls;
- viii. H5.6.16 Minimum dwelling size;
- i. H5.8.1(5) – for buildings that do not comply with standard H5.6.5 Height in relation to boundary but comply with standard H5.6.6 Alternative height in relation to boundary; and
- j. Regulation 10(3) – for restricted discretionary activities under the NES:CS

No other effects have been taken into account in this assessment.

2. In accordance with an assessment under ss104(1)(a) and (ab) of the RMA the actual and potential effects from the proposal will be acceptable as:
 - a. The proposal provides for apartment typologies in two blocks that are three storeys in height, which achieves an overall built form; building scale; and density that will be compatible in the context of the existing and anticipated character of the surrounding residential area.
 - b. From an urban design perspective, the proposal is appropriate to its context; will result in a development that responds positively to the unique characteristics of the site and the surrounding neighbourhood; and is consistent with the type of activity and design quality anticipated within the zone.
 - c. The stormwater management regime comprising treatment, detention, retention and soakage devices (incorporating recharge) is appropriate in the context of the site's ground conditions (peat soils) and absence of available public stormwater network.
 - d. The timing, duration and scale of the proposed earthworks programme is commensurate with residential development. In particular, the building platform pre-loading is necessary due to ground conditions (peat soils). Overall, the earthworks can be appropriately managed and mitigated through compliance with the AUP(OP) noise and vibration standards; and suitable consent conditions to ensure that the proposed earthworks are acceptable, including the preparation and approval of a construction noise and vibration management plan prior to earthworks commencing.
 - e. All geotechnical and land stability matters will be addressed through the robust suite of geotechnical related conditions agreed to by the applicant. Overall, the site is suitable for the residential activities subject to adherence to the geotechnical conditions.
 - f. No significant OLFP spills into the property from the road as the levels in the road reserve limit flow into the site and existing levels in the site prevent the small flow that does enter the site from the east from crossing the site. As such, the development will not impact on overland flow paths. The potential for flood hazards affecting habitable dwellings will be addressed by providing a minimum floor level of 21.1m.
 - g. The existing road network will adequately cater for the additional traffic generated by the proposed development and the vehicle crossing on Walters Road will adequately cater for the anticipated vehicle movements to and from the site. As such, the traffic generation

associated with the proposal will not result in any discernible adverse effects on the local or regional roading networks.

- h. Adequate onsite circulation and manoeuvring space will be achieved for all parking spaces including the onsite refuse collection point; therefore, vehicles will exit the site in a forward gear.
- i. The pedestrian refuge island within Walters Road will be relocated 2m to the east to enable sufficient tracking space for an 8m medium rigid truck exiting the subject site from the vehicle crossing and turning right.
- j. The driveway and pedestrian access points are the only impervious surfaces that will be visible from the streetscape, as the car parking area is located behind the apartment blocks. This design response, in combination with the front yard achieving greater than 50% of the total area as landscape and planting ensures that the increased paved surface does not detract from the amenity values of the streetscape.
- k. The quality and quantity of landscaping elements in the car parking area and front yard complements the built form and incorporates a range of strategically placed specimen trees, screening shrubs, low level shrubs, ground covers and planted buffers between various thresholds, all of which assist in providing privacy, visual interest and residential amenity.
- l. The apartment layouts achieve outlook and outdoor living spaces of sufficient width and depth to ensure an appropriate level of onsite amenity for future residents is achieved.
- m. The effects of the combined retaining and fence will not affect privacy or passive surveillance over the street as it occurs in the south western corner of the site, in the car parking area which adjoins neighbouring properties.
- n. The length of fence over 2m is 3m along the southern boundary and 5.2m along the western boundary and is a maximum of 0.4m over the 2m height. The neighbouring land to the west is part of a right of way serving 43 and 45 Walters Road; therefore, the over height will not affect the amenity of persons associated with these properties. The neighbouring land to the south is the rear corner of 2/162 Porchester Road and is primarily used for car parking; therefore, the over height will not affect the amenity of persons associated with these properties.
- o. The one-bedroom units at 43m² and 44m² have been demonstrated to be functional and of sufficient size to cater for future resident's liveability through provision of appropriately dimensioned living, dining and kitchen areas and generous double bedrooms.
- p. The use of the AHIRB standard is not considered to detract from the streetscape or the amenity of adjoining neighbours and is reflective of an appropriate built form for the site. The units have been provided with balconies which overlook the streetscape enabling passive surveillance, front yard landscaping has been optimised as described above, car parking has been located to the rear of the buildings avoiding visual effects of car parking on the streetscape and pedestrian access have been provided to the development from three separate entry points. While there are some windows which are located along the southern boundary, these are located in excess of 4m from the adjoining boundary. Furthermore, the main outlook is via the balcony which overlooks Porchester Road.

- q. Following site remediation, (accordance with the SMP with site validation sampling), the risks to human health and the environment as a result of soil contamination will be adequately addressed.
 - r. With reference to s104(1)(ab), there are no specific offsetting or environmental compensation measures proposed or agreed to by the applicant to ensure positive effects on the environment.
3. In accordance with an assessment under s104(1)(b) of the RMA the proposal is consistent with the relevant statutory documents for the reasons outlined in section 2 of this decision. In this instance, the proposal specifically accords with the following:
- a. Objectives: E1.2(1)-(3) – Chapter E1 (Water quality and integrated management)
 - b. Policies: E1.3(1)-(2), (4)-(6), (9)-(16) – Chapter E1 (Water quality and integrated management)
 - c. Objectives: E12(2)(1) – Chapter E12 (Land Disturbance – District)
 - d. Policies: E12.3.(1) to E12.3(6) – Chapter E12 (Land Disturbance – District)
 - e. Objectives: E36.2(2) and (5) – Chapter E36 (Natural hazards and flooding)
 - f. Policies: E36.2.(3) (4), (13)-(15), (21), (25), and (26) – Chapter E36 (Natural hazards and flooding)
 - g. Objectives: H5.2 (1)-(3) – Chapter H5 (Residential – Mixed Housing Urban Zone)
 - h. Policies: H5.2(1)-(7) – Chapter H5 (Residential – Mixed Housing Urban Zone)
4. In accordance with an assessment under s104(1)(c) no other matters are considered relevant.
5. In terms of s 105 and s107 of the RMA, the proposal is acceptable for the following reasons:
- a. The discharge proposed is for onsite disposal and soakage into the peat soils – this allows for recharge of peat soils but also is required as there is no public connection for stormwater available to the site.
 - b. Contaminant removal has been provided for the whole site, thereby ensuring that effects on the receiving environment (being the soils and groundwater as applicable to the site) are managed in accordance with best practice.
 - c. The proposed system is required as there is no available stormwater connection. Specific devices have been chosen based on their known consistency to reduce contaminants in a manner which accords with best practice guidelines.
 - d. The alternative is to provide a new piped system which can connect to an existing system (which must be first tested to have capacity) or a new outfall to the Manukau Harbour for discharge. These options are not considered practicable in the case of this application.
 - e. Overall, the applicant has demonstrated that stormwater management regime will minimise the risk of discharges giving rise to:

- i. The production of any conspicuous oil / grease films, scums / foams, floatable or suspended materials;
 - ii. any conspicuous change in colour or visual clarity;
 - iii. any emission of objectionable odours;
 - iv. the rendering of fresh water unsuitable for consumption by farm animals; or
 - v. any significant adverse effects on aquatic life.
6. In the context of this restricted discretionary activity, where the objectives and policies of the relevant statutory documents were prepared having regard to Part 2 of the RMA, they capture all relevant planning considerations and contain a coherent set of policies designed to achieve clear environmental outcomes. They also provide a clear framework for assessing all relevant potential effects and there is no need to go beyond these provisions and look to Part 2 in making this decision as an assessment against Part 2 would not add anything to the evaluative exercise.
7. Overall, the proposal is acceptable and achieves the sustainable management purpose of Part 2 of the RMA avoiding, remedying, or mitigating adverse environmental effects as appropriate.

Conditions

Under sections 108 and 108AA of the RMA, this consent is subject to the following conditions:

Activity in accordance with application

1. The construction, and use, of the residential development and associated site preparatory work (i.e. the activity) shall be carried out in accordance with the documents and drawings and all supporting additional information submitted with the application, detailed below, and all referenced by the council as resource consent number BUN60343865.
 - Application Form, and Assessment of Effects prepared by Renee Fraser-Smith of Tollemache Consultants Limited, titled: "Resource consents for 42 Residential Units and associated works; 164-166 Porchester Road, Takanini" and dated August 2019.

Report title and reference	Author	Rev	Dated
Infrastructure Report 164-166 Porchester Road, Takanini Project No. 2094	CivilPlan Consultants	R001v3	30 July 2019
Transportation Assessment 164 to 166 Porchester Road, Takanini	Flow Transportation Consultants	B	25 July 2019

Urban Design Assessment Residential Development at 164 and 166 Porchester Road, Takanini	Transurban	-	26 July 2019
Detailed Site Investigation – Contamination 164A, 164B and 166 Porchester Road, Papakura Project No. 32709	Fraser Thomas	1	26 March 2019
Remedial Action Plan / Site Management Plan 164A, 164B and 166 Porchester Road, Papakura Project No. 32709	Fraser Thomas	1	1 October 2019
Geotechnical Memorandum 164-166 Porchester Road – Preload Design and Settlement Monitoring	Lander Geotechnical	-	8 May 2019
Geotechnical Report Proposed Three Storeys Apartment Development at 164 to 166 Porchester Road, Papakura Project No. J01138	Lander Geotechnical	A	16 April 2020

Plan title and reference	Author	Rev	Dated
2094-01-000 COVER SHEET	CivilPlan Consultants	C2	June 2019
2094-01-130 PROPOSED SITE LAYOUT	CivilPlan Consultants	C2	26 June 2019
2094-01-150 TOPOGRAPHICAL PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-200 PROPOSED CONTOUR PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-210 PRE-LOAD PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-220 ISOPACH PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-230 SEDIMENT AND EROSION CONTROL PLAN	CivilPlan Consultants	C2	26 June 2019

2094-01-235 SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 1	CivilPlan Consultants	C2	26 June 2019
2094-01-236 SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 2	CivilPlan Consultants	C2	26 June 2019
2094-01-370 VEHICLE CROSSING - PLAN AND DETAIL	CivilPlan Consultants	C2	26 June 2019
2094-01-400 PROPOSED DRAINAGE- OVERALL LAYOUT	CivilPlan Consultants	C3	29 July 2019
2094-01-401 PROPOSED DRAINAGE- DETAIL PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-410 EXISTING FLOODPLAIN - 100YR WITH CLIMATE CHANGE	CivilPlan Consultants	C2	26 June 2019
2094-01-411 PROPOSED FLOODPLAIN - 100YR WITH CLIMATE CHANGE	CivilPlan Consultants	C3	29 July 2019
2094-01-450 WASTEWATER LONGITUDINAL SECTIONS	CivilPlan Consultants	C2	26 June 2019
2094-01-475 PROPOSED DRAINAGE - STORMWATER360 - STORMFILTER DETAIL	CivilPlan Consultants	C2	26 June 2019
2094-01-476 PROPOSED DRAINAGE - STORMWATER360 - STORMFILTER CALCULATION	CivilPlan Consultants	C2	26 June 2019
2094-01-477 PROPOSED DRAINAGE - RECHARGE PIT DETAILS	CivilPlan Consultants	C2	26 June 2019
2094-01-490 PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 1	CivilPlan Consultants	C2	26 June 2019
2094-01-491 PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 2	CivilPlan Consultants	C2	26 June 2019

2094-01-495 WASTEWATER STANDARD DETAILS	CivilPlan Consultants	C2	26 June 2019
2094-01-500 PROPOSED WATERMAIN - LAYOUT PLAN	CivilPlan Consultants	C2	26 June 2019
2094-01-590 WATERMAIN STANDARD DETAILS	CivilPlan Consultants	C2	26 June 2019
A000 COVER SHEET	RCG	-	July 2019
A100 SITE & GROUND FLOOR PLAN	RCG	G	23 July 2019
A101 FIRST FLOOR	RCG	D	23 July 2019
A102 SECOND FLOOR	RCG	D	23 July 2019
A103 THIRD FLOOR	RCG	D	23 July 2019
A104 ELEVATIONS (WALTERS, PORCHESTER & SOUTH)	RCG	E	8 November 2019
A105 ELEVATIONS (WEST, SOUTH & CROSS SECTIONS)	RCG	E	8 November 2019
A106 OUTLOOK SPACE	RCG	B	23 July 2019
A107 LETTERBOXES	RCG	B	23 July 2019
A200 3D VIEWS (WALTERS & PORCHESTER CORNER)	RCG	A	23 July 2019
A201 3D VIEWS (PORCHESTER)	RCG	A	23 July 2019
A202 3D VIEWS (CAR PARK & BUILDING REAR)	RCG	A	23 July 2019
LA00 LANDSCAPE NOTES AND PLANTING SCHEDULES	Transurban	-	26 July 2019
LA01 SITE PLAN	Transurban	-	26 July 2019

LA02 SITE PLAN	Transurban	-	26 July 2019
LA03 DETAILS	Transurban	-	26 July 2019
LA04 PLANT INFORMATION	Transurban	-	26 July 2019
SHEET 1 OF 2 164 TO 166 PORCHESTER ROAD (RESIDENTIAL) VEHICLE TRACKING	Flow Transportation Consultants	F	24 September 2019
SHEET 2 OF 2 164 TO 166 PORCHESTER ROAD (RESIDENTIAL) VEHICLE TRACKING	Flow Transportation Consultants	F	24 September 2019
2094-00-SK04 VEHICLE TRACKING	CivilPlan Consultants	1	26 May 2020

Other additional information	Author	Rev	Dated
S92 response	Mike Smith of CivilPlan Consultants	-	21 November 2019
s92 response	Terry Church of Flow Transportation Consultants	-	24 September 2019
Email s92 response Fwd_ BUN60343865_ 164-166 Porchester Road, Takanini - Urban Design s92 response	Renee Fraser-Smith of Tollemache Consultants Limited	-	14 January 2020
s92 response	Transurban	-	14 January 2020 (received via email)
s92 response	John Lenihan of RCG	-	14 January 2020
s92 response	Alister Hood of CivilPlan Consultants	-	3 March 2020
s92 response	Shane Lander of Lander Geotechnical	-	12 March 2020
Email s92 response Re_ FW_ BUN60343865_ 164- 166 Porchester Road, Takanini - Update	Renee Fraser-Smith of Tollemache Consultants Limited	-	21 May 2020

Lapsing of consent

2. Under section 125 of the RMA, this consent lapses five years after the date it is granted unless:
 - a. The consent is given effect to; or

- b. The council extends the period after which the consent lapses.

Duration – DIS60343867

3. Stormwater diversion and discharge permit DIS60343867 shall expire 35 years from the decision date of this consent unless it has lapsed, been surrendered or been cancelled at an earlier date pursuant to the RMA.

Monitoring fee

4. The consent holder shall pay the council an initial consent compliance monitoring charge of \$660 (inclusive of GST), plus any further monitoring charge or charges to recover the actual and reasonable costs incurred to ensure compliance with the conditions attached to these consents.

Advice Note:

The initial monitoring deposit is to cover the cost of inspecting the site, carrying out tests, reviewing conditions, updating files, etc., all being work to ensure compliance with the resource consent. In order to recover actual and reasonable costs, monitoring of conditions, in excess of those covered by the deposit, shall be charged at the relevant hourly rate applicable at the time. The consent holder will be advised of the further monitoring charge. Only after all conditions of the resource consent have been met, will the council issue a letter confirming compliance on request of the consent holder.

Pre-commencement meeting

5. Prior to the commencement of the earthworks activity, the consent holder shall hold a pre-start meeting that:
 - a. is located on the subject site
 - b. is scheduled not less than 5 days before the anticipated commencement of earthworks
 - c. includes monitoring officers
 - d. includes representation from the contractors who will undertake the works and any suitably qualified professionals if required by other conditions

The following matters shall be discussed at the meeting:

- e. the erosion and sediment control measures
- f. the earthworks methodology
- g. shall ensure all relevant parties are aware of and familiar with the necessary conditions of this consent

The following information shall be made available at the pre-start meeting:

- h. Timeframes for key stages of the works authorised under this consent
- i. Resource consent conditions
- j. Geotechnical detail design report
- k. Settlement monitoring report
- l. *Remedial Action Plan/Site Management Plan* – 164a,164B and 166 Porchester Road, Papakura, prepared by Fraser Thomas Limited, dated 1 October 2019 ('RAP/SMP').

Advice Note:

To arrange the pre-start meeting required by condition 4 please contact the Team Leader Monitoring (South) to arrange this meeting or email monitoring@aucklandcouncil.govt.nz . The conditions of consent should be discussed at this meeting. All information required by the council and listed in that condition should be provided 2 days prior to the meeting.

Architectural design plans

- 6. Prior to the approval of Building Consent, a finalised set of architectural detail drawings and materials specifications shall be submitted to Council's Team Leader Monitoring (South) for written certification. The information shall include the following:
 - a. details of the building's façade treatment / architectural features;
 - b. materials schedule and specification, sample palette of materials, surface finishes, and colour schemes (including colour swatches) referenced on the architectural elevations; and
 - c. external / rooftop services / plant, and visual.

The finalised set of drawings shall ensure that the building's proposed architectural treatment and finished appearance is consistent with the plans and information referenced at condition 1. All works shall then be carried out with the details certified by council, and thereafter retained and maintained, to the satisfaction of Team Leader Monitoring (South).

Advice Note:

As part of the condition monitoring process, Council's monitoring inspectors will liaise with members of the Council's Auckland Design Office to ensure that the submitted details are consistent with the approved plans and information.

Finalised landscape design drawings, specifications and maintenance requirements

7. Prior to the commencement of apartment building construction on site, the consent holder shall provide to the Team Leader Monitoring (South) for approval, a finalised set of detailed landscape design drawings and supporting written documentation which have been prepared by a landscape architect or suitably qualified professional. The submitted information shall be consistent with the consented landscape concept plans prepared by TransUrban dated 26.07.2019 and, at a minimum, shall include landscape design drawings, specifications and maintenance requirements including:
 - a. An annotated planting plans which communicate the proposed location and extent of all areas of planting
 - b. Annotated cross-sections and/or design details with key dimensions to illustrate that adequate widths and depths are provided for any planter boxes / garden beds
 - c. A plant schedule based on the submitted planting plans which details specific plant species, plant sourcing, the number of plants, height and/or grade (litre) / Pb size at time of planting, and estimated height / canopy spread at maturity
 - d. Details of draft specification documentation for any specific drainage, soil preparation, tree pits, staking, irrigation and mulching requirements
 - e. An annotated pavement plan and related specifications, detailing proposed site levels and the materiality and colour of all proposed hard surfacing
 - f. An annotated plan and related specifications which confirm the location and type of any seats, bins, lights, fences, walls and other structural landscape design elements
 - g. A landscape maintenance plan (report) and related drawings and specifications for all aspects of the finalised landscape design, including in relation to the following requirements:
 - i. Irrigation
 - ii. Weed and pest control
 - iii. Plant replacement
 - iv. Inspection timeframes
 - v. Contractor responsibilities

The finalised landscape design shall be consistent with the landscape design intent / objectives identified in the conceptual plans and information referenced at condition 1 and confirm responsibilities for ongoing maintenance requirements.

Implementation and maintenance of approved landscape design

8. Prior to the occupation of the apartment buildings or within the first planting season following occupation, the consent holder shall implement the landscape design which has been approved by the council under condition 6 and thereafter retain and maintain this landscape (planting and pavement) in perpetuity to the satisfaction of the Team Leader Monitoring (South) in accordance with the maintenance plan which has been approved under condition 7.
9. All planting located within the front yard of this development (excluding specimen trees) should be maintained to a maximum height of 1.5m in order to ensure a high degree of activation and surveillance is maintained over the public realm.

Advice Note:

It is recommended that the consent holder consider a minimum three-year management / maintenance programme for plant establishment and provide, in particular, details of maintenance methodology and frequency, allowance for fertilising, weed removal / spraying, replacement of plants, including specimen trees in case plants are severely damaged / die over the first five years of the planting being established and watering to maintain soil moisture. As part of the approval process, the council's monitoring team will liaise with landscape architects from the council's Auckland Design Office to ensure that the submitted drawings and related information are consistent with the originally consented landscape concept plan(s).

Lighting plans

10. Prior to the lodgement of Building Consent, the consent holder shall provide a Lighting Plan and Certification/ Specifications prepared by a qualified Lighting Engineer, to Team Leader Monitoring (South). The purpose of this condition is to provide adequate lighting for the safety of people residing, working or visiting the premises and its immediate environs outside of daylight hours. The Lighting Plan shall:
 - a. include all accessible areas of the premises where movement of people are expected. Such locations include, but are not limited to the shared accessways, building entrances, communal carpark and footpaths.
 - b. include proposed locations, lux levels and types of lighting (i.e. manufacturer's specifications once a lighting style has been determined) and any light support structures required to control timing, level of lighting, or to minimise light spill, glare, and loss of night time viewing.
 - c. Demonstrate compliance with the relevant standards in E24.6.1 Lighting of the Auckland Unitary Plan (Operative in Part).

- d. Demonstrate compliance with the AS/NZS 1158 P requirements and clearly specify what P Category the lighting design will achieve. The selection criteria for the chosen lighting category should also be presented (i.e. pedestrian/cycle activity, risk of crime etc.)
- e. Demonstrate the vertical illuminance by means of lux contours or a similar method to assess light spill on neighbouring properties (where relevant). The limits of the vertical illuminance should comply with Auckland Unitary Plan (Operative in Part) Standard E24.6.1.3.
- f. Include an executive summary (i.e. non-technical summary) of the above information that outlines the relevant requirements to their application and their design response to them.

The finalised design details certified by the qualified Lighting Engineer shall be established prior to the development hereby consented being first occupied, and thereafter retained and maintained, to the satisfaction of the Team Leader Monitoring (South).

Advice Note:

The purpose of this condition is to ensure that adequate lighting is provided to frequently used areas within the proposed development for the safety of users. Adequate lighting is the amount of lighting at eye level for a person with average eyesight so they can identify any potential threat approaching them from at least a 15-metre distance. Council's monitoring officers will liaise with members of the Council's Auckland Design Office to ensure that the submitted details are consistent with the above condition.

Contamination

11. Earthworks shall be undertaken in accordance with *Remedial Action Plan/Site Management Plan* – 164a, 164B and 166 Porchester Road, Papakura, prepared by Fraser Thomas Limited, dated 1 October 2019 ('RAP/SMP'). Any variations to the RAP/SMP shall be submitted to the Team Leader Monitoring (South) for approval.

Advice Note:

The Council acknowledges that the Remedial Action Plan/ Site Management Plan is intended to provide flexibility of the management of the works. Accordingly, the plan may need to be updated. Any updates should be limited to the scope of this consent and be consistent with the conditions of this consent. If you would like to confirm that any proposed updates are within scope, please contact the Team Leader Monitoring (South).

12. During earthworks all necessary action shall be taken to prevent dust generation and sufficient water shall be available to dampen exposed soil, and/or other dust suppressing measures shall be available to avoid dust formation. The consent holder shall ensure that dust management during the excavation works generally complies with the *Good Practice Guide for Assessing and Managing Dust* (Ministry for the Environment, 2016).

13. In the event of the unexpected discovery of contamination during earthworks which has not been previously identified, the consent holder shall immediately cease the works in the vicinity of the contamination, notify the Team Leader Monitoring (South), and engage a Suitably Qualified and Experienced Practitioner (SQEP) to assess the situation (including possible sampling and testing) and decide in conjunction with a council compliance officer on the best option for managing the material.
14. Any excavated material that is not re-used on site shall be disposed of at an appropriate facility licensed to accept the levels of contamination identified.
15. The consent holder shall ensure that the imported soil complies with condition 30 of this consent.
16. Within three (3) months of the completion of earthworks on the site, a Site Validation Report (SVR) shall be provided to the Team Leader Monitoring (South). The SVR shall be prepared by a Suitably Qualified and Experienced Professional (SQEP) in accordance with the Contaminated Land Management Guidelines No. 1: Reporting on Contaminated Sites in New Zealand (Ministry for the Environment, 2011) and contain sufficient detail to address the following matters:
 - a. A summary of the works undertaken, including the location and dimensions of the excavations carried out and the volume of soil excavated;
 - b. Details and results of any testing, including validation testing, undertaken and interpretation of the results in the context of the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health and the AUP(OP);
 - c. Copies of the disposal dockets for any material removed from the site;
 - d. Records of any unexpected contamination encountered during the works and response actions, if applicable;
 - e. Conditions of the final site ground surface and details of any validation sampling undertaken on materials re-used on site or imported to site; and
 - f. A statement certifying that all works have been carried out in accordance with the requirements of the consent.

Advice Notes:

If you are demolishing any building or structure that may have asbestos containing materials (ACM) in it:

- *You have obligations under the relevant regulations for the management and removal of asbestos, including the need to engage a Competent Asbestos Surveyor to confirm the presence or absence of any ACM.*
- *Work may have to be carried out under the control of person holding a WorkSafe NZ Certificate of Competence (CoC) for restricted works.*
- *If any ACM is found, removal or demolition will have to meet the Health and Safety at Work (Asbestos) Regulations 2016.*

Information on asbestos containing materials and your obligations can be found at www.worksafe.govt.nz.

If ACM is found on site following the demolition or removal of the existing buildings, you may be required to further remediate the site and carry out validation sampling. Dependent on the amount of soil disturbance, a further consent application may be required.

Geotechnical

17. All earthworks on the site are to be carried out in accordance with the requirements of the Geotechnical Investigation report titled 'Geotechnical Investigation for Proposed Three Storey Apartment Development at 164 to 166 Porchester Road, Papakura' ref J01138- rev A, dated 16/04/2020.
18. An Engineer's certificate and Geotechnical Completion Report (GCR) prepared by an appropriately qualified engineering professional responsible for supervising the works must be provided for written certification by the Team Leader Regulatory Engineering South, confirming that the works have been completed in accordance with the approved plans prior to the occupation of the apartment buildings. The GCR is to cover the following (as a minimum):
 - a. That the works were undertaken in accordance with NZS4431:1989 Code of Practice for Earth Fill for Residential Subdivisions and the site-specific designs outlined in the Geotechnical Investigation Report; and
 - b. Recommendations, confirming adequate factors of safety, and including as-built records of earthworks, groundwater levels and drainage; and
 - c. Confirmation that settlement criteria and/or ground improvement as defined in the Geotechnical Investigation Report and Detail Geotechnical Design Report have been met prior to commencement of house construction; and
 - d. The extent to which settlement of the site is expected and its impact on future house/structure construction; and

- e. Include a statement of professional opinion for the suitability of the site for residential development; and
- f. Details of all earthworks and as-built plans including, the depth, extent of fill and drainage, subsoil drains, shear keys and soil reinforcement. As-built plans should be certified by a licensed Cadastral Surveyor; and
- g. Specific requirements for future earthworks, building foundations, retaining walls and other works which may occur in each lot. This shall include requirements for works which may take place within the vicinity of subsoil drains; and
- h. The ongoing maintenance requirements of landowners to ensure efficient functioning of the privately-owned subsoil drains and subsoil drain outlets. This shall include a requirement to provide CCTV monitoring of subsoil drain outlets to Council; and
- i. If subsoil drains are installed to a 'zero maintenance' standard, the Completion Report shall include requirements to avoid damage to the subsoil drains including the extent to which modifications can be made to the capping (covering) at ground level; and
- j. Any related matters identified in other conditions of this consent.

Advice Note:

Auckland Council may appoint an independent geotechnical engineer to observe critical aspects of the construction works and/ or peer review the GCR, at the Consent Holder's expense. Critical observations are to be agreed between the independent geotechnical engineer and the Supervising Engineer prior to construction commencing.

- 19. Prior the commencement of the construction of the apartment buildings on site, a detailed Geotechnical Design Report including specific foundation design shall be prepared and submitted to the Team Leader Monitoring (South) for written certification.
- 20. Detailed assessment of the expansive site class by specific laboratory tests in accordance with published information (AS2870:2011 or BRANZ 120A) shall be undertaken as a part of Geotechnical Completion Report or Geotechnical Design Report.

Advice Note:

The approved Detail Geotechnical Design Report shall be included as part of the building consent support documents.

Supervision and certification of geotechnical works

- 21. The construction of permanent building platform, settlement mitigation measures, building foundations, pavements and floor slabs and the placement and compaction of fill material shall be supervised by a suitably qualified engineering professional. In supervising the works, the suitably qualified engineering professional shall ensure that they are constructed and

otherwise completed in accordance with the engineering plans and geotechnical recommendations, relevant engineering codes of practice and detailed plans forming part of the application.

22. Certification from a suitably qualified engineering professional responsible for supervising the works shall be provided to the Team Leader Monitoring (South), confirming that the works have been completed in accordance with Geotechnical Investigation report and Detail Geotechnical Design report, within ten (10) working days following completion. Written certification shall be in the form of a geotechnical completion report, or any other form acceptable to the council.

Groundwater

23. An on-site monitoring system is to be established by the consent holder to:
 - a. determine if the groundwater levels post-construction are similar to the pre-construction levels.
 - b. demonstrate that groundwater equilibrium has been maintained such that no detrimental effect to soil structures/ strength or stability has occurred.
 - c. the required groundwater monitoring systems, and devices are to be established across the site prior occupation of the apartment buildings.
 - d. an on-site groundwater monitoring plan and methodology is to be submitted to the Council for approval prior to commencement of the works and prior to occupation of the apartment buildings.

The monitoring is to be carried out on a regular basis throughout the development phases starting a minimum of two months prior to commencement of works on the site, through the development construction phases, post-construction and continuing for sufficient time to confirm that the natural ground water levels have stabilised. The monitoring plan must take account of seasonal changes so that levels are compared over the same calendar period to avoid natural variations influencing the interpretation of the results.

24. Geotechnical/geo-hydrologist certification is to be provided to the Team Leader Monitoring (South) prior to occupation of the apartment buildings. This must include confirmation that:
 - a. the groundwater levels have been maintained or re-established; and
 - b. there were no significant groundwater fluctuations that may have resulted in irreversible shrinkage of the organic component in the peaty soils, and that no detrimental effect to soil structure/strength or stability has occurred and will not occur in the future.

Ground settlement

25. Prior to placement of any fill on-site the consent holder shall:

- a. Submit to Council for written certification a settlement monitoring and contingency plan which shall detail settlement monitoring to be undertaken during and following construction and measures to be taken should settlements exceed expected values.
 - b. Install specific settlement monitoring points and, if applicable, undertake structural survey of surrounding buildings to ensure settlement is as anticipated and there are no detrimental effects on neighbouring land & buildings
26. Settlement shall be monitored at the site for a period commencing from the initial baseline measurements prior to construction, until the time when all the specific design criteria has been met as per the Geotechnical Consultant's report and subject to Council's approval.
27. Geotechnical certification that the settlement criteria have been met is to be provided to Council prior submitting the Geotechnical Completion Report.
28. No building works are to commence until the above requirements of conditions 24, 25 and 26 have been certified by Auckland Council's Geotechnical Engineer.

Ongoing monitoring

29. After the CCC has been granted for the two apartment blocks, a post housing construction geotechnical groundwater and settlement monitoring plan is to be prepared and submitted to Team Leader Regulatory Engineering South for approval. The post housing construction on-site monitoring system is to be established to:
- a. provide evidence that ground settlement has attenuated; and
 - b. demonstrate that groundwater level fluctuations resulting from works carried out for the development have been less than significant and are such that no detrimental effect to soil structures/ strength or stability has occurred.
 - c. All monitoring sites are to be identified on a plan clearly showing their location and set out positions in relation to property boundaries for ease of locating in the future. Attached to the plan is to be a copy of the monitoring device design, an as-built detail drawing and the maintenance plan for each device. Each of the monitoring sites is to be provided with adequate protection to ensure they are not damaged during construction works and/or over their intended life span.
 - d. If the monitoring site is on private property a consent notice is to be registered on the Certificate(s) of Title detailing the location, the need for protection of the systems and the need for ongoing monitoring by the consent holder.
 - e. Each monitoring site is to be monitored and readings recorded at regular intervals throughout the development period and for a minimum five-year period commencing from the date CCC being issued. The records are to be provided to the Council to the satisfaction of Team Leader Regulatory Engineering South on completion of each year (or if on request then monthly). If at any stage the results show any indication of settlement or the possibility of detrimental changes in water levels the consent holder must immediately advise the Council, the geotechnical engineer and the consent holder's engineer.

- f. If after a three-year monitoring period it is demonstrated that no further monitoring is required, to the satisfaction of the Council's Manager Regulatory Engineering South monitoring activities may cease.
30. The Council may appoint an independent geotechnical engineer to observe critical aspects of the construction works (e.g. geotechnical drainage installations, shear key excavations, retaining walls, capping requirements for rock if encountered in deep cuts, etc.), and/ or peer review the Geotechnical Completion Report, at the Consent Holder's expense. Critical observations are to be agreed between the independent geotechnical engineer and the Supervising Engineer prior to construction commencing.

Quality of fill

31. All imported fill used shall:
- a. comply with the definition for 'cleanfill' in the Ministry for the Environment publication 'A Guide to the Management of Cleanfills' (2002)
 - b. be solid material of a stable, inert nature; and
 - c. not contain hazardous substances or contaminants above recorded natural background levels of the receiving site.

General sediment control

32. All earthworks shall be managed to minimise any discharge of debris, soil, silt, sediment or sediment-laden water beyond the subject site to either land, stormwater drainage systems, watercourses or receiving waters. In the event that a discharge occurs, works shall cease immediately, and the discharge shall be mitigated and/or rectified to the satisfaction of the Team Leader Monitoring (South).

Advice Note:

In accordance with condition 32 all earthworks shall be undertaken to ensure that all potential sediment discharges are appropriately managed. Such means and measures may include:

- *Catchpit protection*
- *run-off diversions*
- *sediment retention ponds*
- *silt and sediment traps*
- *decanting earth bunds*
- *silt fences*

During excavation, the ingress and accumulation of surface run-off water and/or perched groundwater can be minimised by:

- *maintaining a waterproof cover over any excavation trenches and pits outside of working hours,*
- *diversion of surface water flow around the works area, and*
- *regular disposal of the water into an appropriate sediment control device, if ponding occurs within the excavation.*

Please note that the diversion of stormwater and/or groundwater may require a consent in accordance with Chapters 5 and 6 of the Auckland Council Regional Plan (Air, Land and Water) or the Proposed Auckland Unitary Plan.

It is recommended that you discuss any potential measures with the council's monitoring officer who may be able to provide further guidance on the most appropriate approach to take. Please contact the Council's Team Leader Monitoring (South) on monitoring@aucklandcouncil.govt.nz for more details. Alternatively, please refer to "Auckland Regional Council, Technical Publication No. 90, Erosion & Sediment Control Guidelines for Land Disturbing Activities in the Auckland Region".

Construction noise and vibration management plan

33. At least five days prior to the commencement of construction and / or any earthworks activity, a Construction Noise and Vibration Management Plan ('CNVMP') shall be prepared by a suitably qualified person and submitted to the Team Leader Monitoring (South) for review and written certification. At a minimum, the CNVMP shall address the measures in Annex E of NZS 6803:1999 "Acoustics – Construction Noise". The approved CNVMP shall be implemented throughout the construction phase of the project.

The plan may be updated as necessary to the satisfaction of the Team Leader Monitoring (South). The objectives of the CNVMP shall be:

- a. Applicable site noise and vibration criteria.
- b. Programme of works and hours of operation.
- c. Identification of surrounding noise and/or vibration sensitive receivers.
- d. An assessment of vibration from the proposed construction activities.
- e. The proposed neighbour liaison approach, including communication with occupants of all buildings within 50 m of the site of the works in writing at least ten (10) days prior to the commencement of construction and / or any earthworks activity activities on site. The written advice shall set out:
 - (i) a brief overview of the construction works
 - (ii) the working hours and expected duration

- (iii) all mitigation measures to be implemented
 - (iv) the procedure for recording concerns/complaints regarding noise and vibration
 - (v) the procedure for noise and vibration monitoring where concerns are raised by receivers
 - (vi) contact details for site personnel for any concerns regarding noise and vibration
- f. Details of the management and mitigation measures required to comply with the relevant noise and vibration criteria.
 - g. A requirement to undertake pre- and post-building condition surveys of neighbouring buildings/structures with the potential to receive vibration levels in excess of the limits in AUP rule E25.6.30, assuming access is granted by the owner/occupier.
 - h. The requirement to measure construction vibration at the most exposed surrounding property(ies), as identified in Item (g). Where the vibration levels exceed or are likely to exceed the DIN 4150-3 guideline values then the construction activity shall only proceed if there is appropriate monitoring of vibration levels and effects on those buildings at risk of exceeding the DIN 4150-3 guideline values, by suitably qualified experts.

Minimum floor level

34. The approved minimum floor level of RL 21.1m shall apply to both apartment buildings unless a lower floor level is approved by the Auckland Council that has been subject to specific engineering design.

All levels are in terms of the Lands & Survey Auckland Datum, 1946 and in accordance with the approved plans referenced in condition 1.

Certification by Registered Professional Surveyor shall be submitted to the Team Leader Monitoring (South) for compliance of the above condition.

Engineering plans (major)

35. Prior to commencement of any public works related on site, the consent holder shall provide design plans and specifications detailing the following works required in respect to this Land use, to the satisfaction of the Team Leader Regulatory Engineering South.

The engineering plans submitted for approval shall detail all works associated with, and be in accordance with current Council Engineering Standards, including but not limited to;

- a. Roothing Works
- b. Stormwater Reticulation
- c. Wastewater Reticulation

Advice Notes:

The engineering plan application forms including fees can be found at the following Auckland Council website:

<https://www.aucklandcouncil.govt.nz/building-and-consents/engineering-approvals/Pages/default.aspx>

In the former Papakura District water and wastewater services are provided by Veolia under a Franchise Agreement. The consent holder deals directly with Veolia in relation to all water and wastewater servicing matters throughout the development process. Compliance with Veolia's water and wastewater requirements (as evidenced by the issuing by Veolia of a Compliance Certificate) is necessary prior to the occupation of the apartment buildings. (papakura.developments@veolia.com)

Public road

36. The consent holder shall apply for Engineering Plan Approval for relocating the traffic island 2m away from the current location to ensure that vehicle exiting the site will not encroach onto the existing traffic island. The works to relocate and complete the new traffic island shall be completed prior to occupation of the apartment buildings.

Advice Notes:

The line marking changes shall require resolution process approval.

In the case that the proposed modification of Auckland Transport's assets as part of the applicant's proposal require further resolutions, this will be completed by the applicant following Auckland Transport's Resolution & Approval Reports Guidebook (Transport Controls Team, June 2015).

37. The traffic Island works shall be constructed in accordance with the approved Engineering Plans to the satisfaction of the Team Leader Regulatory Engineering South prior to occupation of the apartment buildings.
38. An Engineering Completion Certificate certifying that all the ancillary structures on the roads have been constructed in accordance with the approved Engineering Plans prior to occupation of the apartment buildings.

Vehicle crossings and driveways

39. The driveway shall be formed, paved and drained to the Council's current Local Engineering Standards, including the provision of stormwater catch pits and/or slot drains, within the boundaries of the common area and when necessary the provision of kerbing or other mechanism to prevent water flowing on to other property including footpaths.

40. Certification shall be provided by a Chartered Professional Engineer confirming the construction and stormwater runoff from the driveway is in accordance with the Council's current Local Engineering Standards prior to occupation of the apartment buildings.
41. The vehicle crossing shall be constructed in accordance with Auckland Transport Code of Practice Standards GD017A-Residential Vehicle Crossing for more than 4 dwellings. The berm shall be re-instated to Council's "Code Of Practice For Working In The Road." (<https://at.govt.nz/about-us/working-on-the-road/>).

Advice Note:

A vehicle crossing permit is required to be obtained from Auckland Transport prior to the construction of the vehicle crossing on existing public roads. See Auckland Transport's website <https://at.govt.nz/about-us/working-on-the-road/vehicle-crossing-application/> for more information.

Private stormwater system

42. The stormwater detention system shall have a minimum of 136m³ storage capacity to mitigate the stormwater run-off generated from all impervious area prior discharge to the recharge pits on site. Detail design of the tank and calculations shall be provided at building consent stage.

Advice Notes:

Due to high ground water in the Peat soil during winter, the rainsmart module shall be designed to prevent ground water seepage which will reduce the detention capacity of this detention system. Buoyancy also a major consideration for this design

Recharge pits

43. A residential recharge design and pit maintenance guide is to be created by the consent holder. This guide is to also include standard detail drawings for recharge pit with recommendations in terms of impervious area discharge before overflow to the public stormwater reticulation system.

A producer statement 1 (PS1) and geotechnical recommendation memo are to be included as part of the building consent application due to specific design of recharge pit.

44. This condition shall be complied with to the satisfaction of the Team Leader Monitoring (South) prior to occupation of the apartment buildings.

Water and wastewater certification

45. A Completion Certificate certifying that all public water and wastewater and individual including connections have been constructed in accordance with the approved Engineering Plan and shall be provided to Team Leader Monitoring (South) prior to occupation of the apartment buildings.

Advice Note:

In the former Papakura District, water and wastewater services are provided by Veolia under a Franchise Agreement. The consent holder shall deal directly with Veolia for approval in relation to all water and wastewater servicing matters throughout the development. Compliance certification shall be obtained from Veolia prior to connection to any Veolia network. (papakura.developments@veolia.com)

Wastewater and water supply capacity

46. The consent holder shall demonstrate that there is adequate wastewater and water supply capacity to services the proposal development at Engineering Plan approval state to the satisfactory of the Team Leader Regulatory Engineering South.

Any required upgrade for the existing public wastewater and water supply to services this proposed development shall be fully funded by the consent holder.

Electricity supply

47. The consent holder shall provide and install an underground electricity supply system to service the apartment buildings as shown on the approved plans. The system shall be installed in accordance with the requirements of relevant network utility operator. The consent holder shall provide certification from the network utility operator to Team Leader Monitoring (South), that the system has been installed in accordance with their requirements prior to occupation of the apartment buildings.

Telecommunications services

48. The consent holder shall provide and install an underground telecommunications system to service the apartment buildings generally as shown on the approved plans. The system shall be installed in accordance with the relevant network utility operator. The consent holder shall provide certification from the network utility operator to Team Leader Monitoring (South), that the system has been installed in accordance with their requirements prior to occupation of the apartment buildings.

Advice notes

1. Any reference to number of days within this decision refers to working days as defined in s2 of the RMA.
2. For the purpose of compliance with the conditions of consent, “the council” refers to the council’s monitoring inspector unless otherwise specified. Please email monitoring@aucklandcouncil.govt.nz to identify your allocated officer.
3. For more information on the resource consent process with Auckland Council see the council’s website: www.aucklandcouncil.govt.nz. General information on resource consents, including making an application to vary or cancel consent conditions can be found on the Ministry for the Environment’s website: www.mfe.govt.nz.

4. *If you disagree with any of the above conditions, and/or disagree with the additional charges relating to the processing of the application(s), you have a right of objection pursuant to sections 357A and/or 357B of the Resource Management Act 1991. Any objection must be made in writing to the council within 15 working days of your receipt of this decision (for s357A) or receipt of the council invoice (for s357B).*
5. *The consent holder is responsible for obtaining all other necessary consents, permits, and licences, including those under the Building Act 2004, and the Heritage New Zealand Pouhere Taonga Act 2014. This consent does not remove the need to comply with all other applicable Acts (including the Property Law Act 2007 and the Health and Safety at Work Act 2015), regulations, relevant Bylaws, and rules of law. This consent does not constitute building consent approval. Please check whether a building consent is required under the Building Act 2004.*
6. *This consent approval does not authorise the construction of the necessary engineering works shown on the plans. A separate Engineering Plan Approval is required to undertake any engineering works related to the proposal.*
7. *A Corridor Access Request, (CAR), is required for all works undertaken within the 'road corridor'. See Auckland Transport's website <https://at.govt.nz/about-us/working-on-the-road/corridor-access-requests/#applycar> for more information.*

Delegated decision maker:

Name: Colin Hopkins
Title: Principal Project Lead
Premium, Resource Consents

Signed:



Date: 06 July 2020

BUN60343865

Approved Resource Consent Plan

06/07/2020



DRAWLING LIST	
Sheet Number	Sheet Name
A000	COVER SHEET
A100	SITE & GROUND FLOOR PLAN
A101	FIRST FLOOR
A102	SECOND FLOOR
A103	THIRD FLOOR
A104	ELEVATIONS
A105	ELEVATIONS
A106	OUTLOOK SPACE
A107	LETTERBOXES
A200	3D VIEWS
A201	3D VIEWS
A202	3D VIEWS



LOCATION PLAN

PORCHESTER ROAD

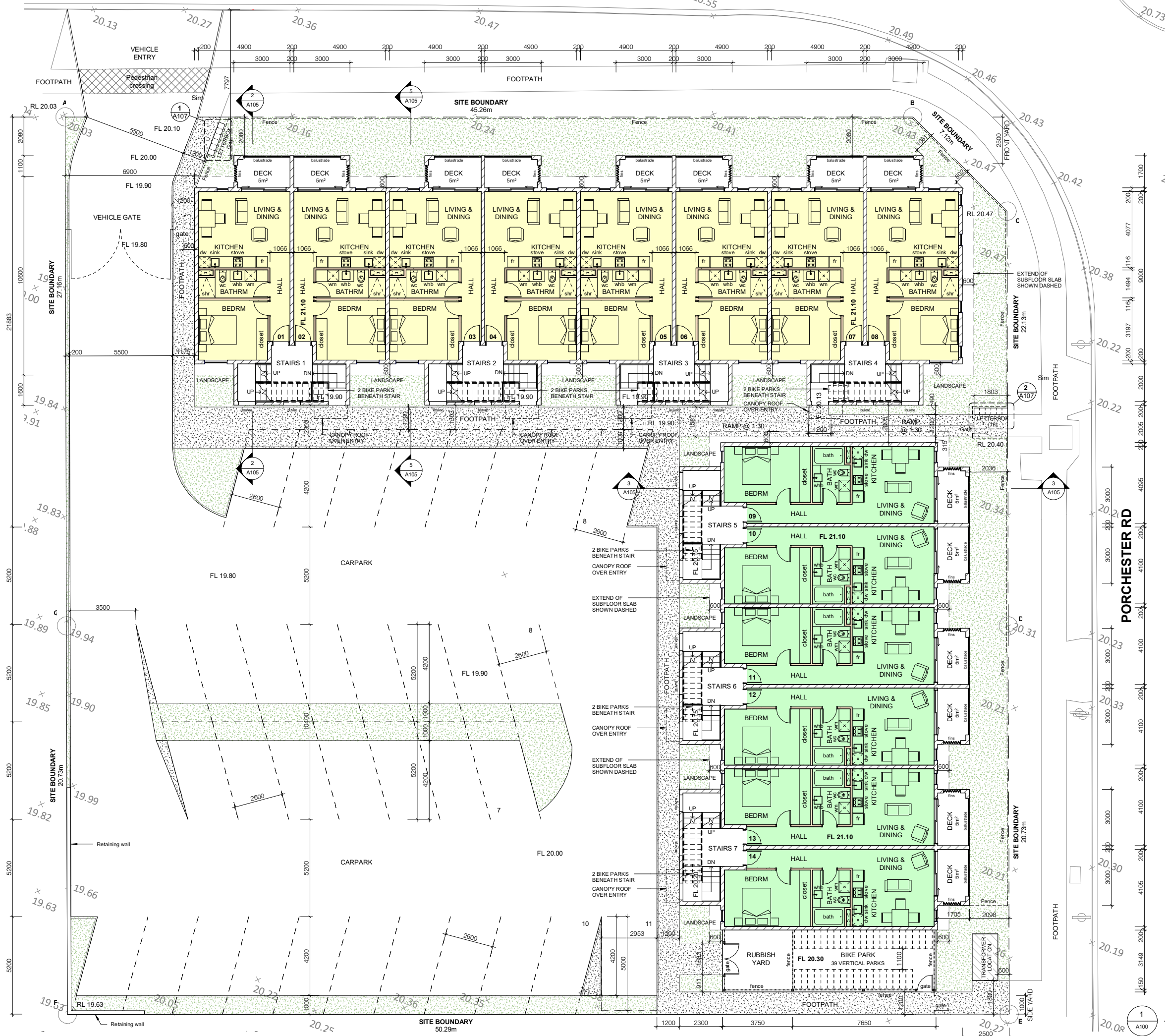
**164-166 PORCHESTER RD
TAKANINI**

**ALDA INVESTMENTS
LTD**



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delivered.

RCG PROJECT NO: 19001
ISSUE DATE: JUL 2019
ISSUED FOR: RESOURCE CONSENT



KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53

CONSULTANTS REFERENCE NOTE		
Refer to specific consultant documentation for relevant information on:		
<ul style="list-style-type: none"> Civil Engineering Structural Engineering Plumbing & Drainage Fire Landscape Traffic 		

LETTERBOX NOTES		
<ul style="list-style-type: none"> Allow for two Letterbox units, one per Building Block: North Block with 24 units, West Block with 18 units. Letterbox numbers to correspond to unit numbers. Allow for 190(h)x250(w)x400(d) Contemporary Apartment Box Mailboxes to be built into block wall. 		

NOTES:

All work shown or implied to be carried out in strict accordance with NZS3604, the NZ Building Code and Local Body requirements.

Refer to consultant drawings for specific details on systems flashing, other such services where not specified in these documents.

All drawings to be read in conjunction with the architectural specification and structural engineers drawings and specification.

Refer to Fire Safety Design Report for fire safety requirements and precautions.

The plumbing and drainage installation must comply with AS/NZ 3500.2003, the NZ Building Code and requirements of the local Territorial Authority.

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NOT FOR CONSTRUCTION

Revisions		
Ref	Description	Date
D	FOR CONSENT	03.07.19
E	FOR CONSENT	08.07.19
F	FOR CONSENT	19.07.19
G	RESOURCE CONSENT	23.07.19

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PO Box 137313, Auckland 1151, New Zealand
T: +64 9 303 1501 W: rcg.co.nz

PORCHESTER ROAD

ADDRESS:
**164-166 PORCHESTER RD
TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

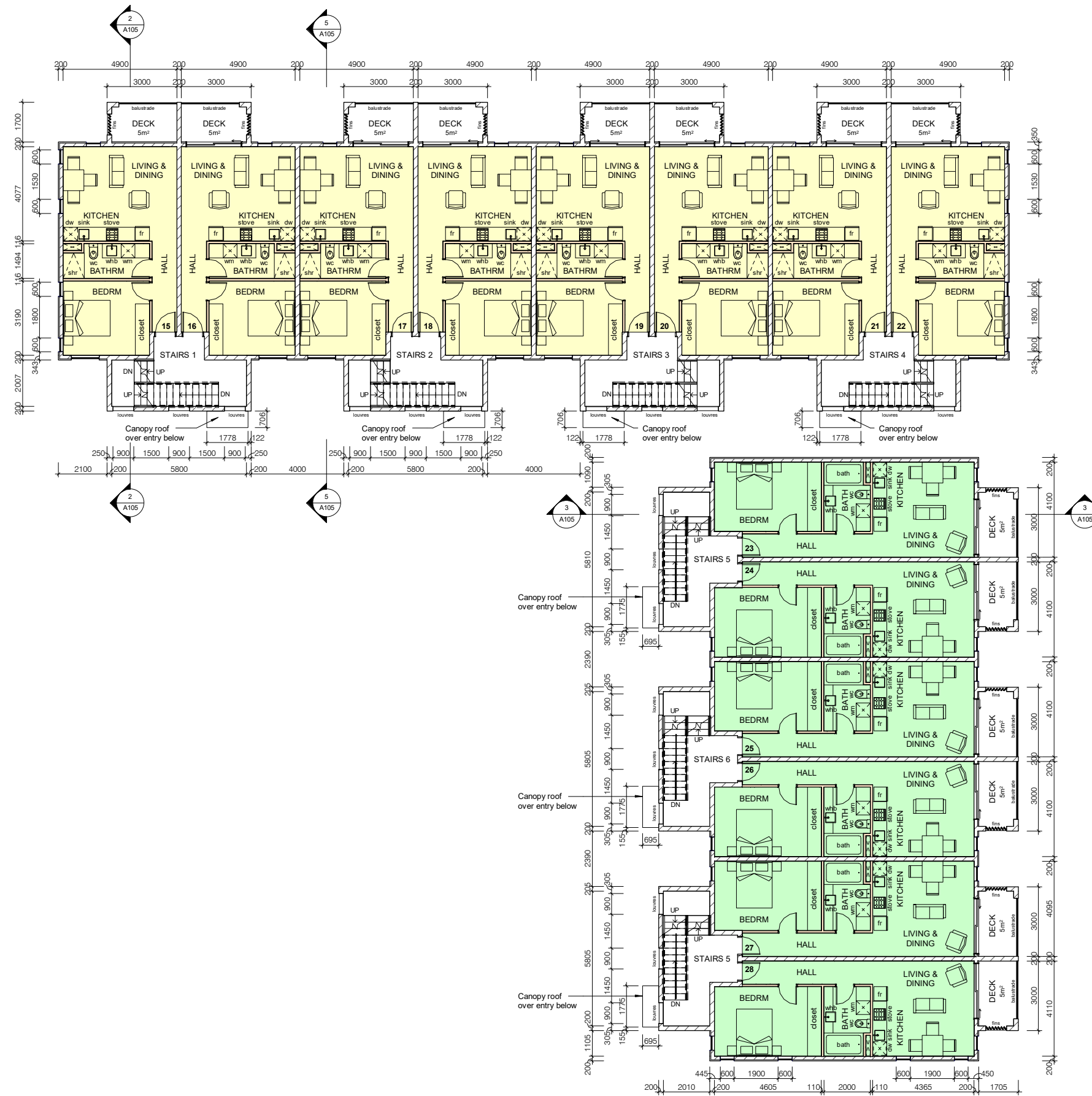
SHEET NAME:
**SITE & GROUND
FLOOR PLAN**

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A100** REV: **G**

SITE & GROUND FLOOR
1 : 100

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1 FIRST FLOOR
A101 1:100

KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedm.	43sqm	16
North Block - 2 Bedm.	58sqm	8
West Block - 1 Bedm.	44sqm	13
West Block - 2 Bedm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53

AREAS		
Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%

CONSULTANTS REFERENCE NOTE

Refer to specific consultant documentation for relevant information on:

- Civil Engineering
- Structural Engineering
- Plumbing & Drainage
- Fire
- Landscape
- Traffic

NOTES:

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Revisions		
Ref	Description	Date
A	UPDATED DRAWINGS	29.04.19
B	FOR CONSENT	17.06.19
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19



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PROJECT:

PORCHESTER ROAD

ADDRESS:
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TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

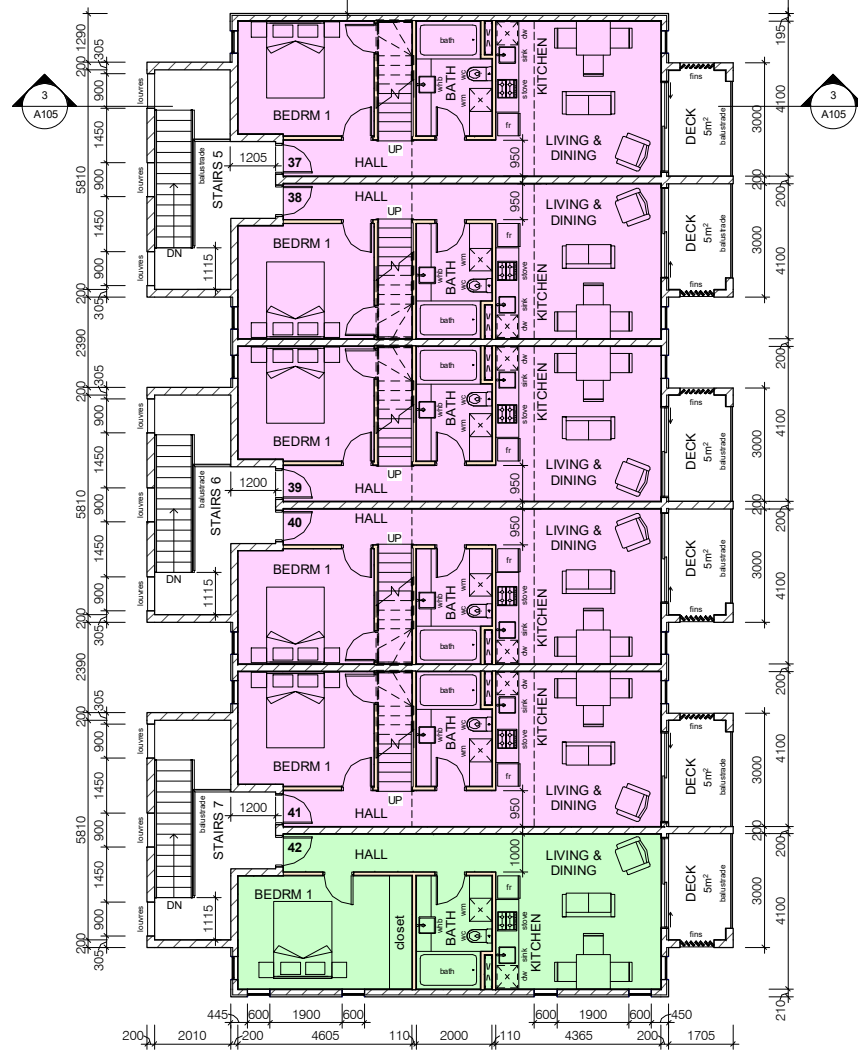
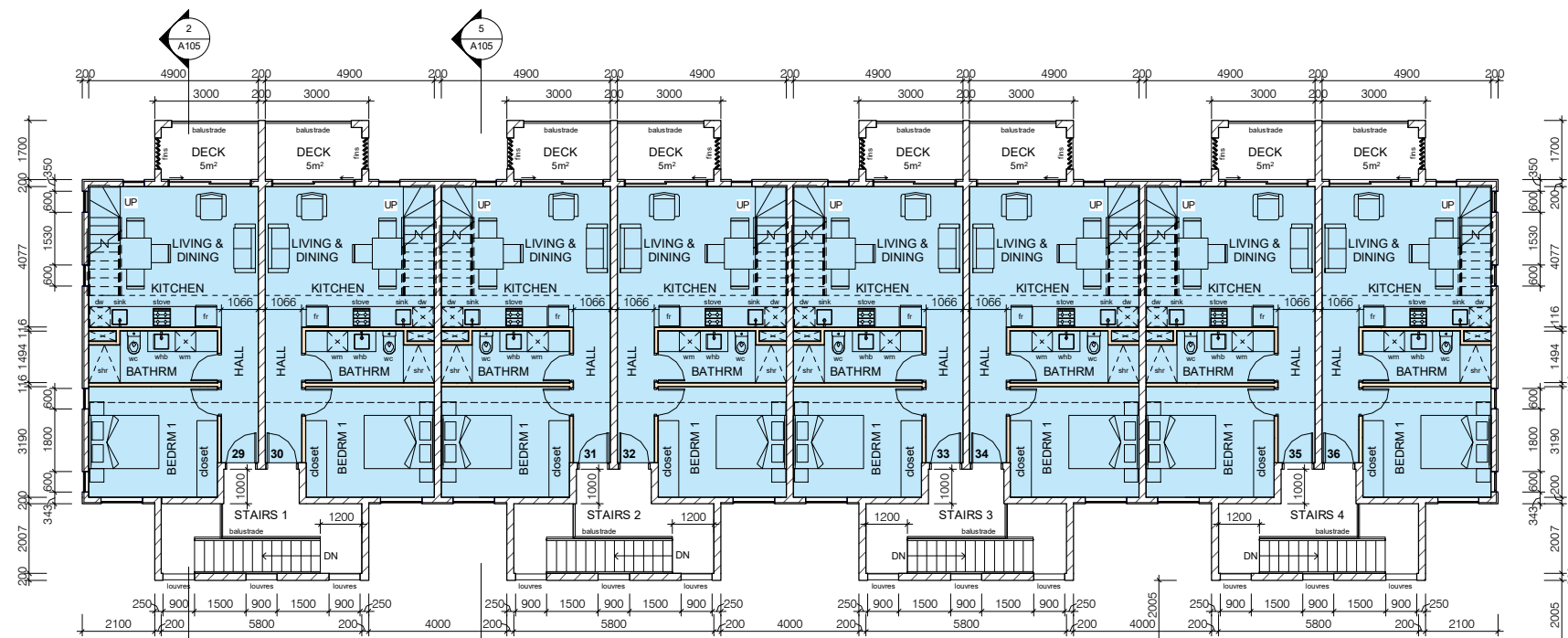
ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
FIRST FLOOR

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As indicated@A1

SHEET NO: **A101** REV: **D**

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1
A102
SECOND FLOOR
1 : 100

KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
North Block - 2 Bedrm.	58sqm	8
West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
Total:	2,004sqm	42
Total Carparks:		34
Total Bike Parks:		53

CONSULTANTS REFERENCE NOTE		
Refer to specific consultant documentation for relevant information on:		
<ul style="list-style-type: none"> • Civil Engineering • Structural Engineering • Plumbing & Drainage • Fire • Landscape • Traffic 		

NOTES:

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A	UPDATED DRAWINGS	29.04.19
B	FOR CONSENT	17.06.19
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D	RESOURCE CONSENT	23.07.19



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PROJECT:

PORCHESTER ROAD

ADDRESS:
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CLIENT:
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NO: **19001** ISSUE DATE: **JUL 2019**

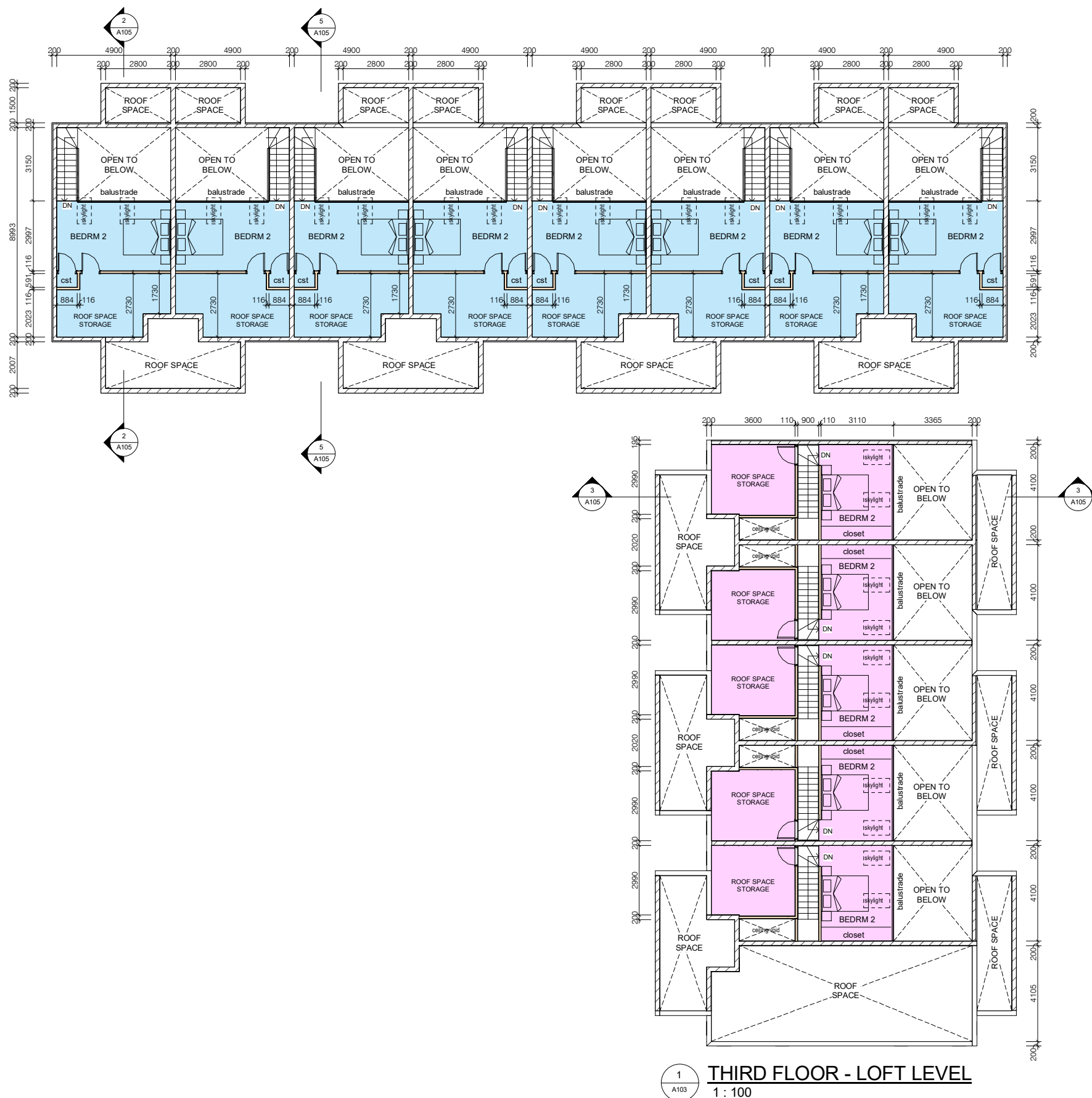
ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
SECOND FLOOR

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: As Indicated@A1

SHEET NO: **A102** REV: **D**

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1 THIRD FLOOR - LOFT LEVEL
1 : 100

KEY LEGEND		
Description:	Internal Area:	Qty:
North Block - 1 Bedrm.	43sqm	16
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West Block - 1 Bedrm.	44sqm	13
West Block - 2 Bedrm.	56sqm	5
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Total Bike Parks:		53
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Site Area:	2,395sqm	
Building Area:	894sqm	37%
Carpark Area:	937sqm	39%
Landscape Area:	520sqm	22%
CONSULTANTS REFERENCE NOTE		
Refer to specific consultant documentation for relevant information on:		
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B	FOR CONSENT	17.06.19
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19



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PROJECT:

PORCHESTER ROAD

ADDRESS:

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TAKANINI**

CLIENT:

ALDA INVESTMENTS LTD

NO: 19001 ISSUE DATE: JUL 2019

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

THIRD FLOOR

DESIGN **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: As Indicated@A1

SHEET NO: REV:

A103 D

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1 NORTH ELEVATION - WALTERS ST
1 : 100



2 EAST ELEVATION - PORCHESTER RD
1 : 100



3 NORTH BLOCK - SOUTH ELEVATION
1 : 100

- FINISHES: EXTERIOR**
- FLOORS:**
1.1 Timber floor construction.
- WALLS:**
2.1 Timber frame wall with N2 Brick Designer series cladding, colour: dark grey.
2.2 Timber frame wall with Metal Cladding, colour: Ironsand.
2.3 20-series concrete block with Cemintel.
2.4 Aluminium balustrade.
2.5 Timber frame wall with N2 Brick Designer series cladding, colour: light grey.
- ROOFS:**
3.1 Metal profile roofing, colour: Ironsand.
- WINDOWS / DOORS:**
4.1 Aluminium frame doors & windows, colour: Ironsand.
4.2 Aluminium frame roof window, colour: Ironsand.
4.3 Frosted glass to bottom pane of all slot windows.
- FENCES & GATES:**
5.1 Steel fence & gate, paint finish.
- PAVEMENT:**
6.1 Exposed aggregate concrete paving.
- LOUVRES / FINIS:**
7.1 Timber horizontal louvres.
7.2 Timber vertical fins.

- CONSULTANTS REFERENCE NOTE**
- Refer to specific consultant documentation for relevant information on:
- Civil Engineering
 - Structural Engineering
 - Plumbing & Drainage
 - Fire
 - Landscape
 - Traffic

NOTES:

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All drawings to be read in conjunction with the architectural specification and structural engineers drawings and specification.

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Timber framing treatments to DBH, NZS3602/NZS3604 and NZBC.

Refer to consultant drawings for specific details on systems flashing and other such services where not specified in these documents.

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Revisions		
Ref	Description	Date
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19
E	RESOURCE CONSENT	08.11.19

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PORCHESTER ROAD

ADDRESS:
**164-166 PORCHESTER RD
TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
ELEVATIONS

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A104** REV: **E**

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FINISHES: EXTERIOR

- FLOORS:**
1.1 Timber floor construction.
- WALLS:**
2.1 Timber frame wall with N2 Brick Designer series cladding, colour: dark grey.
2.2 Timber frame wall with Metal Cladding, colour: Ironsand.
2.3 20-series concrete block with Cemintel.
2.4 Aluminium balustrade.
2.5 Timber frame wall with N2 Brick Designer series cladding, colour: light grey.
- ROOFS:**
3.1 Metal profile roofing, colour: Ironsand.
- WINDOWS / DOORS:**
4.1 Aluminium frame doors & windows, colour: Ironsand.
4.2 Aluminium frame roof window, colour: Ironsand.
4.3 Frosted glass to bottom pane of all slot windows.
- FENCES & GATES:**
5.1 Steel fence & gate, paint finish.
- PAVEMENT:**
6.1 Exposed aggregate concrete paving.
- LOUVRES / FINS:**
7.1 Timber horizontal louvres.
7.2 Timber vertical fins.

NOTES:

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Refer to consultant drawings for specific details on systems flashing and other such services where not specified in these documents.

DISCLAIMER:

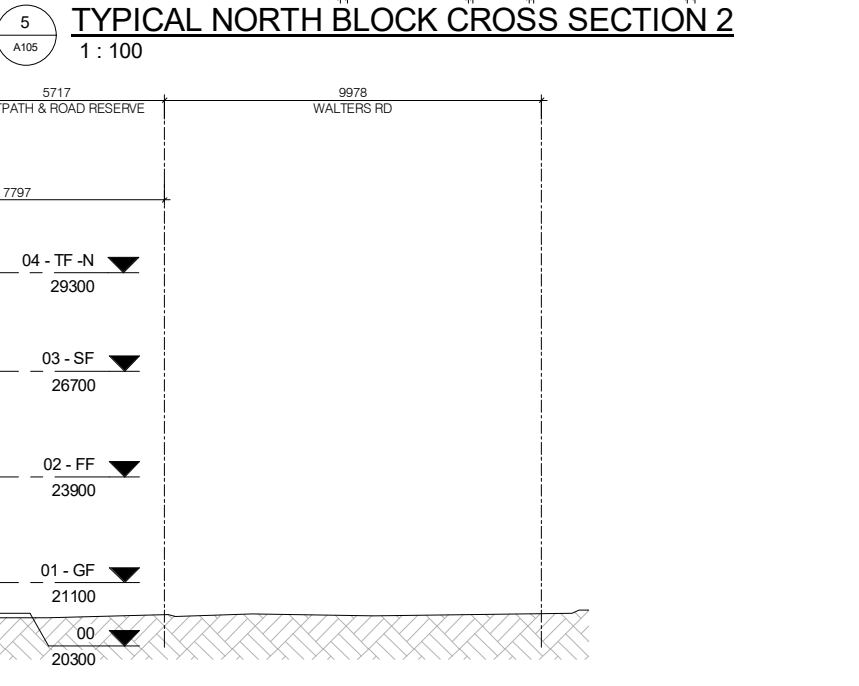
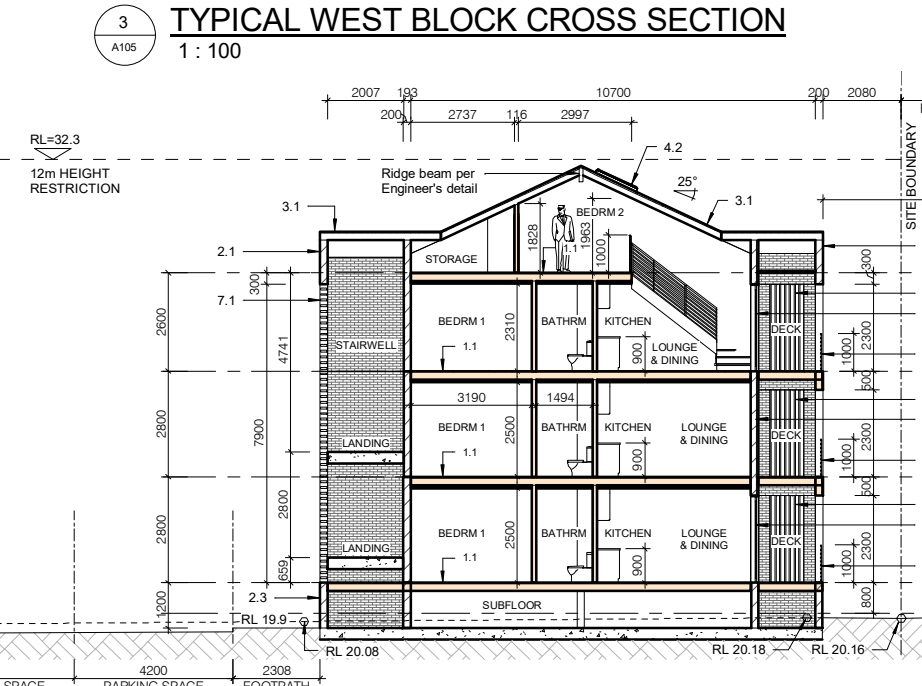
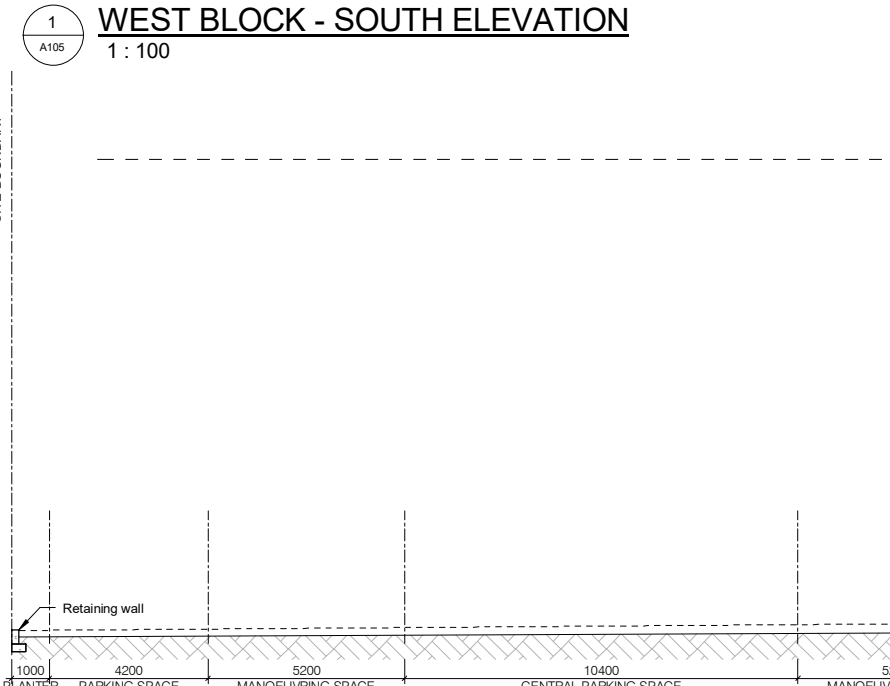
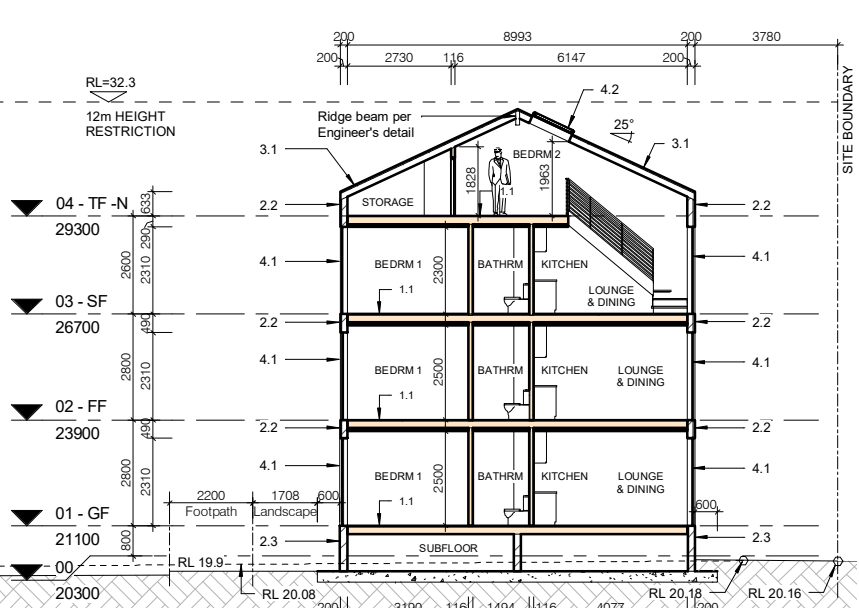
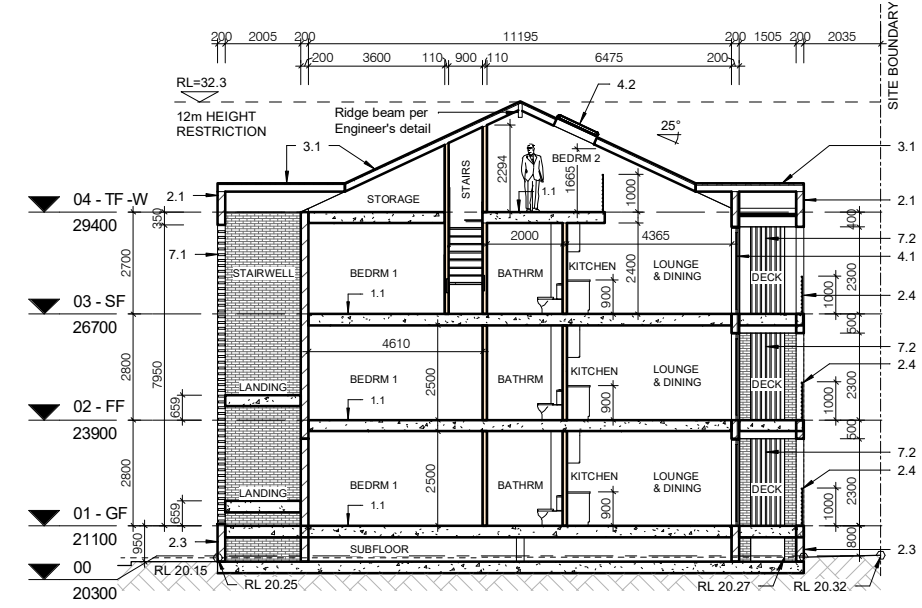
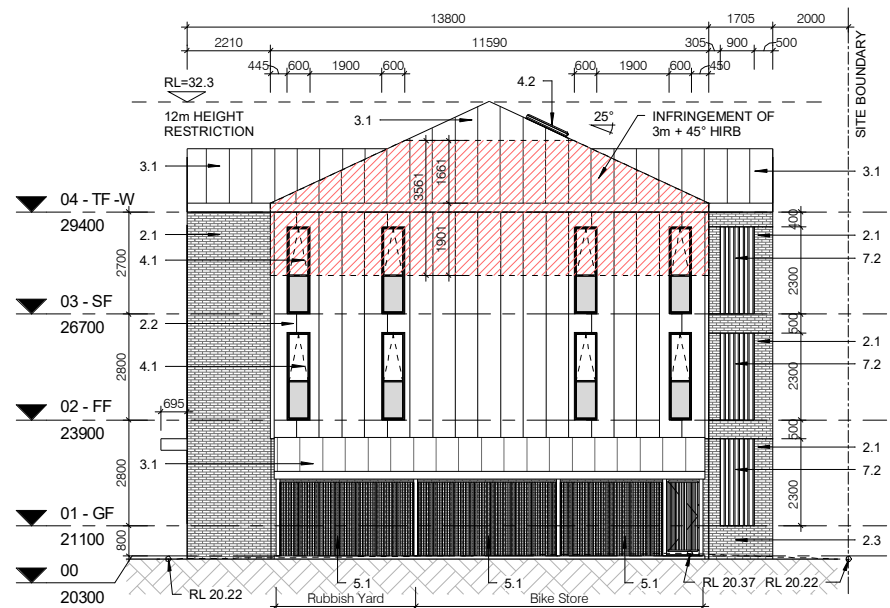
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CONSULTANTS REFERENCE NOTE

- Refer to specific consultant documentation for relevant information on:
- Civil Engineering
 - Structural Engineering
 - Plumbing & Drainage
 - Fire
 - Landscape
 - Traffic

Revisions		
Ref	Description	Date
C	FOR CONSENT	08.07.19
D	RESOURCE CONSENT	23.07.19
E	RESOURCE CONSENT	08.11.19



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PO Box 137313, Auckland 1151, New Zealand
T: +64 9 303 1501 W: rcg.co.nz

PORCHESTER ROAD

ADDRESS:
**164-166 PORCHESTER RD
TAKANINI**

CLIENT:
ALDA INVESTMENTS LTD

NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:
ELEVATIONS

DESIGN **Designer**
DRAWN: **Author**
CHECKED: **Checker** SCALE: **As Indicated@A1**

SHEET NO: **A105** REV: **E**

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NOTES:

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Ref	Description	Date
A	FOR CONSENT	08.07.19
B	RESOURCE CONSENT	23.07.19



RCG, 11 Cheshire Street, Parnell, Auckland 1052
 PO Box 137313, Auckland 1151, New Zealand
 T: +64 9 303 1501 W: rcg.co.nz

PROJECT:

PORCHESTER ROAD

ADDRESS:

**164-166 PORCHESTER RD
 TAKANINI**

CLIENT:

ALDA INVESTMENTS LTD

NO: 19001 ISSUE DATE: JUL 2019

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

OUTLOOK SPACE

DESIGN **Designer**

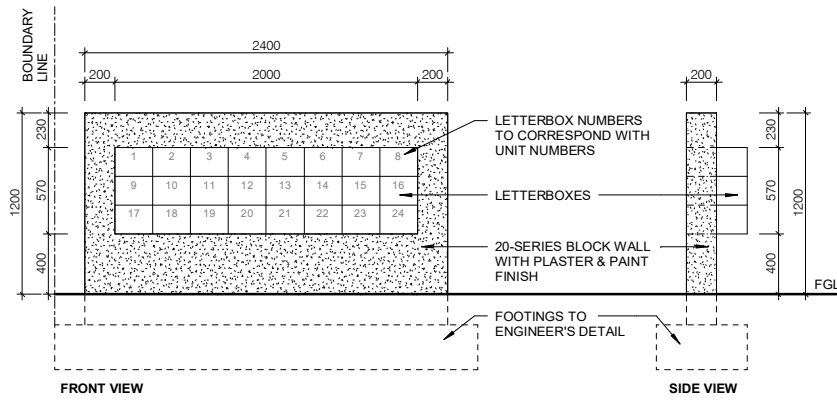
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CHECKED: **Checker** SCALE: 1 : 100@A1

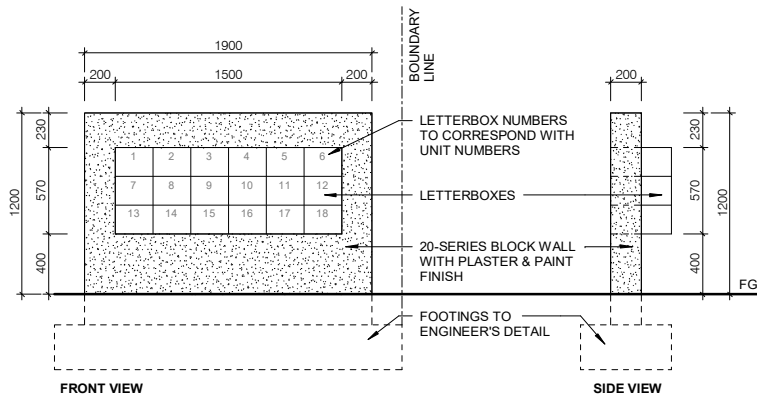
SHEET NO: REV:

A106 B

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 23/07/2019 11:54:33 AM



1 **LETTERBOX - 24 UNITS**
1:25



2 **LETTERBOX - 18 UNITS**
1:25

- LETTERBOX NOTES**
- Allow for two Letterbox units, one per Building Block; North Block with 24 units, West Block with 18 units.
 - Letterbox numbers to correspond to unit numbers.
 - Allow for 190(h)x250(w)x400(d) Contemporary Apartment Box Mailboxes to be built into block wall.

NOTES:
All work shown or implied to be carried out in strict accordance with NZS3604, the NZ Building Code and Local Body requirements.

Refer to consultant drawings for specific details on systems flashing, other such services where not specified in these documents.

All drawings to be read in conjunction with the architectural specification and structural engineers drawings and specification.

Refer to Fire Safety Design Report for fire safety requirements and precautions.

The plumbing and drainage installation must comply with AS/NZ 3500.2003, the NZ Building Code and requirements of the local Territorial Authority.

Timber framing treatments to DBH, NZS3602/NZS3604 and NZBC.

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Revisions		
Ref	Description	Date
A	FOR CONSENT	19.07.19
B	RESOURCE CONSENT	23.07.19



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PROJECT:

PORCHESTER ROAD

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164-166 PORCHESTER RD
TAKANINI

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ALDA INVESTMENTS LTD

NO: 19001 ISSUE DATE: JUL 2019

ISSUED FOR: RESOURCE CONSENT

SHEET NAME:

LETTERBOXES

DESIGN: Designer
DRAWN: Author
CHECKED: Checker SCALE: As indicated@A1

SHEET NO: REV:

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CLIENT:

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NO: **19001**

ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

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SHEET NO: REV:

A200 A

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1 VIEW OF CORNER - WALTERS AND PORCHESTER
A200 1 : 1



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CLIENT:

ALDA INVESTMENTS LTD

NO: **19001**

ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: 1 : 1@A1

SHEET NO: **A201** REV: **A**

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1 VIEW OF PORCHESTER ELEVATION
A201 1 : 1

421



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PROJECT:

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TAKANINI**

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NO: **19001** ISSUE DATE: **JUL 2019**

ISSUED FOR: **RESOURCE CONSENT**

SHEET NAME:

3D VIEWS

DESIGN: **Designer**

DRAWN: **Author**

CHECKED: **Checker** SCALE: 1 : 1@A1

SHEET NO: REV:

A202 A

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23/07/2019 11:54:37 AM **422**



1 VIEW OF CARPARK AND REAR OF BUILDING
A202 1 : 1

PLAN SET

- LA00 Landscape Notes and Plant Schedules
- LA01 Landscape Northern Site
- LA02 Landscape Southern Site
- LA03 Details
- LA04 Plant Information

TYPICAL SOIL DEPTH

Shrubs: 300mm min.
 Trees: 3m³ min. per tree, 1m depth

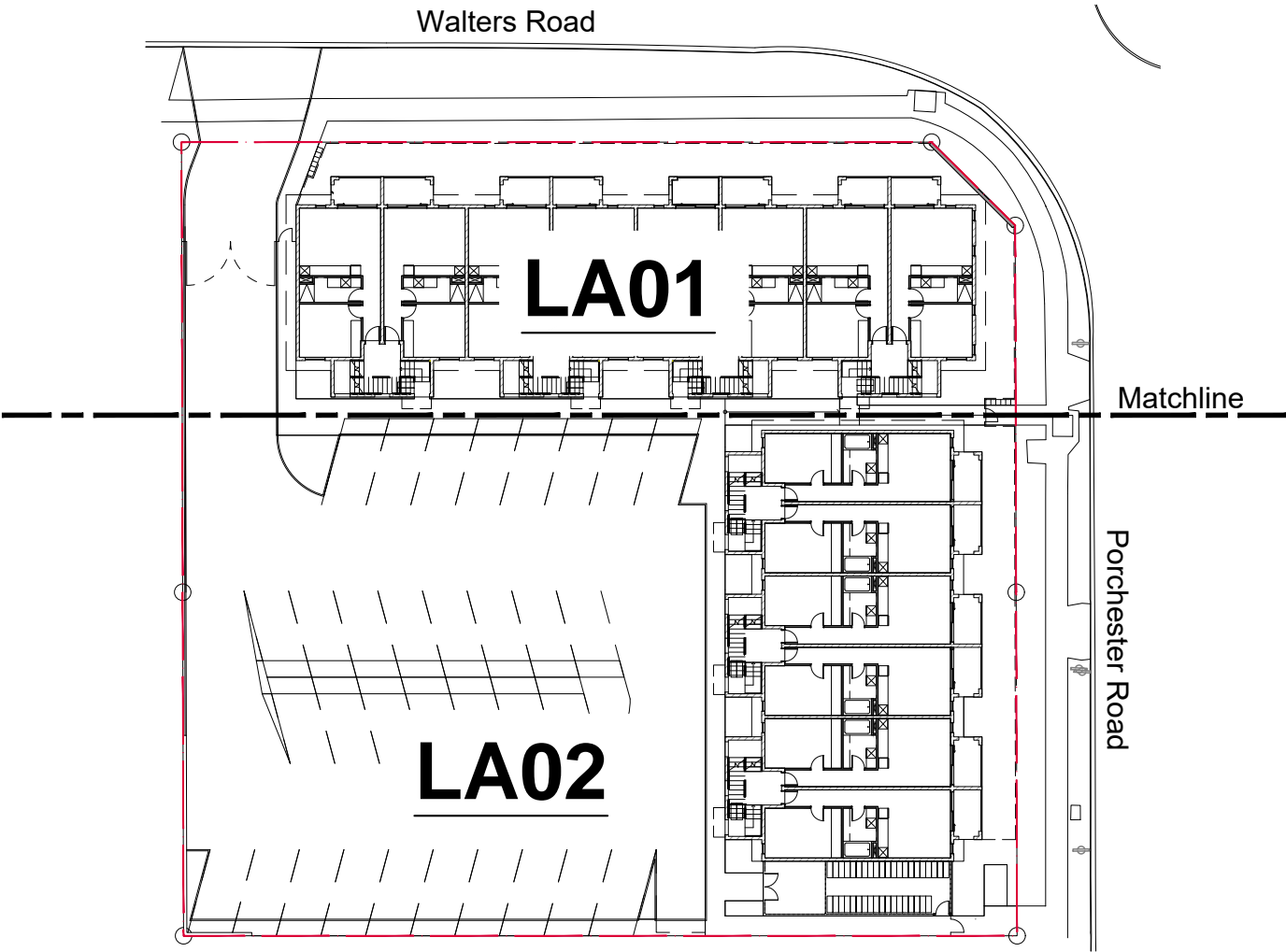
PLANT LIST

TREES

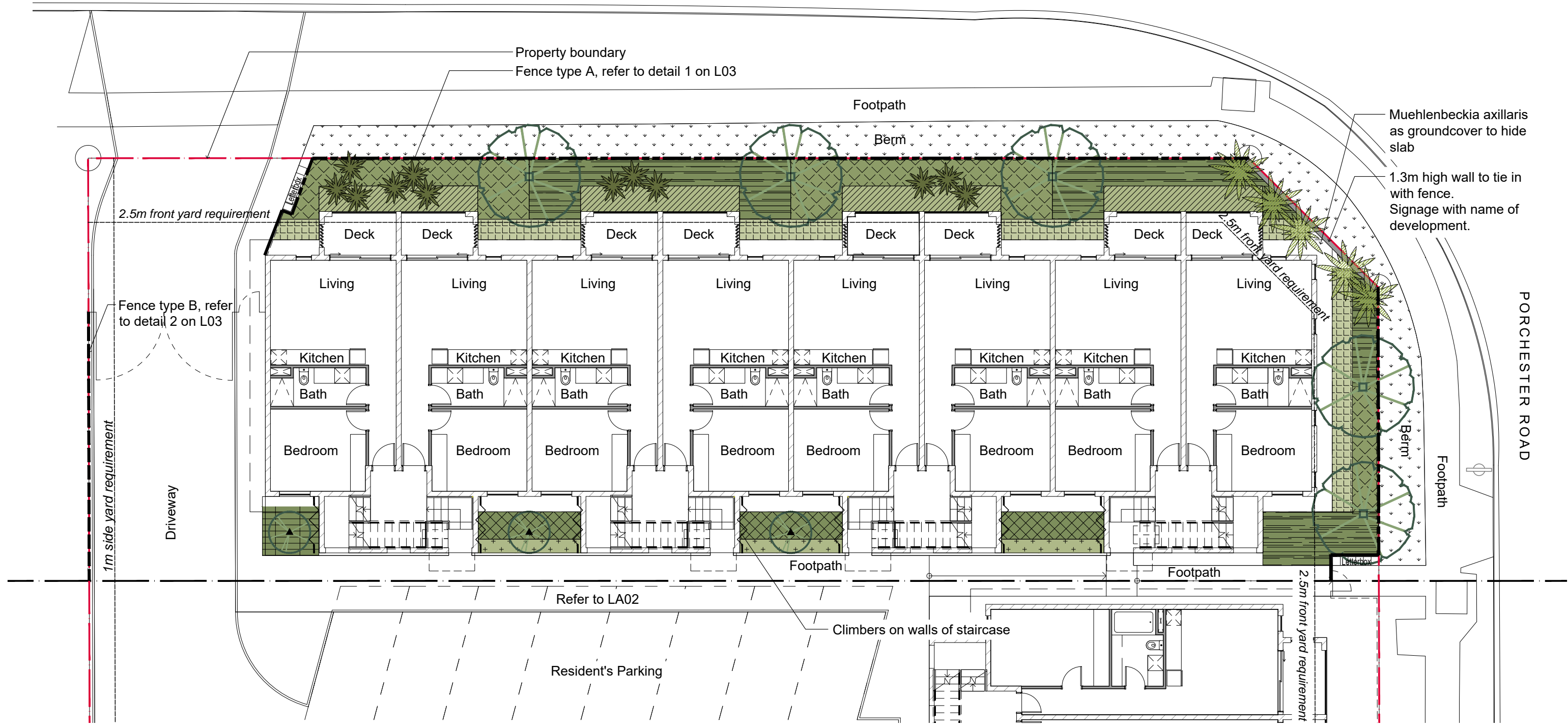
Latin Name	Common Name	Size	Qty
Cordyline australis	Cabbage Tree	25l	26
Liriodendron tulipifera fastigiata	Tulip Tree	160l	9
Pseudopanax crassifolius	Horoeka	45l	8
Rhopalostylis sapida	Nikau	45l	5
Strebulus banksii	Ewekuri	45l	4

SHRUBS, CLIMBERS & GRASSES

Latin Name	Common Name	Size	Spacing	Qty
Carex testaceae	Orange sedge	2l	5/m ²	77
Clivia miniata	Flame Lily	2l	4/m ²	51
Corokia Yellow Wonder	Korokia	10l	1/m	43
Dietes bicolor	Fortnight Lily	2l	2/m ²	50
Griselinia littoralis 'Ardmore Green'	Kapuka	10l	1/m	36
Hebe flame	Hebe	3l	3/m ²	44
Hebe stricta	Hebe	3l	3/m ²	12
Hemerocallis Stella Bella	Daylily	2l	2/m ²	43
Lobelia angulata	Panakenake	2l	1/m ²	71
Lomandra 'Little Con'		2l	4/m ²	124
Lomandra hystrix major		2L	1/m ²	28
Loropetalum chinense rubrum	Chinese fringe flower	3.5l	1/m ²	16
Muehlenbeckia axillaris		2l	2/m ²	4
Parthenocissus tricuspidata	Boston Ivy	2.5l	1/2m	20
Phormium 'Purple Haze'	Flax	2l	2/m ²	32
Trachelospermum jasminoides	Star Jasmine	8l	1/1m	36



Keyplan



Legend

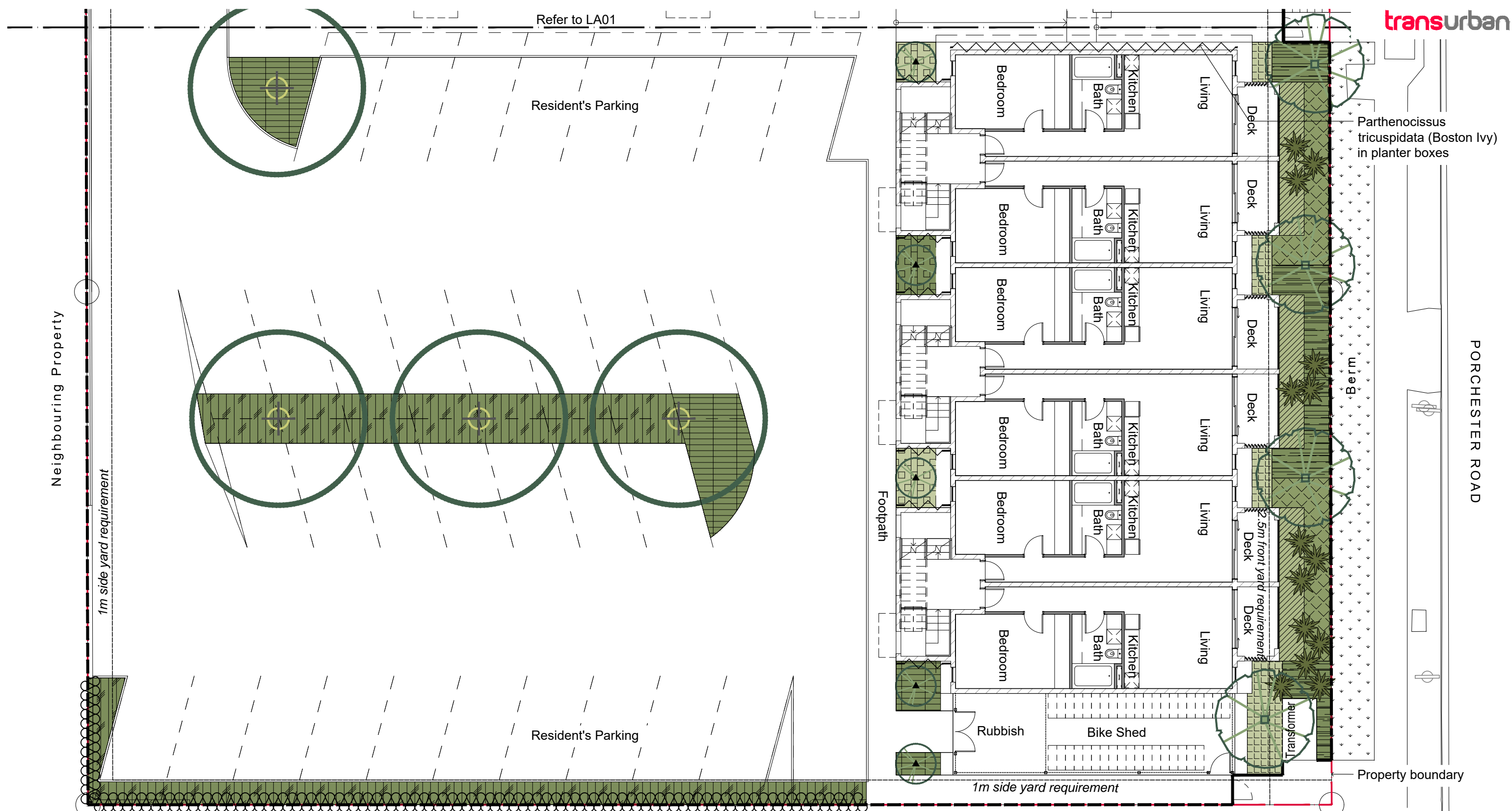
- Cordyline australis (Cabbage Tree)
- Liriodendron tulipifera fastigiata (45L)
- Pseudopanax crassifolius (45L)
- Rhopalostylis sapida (45L)
- Streblus banksii (45L)

- Carex testacea
- Lomandra hystrix major
- Clivia miniata
- Corokia Yellow Wonder (as hedge)
- Dietes bicolor
- Hebe flame
- Lomandra 'Little Con'
- Griselinia littoralis 'Ardmore Green' (as hedge)
- Hebe flame
- Hebe stricta
- Hemerocallis 'Stella Bella'
- Loropetalum chinense Rubrum
- Phormium cookianum 'Black Magic'
- Lobelia angulata
- Lomandra hystrix major
- Parthenocissus tricuspidata
- Trachelospermum jasminoides

- Fence Type A - 1.3m (Refer to LA03, Detail 1)
- Fence Type B - 1.8m (Refer to LA03, Detail 2)
- Wall - 1.3m with Development Name

-Resource Consent-
-Not for construction-

Siteplan LA01
166 & 164 Porchester Road - Residential Development
Scale 1:150 @A3 Date : 26 July 2024



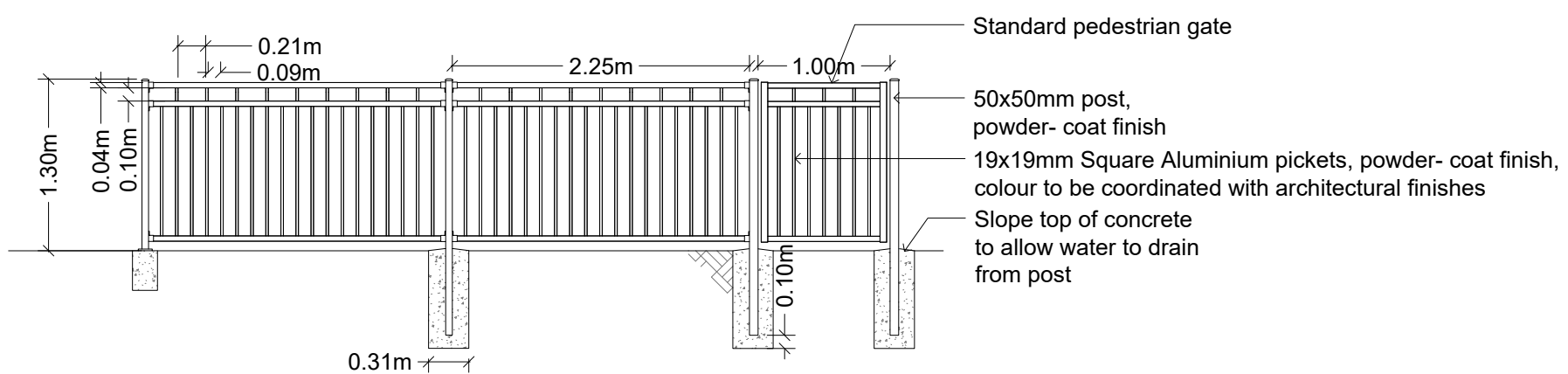
Legend

- | | | | | | | | | | |
|--|--|--|----------------------------------|--|--|--|-----------------------------|--|---|
| | Cordyline australis (Cabbage Tree) | | Carex testacea | | Griselinia littoralis 'Ardmore Green' (as hedge) | | Lobelia angulata | | Fence Type A - 1.3m (Refer to LA03, Detail 1) |
| | Liriodendron tulipifera fastigiata (45L) | | Lomandra hystrix major | | Hebe flame | | Lomandra hystrix major | | Fence Type B - 1.8m (Refer to LA03, Detail 2) |
| | Pseudopanax crassifolius (45L) | | Clivia miniata | | Hebe stricta | | Parthenocissus tricuspidata | | Wall - 1.3m with Development Name |
| | Rhopalostylis sapida (45L) | | Corokia Yellow Wonder (as hedge) | | Hemerocallis 'Stella Bella' | | Trachelospermum jasminoides | | |
| | Streblus banksii (45L) | | Dietes bicolor | | Loropetalum chinense Rubrum | | | | |
| | | | Lomandra 'Little Con' | | Phormium cookianum 'Black Magic' | | | | |

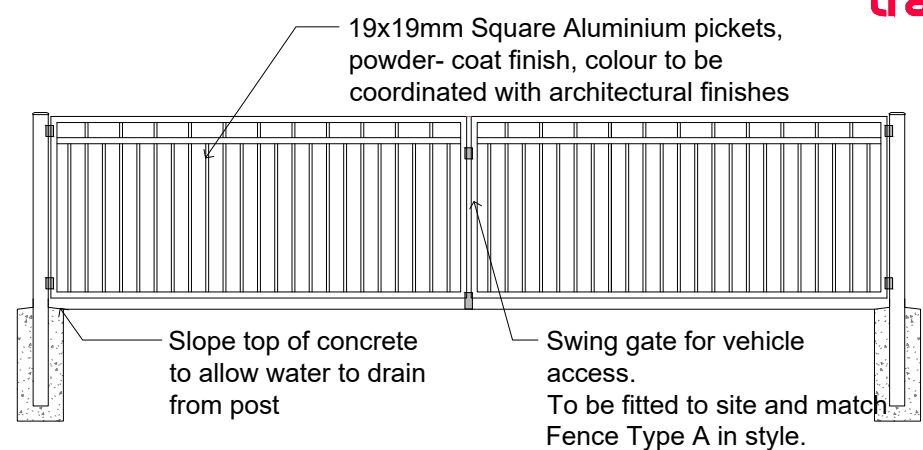


-Resource Consent-
-Not for construction-

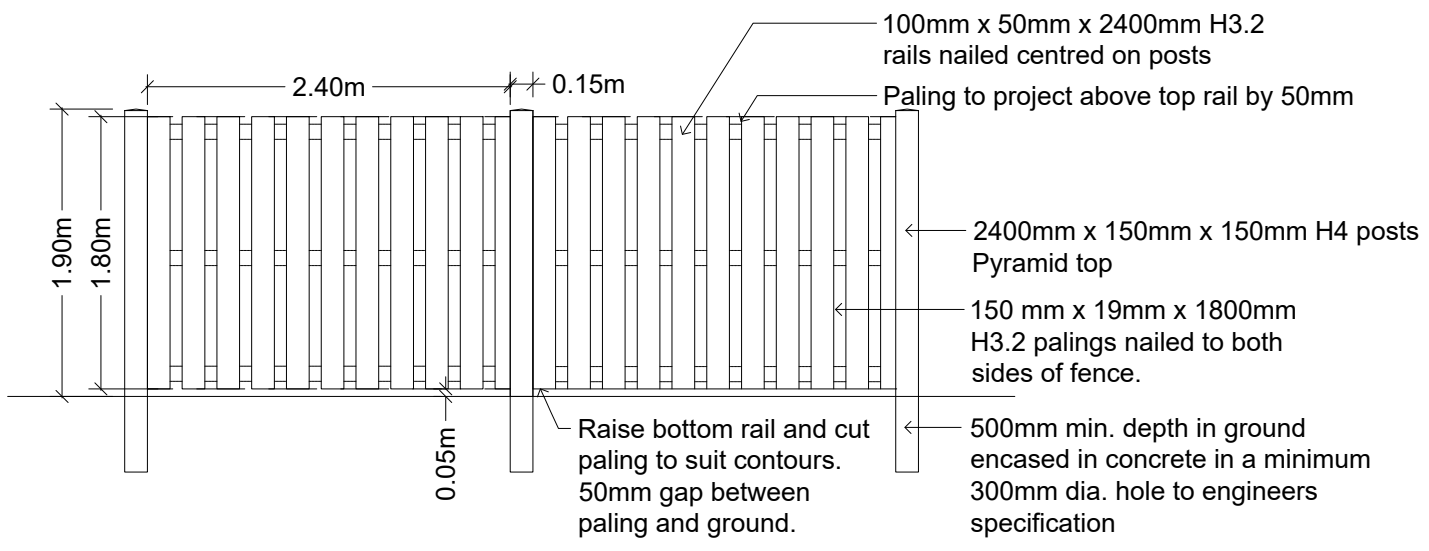
Siteplan LA02
166 & 164 Porchester Road - Residential Development
Scale 1:150 @A3 Date : 26 July 2025



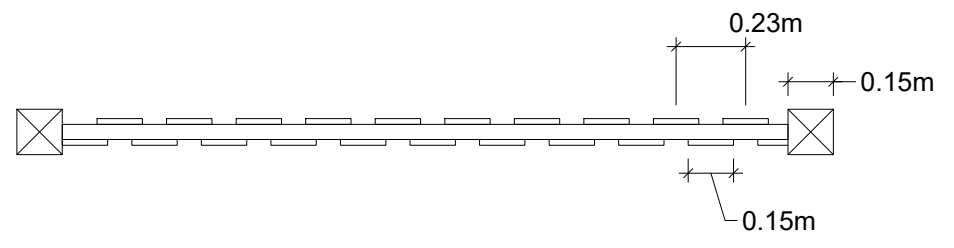
Detail 1 - Standard Fence and Pedestrian Gate Type A Scale 1:50@A3



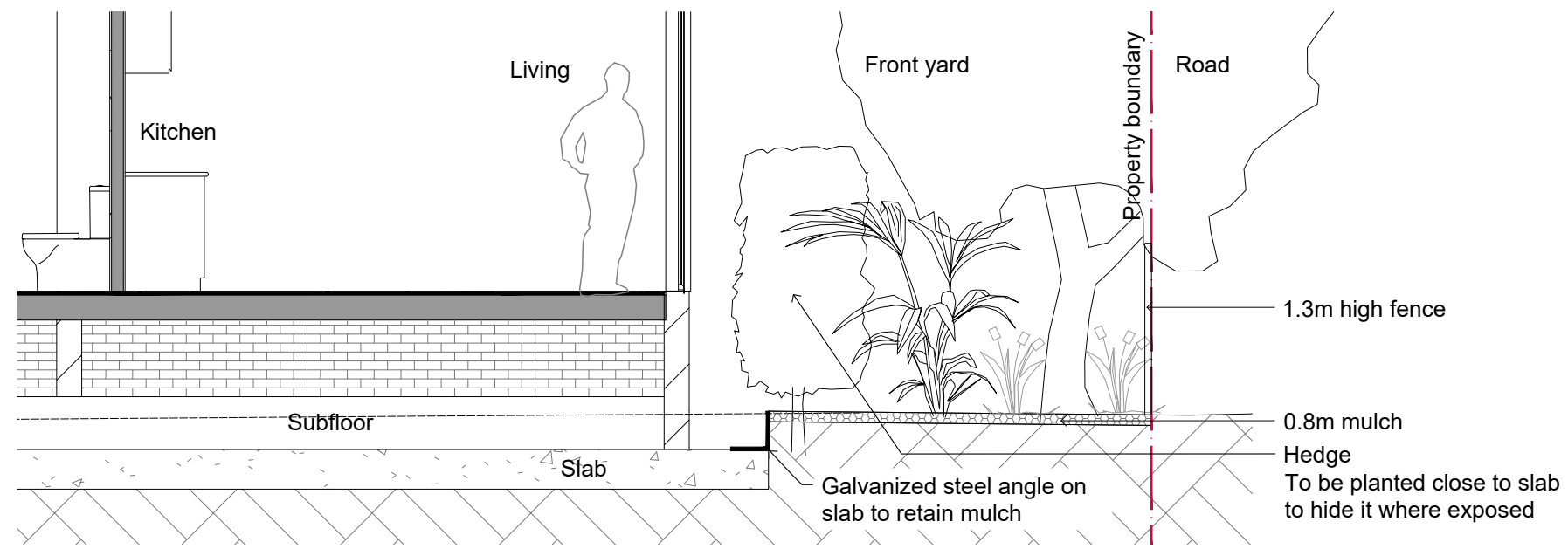
Detail 1 - Vehicle Gate Type A Scale 1:50@A3



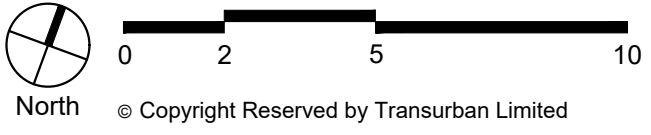
Detail 2 - Standard Fence Type B Scale 1:50@A3



Detail 2a - Standard Fence Type B - planview Scale 1:25@A3



Detail 3 - Planting detail at slab Scale 1:50@A3



-Resource Consent-
-Not for construction-

Shrubs



Carex testacea (NZ sedge)
A native grass with a distinctive rich orange and green colour and a weeping form, very popular for amenity planting. Best planted in mass for effect.
Full sun, intolerant of wet conditions, hardy
H x W: 0.4 x 0.4m



Clivia miniata (Flame lily)
Excellent plant for the shade, especially under trees. In spring (& sometimes autumn) clusters of orange tubular flowers are borne above the dark green, broad, strap-like foliage.
Full to semi-shade. Good drainage.
H x W: 0.8m x 1m



Corokia Yellow Wonder (Korokio)
Upright densely branched. Green small foliage. Starry yellow flower followed by masses of golden-yellow berries. Ideal for hedging.
Full sun, free draining soil
H x W: 2 x 1.2m.



Dietes bicolor (Fortnight Lily)
Easy growing perennial with erect, sword shaped green leaves. Iris like pale yellow flowers. Flowers abundantly throughout the warmer weather.
Full sun to part shade, frost tolerant, hardy.
H x W: 60 x 40cm



Griselinia littoralis 'Ardmore Green' (Kapuka)
Dark green, smaller leaf Griselinia with a compact tight habit. Easier to keep as a low formal hedge as slower growth than other griselinias.
Hardy, trim once a year to form a formal hedge
H: 0.5m to 2m hedge



Hebe 'Flame'
Shiny leaves which are green during summer and turning a bronze purple colour during winter. The leaves have a purple midrib and purple stems.
H x W: 0.6 x 0.75m



Hebe stricta (Koromiko)
Bushy shrub bearing pairs of long narrow pointed thin leaves. Flowers white or pinkish. It will not grow under kauris.
Full sun only, tolerates wet, hardy
H x W: 1 x 1m



Hemerocallis 'Stella Bella' (Dayily)
Extremely tolerant and hardy, can bloom for up to 6 months. Yellow flowers. Perfect for mass planting. Evergreen.
Full sun, but will tolerate some shade.
H x W: 0.5-0.8 x .4-1.0m



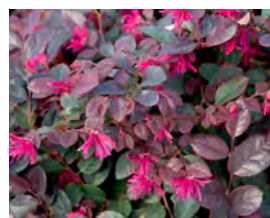
Lobelia angulata (Panakenake)
A wide spreading groundcover, fast growing and easy to establish.
Prefers a damp situation, but does well in most places.
H x W: 50 x 50cm



Lomandra 'Little Con'
Grasslike plant, forming a compact spiky ball.
Very tough, can grow on dry soils.
H x W: 30 x 30cm



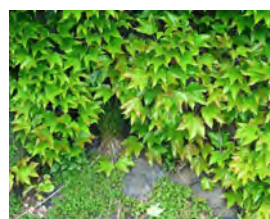
Lomandra hystrix major
Use this giant grass as a specimen feature in gardens or as a large back drop effect.
H x W: 1.5 x 1.5m



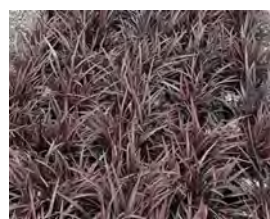
Loropetalum chinense Rubrum (Chinese fringe flower)
Evergreen shrub, burgundy wine foliage, china pink flowers in spring. Its dense and spreading habit, requires little if any maintenance.
Hardy, tolerates most climatic situations throughout NZ.
H x W: 1.5m x 2m



Muehlenbeckia axillaris (Creeping Pohuehue)
Hardy groundcover with wiry stems and small dark green leaves.
Hardy, full sun, tolerant of dry situations.
H x W: 0.15 x 1m



Parthenocissus tricuspidata (Boston Ivy)
Vigorous, self-clinging large deciduous climber. Full sun or shade, fertile, well-drained soil.



Phormium Cookianum Black Magic
Deep purple, strap-shaped leaf. Great in low maintenance gardens and pots. Ideal in gardens, border or for mass planting. Frost tolerant and drought hardy once established.
H x W: .45 x .45m



Trachelospermum jasminoides (Star Jasmine)
Deep green, glossy foliage with highly scented, white flowers. Suitable in warm, sunny position and a cool climate, most soils and can withstand mild frosts.
H x W: up to 3 x 3m

Trees



Cordyline australis (Cabbage Tree)
Creamy, fragrant white flowers in late spring, followed by white-blue berries. Can be grown in a variety of conditions. Bird distributed seed, provides food for native birds.
Full sun and semi-shade, tolerates wet and dry conditions, hardy.
H x W: 8 x 3m



Liriodendron tulipifera Fastigiata (Tulip Tree)
Deciduous tree, crown has narrow pyramid shape. Prefers deep, fertile, free- draining soil and full sun
H : up to 8 m



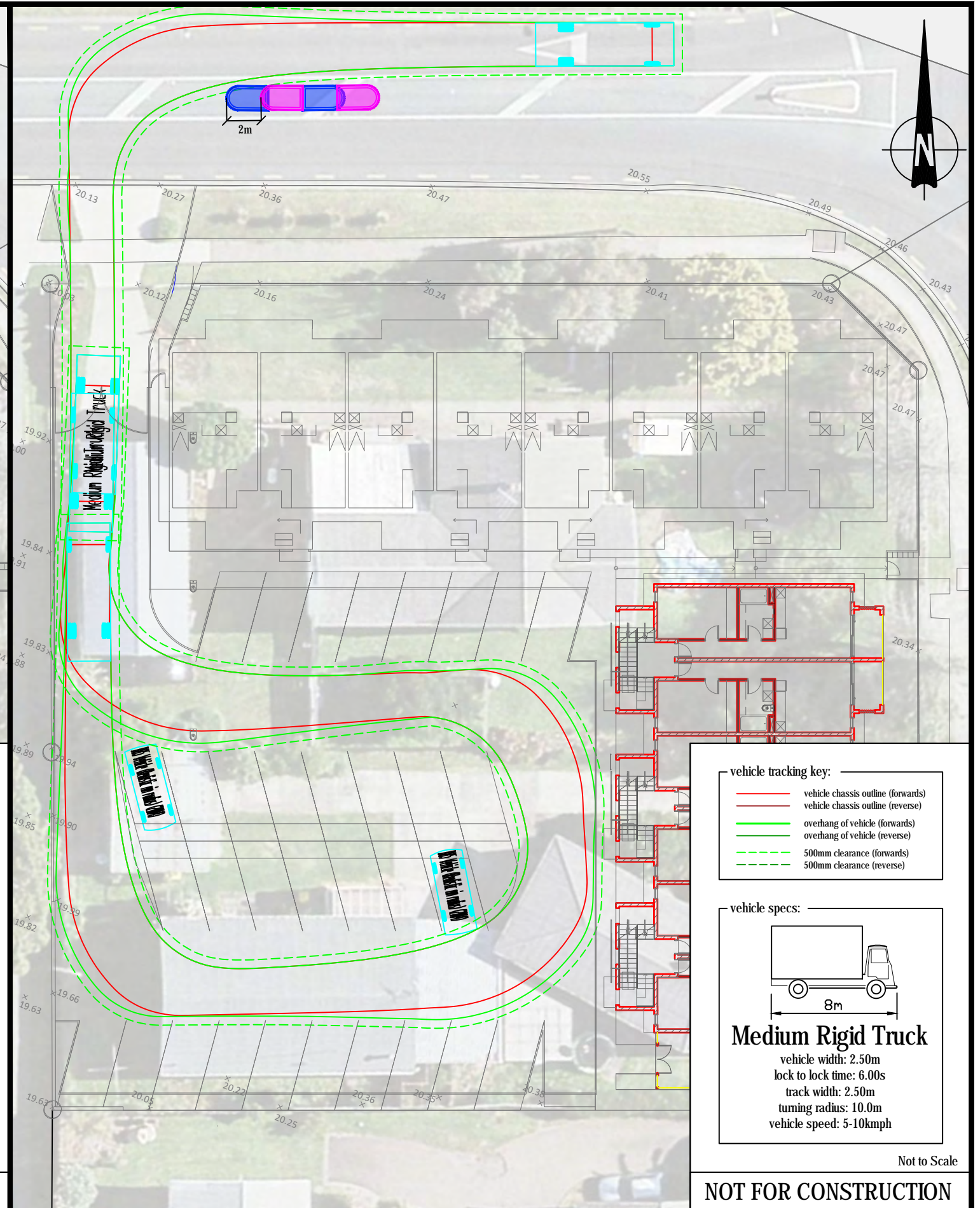
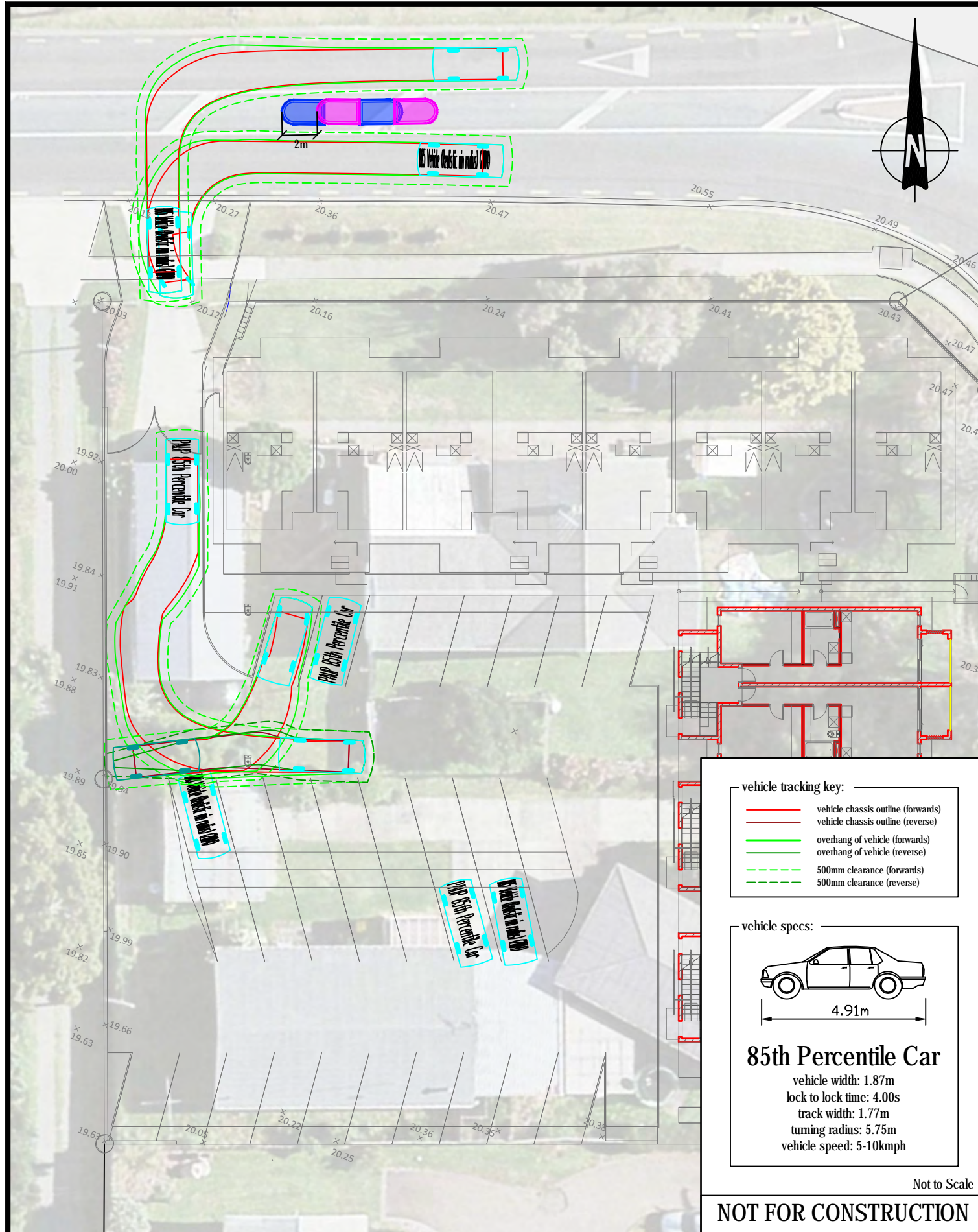
Pseudopanax crassifolius (Lancewood)
Round-headed tree with straight, clean trunk when mature. Long upright juvenile phase with long descending dark-green leaves gives an architectural look to even the smallest of spaces. Attractive to birds and bees. Evergreen.
Shade tolerant and wind resistant, frost tender when young.
H x W: 12m x 3m



Rhopalostylis sapida (Pitt Island Nikau)
Flowering doesn't occur until it is at least 30 years old, followed by red fruits which take a year to ripen. Provides food for native birds.
Semi shade or shade, tolerant of light frosts, prefers a sheltered position
H : 10m+



Streblus banksii (Ewekuri)
Tree with grey spotted bark and dark green elliptical leaves. Flowers small, in clusters of long spikes.
Prefers deep, fertile soils
H: up to 12m

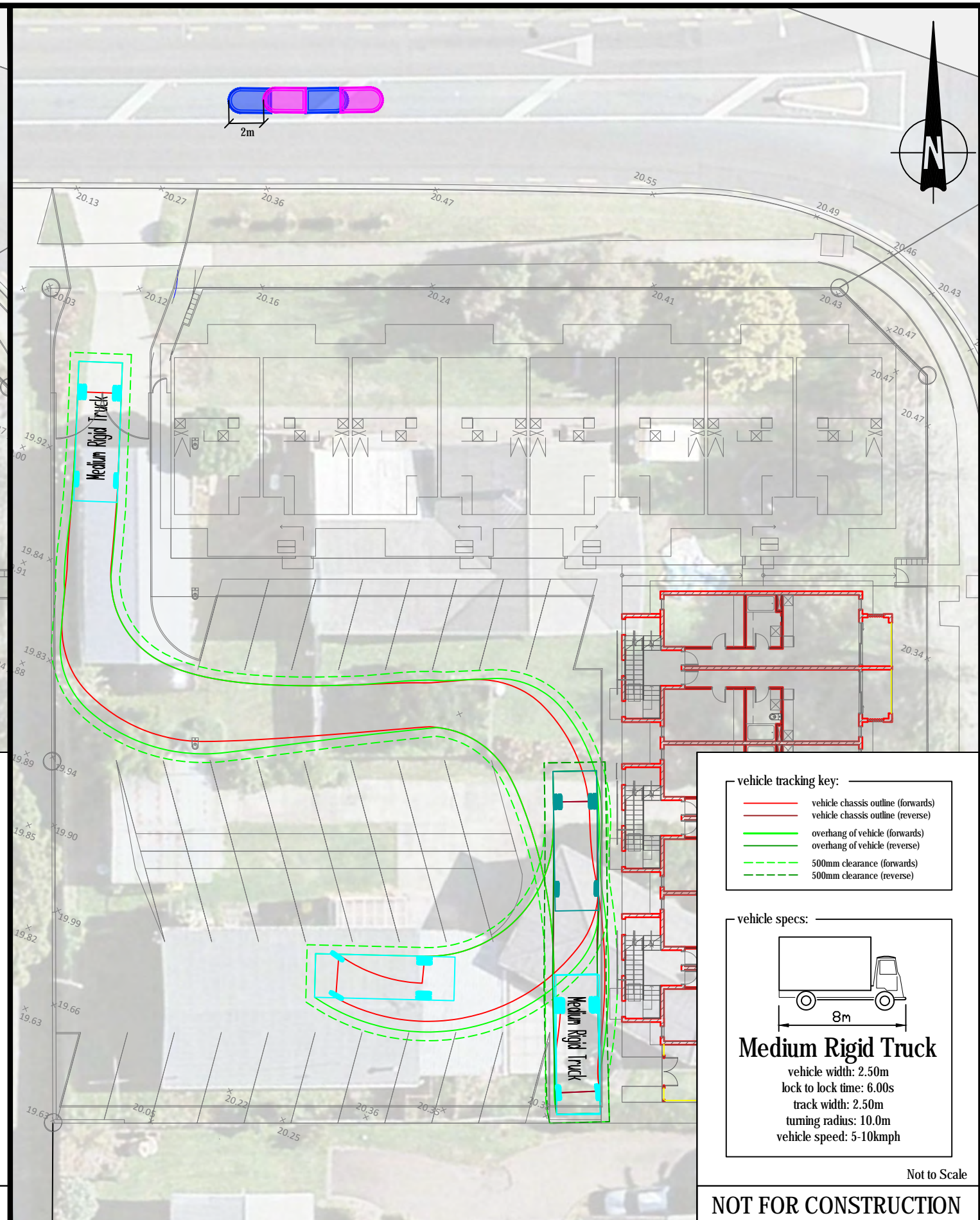
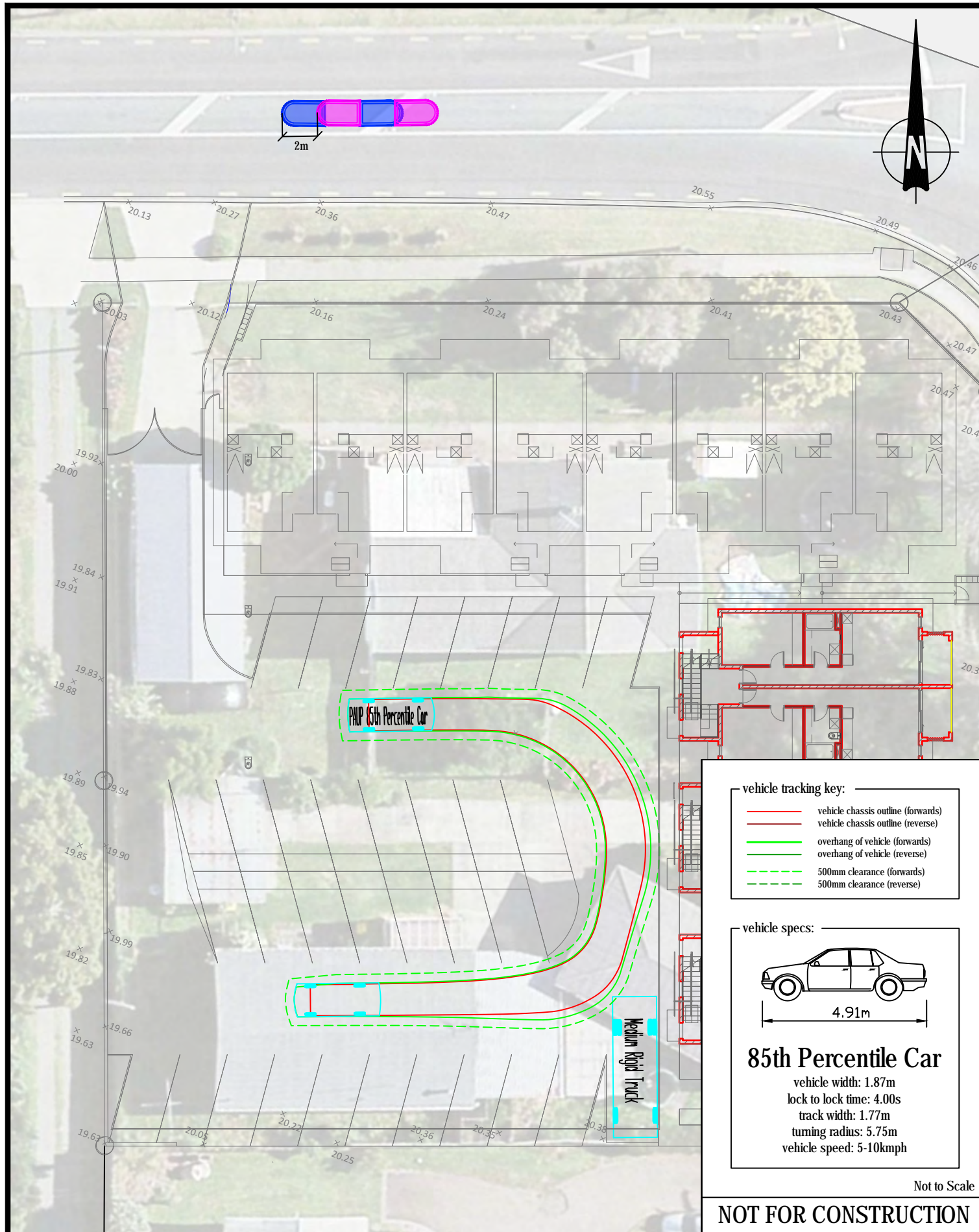


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ref:	TNGX 012	checked:	TC

status	issued
c	19/02/2019
d	12/04/2019
e	12/07/2019
f	24/09/2019

164 to 166 Porchester Road (Residential) Vehicle Tracking



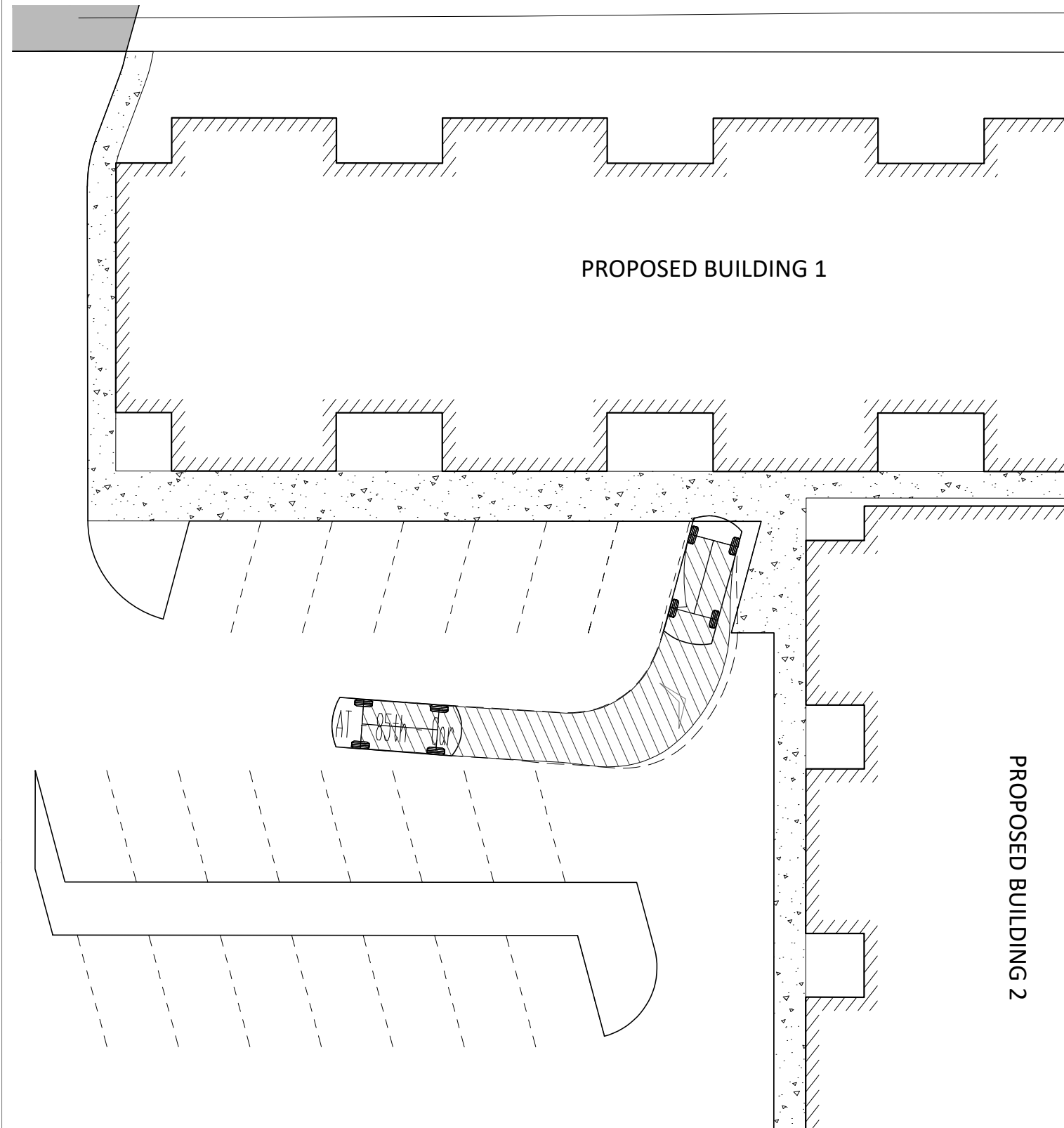
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f	24/09/2019

164 to 166 Porchester Road (Residential) Vehicle Tracking

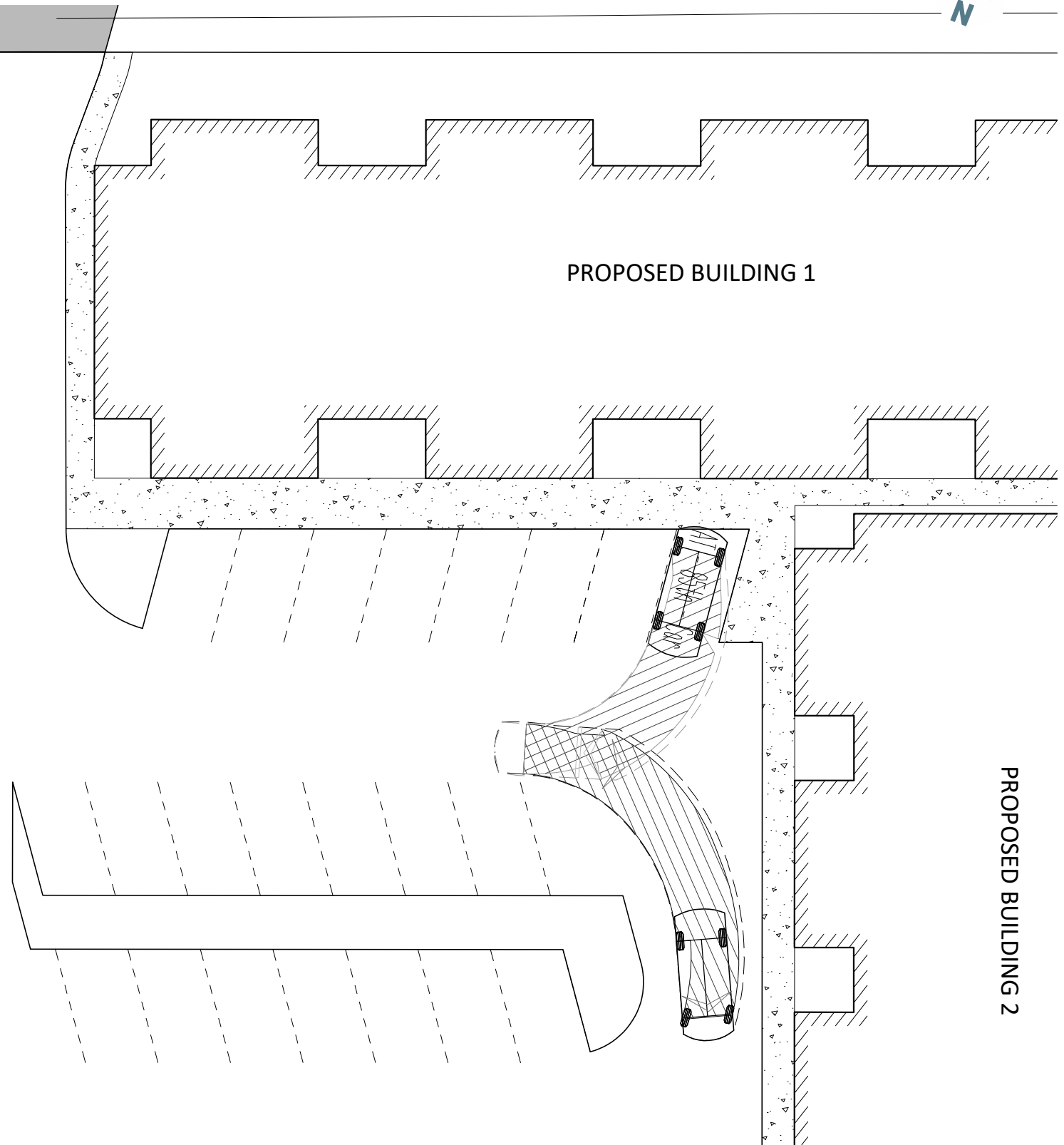
flow
 TRANSPORTATION SPECIALISTS
 Level 1, 11 Blake Street, Ponsonby, Auckland 0612 | Phone: 09 970 3820 | Fax: 09 970 3890 | www.flownz.co.nz



PROPOSED BUILDING 1

PROPOSED BUILDING 2

85TH PERCENTILE CAR - FORWARD
SCALE: (A1/A3) 1:100/1:200



PROPOSED BUILDING 1

PROPOSED BUILDING 2

85TH PERCENTILE CAR - REVERSE
SCALE: (A1/A3) 1:100/1:200

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REV	REVISION DETAILS	BY	DATE
1	TRACKING INTO CARPARK	RJP	26.05.20

DRAWN:	RJP	DATE:	05.2020
CHECKED:	RJP	DATE:	26.05.20
APPROVED:	RJP	DATE:	26.05.20

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CONSULTANTS

Level 9, Laidlaw House, 20 Amersham Way, Manukau, Auckland. Phone: 09 222 2445

PROJECT TITLE:
ALDA INVESTMENTS
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

SHEET TITLE:
VEHICLE TRACKING

ISSUE STATUS: **DRAFT@26.05.20**

SCALE: (A1/A3) 1:100 / 1:200

SCALE BAR 1:200@A3

DRAWING NUMBER: **2094-00-5004** REV: **1**

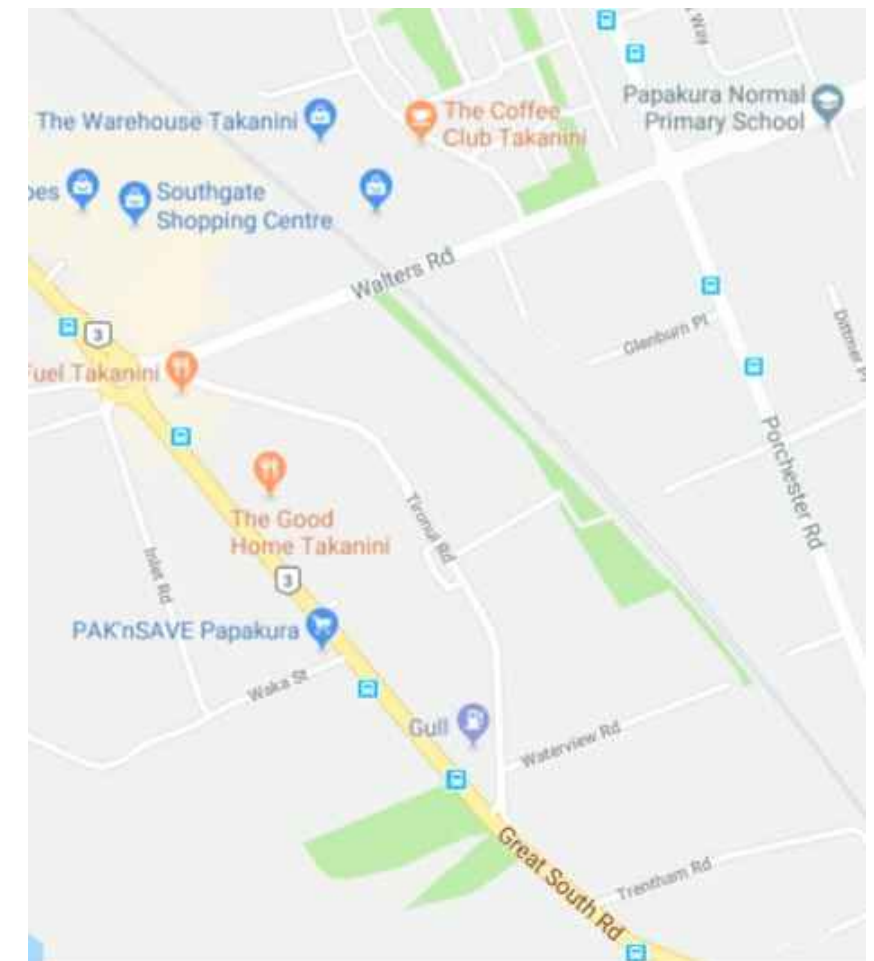
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**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND
PROJECT NUMBER: 2094**

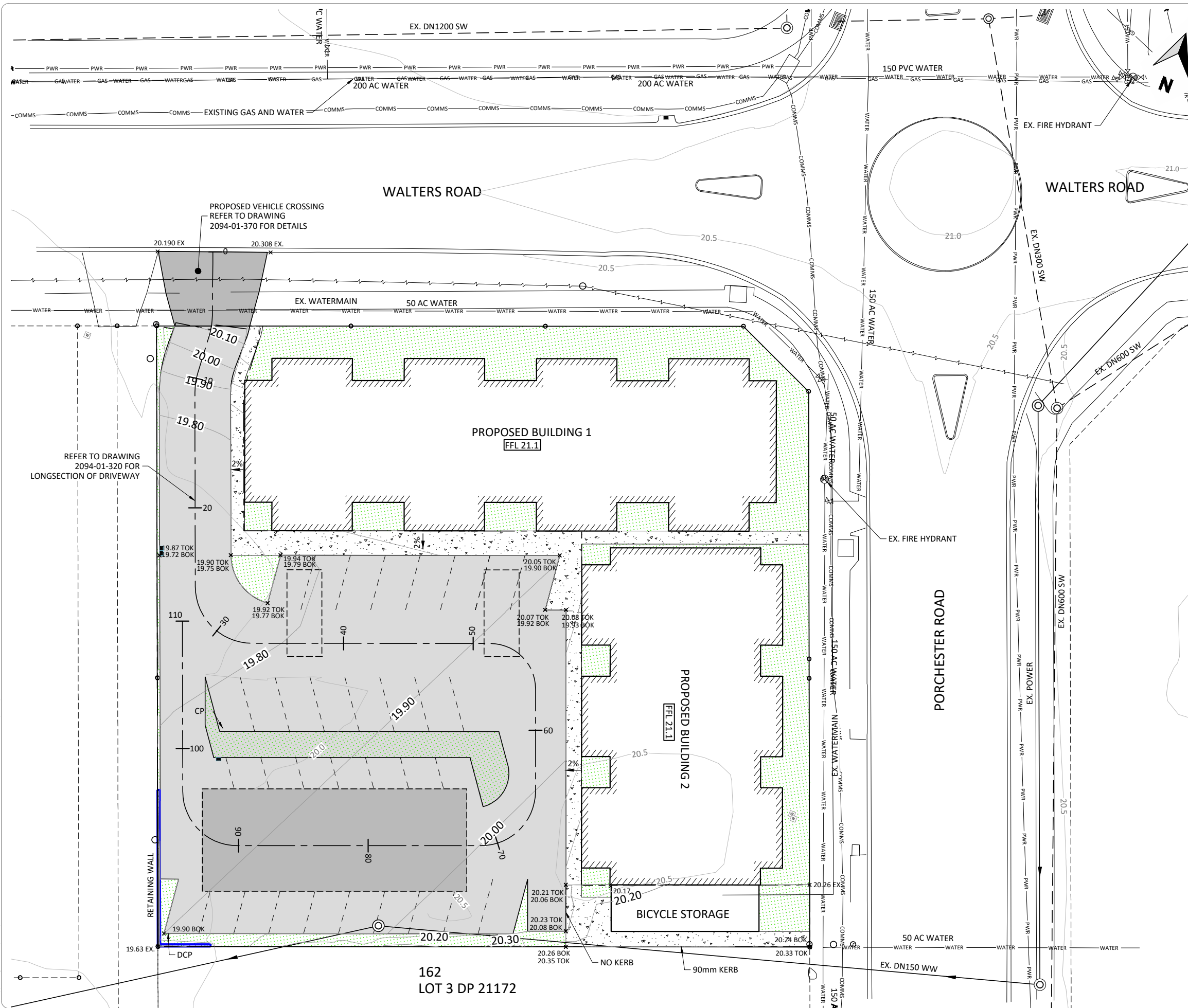
**ISSUED FOR RESOURCE CONSENT
JUNE 2019**

DRAWING INDEX

Sheet Number	Sheet Title
2094-01-000	COVER SHEET
2094-01-130	PROPOSED SITE LAYOUT
2094-01-150	TOPOGRAPHICAL PLAN
2094-01-200	PROPOSED CONTOUR PLAN
2094-01-210	PRE-LOAD PLAN
2094-01-220	ISOPACH PLAN
2094-01-230	SEDIMENT AND EROSION CONTROL PLAN
2094-01-235	SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 1
2094-01-236	SEDIMENT AND EROSION CONTROL - STANDARD DETAILS - SHEET 2
2094-01-370	VEHICLE CROSSING - PLAN AND DETAIL
2094-01-400	PROPOSED DRAINAGE-OVERALL LAYOUT
2094-01-401	PROPOSED DRAINAGE-DETAIL PLAN
2094-01-410	EXISTING FLOODPLAIN - 100YR WITH CLIMATE CHANGE
2094-01-411	PROPOSED FLOODPLAIN - 100YR WITH CLIMATE CHANGE
2094-01-450	WASTEWATER LONGITUDINAL SECTIONS
2094-01-475	PROPOSED DRAINAGE - STORMWATER360 -STORMFILTER DETAIL
2094-01-476	PROPOSED DRAINAGE - STORMWATER360 -STORMFILTER CALCULATION
2094-01-477	PROPOSED DRAINAGE - RECHARGE PIT DETAILS
2094-01-490	PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 1
2094-01-491	PROPOSED DRAINAGE - STORMWATER STANDARD DETAILS - SHEET 2
2094-01-495	WASTEWATER STANDARD DETAILS
2094-01-500	PROPOSED WATERMAIN - LAYOUT PLAN
2094-01-590	WATERMAIN STANDARD DETAILS



LOCALITY PLAN
NOT TO SCALE



- NOTES:**
- ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS AND NZ BUILDING CODE.
 - ALL WORKS ON EXISTING STORMWATER, WASTEWATER LINES AND WATERMANS TO BE CARRIED OUT BY AN APPROVED LICENSED CONTRACTOR AT THE DEVELOPERS EXPENSE.
 - CONTRACTOR TO LOCATE / MARK AND CONFIRM ALL EXISTING SERVICES PRIOR TO COMMENCEMENT OF WORK ON SITE AND PROTECTION OF SAME.

LEGEND

EXISTING

- DN1350 SW STORMWATER LINE AND MANHOLE
- WASTEWATER LINE AND MANHOLE
- CONTOUR
- POWER
- WATER
- COMMS
- GAS
- COMMS PLINTH
- POWER PLINTH
- POWER POLE
- GAS PIT
- CATCHPIT
- WATERMAIN
- FIRE HYDRANT
- WATER METER

NEW

- STORMWATER DEVICE REFER TO DRAINAGE DRAWINGS
- CONCRETE CARPARK
175mm THICK 25MPa CONCRETE WITH 1 LAYER OF 663 MESH PLACED CENTRALLY, ON 150mm COMPACTED GAP65 SUBBASE. SAW-CUT EXPANSION JOINTS AT 4m CRS MAX. (SUBGRADE CBR ≥5%)
- LANDSCAPED AREAS
- RETAINING WALL

KERB BLOCK
SCALE: (A1/A3) N.T.S.

- 150mm CONCRETE GRADE
BOTTOM OF KERB(BOK)
- 150mm CONCRETE GRADE
BOTTOM OF KERB(BOK)

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REV	REVISION DETAILS	BY	DATE
C2	ISSUED FOR CONSENT	MSS	26.06.19
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PROJECT TITLE:
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164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

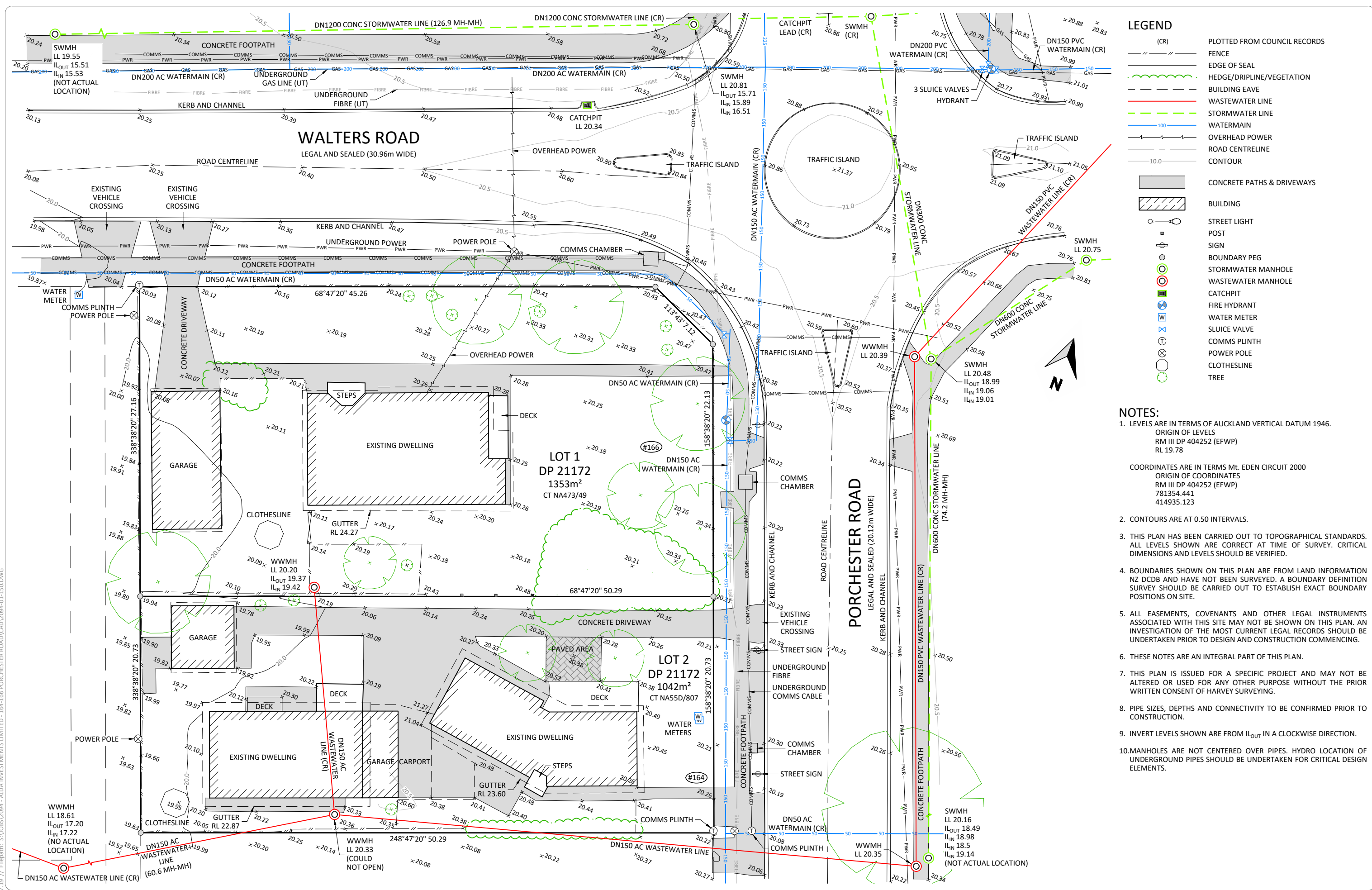
SHEET TITLE:
PROPOSED SITE LAYOUT

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: 2094-01-130
REV: C2



LEGEND

- (CR) PLOTTED FROM COUNCIL RECORDS
- FENCE
- EDGE OF SEAL
- HEDGE/DRIPLINE/VEGETATION
- BUILDING EAVE
- WASTEWATER LINE
- STORMWATER LINE
- WATERMAIN
- OVERHEAD POWER
- ROAD CENTRELINE
- CONTOUR
- CONCRETE PATHS & DRIVEWAYS
- BUILDING
- STREET LIGHT
- POST
- SIGN
- BOUNDARY PEG
- STORMWATER MANHOLE
- WASTEWATER MANHOLE
- CATCHPIT
- FIRE HYDRANT
- WATER METER
- SLUICE VALVE
- COMMS PLINTH
- POWER POLE
- CLOTHESLINE
- TREE

- ### NOTES:
- LEVELS ARE IN TERMS OF AUCKLAND VERTICAL DATUM 1946. ORIGIN OF LEVELS RM III DP 404252 (EFWP) RL 19.78
 - COORDINATES ARE IN TERMS M.L. EDEN CIRCUIT 2000 ORIGIN OF COORDINATES RM III DP 404252 (EFWP) 781354.441 414935.123
 - CONTOURS ARE AT 0.50 INTERVALS.
 - THIS PLAN HAS BEEN CARRIED OUT TO TOPOGRAPHICAL STANDARDS. ALL LEVELS SHOWN ARE CORRECT AT TIME OF SURVEY. CRITICAL DIMENSIONS AND LEVELS SHOULD BE VERIFIED.
 - BOUNDARIES SHOWN ON THIS PLAN ARE FROM LAND INFORMATION NZ DCDB AND HAVE NOT BEEN SURVEYED. A BOUNDARY DEFINITION SURVEY SHOULD BE CARRIED OUT TO ESTABLISH EXACT BOUNDARY POSITIONS ON SITE.
 - ALL EASEMENTS, COVENANTS AND OTHER LEGAL INSTRUMENTS ASSOCIATED WITH THIS SITE MAY NOT BE SHOWN ON THIS PLAN. AN INVESTIGATION OF THE MOST CURRENT LEGAL RECORDS SHOULD BE UNDERTAKEN PRIOR TO DESIGN AND CONSTRUCTION COMMENCING.
 - THESE NOTES ARE AN INTEGRAL PART OF THIS PLAN.
 - THIS PLAN IS ISSUED FOR A SPECIFIC PROJECT AND MAY NOT BE ALTERED OR USED FOR ANY OTHER PURPOSE WITHOUT THE PRIOR WRITTEN CONSENT OF HARVEY SURVEYING.
 - PIPE SIZES, DEPTHS AND CONNECTIVITY TO BE CONFIRMED PRIOR TO CONSTRUCTION.
 - INVERT LEVELS SHOWN ARE FROM I_{OUT} IN A CLOCKWISE DIRECTION.
 - MANHOLES ARE NOT CENTERED OVER PIPES. HYDRO LOCATION OF UNDERGROUND PIPES SHOULD BE UNDERTAKEN FOR CRITICAL DESIGN ELEMENTS.

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PROJECT TITLE:
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 TAKANINI, AUCKLAND**

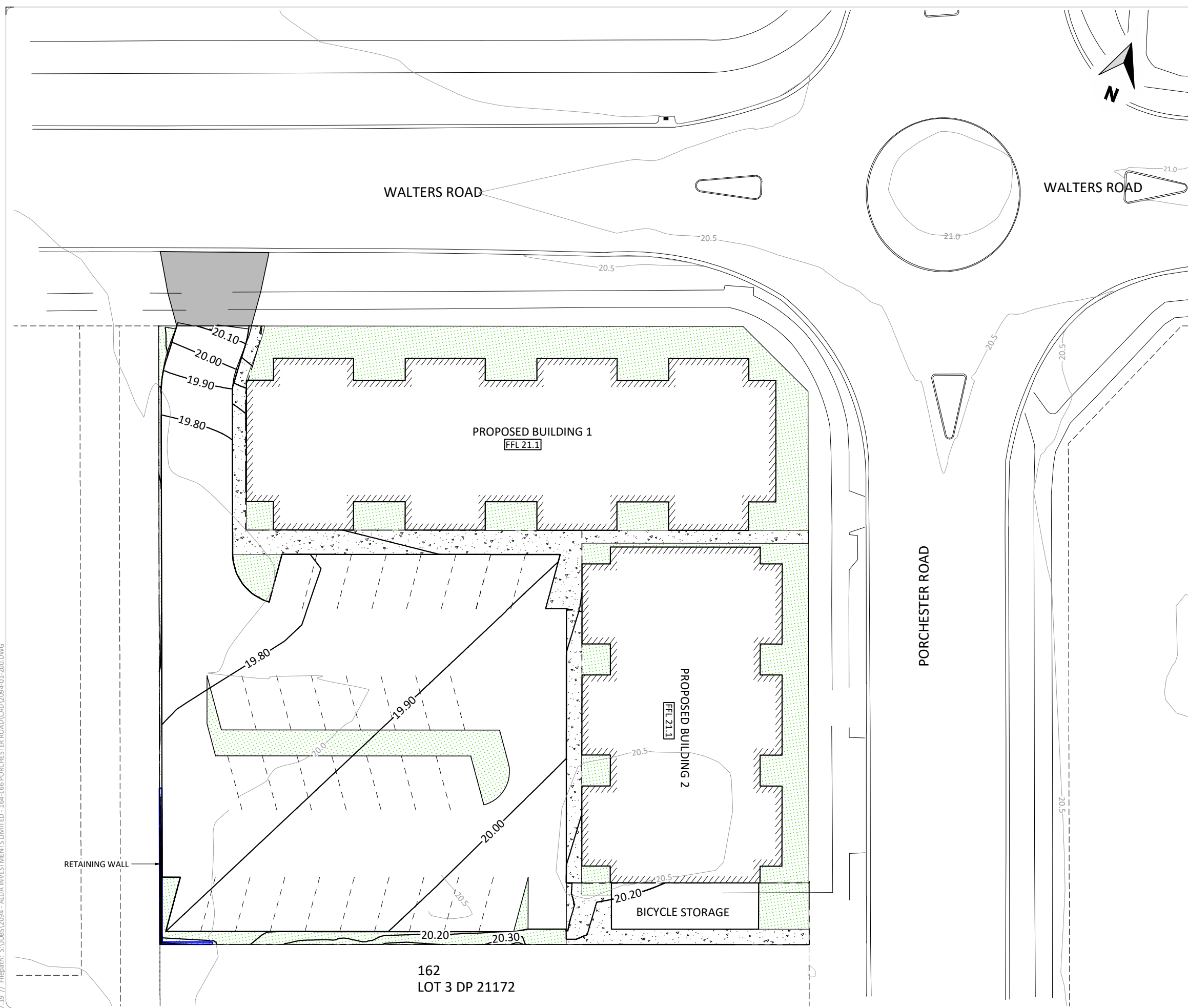
SHEET TITLE:
**TOPOGRAPHICAL PLAN OF
 LOTS 1 AND 2 DP 21172**

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-150** REV: **C2**



- NOTES:**
- EXISTING CONTOURS ARE SHOWN AT 0.5m INTERVALS.
 - DESIGN CONTOURS ARE SHOWN AT 0.1m INTERVALS.
 - CONTOURS SHOWN ARE FINISHED SURFACE LEVELS.
 - SILT AND STORMWATER CONTROL IS TO BE IMPLEMENTED DAILY AND COMPLY WITH THE GENERAL SPECIFICATION.
 - THE CONTRACTOR MUST BE AWARE OF AND COMPLY WITH AUCKLAND COUNCIL REQUIREMENTS FOR EARTHWORKS, AT ALL TIMES.
 - IT IS INTENDED THAT THE CONTRACTOR SHALL GRADE THE EARTHWORKS TO THE FINISHED CONTOURS SHOWN. HOWEVER, THE FINAL MARRYING AND SHAPING OF THE EARTHWORKS AREAS ARE SUBJECT TO THE ENGINEERS APPROVAL.
 - ROAD LONGSECTION SUBGRADE LEVELS, TYPICAL CROSS-SECTION LEVELS AND BOUNDARY LEVELS TAKE PRECEDENCE OVER PLAN CONTOUR LEVELS.

- LEGEND**
- 10.0 ——— EXISTING CONTOUR
 - 10.0 ——— PROPOSED MAJOR CONTOUR

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C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

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ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

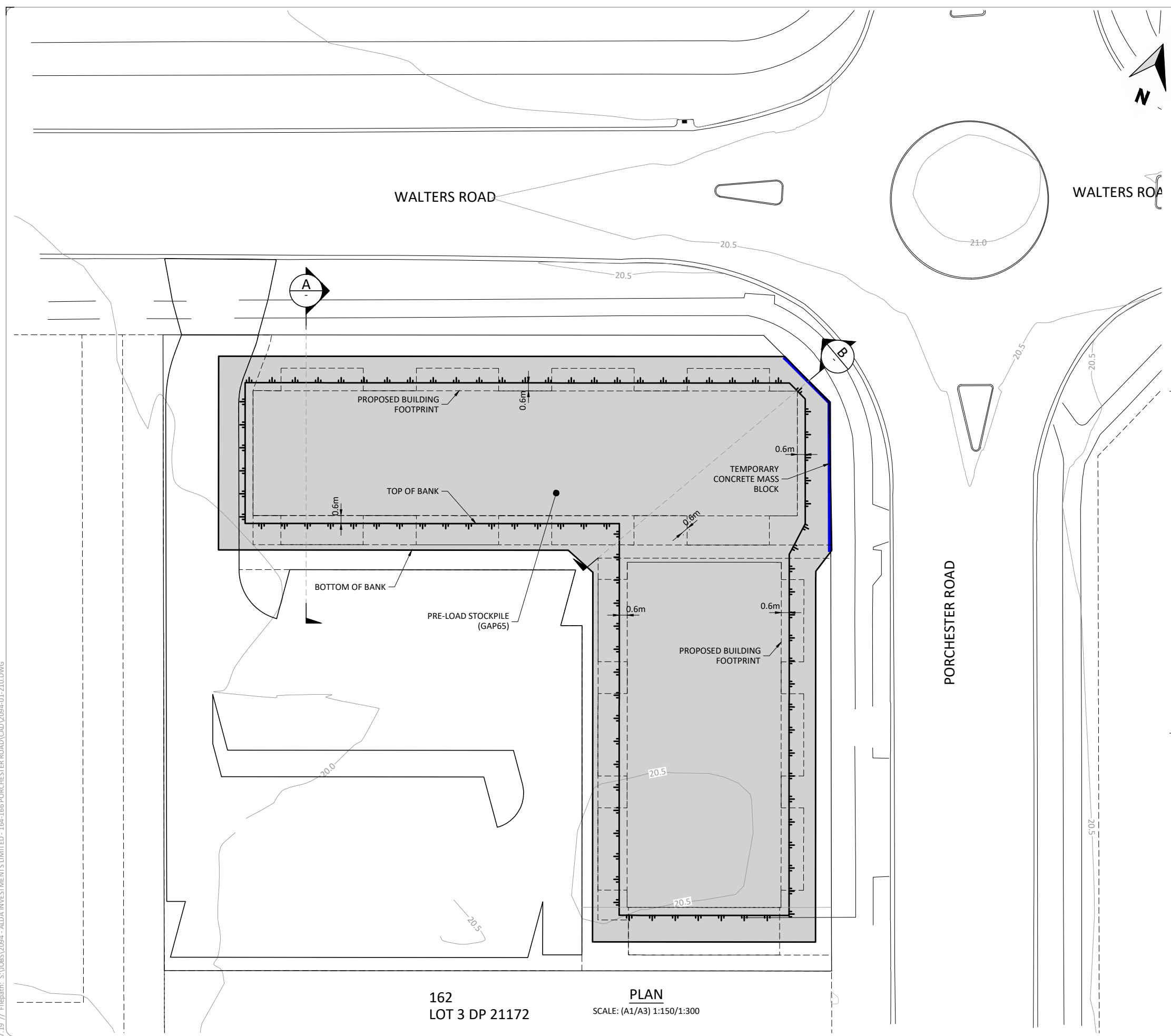
SHEET TITLE:
PROPOSED CONTOUR PLAN

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR 1:300@A3 0 3 6 9 12 15m

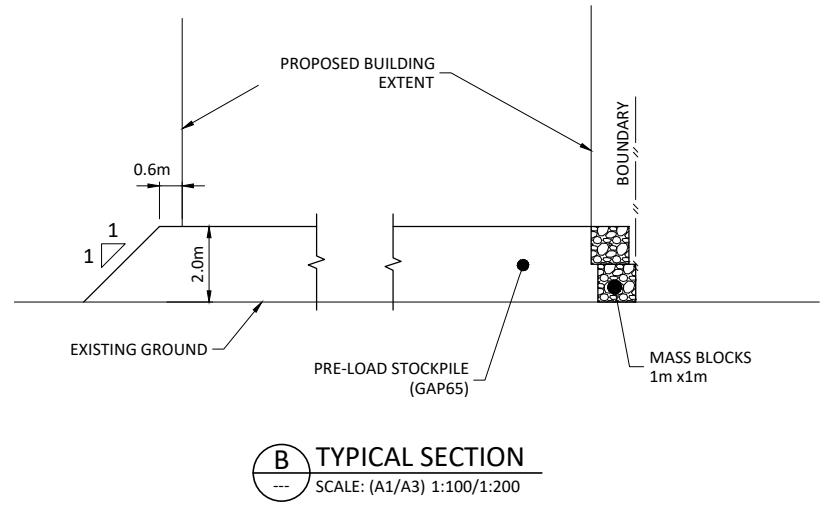
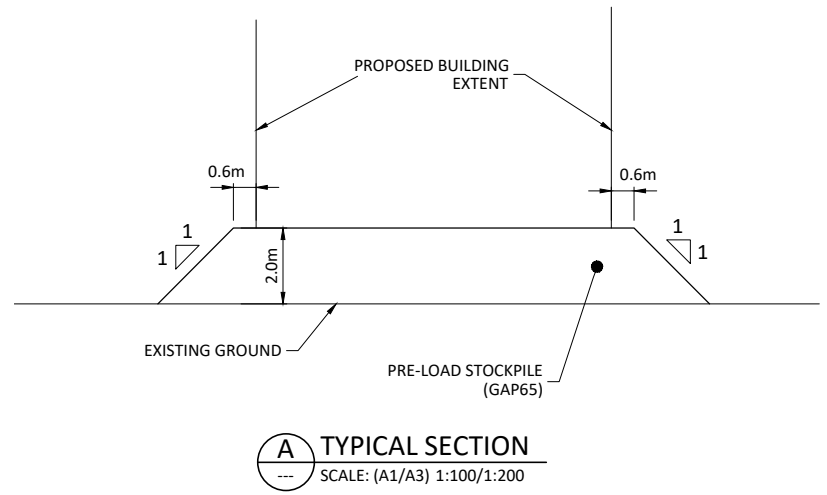
DRAWING NUMBER: 2094-01-200
REV: C2



NOTES:
 1. REFER TO LANDER GEOTECHNICAL PRELOAD DESIGN REPORT FOR DETAILS.

LEGEND

■ PRE-LOAD STOCKPILE (GAP65)



162 LOT 3 DP 21172
 PLAN
 SCALE: (A1/A3) 1:150/1:300

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MM	03.2019		
MM	19.06.19		
MSS	19.06.19		
C2	ISSUED FOR CONSENT	MSS	26.06.19
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
PRE-LOAD PLAN

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) AS SHOWN

SCALE BAR
 N.T.S.

DRAWING NUMBER: **2094-01-210** REV: **C2**



NOTES:

1. PROPOSED ISOPACHS ARE SHOWN AT 0.1m INTERVALS.
2. ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS.
3. ALL TOPSOIL TO BE STRIPPED AND STOCKPILED CLEAR OF THE PROPOSED EARTHWORKS IN A LOCATION AGREED WITH THE ENGINEER.
4. ALL EROSION AND SEDIMENT CONTROL DEVICES TO BE INSTALLED, AS-BUILT AND INSPECTED BY THE ENGINEER / COUNCIL REPRESENTATIVE PRIOR TO COMMENCING THE EARTHWORKS ON THE SITE.
5. AREA OF EARTHWORKS = 0.12ha.
6. EARTHWORK VOLUMES ARE FROM EXISTING SURFACE TO FINISHED SURFACE AND ARE:
 CUT VOLUME= 277 m³
 FILL VOLUME= 3 m³

LEGEND

- 10.0— PROPOSED CUT ISOPACH
 - 10.0— PROPOSED FILL ISOPACH
 - - - - - EARTHWORKS ZERO CUT/FILL LINE
 - - - - - EARTHWORK EXTENT
- CUT** PROPOSED CUT
- FILL** PROPOSED FILL

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REV	REVISION DETAILS	BY	DATE
C2	ISSUED FOR CONSENT	MSS	26.06.19
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REV	REVISION DETAILS	BY	DATE

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
 ISOPACH PLAN**

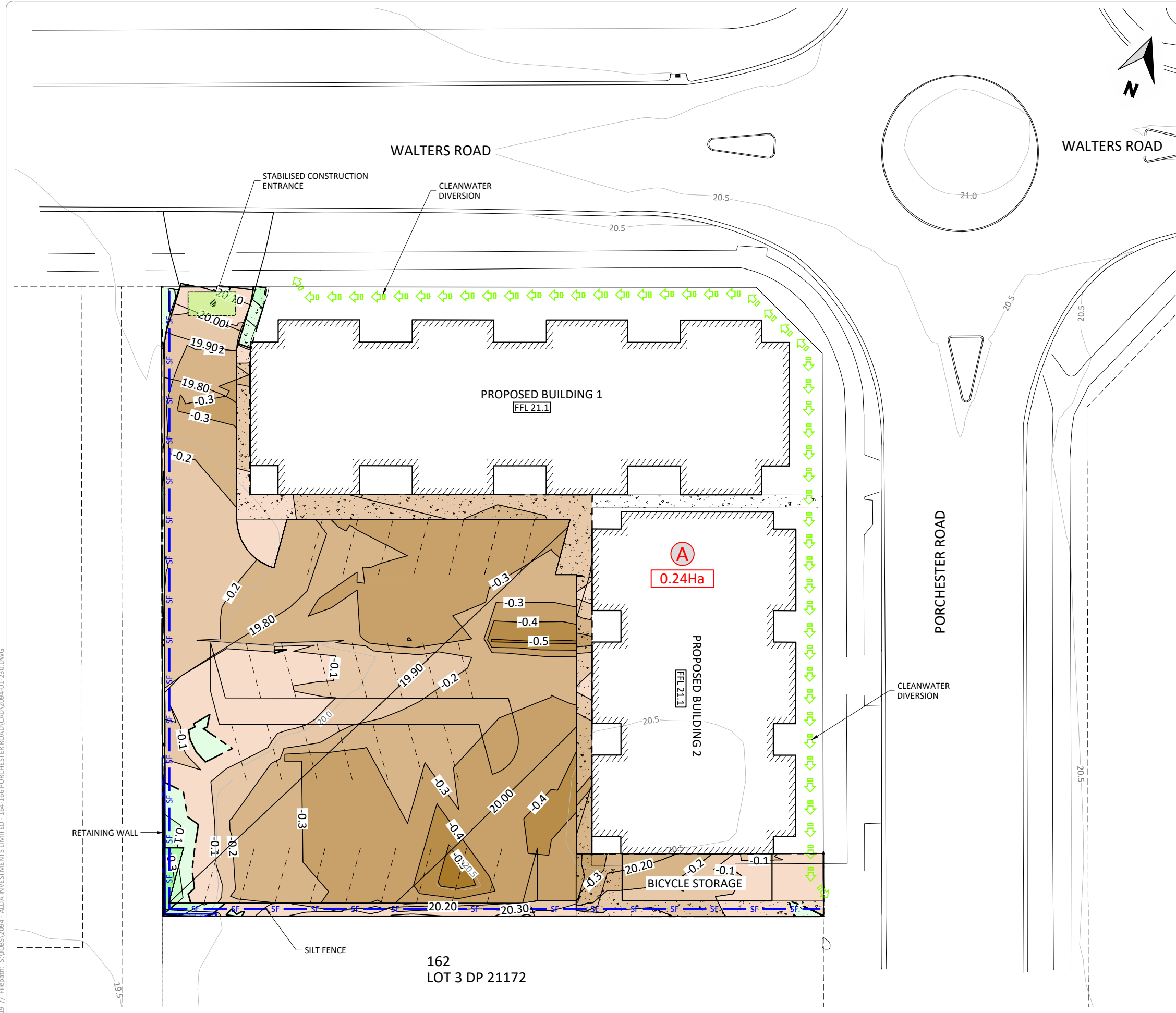
ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-220** REV: **C2**

162
 LOT 3 DP 21172



NOTES:

1. EXISTING CONTOURS ARE SHOWN AT 0.5m INTERVALS.
2. FINISHED CONTOURS ARE SHOWN AT 0.5m INTERVALS.
3. TOTAL SITE AREA = 0.24ha.
4. ALL WORKS TO COMPLY WITH COUNCIL ENGINEERING QUALITY STANDARDS AND ALL MATERIALS TO COMPLY WITH COUNCIL REQUIREMENTS.
5. ALL EROSION AND SEDIMENT CONTROL MEASURES MUST BE OPERATIONAL PRIOR TO ANY WORKS COMMENCING.
6. SEE SHEET 2094-01-235 TO 236 FOR STANDARD DETAILS
7. A COPY OF THE SEDIMENT CONTROL PLAN SHALL BE AVAILABLE ON THE SITE DURING WORK HOURS AND ALL PERSONNEL INVOLVED IN EARTHWORK ACTIVITIES ON THE SITE (INCLUDING OF SUB-CONTRACTORS) SHALL BE FAMILIAR WITH THE PLAN REQUIREMENTS AS THEY RELATE TO EROSION AND SEDIMENT CONTROL.
8. ALL "CLEANWATER" RUNOFF FROM STABILISED SURFACES INCLUDING CATCHMENT AREAS ABOVE THE SITE SHALL BE DIVERTED AWAY FROM EARTHWORK AREA VIA STABILISED SYSTEM, SO AS TO PREVENT SURFACE EROSION.
9. CUTOFF DRAINS TO BE INSTALLED AT THE MAXIMUM INTERVAL OF 50m AT THE END OF EACH DAY UNTIL SLOPES ARE STABILISED.
10. FURTHER SEDIMENT CONTROL WORKS MAY BE REQUIRED BY THE ENGINEER AS THE PROJECT ADVANCES. THESE WILL BE INSTALLED AS AND WHERE DIRECTED BY THE ENGINEER. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ENSURING THAT THE SITE HAS EFFECTIVE SILT DETENTION FACILITIES OPERATING AT ALL TIMES.
11. EITHER DECANTING EARTH BUND (DEB), POND, OR SILT FENCE TO BE UTILISED AS DIRECTED BY THE ENGINEER IN CONSULTATION WITH AUCKLAND COUNCIL.
12. ALL EROSION AND SEDIMENT CONTROL MEASURES TO COMPLY WITH AUCKLAND COUNCIL'S GD05.
13. SEDIMENT AND EROSION CONTROL MEASURES SHOWN ON THIS DRAWING ARE WORST CASE AND ASSUME A FULLY OPEN SITE WITH UNSTABILISED CLAY. CONTRACTOR METHODOLOGY AND MATERIALS USED WILL HAVE A BEARING ON REQUIRED SEDIMENT AND EROSION CONTROL. FOR EXAMPLE, AS FINAL LAYERS OF FILL START TO SLOPE TOWARDS BOUNDARIES, IF METAL IS USED IN TO COVER THESE AREAS, THE AREA IS DEEMED STABILISED AND REQUIRED SEDIMENT AND EROSION CONTROL MEASURES WILL BE REDUCED UPON APPROVAL BY COUNCIL.
14. DEVICES SHALL NOT BE DE-COMMISSIONED WITHOUT THE EXPRESS APPROVAL OF COUNCILS MONITORING OFFICER.

LEGEND

- 10.00- EXISTING CONTOURS
- 10.00- PROPOSED CONTOURS
- STABILISED CONSTRUCTION ACCESS
- SF SILT FENCE
- CLEAN WATER DIVERSION
- CATCHMENT NUMBER
- CATCHMENT AREA

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MSS	05.06.19	DATE	

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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
SEDIMENT AND EROSION CONTROL
PLAN**

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-230** REV: **C2**

SILT FENCE:

APPLICATION

- ON LOW GRADIENT SITES OR FOR CONFINED AREAS WHERE THE CONTRIBUTING CATCHMENT IS SMALL, SUCH AS SHORT STEEP BATTER FILLS AND AROUND WATERCOURSES.
- TO DELINEATE THE LIMIT OF DISTURBANCE ON AN EARTHWORKS SITE SUCH AS RIPARIAN AREAS OR BUSH RESERVES.
- TO STORE RUNOFF BEHIND THE SILT FENCE WITHOUT DAMAGING THE FENCE OR THE SUBMERGED AREA BEHIND THE FENCE.
- DO NOT INSTALL SILT FENCES ACROSS WATERCOURSES OR IN AREAS OF CONCENTRATED FLOWS.

DESIGN

- ENSURE THE SILT FENCE HEIGHT IS A MINIMUM OF 600mm ABOVE AND 200mm BELOW GROUND LEVEL.
- PLACE SUPPORTING POSTS/WARATAHS FOR SILT FENCES 2m-4m WITH SUPPORT PROVIDED BY TENSIONED WIRE (2.5mm HT) ALONG THE TOP OF THE SILT FENCE. WHERE A STRONG WOVEN FABRIC IS USED IN CONJUNCTION WITH A WIRE SUPPORT, THE DISTANCE BETWEEN POSTS CAN BE EXTENDED UP TO 4m. DOUBLE THE SILT FENCE FABRIC OVER AND FASTEN TO THE WIRE AND POSTS WITH SILT FENCE CLIPS AT 500mm SPACINGS. ENSURE SUPPORTING POSTS ARE EMBEDDED A MINIMUM OF 400mm INTO THE GROUND.
- ALWAYS INSTALL SILT FENCES ALONG THE CONTOUR (AT A BREAK IN SLOPE). WHERE THIS IS NOT POSSIBLE OR WHERE THERE ARE LONG SECTIONS OF SILT FENCE, INSTALL SHORT SILT FENCE RETURNS PROJECTING UP SLOPE TO MINIMISE CONCENTRATION OF FLOWS. SILT FENCE RETURNS ARE A MINIMUM 2m IN LENGTH, CAN INCORPORATE A TIE BACK AND ARE GENERALLY CONSTRUCTED BY CONTINUING THE SILT FENCE AROUND THE RETURN AND DOUBLING BACK, ELIMINATING JOINS.
- JOIN LENGTHS OF SILT FENCE BY DOUBLING OVER FABRIC ENDS AROUND A WARRATAH, WOODEN POST OR BATTEN OR BY STAPLING THE FABRIC ENDS TO A BATTEN AND BUTTING THE TWO BATTENS TOGETHER OR BY OVERLAPPING AT LEAST 2m.
- MAXIMUM SLOPE LENGTHS, SPACING OF RETURNS AND ANGLES FOR SILT FENCES ARE SHOWN IN THE TABLE BELOW.
- INSTALL SILT FENCE RETURNS AT EITHER END OF THE SILT FENCE PROJECTING UPSLOPE TO A SUFFICIENT HEIGHT TO PREVENT OUTFLANKING.
- WHERE IMPOUNDED FLOW MAY OVERTOP THE SILT FENCE, CROSSING NATURAL DEPRESSIONS OR LOW POINTS, MAKE PROVISION FOR A RIPRAP SPLASH PAD OR OTHER OUTLET PROTECTION DEVICE.

MAINTENANCE

- INSPECT SILT FENCES AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL. MAKE ANY NECESSARY REPAIRS WHEN BULGES OCCUR OR WHEN SEDIMENT ACCUMULATION REACHES 50% OF THE FABRIC HEIGHT.
- ANY AREAS OF COLLAPSE, DECOMPOSITION OR INEFFECTIVENESS NEED TO BE IMMEDIATELY REPLACED.
- REMOVE SEDIMENT DEPOSITS AS NECESSARY TO CONTINUE TO ALLOW FOR ADEQUATE SEDIMENT STORAGE AND REDUCE PRESSURE ON THE SILT FENCE. ENSURE THAT THE SEDIMENT IS REMOVED TO A SECURE AREA.
- DO NOT REMOVE SILT FENCE MATERIALS AND SEDIMENT DEPOSITION UNTIL THE CATCHMENT AREA HAS BEEN APPROPRIATELY STABILISED. STABILISE THE AREA OF THE REMOVED SILT FENCE.

SUPER SILT FENCE DESIGN CRITERIA:

SLOPE STEEPNESS %	SLOPE LENGTH (m) (MAXIMUM)	SPACING OF RETURNS (m)	SUPER SILT FENCE LENGTH (m) (MAXIMUM)
0-10%	UNLIMITED	60	UNLIMITED
10-20%	60	50	450
20-33%	30	40	300
33-50%	30	30	150
>50%	15	20	75

SUPER SILT FENCE:

APPLICATION

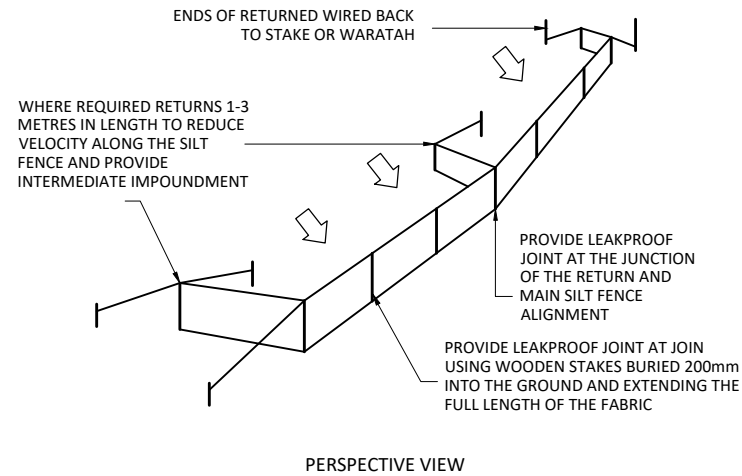
- PROVIDES A BARRIER THAT CAN COLLECT AND HOLD DEBRIS AND SOIL, PREVENTING THE MATERIAL FROM ENTERING CRITICAL AREAS, WATERCOURSES AND STREETS.
- CAN BE USED WHERE THE INSTALLATION OF AN EARTH OR TOPSOIL BUND WOULD DESTROY SENSITIVE AREAS SUCH AS BUSH AND WETLANDS.
- SHOULD BE PLACED AS CLOSE TO THE CONTOUR AS POSSIBLE. NO SECTION OF THE FENCE SHOULD EXCEED A GRADE OF 5% FOR A DISTANCE OF MORE THAN 15m.

DESIGN

- ENSURE THE SILT FENCE HEIGHT IS A MINIMUM OF 600mm ABOVE AND 200mm BELOW GROUND LEVEL.
- WHEN CONSIDERING SUPER SILT FENCE INSTALLATION FOR LARGER CATCHMENTS (GREATER THAN 0.5ha) AS IN THE TABLE BELOW, CAREFULLY CONSIDER THE SPECIFIC SITE CONDITIONS AND OTHER ALTERNATIVE CONTROL MEASURES AVAILABLE. BASE THE LENGTH OF THE SUPER SILT FENCE ON THE LIMITS SHOWN IN THE TABLE LEFT.
- ALWAYS INSTALL SILT FENCES ALONG THE CONTOUR (AT A BREAK IN SLOPE). WHERE THIS IS NOT POSSIBLE OR WHERE THERE ARE LONG SECTIONS OF SILT FENCE, INSTALL SHORT SILT FENCE RETURNS PROJECTING UP SLOPE TO MINIMISE CONCENTRATION OF FLOWS. SILT FENCE RETURNS ARE A MINIMUM 2m IN LENGTH, CAN INCORPORATE A TIE BACK AND ARE GENERALLY CONSTRUCTED BY CONTINUING THE SILT FENCE AROUND THE RETURN AND DOUBLING BACK, ELIMINATING JOINS.
- JOIN LENGTHS OF SILT FENCE BY DOUBLING OVER FABRIC ENDS AROUND A WARRATAH, WOODEN POST OR BATTEN OR BY STAPLING THE FABRIC ENDS TO A BATTEN AND BUTTING THE TWO BATTENS TOGETHER OR BY OVERLAPPING AT LEAST 2m.
- LIMITS IMPOSED BY ULTRA VIOLET LIGHT AFFECT THE STABILITY OF THE FABRIC AND WILL DICTATE THE MAXIMUM PERIOD THAT THE SUPER SILT FENCE MAY BE USED.
- WHERE ENDS OF THE GEOTEXTILE FABRIC COME TOGETHER, OVERLAP, FOLD AND STAPLE THE FABRIC ENDS TO PREVENT SEDIMENT BYPASS.

MAINTENANCE

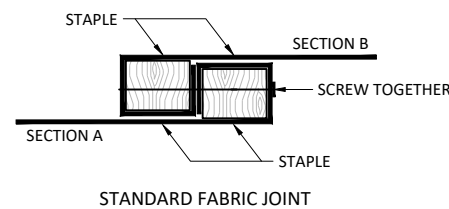
- INSPECT SILT FENCES AT LEAST ONCE A WEEK AND AFTER EACH RAINFALL. MAKE ANY NECESSARY REPAIRS WHEN BULGES OCCUR OR WHEN SEDIMENT ACCUMULATION REACHES 50% OF THE FABRIC HEIGHT.
- ANY AREAS OF COLLAPSE, DECOMPOSITION OR INEFFECTIVENESS NEED TO BE IMMEDIATELY REPLACED.
- REMOVE SEDIMENT DEPOSITS AS NECESSARY TO CONTINUE TO ALLOW FOR ADEQUATE SEDIMENT STORAGE AND REDUCE PRESSURE ON THE SILT FENCE. ENSURE THAT THE SEDIMENT IS REMOVED TO A SECURE AREA.
- DO NOT REMOVE SILT FENCE MATERIALS AND SEDIMENT DEPOSITION UNTIL THE CATCHMENT AREA HAS BEEN APPROPRIATELY STABILISED. STABILISE THE AREA OF THE REMOVED SILT FENCE.



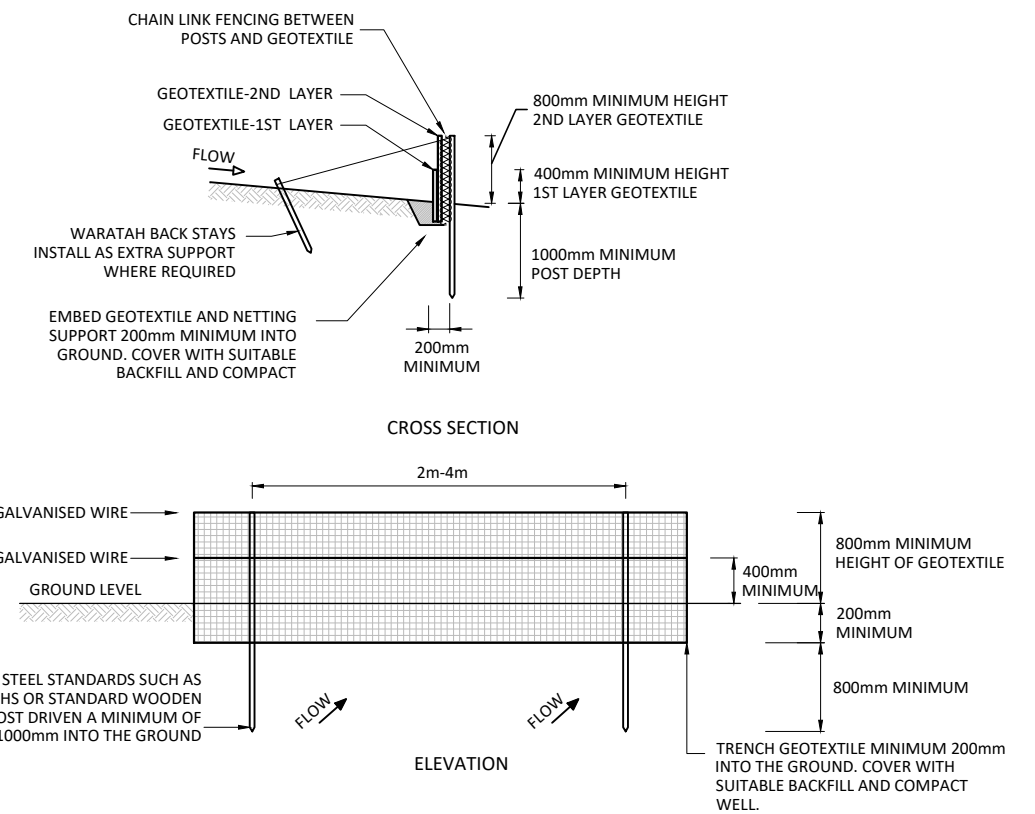
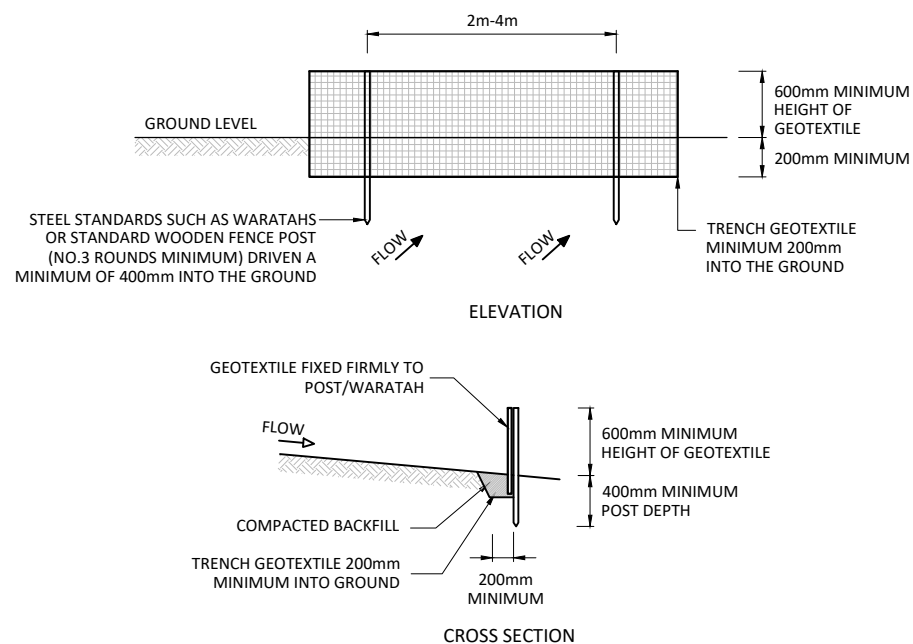
SILT FENCE DESIGN CRITERIA:

SLOPE STEEPNESS %	SLOPE LENGTH (m) (MAXIMUM)	SPACING OF RETURNS (m)	SILT FENCE LENGTH (m) (MAXIMUM)
< 2%	UNLIMITED	N/A	UNLIMITED
2-10%	40	60	300
10-20%	30	50	230
20-33%	20	40	150
33-50%	15	30	75
>50%	6	20	40

GRAB TENSILE STRENGTH: >440N (ASTM D4632)
 TENSILE MODULUS: 0.140 pa (MINIMUM)
 APPARENT OPENING SIZE: 0.1-0.5mm (ASTM D4751)



SILT FENCE CONSTRUCTION



SUPER SILT FENCE CONSTRUCTION

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REV	REVISION DETAILS		

DRAWN	DATE
MM	03.2019
CHECKED	DATE
MSS	05.06.19
APPROVED	DATE
MSS	05.06.19

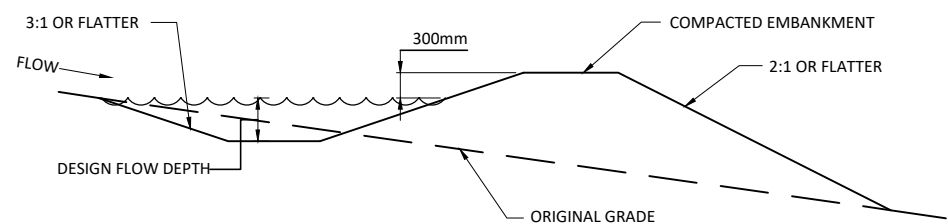
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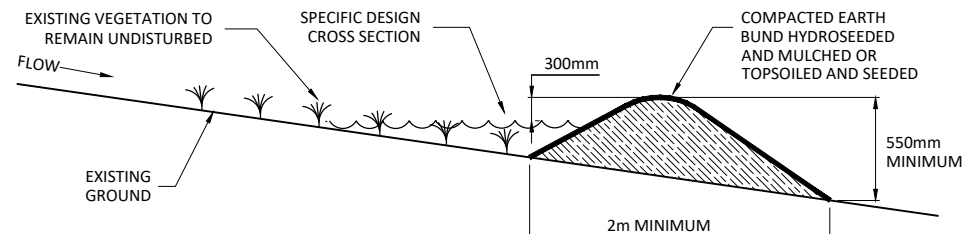
PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
 SEDIMENT AND EROSION CONTROL
 STANDARD DETAILS-SHEET 1**

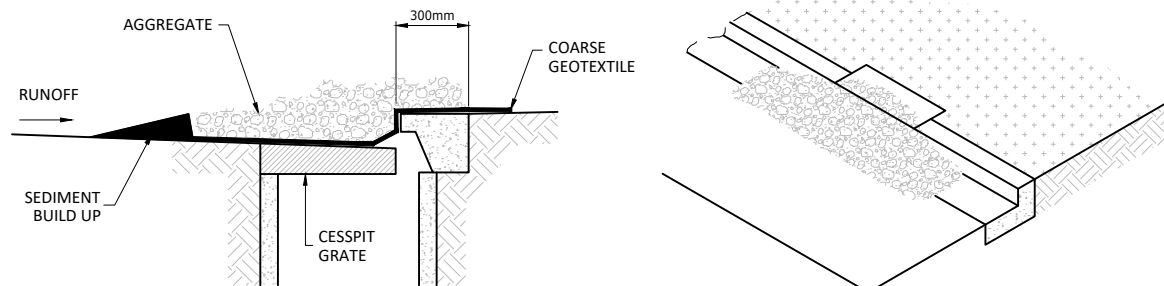
ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) **NOT TO SCALE**
 SCALE BAR
 N.T.S.
 DRAWING NUMBER: **2094-01-235**
 REV: **C2**



DIRTY WATER DIVERSION - CROSS SECTION



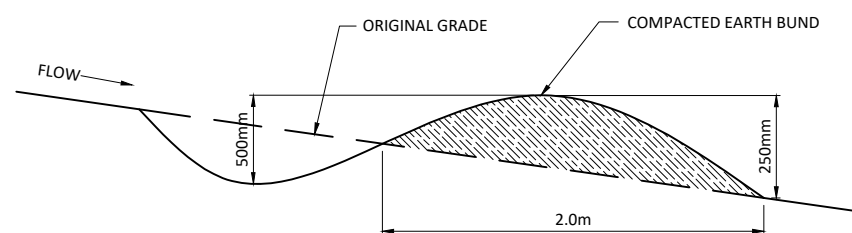
CLEANWATER DIVERSION - CROSS SECTION



CROSS SECTION

PERSPECTIVE VIEW

STORMWATER INLET PROTECTION



CONTOUR DRAIN

STABILISED CONSTRUCTION ENTRANCE:

APPLICATION

- USE A STABILISED CONSTRUCTION ENTRANCE AT ALL POINTS OF CONSTRUCTION SITE INGRESS AND EGRESS WITH A CONSTRUCTION PLAN LIMITING TRAFFIC TO THESE ENTRANCES ONLY.

DESIGN

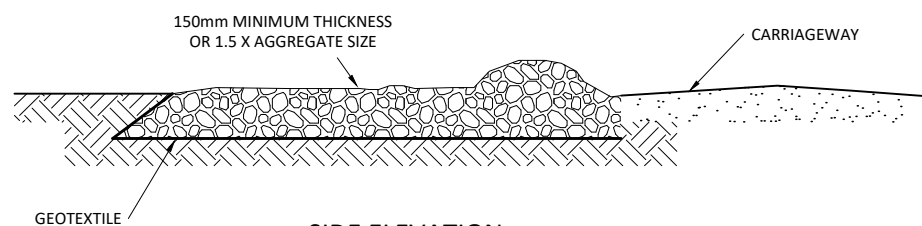
- CLEAR THE ENTRANCE AND EXIT AREA OF ALL VEGETATION, ROOTS AND OTHER UNSUITABLE MATERIAL AND PROPERLY GRADE IT.
- LAY WOVEN GEOTEXTILE; PIN DOWN EDGES AND OVERLAP JOINTS.
- PROVIDE DRAINAGE TO CARRY RUNOFF FROM THE STABILISED CONSTRUCTION ENTRANCE TO A SEDIMENT CONTROL MEASURE.
- PLACE AGGREGATE TO THE SPECIFICATIONS BELOW AND SMOOTH IT.

STABILISED CONSTRUCTION ENTRANCE AGGREGATE SPECIFICATIONS:

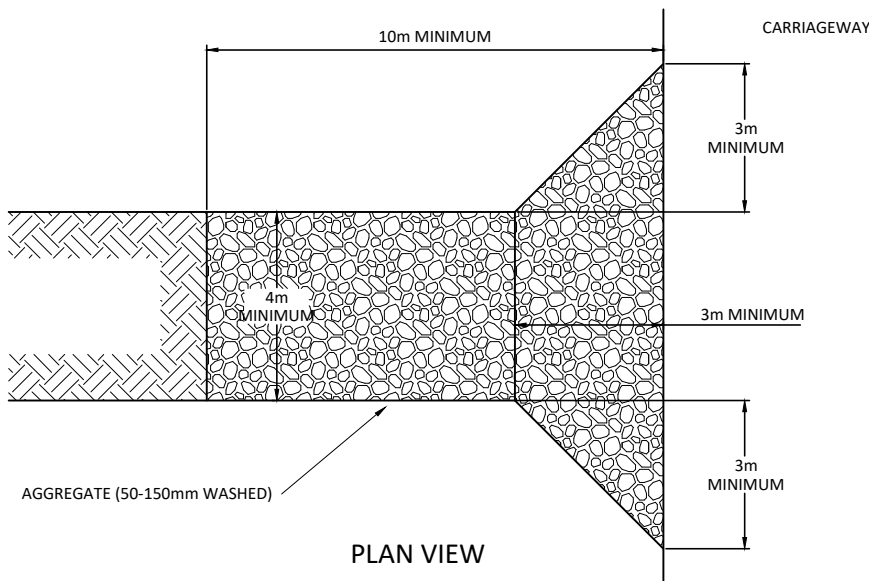
AGGREGATE SIZE	50-150mm WASHED AGGREGATE
THICKNESS	150mm MINIMUM OR 1.5 X AGGREGATE SIZE
LENGTH	10m MINIMUM LENGTH RECOMMENDED
WIDTH	4m MINIMUM WIDTH

MAINTENANCE

- MAINTAIN THE STABILISED CONSTRUCTION ENTRANCE IN A CONDITION TO PREVENT SEDIMENT FROM LEAVING THE CONSTRUCTION SITE. AFTER EACH RAINFALL INSPECT ANY STRUCTURE USED TO TRAP SEDIMENT FROM THE STABILISED CONSTRUCTION ENTRANCE AND CLEAN OUT AS NECESSARY.
- WHEN WHEEL WASHING IS ALSO REQUIRED, ENSURE THIS IS DONE ON AN AREA STABILISED WITH AGGREGATE WHICH DRAINS TO AN APPROVED SEDIMENT RETENTION FACILITY.

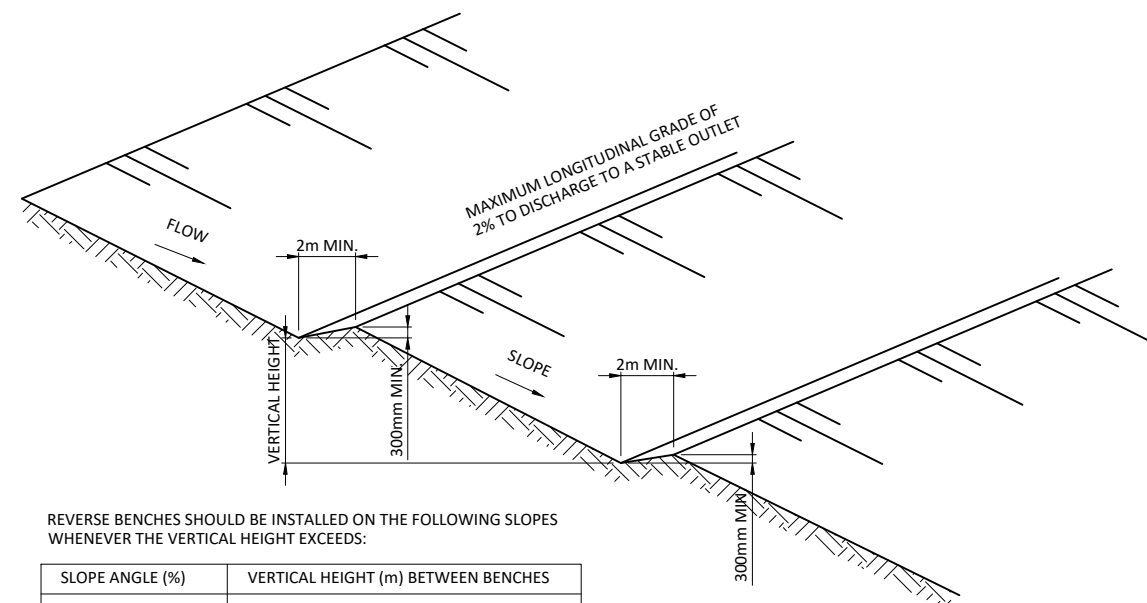


SIDE ELEVATION



PLAN VIEW

STABILISED CONSTRUCTION ENTRANCE



REVERSE BENCHES SHOULD BE INSTALLED ON THE FOLLOWING SLOPES WHENEVER THE VERTICAL HEIGHT EXCEEDS:

SLOPE ANGLE (%)	VERTICAL HEIGHT (m) BETWEEN BENCHES
50	10
33	15
25	20

BENCHED SLOPE

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MSS	05.06.19		
MSS	05.06.19		
C2	ISSUED FOR CONSENT	MSS	26.06.19
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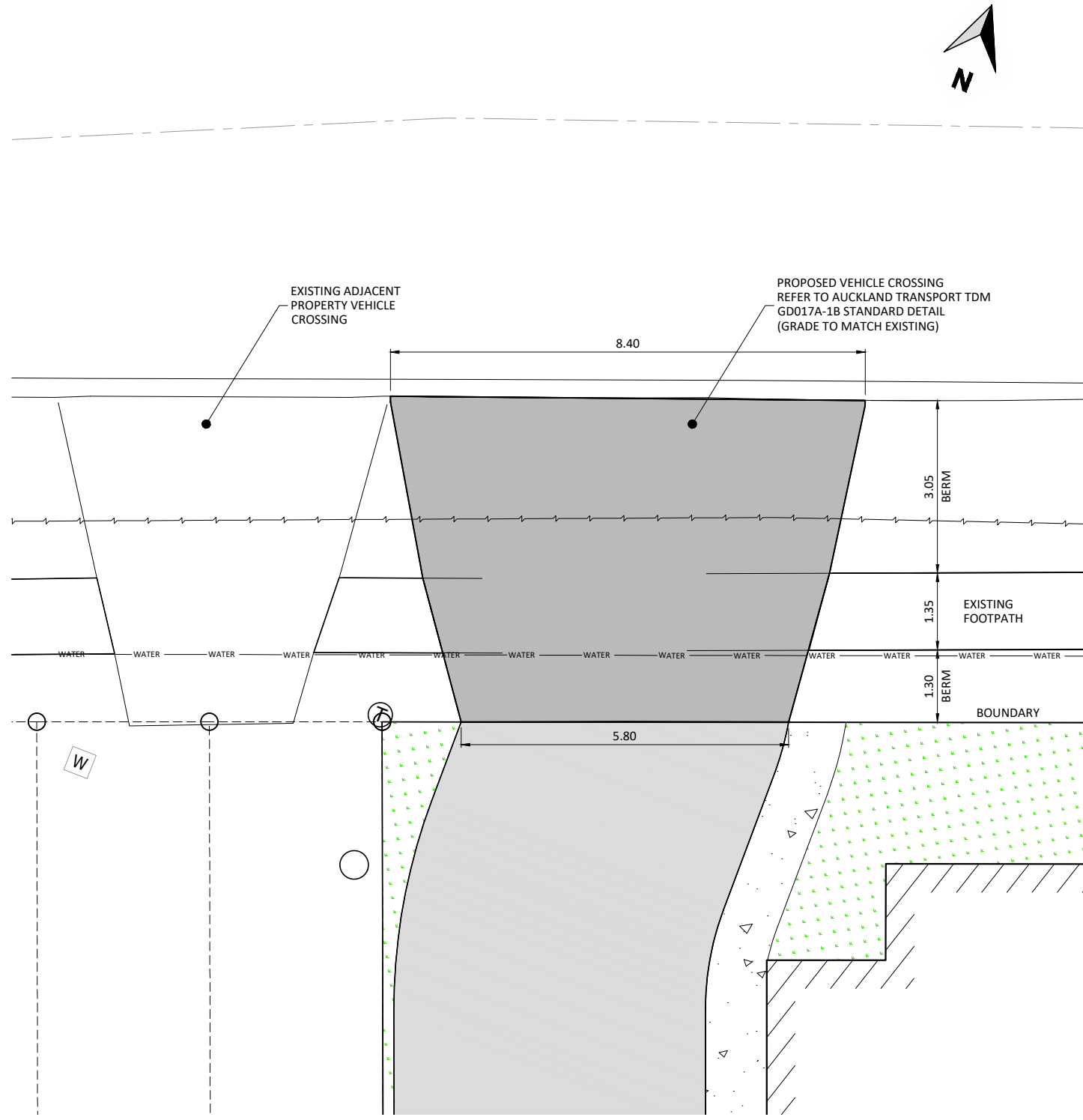
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED EARTHWORKS
SEDIMENT AND EROSION CONTROL
STANDARD DETAILS-SHEET 2**

ISSUE STATUS: **CONSENT**
SCALE: (A1/A3) **NOT TO SCALE**
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VEHICLE CROSSING PLAN
SCALE: (A1/A3) 1:50/1:100

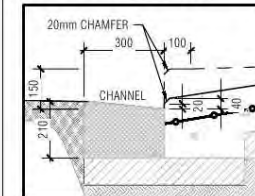
ORIGINAL SIZE A4 DO NOT SCALE



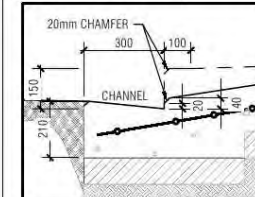
3D VIEW
N.T.S.

GD017A-1B

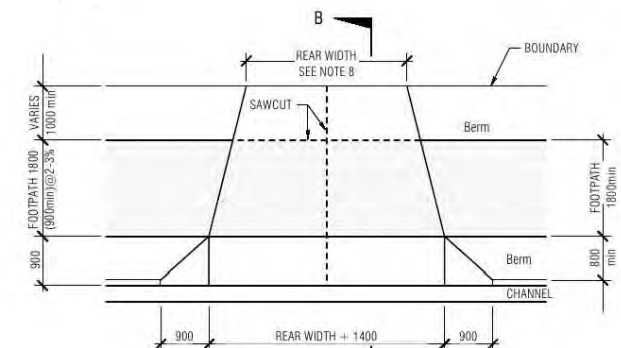
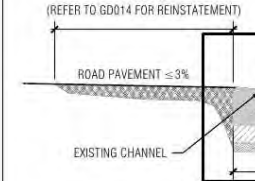
KEEP EXISTING CHANNEL



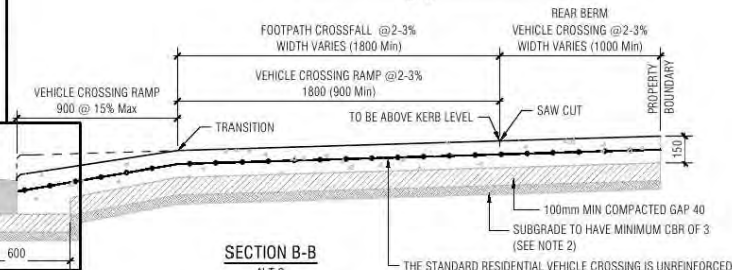
REBUILD NEW CHANNEL



REINSTATE ROAD PAVEMENT
1000 MIN
(REFER TO GD014 FOR REINSTATEMENT)



VEHICLE CROSSING
FOOTPATH SEPARATED FROM KERB
N.T.S.



Notes:

- All dimensions are in millimetres unless noted otherwise.
- If CBR of existing Subgrade is < 3. Pavement Design should be provided and approved by Auckland Transport.
- All concrete to be 20 Mpa and constructed in accordance with NZS 3109 with a broom finish and may contain upto 4% oxide.
- Saw cut expansion joints at 4m centres maximum each way in addition to saw cuts shown on dwg.
- All work must comply with the NZTA's 'CoPTM' (code of practice for temporary traffic management).
- Construct in same material and finish as surrounding footpath.
- Existing channel may be retained if:
 - kerb can be removed without disturbing channel
 - road crossfall does not exceed 3%
- Rear Width to be as permitted under Auckland unitary Plan:
 - 2750-3000 - Single vehicle crossing
 - 5500-6000 - Two-Way Shared Access
 - 3000-3500 - One-Way Shared Access

GO TO COVER SHEET



Drawn: Ulysses Gabriel
Checked: Richard Batty
Approved: Chris Beasley
Authorised: Chief Engineer

TDM TECHNICAL STANDARDS
Road layout and geometric design
Residential Vehicle Crossing (Sheet 3 of 4)

Date: -
Scale: N.T.S.
Drawing No: GD017A-1B

C2	ISSUED FOR CONSENT	MSS	26.06.19
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MSS	05.06.19	
APPROVED:	DATE:	
MSS	05.06.19	

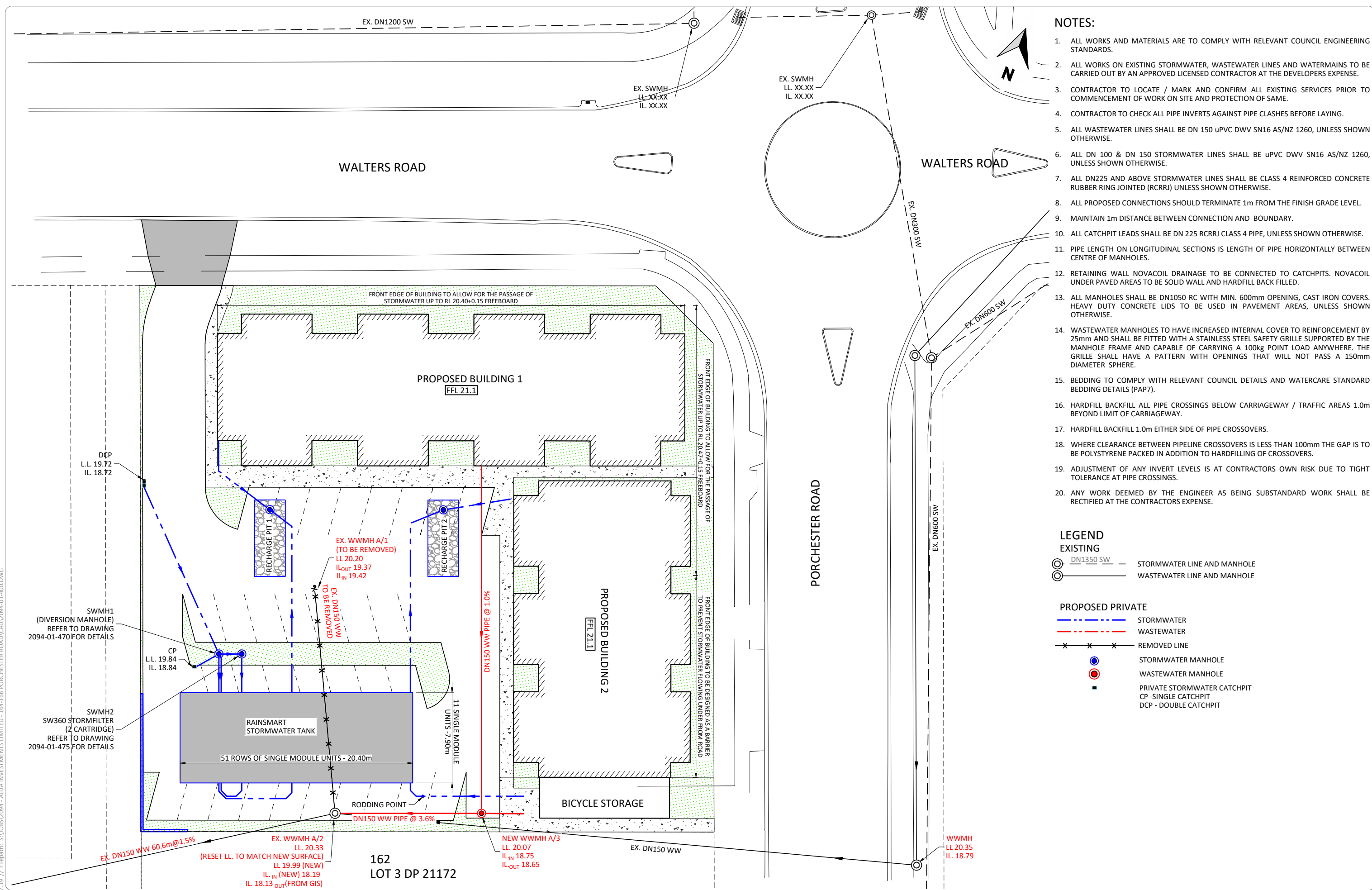
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PROJECT TITLE:
ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

SHEET TITLE:
VEHICLE CROSSING
PLAN AND DETAIL

ISSUE STATUS: **CONSENT**
SCALE: (A1/A3) 1:50 / 1:100
SCALE BAR: 0 1 2 3 4 5m
DRAWING NUMBER: 2094-01-370
REV: C2



- NOTES:**
- ALL WORKS AND MATERIALS ARE TO COMPLY WITH RELEVANT COUNCIL ENGINEERING STANDARDS.
 - ALL WORKS ON EXISTING STORMWATER, WASTEWATER LINES AND WATERMANS TO BE CARRIED OUT BY AN APPROVED LICENSED CONTRACTOR AT THE DEVELOPERS EXPENSE.
 - CONTRACTOR TO LOCATE / MARK AND CONFIRM ALL EXISTING SERVICES PRIOR TO COMMENCEMENT OF WORK ON SITE AND PROTECTION OF SAME.
 - CONTRACTOR TO CHECK ALL PIPE INVERTS AGAINST PIPE CLASHES BEFORE LAYING.
 - ALL WASTEWATER LINES SHALL BE DN 150 uPVC DWV SN16 AS/NZ 1260, UNLESS SHOWN OTHERWISE.
 - ALL DN 100 & DN 150 STORMWATER LINES SHALL BE uPVC DWV SN16 AS/NZ 1260, UNLESS SHOWN OTHERWISE.
 - ALL DN225 AND ABOVE STORMWATER LINES SHALL BE CLASS 4 REINFORCED CONCRETE RUBBER RING JOINTED (RCRRJ) UNLESS SHOWN OTHERWISE.
 - ALL PROPOSED CONNECTIONS SHOULD TERMINATE 1m FROM THE FINISH GRADE LEVEL.
 - MAINTAIN 1m DISTANCE BETWEEN CONNECTION AND BOUNDARY.
 - ALL CATCHPIT LEADS SHALL BE DN 225 RCRRJ CLASS 4 PIPE, UNLESS SHOWN OTHERWISE.
 - PIPE LENGTH ON LONGITUDINAL SECTIONS IS LENGTH OF PIPE HORIZONTALLY BETWEEN CENTRE OF MANHOLES.
 - RETAINING WALL NOVA COIL DRAINAGE TO BE CONNECTED TO CATCHPITS. NOVA COIL UNDER PAVED AREAS TO BE SOLID WALL AND HARDFILL BACK FILLED.
 - ALL MANHOLES SHALL BE DN1050 RC WITH MIN. 600mm OPENING, CAST IRON COVERS. HEAVY DUTY CONCRETE LIDS TO BE USED IN PAVEMENT AREAS, UNLESS SHOWN OTHERWISE.
 - WASTEWATER MANHOLES TO HAVE INCREASED INTERNAL COVER TO REINFORCEMENT BY 25mm AND SHALL BE FITTED WITH A STAINLESS STEEL SAFETY GRILLE SUPPORTED BY THE MANHOLE FRAME AND CAPABLE OF CARRYING A 100kg POINT LOAD ANYWHERE. THE GRILLE SHALL HAVE A PATTERN WITH OPENINGS THAT WILL NOT PASS A 150mm DIAMETER SPHERE.
 - BEDDING TO COMPLY WITH RELEVANT COUNCIL DETAILS AND WATERCARE STANDARD BEDDING DETAILS (PAP7).
 - HARDFILL BACKFILL ALL PIPE CROSSINGS BELOW CARRIAGEWAY / TRAFFIC AREAS 1.0m BEYOND LIMIT OF CARRIAGEWAY.
 - HARDFILL BACKFILL 1.0m EITHER SIDE OF PIPE CROSSOVERS.
 - WHERE CLEARANCE BETWEEN PIPELINE CROSSOVERS IS LESS THAN 100mm THE GAP IS TO BE POLYSTYRENE PACKED IN ADDITION TO HARDFILLING OF CROSSOVERS.
 - ADJUSTMENT OF ANY INVERT LEVELS IS AT CONTRACTORS OWN RISK DUE TO TIGHT TOLERANCE AT PIPE CROSSINGS.
 - ANY WORK DEEMED BY THE ENGINEER AS BEING SUBSTANDARD WORK SHALL BE RECTIFIED AT THE CONTRACTORS EXPENSE.

- LEGEND**
- EXISTING**
- DN1350 SW — STORMWATER LINE AND MANHOLE
 - WASTEWATER LINE AND MANHOLE
- PROPOSED PRIVATE**
- STORMWATER
 - WASTEWATER
 - REMOVED LINE
 - STORMWATER MANHOLE
 - WASTEWATER MANHOLE
 - PRIVATE STORMWATER CATCHPIT
 - CP - SINGLE CATCHPIT
 - DCP - DOUBLE CATCHPIT

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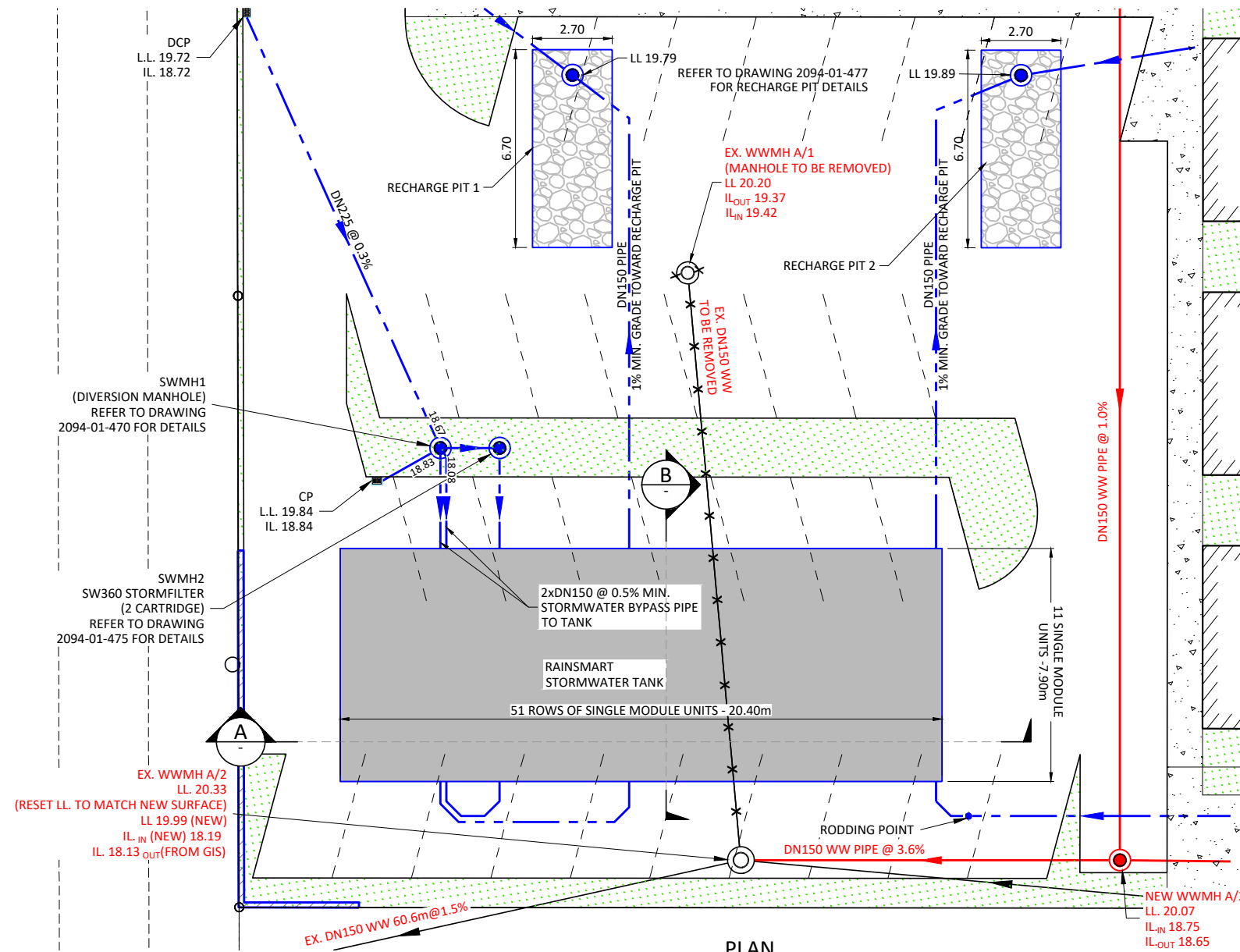
SHEET TITLE:
PROPOSED DRAINAGE
OVERALL LAYOUT PLAN

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) **1:150 / 1:300**

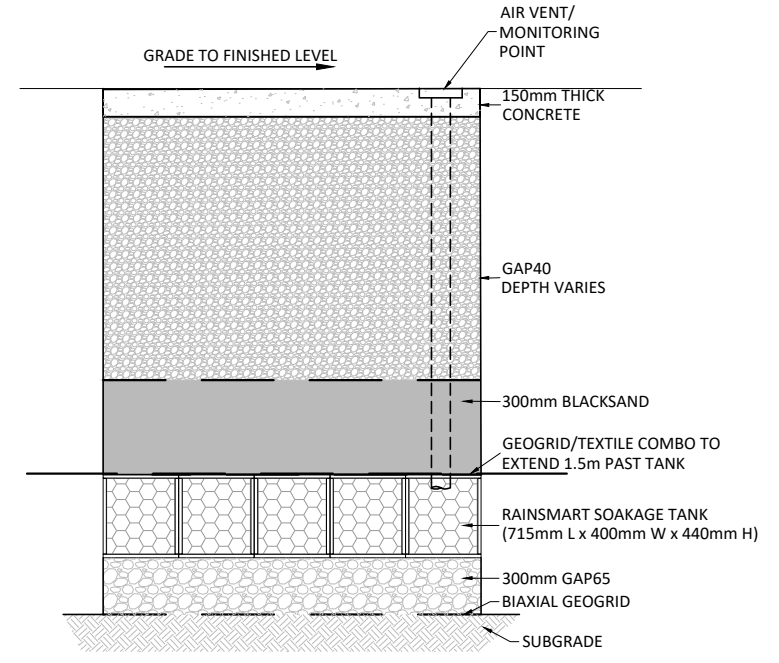
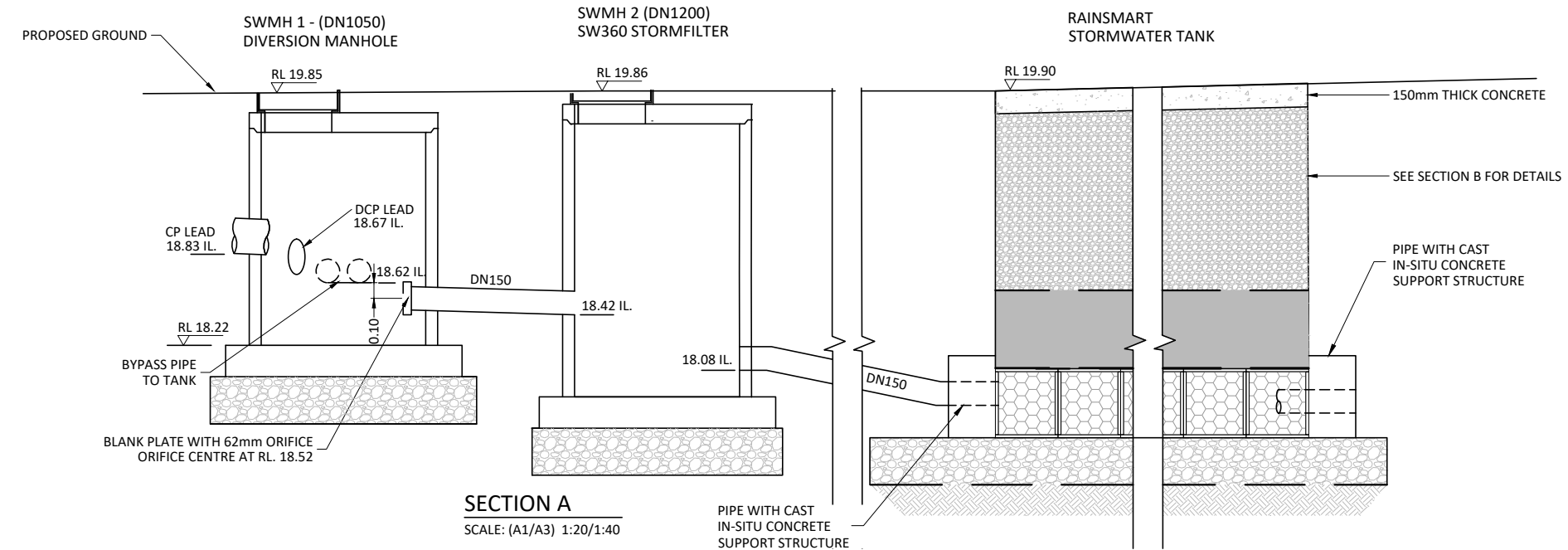
SCALE BAR: 0 3 6 9 12 15m

DRAWING NUMBER: **2094-01-400** REV: **C3**



PLAN
SCALE: (A1/A3) 1:100/1:200

- NOTES:**
1. ALL STORAGE TANK DRAINAGE WORKS SHALL BE CARRIED OUT IN ACCORDANCE WITH THE SPECIFICATIONS, METHOD STATEMENTS AND RECOMMENDATIONS OF RAINSMART SOLUTIONS PTY.
 2. ALL LEVELS AND DIMENSIONS SHALL BE VERIFIED ON SITE PRIOR TO THE COMMENCEMENT OF ANY WORKS. ANY DISCREPANCIES SHALL BE IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER.



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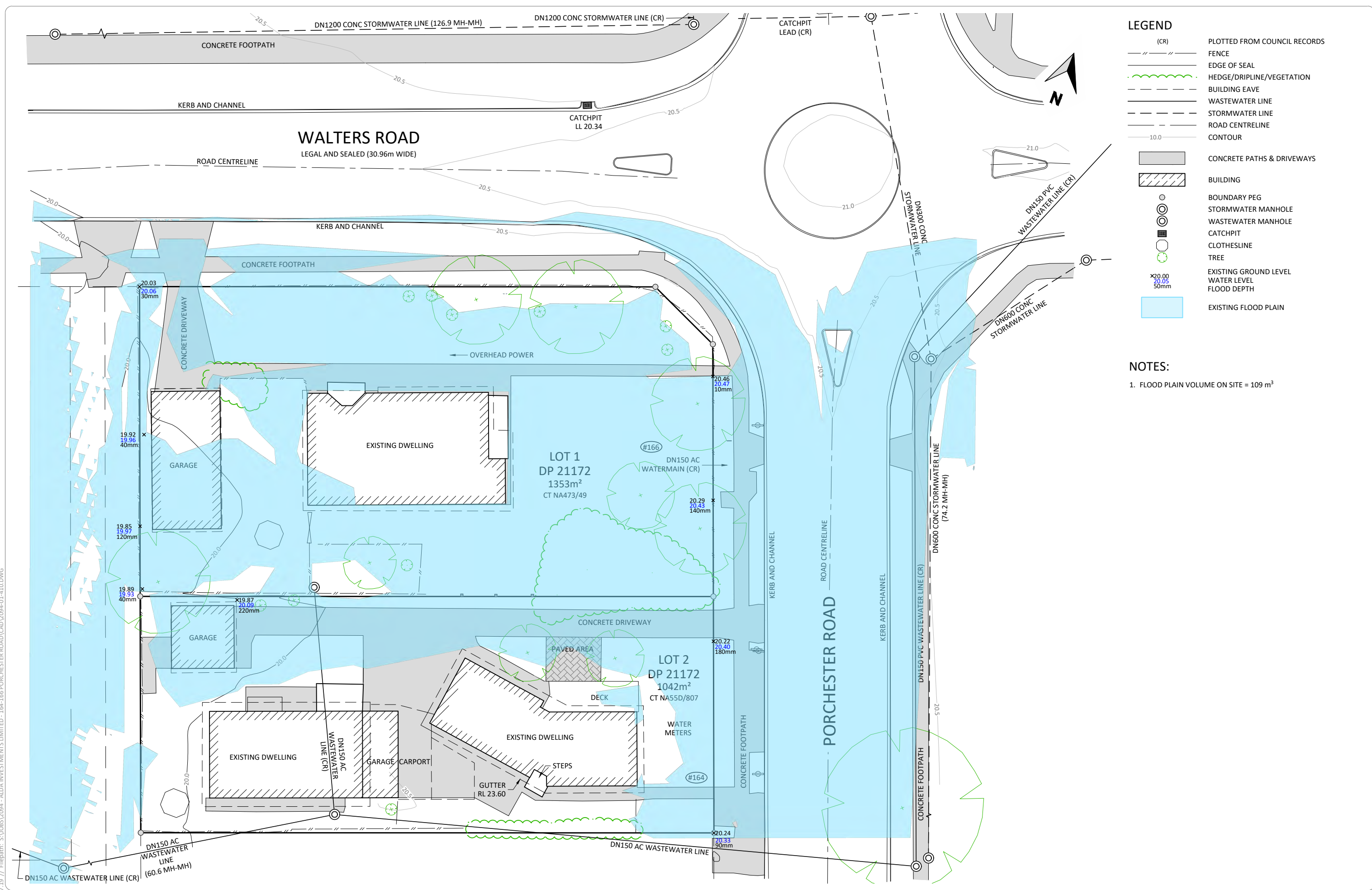
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PROJECT TITLE:
**ALDA INVESTMENTS LIMITED
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TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED DRAINAGE
DETAIL PLAN**

ISSUE STATUS: **CONSENT**
SCALE: (A1/A3) **AS SHOWN**
SCALE BAR
DRAWING NUMBER: **2094-01-401** REV: **C2**



LEGEND

- (CR) PLOTTED FROM COUNCIL RECORDS
- FENCE
- EDGE OF SEAL
- HEDGE/DRIPLINE/VEGETATION
- BUILDING EAVE
- WASTEWATER LINE
- STORMWATER LINE
- ROAD CENTRELINE
- 10.0 CONTOUR
- CONCRETE PATHS & DRIVEWAYS
- BUILDING
- BOUNDARY PEG
- STORMWATER MANHOLE
- WASTEWATER MANHOLE
- CATCHPIT
- CLOTHESLINE
- TREE
- EXISTING GROUND LEVEL
- WATER LEVEL
- FLOOD DEPTH
- EXISTING FLOOD PLAIN

NOTES:

1. FLOOD PLAIN VOLUME ON SITE = 109 m³

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MM	03.2019
CHECKED	DATE
MSS	05.06.19
APPROVED	DATE
MSS	05.06.19

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TAKANINI, AUCKLAND

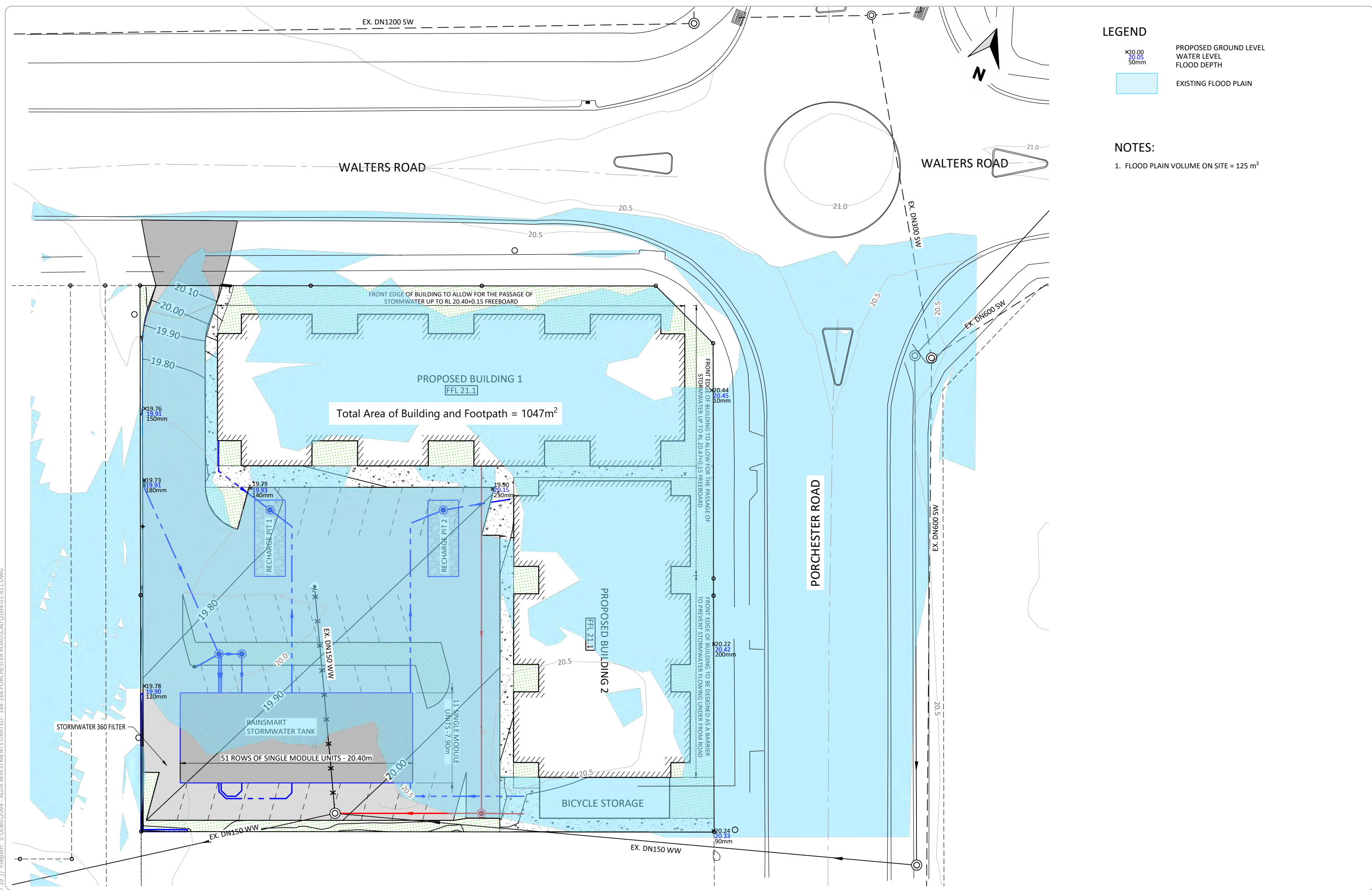
SHEET TITLE:
EXISTING FLOODPLAIN
100 YR WITH CLIMATE CHANGE

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR 1:300@A3 0 3 6 9 12 15m

DRAWING NUMBER: 2094-01-410
REV: C2



LEGEND

- x20.00
20.05
50mm
- PROPOSED GROUND LEVEL
- WATER LEVEL
- FLOOD DEPTH
- EXISTING FLOOD PLAIN

NOTES:

- FLOOD PLAIN VOLUME ON SITE = 125 m³

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C3	ISSUED FOR CONSENT	MSS	29.07.19
C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19

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PROJECT TITLE:
ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

SHEET TITLE:
PROPOSED FLOODPLAIN
100 YR WITH CLIMATE CHANGE

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) 1:150 / 1:300

SCALE BAR 1:300@A3

DRAWING NUMBER: 2094-01-411-444 REV: C3

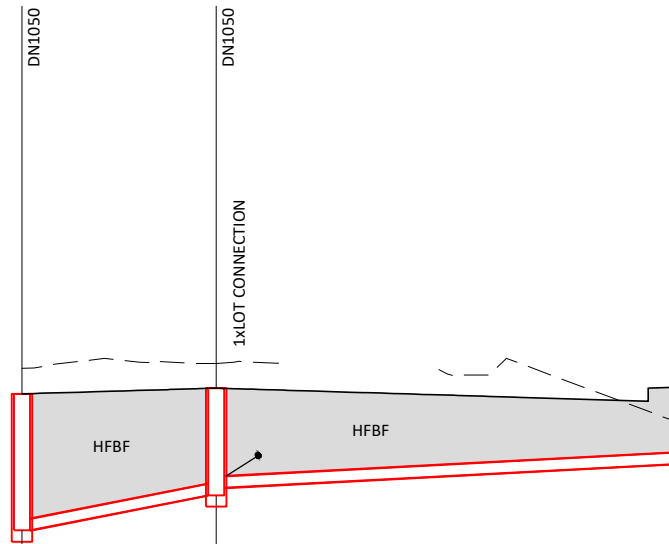
LEGEND

- EXISTING GROUND LINE
- PROPOSED GROUND LINE
- HFBF PROPOSED HARDFILL BACKFILL
- PROPOSED LOT CONNECTIONS
- INTERNAL DROP CONNECTION

NOTES:

1. REFER TO SHEET 2094-01-400 FOR GENERAL NOTES.
2. LOT CONNECTIONS TO TERMINATE 1m FROM PROPOSED GROUND LINE
3. LENGTHS OF RAMP RISERS ARE STATED IN VERTICAL LENGTH OF PIPE. CONTRACTOR TO ENSURE CORRECT LENGTH OF PIPE IS USED SO THAT THE CONNECTION TERMINATES WITHIN THE APPROPRIATE LOT BOUNDARY.

EX.WWMH. A/2 NEW WWMH. A/3



DATUM	14.0		
EXISTING GROUND LEVEL	20.33	20.39	19.62
LID LEVEL	19.99	20.07	20.08
INVERT LEVEL	18.19	18.65	19.05
DEPTH TO INVERT	1.80	1.42	1.03
GRADIENT & PIPE SIZES	3.60% DN150uPVC SN16		1.00% DN150uPVC SN16 LOT CONNECTION
DISTANCE (m)	12.85	30.46	

WASTEWATER LONGSECTION

Printed by: ALUSTAIR@04.07.19 // Filepath: S:\JOBS\2094 - ALDA INVESTMENTS LIMITED - 164-166 PORCHESTER ROAD\CAD\2094-01-450.DWG

C2	ISSUED FOR CONSENT	MSS	26.06.19
C1	ISSUED FOR CONSENT	MSS	05.06.19
REV	REVISION DETAILS	BY	DATE

DRAWN:	MM	DATE:	03.2019
CHECKED:	MSS	DATE:	05.06.19
APPROVED:	MSS	DATE:	05.06.19

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CIVILPLAN
CONSULTANTS

Level 3, 3 Osterley Way, Manukau, Auckland. Phone: 09 222 2445

PROJECT TITLE:

ALDA INVESTMENTS LIMITED
164-166 PORCHESTER ROAD
TAKANINI, AUCKLAND

SHEET TITLE:

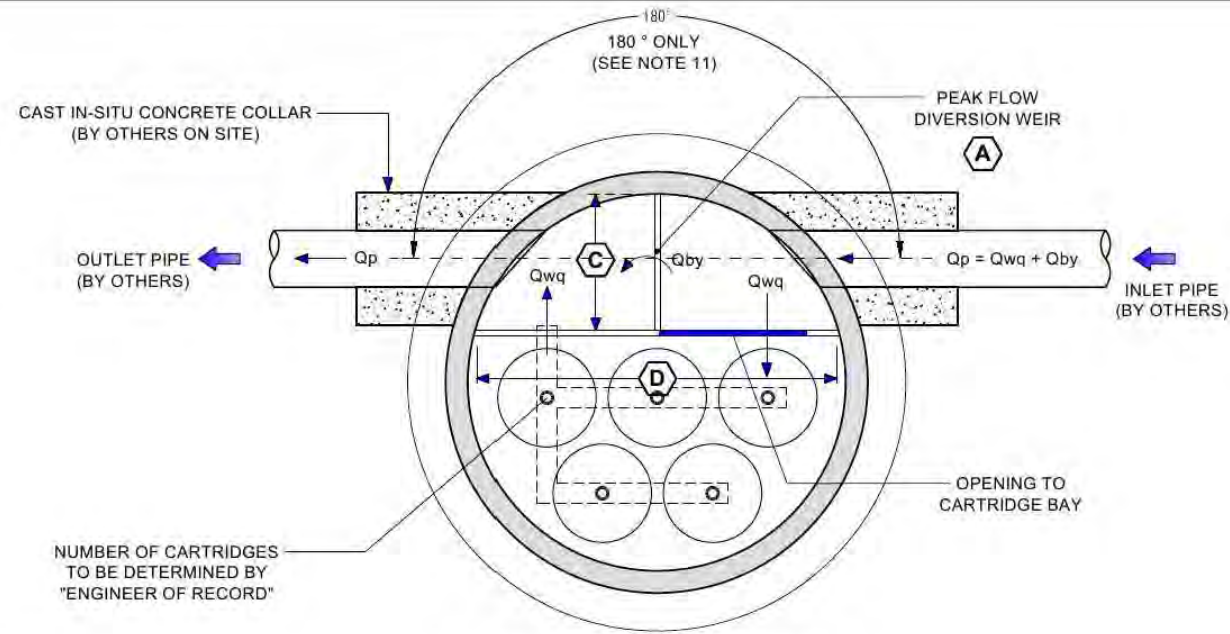
PROPOSED DRAINAGE
WASTEWATER LONGITUDINAL SECTIONS

ISSUE STATUS: CONSENT

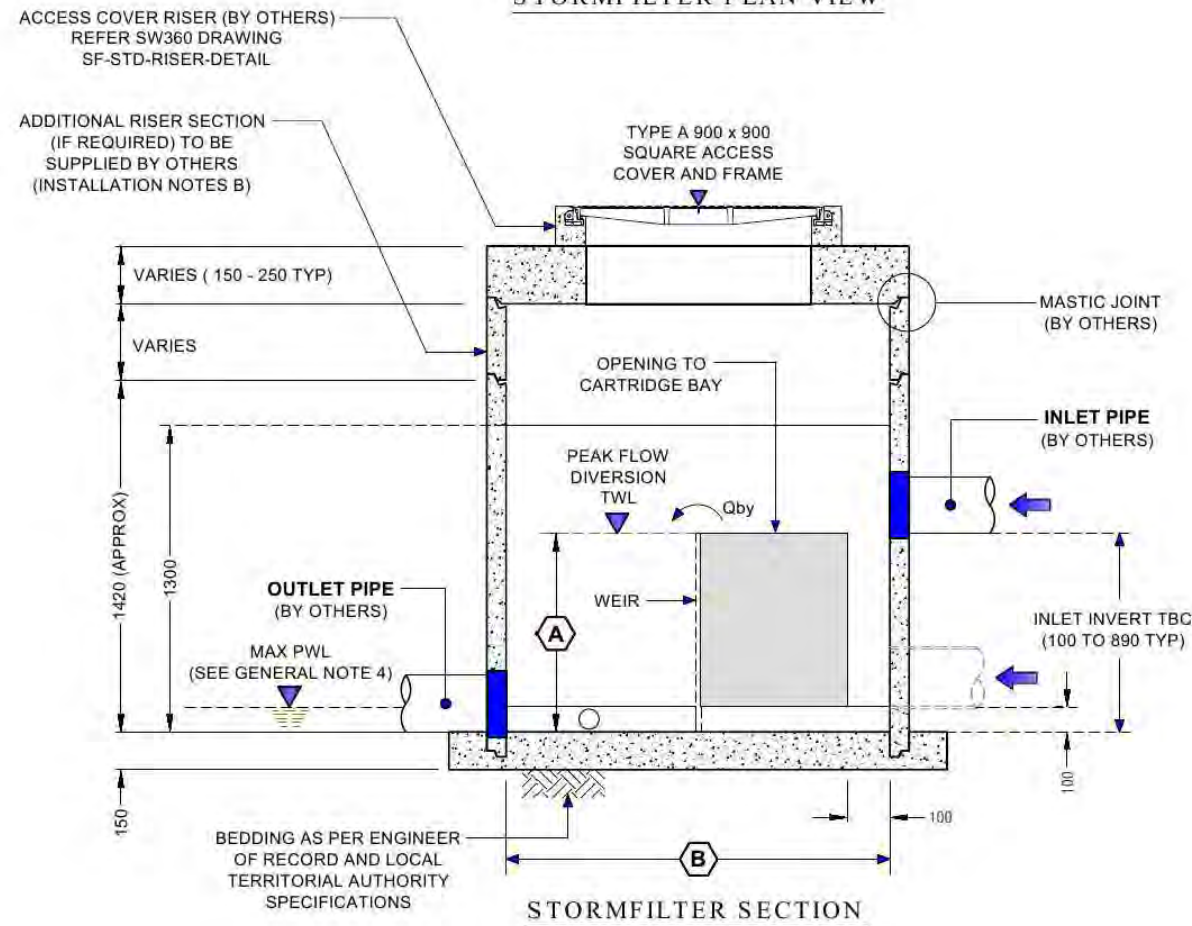
SCALE: (A1/A3) 1:250H 1:50V / 1:500H 1:100V

SCALE BAR
N.T.S.

DRAWING NUMBER: **2094-01-450** REV: **C2**



STORMFILTER PLAN VIEW



STORMFILTER SECTION

STORMFILTER DESIGN NOTES

STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. THE PEAK DIVERSION MODEL INTEGRATES AN "OFFLINE" BYPASS WEIR WITHIN THE STORMFILTER MANHOLE. THE STORMFILTER RATED MAXIMUM TREATMENT CAPACITY PER CARTRIDGE IS SHOWN IN THE TABLE BELOW. MAXIMUM HYDRAULIC INTERNAL BYPASS @ 200 mm HYDRAULIC HEAD IS SHOWN IN THE TABLE BELOW. SITE SPECIFIC PEAK CONVEYANCE CAPACITY TO BE DETERMINED BY ENGINEER OF RECORD. CONTACT YOUR STORMWATER CONSULTANT FOR ADDITIONAL INFORMATION

CARTRIDGE HEIGHT (cm)	69	46	30 (LOW DROP)			
RECOMMENDED HYDRAULIC DROP (mm) (A)	890	660	500			
SPECIFIC FLOW RATE (L/s/m ²)	1.40	0.70	1.40	0.70		
CARTRIDGE FLOW RATE (L/s)	1.42	0.71	0.95	0.475	0.63	0.315

STORMFILTER STRUCTURE MODEL	SFMHPD1015	SFMHPD1215	SFMHPD1515	SFMHPD1815	SFMHPD2015	SFMHPD2315
MAXIMUM No. OF CARTRIDGES	1	2	3	5	6	9
MAX. TREATMENT FLOW RATE Qwq (L/s)	1.42	2.84	4.26	7.10	8.56	12.78
(B) CHAMBER DIAMETER	Ø 1050	Ø 1200	Ø 1500	Ø 1800	Ø 2050	Ø 2300
(C) BYPASS WEIR LENGTH*	400	430	560	650	700	750
(D) BYPASS BAFFLE LENGTH*	1035	1165	1420	1750	1930	2150
BYPASS FLOW, Qby @ 200mm ABOVE WL*	65 L/S	70 L/S	90 L/S	105 L/S	110 L/S	120 L/S

*CUSTOM PEAK DIVERSION CONFIGURATIONS ARE AVAILABLE WHERE GREATER BYPASS FLOWS ARE REQUIRED. CONTACT A SW360 STORMWATER CONSULTANT FOR MORE INFORMATION.

SITE SPECIFIC DATA REQUIREMENTS

STRUCTURE ID	
CATCHMENT AREA	940m ²
WATER QUALITY FLOW RATE - Qwq (L/s)	2.6 L/s
PEAK FLOW RATE - Qp (L/s)	N.A. (EXTERNAL BYPASS PROVIDED)
RETURN PERIOD OF PEAK FLOW (yrs)	N.A. (EXTERNAL BYPASS PROVIDED)
# OF CARTRIDGES REQUIRED x CARTRIDGE HEIGHT (ie 1 x 69 cm)	2 X 69 cm
CARTRIDGE FLOW RATE	1.3 L/s
MEDIA TYPE (ZEO, PER, ZPG, PHS)	PER

PIPE DATA:	R.L.	MATERIAL	DIAMETER
INLET PIPE	18.42	PVC	150
OUTLET PIPE	18.08	PVC	150
LID LEVEL		N/A	N/A

AS PER ENGINEER OF RECORD

GENERAL NOTES :

1. STORMWATER360 TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
2. FOR SITE SPECIFIC DRAWINGS WITH DETAILED STRUCTURE DIMENSIONS AND WEIGHT, PLEASE CONTACT YOUR SW360 STORMWATER CONSULTANT VIA www.stormwater360.co.nz, OR 0800 STORMWATER, OR sales@stormwater360.co.nz.
3. T.W.L. = TREATMENT WATER LEVEL.
4. THE MAXIMUM DOWNSTREAM PERMANENT WATER LEVEL (PWL) IS TO BE SET AT OR BELOW THE BASE OF THE CARTRIDGES. TYPICALLY 100 mm ABOVE OUTLET INVERT.
5. PEAK DIVERSION WEIR CORRESPONDS TO RECOMMENDED HYDRAULIC DROP (A) ON TABLE ABOVE.
6. STRUCTURE SHALL MEET NZTA'S HN-HO-72 OR PER APPROVING JURISDICTION TRAFFICKED LOAD REQUIREMENTS, WHICHEVER IS MORE STRINGENT. COVER AND FRAME ARE TO BE RATED TO EITHER CLASS B (FOR PEDESTRIAN AREAS) OR CLASS D (TRAFFICKED ROADS) IN ACCORDANCE WITH AS 3996 : 2006.
7. STRUCTURE SHALL BE PRECAST CONCRETE CONFORMING TO NZS 3109 : 1997, NZS 3114 : 1987 AND AS/NZS 4058 : 2007.
8. FILTER CARTRIDGES SHALL BE MEDIA-FILLED, PASSIVE, SIPHON ACTUATED, RADIAL FLOW, AND SELF CLEANING. RADIAL MEDIA DEPTH SHALL BE 178 mm. FILTER MEDIA CONTACT TIME SHALL BE AT LEAST 39 SECONDS.
9. SPECIFIC FLOW RATE IS EQUAL TO THE FILTER TREATMENT CAPACITY (L/s) DIVIDED BY THE FILTER CONTACT SURFACE AREA (m²).
10. MINIMUM INVERT DIFFERENCE BETWEEN INLET PIPE AND OUTLET PIPE IS 100 mm.
11. DEVICE SUITABLE FOR INSTALLATION ON STRAIGHT (180°) PIPE RUNS. ALTERNATIVE PIPE ANGLES ARE NOT SUITABLE. CONTACT SW360 FOR ALTERNATE OPTIONS.
12. NO PRODUCT SUBSTITUTIONS SHALL BE ACCEPTED UNLESS SUBMITTED 10 DAYS PRIOR TO PROJECT BID DATE, OR AS DIRECTED BY THE ENGINEER OF RECORD.

INSTALLATION NOTES :

- A. SIZE AND CLASS OF PIPE OR SQUARE KNOCKOUT SIZE TO BE SPECIFIED ON DRAWING BY CLIENT / CONTRACTOR.
- B. ADDITIONAL RISERS TO BE FORMED ON SITE BY CONTRACTOR (IF REQUIRED).
- C. ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- D. CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STORMFILTER STRUCTURE (LIFTING CLUTCHES PROVIDED).
- E. CONTRACTOR TO INSTALL JOINT SEALANT BETWEEN ALL STRUCTURE SECTIONS AND ASSEMBLE STRUCTURE.
- F. CONTRACTOR TO PROVIDE, INSTALL, AND GROUT INLET AND OUTLET PIPES. STRAIGHT PIPES TO BE INSTALLED WITH 180° ANGLES ONLY. A CAST IN-SITU CONCRETE COLLAR IS TO BE POURED ON SITE BY CONTRACTOR AS SHOWN ON DRAWINGS.
- G. CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.



0800 STORMWATER
sales@stormwater360.co.nz
www.stormwater360.co.nz

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STORMFILTER PEAK DIVERSION MANHOLE CONFIGURATION GENERAL ARRANGEMENT
 DRAWING 1
 JOB NO :
 PROJECT :
 DEVICE # :
 DRN : R.P. 30.04.18
 CKD : T.B. 30.04.18
 SCALE : XXX DRG No : SF-STD-MHPD-ALL

Printed by: ALUSTAIR@04.07.19 // Filepath: S:\Jobs\2094 - ALDA INVESTMENTS LIMITED - 164-166 PORCHESTER ROAD\CAD\2094-01-475.DWG

NO	REVISION	DATE
C2	ISSUED FOR CONSENT	MSS 26.06.19
C1	ISSUED FOR CONSENT	MSS 05.06.19
REV	REVISION DETAILS	BY DATE

DRAWN: MM 03.2019
 CHECKED: MSS 05.06.19
 APPROVED: MSS 05.06.19
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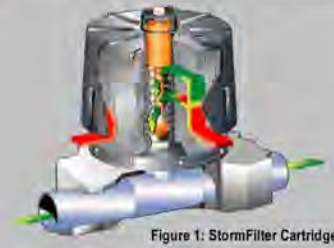
PROJECT TITLE:
 ALDA INVESTMENTS LIMITED
 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND

SHEET TITLE:
 PROPOSED DRAINAGE
 STORMWATER360
 STORMFILTER DETAIL

ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) NOT TO SCALE
 SCALE BAR
 N.T.S.
 DRAWING NUMBER: 2094-01-475
 REV: C2

Project Name **164-166 Porchester Rd, Takanini, Auckland** Location **AKL**
 Job # **5447** Device # **SF1** Option # **A** Revision # **RA**
 Author _____ Date **2 May 2019**

CALCULATIONS - Please Read Instructions First		INSTRUCTIONS																																																																								
1.0 Water Quality Design Storm Peak Runoff Flowrate (RATIONAL METHOD)		1.0 Use the rational method to compute the water quality design storm peak runoff flow rate. Values with blue text require user input. Values in red text are automatically calculated. Values with black text remain constant.																																																																								
1.1 Runoff Co-efficients		1.1 Input the appropriate runoff co-efficient for each sub-catchment.																																																																								
Coefficient of Impervious Roof (Croof)	1.00	Use C=1.0 for impervious roof surfaces runoff co-efficient ¹																																																																								
Coefficient of Impervious Road (Croad)	1.00	Use C=1.0 for impervious paved surfaces runoff co-efficient ¹																																																																								
Coefficient of Pervious Area (Cper)	0.50	Use C=0.4 for pervious surfaces with clay soils runoff co-efficient & C=0.3 for all other pervious surfaces ¹																																																																								
1.2 Catchment Areas		1.2 Input the appropriate catchment area for each sub-catchment.																																																																								
Area Impervious Roof (Aroof)	0 m ²	Enter impervious roof surface catchment area																																																																								
Area Impervious Road (Aroad)	940 m ²	Enter impervious paved surfaces catchment area																																																																								
Area Pervious Area (Aper)	0 m ²	Enter pervious grassed/landscaped surfaces catchment area																																																																								
Area Total Catchment (Acatch)	940 m ²	Total catchment area (i.e. Acatch = Aroof + Aroad + Aper)																																																																								
Product of Area & Coefficients (CA)	940 m ²	Product of catchment areas & runoff co-efficients (i.e. CA = (Croof x Aroof) + (Croad x Aroad) + (Cper x Aper)																																																																								
1.3 Rainfall Intensity		1.3 Input rainfall intensity																																																																								
Water Quality Rainfall Intensity (IWQ)	10 mm/hr	Rainfall intensity of 10mm/hr to be used for StormFilter water quality treatment within Auckland Council boundaries ¹																																																																								
1.4 Water Quality Design Storm Peak Runoff Flowrate		1.4 Compute the water quality design storm peak runoff flow rate via Rational Method																																																																								
Design Water Quality Treatment Flowrate (Qwq)	2.611 L/s	i.e. Q = I.C.A																																																																								
2.0 StormFilter Peak Treatment Flowrate		2.0 Use the stormfilter stage-discharge equation to calculate the StormFilter peak treatment flowrate.																																																																								
2.1 Preliminary		Enter cartridge filtration media (i.e. Perlite or ZPG)																																																																								
Cartridge Media (Media)	Perlite	Enter cartridge height (i.e. 69cm / 46cm / 30cm)																																																																								
Cartridge Height (Hcart)	69 cm	Enter restrictor disc size, refer table below for max disc diameter																																																																								
Diameter Disc Orifice (d)	27.6 mm																																																																									
Internal bypass weir height (Hweir)	0.79 m																																																																									
Priming depth (Hprime)	0.66 m																																																																									
Area of a Cartridge (Acart)	0.181 m ²																																																																									
2.2 StormFilter Cartridge Peak Treatment Flowrate		$Q = 0.111d^{2.06} \Delta h^{0.5}$ [Q]=L min ⁻¹ ; [d]=mm; [h]=m Figure 2: StormFilter Stage Discharge Equation [2]																																																																								
StormFilter cartridge stage-discharge equation	=0.111d ^{2.06} Δh ^{0.5} L/min	<table border="1"> <thead> <tr> <th>Cart Height (cm)</th> <th>Actual Height (m)</th> <th>Priming Depth (m)</th> <th>Max Disc Diam. (mm)</th> <th>Max. Design Q (L/s)</th> <th>Filter Bed Area (m²)</th> <th>Flow Rate (L/s/m²)</th> <th>Bed Depth (mm)</th> <th>Media Volume (m³)</th> <th>Flow Rate (L/s/m³)</th> </tr> </thead> <tbody> <tr> <td>30</td> <td>0.305</td> <td>0.27</td> <td>22.70</td> <td>0.63</td> <td>0.460</td> <td>1.37</td> <td>175</td> <td>0.052</td> <td>12.0</td> </tr> <tr> <td>46</td> <td>0.457</td> <td>0.43</td> <td>25.00</td> <td>0.95</td> <td>0.689</td> <td>1.38</td> <td>175</td> <td>0.078</td> <td>12.1</td> </tr> <tr> <td>69</td> <td>0.686</td> <td>0.66</td> <td>27.60</td> <td>1.42</td> <td>1.034</td> <td>1.37</td> <td>175</td> <td>0.118</td> <td>12.1</td> </tr> </tbody> </table> Table 1: StormFilter Cartridge Specifics [3]		Cart Height (cm)	Actual Height (m)	Priming Depth (m)	Max Disc Diam. (mm)	Max. Design Q (L/s)	Filter Bed Area (m ²)	Flow Rate (L/s/m ²)	Bed Depth (mm)	Media Volume (m ³)	Flow Rate (L/s/m ³)	30	0.305	0.27	22.70	0.63	0.460	1.37	175	0.052	12.0	46	0.457	0.43	25.00	0.95	0.689	1.38	175	0.078	12.1	69	0.686	0.66	27.60	1.42	1.034	1.37	175	0.118	12.1																															
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Design treatment flowrate per cartridge (Qcart)	1.420 L/s/cart	Compute the StormFilter peak treatment flowrate at internal bypass per cartridge via the StormFilter stage-discharge equation																																																																								
Number (actual) of StormFilter cartridges required	1.839 cart(s)	Compute the number of actual StormFilter cartridges required (i.e. nCART = QWQ / QCART)																																																																								
Number (rounded) of StormFilter cartridges required (nCART)	2 cart(s)	Compute the number (rounded up to whole number) of StormFilter cartridges required																																																																								
Design StormFilter Treatment Flowrate (QSF)	2.840 L/s	Compute the StormFilter peak treatment flowrate at internal bypass via the StormFilter stage-discharge equation																																																																								
3.0 Estimate Sediment Mass Loading		3.0 Estimate sediment mass loading (Refer sheet '2. Mass Load Calcs' for more details)																																																																								
3.1 StormFilter Manhole/Vault Dimensions		3.1 Use tables 2 & 3 below to fill in StormFilter Dimensions based on number of cartridges as calculated in 2.2 above																																																																								
Length Cartridge Bay (Lbay)	1.000 m	3.2 Use table 3 below to fill in estimated TSS concentration. For roads with ≥25,000vpd, use minimum 600kg/ha/yr																																																																								
Width Cartridge Bay (Wbay)	1.670 m	3.3 Use 0% pretreatment for vaults/manholes with no forebay. Use 10-15% pretreatment for vaults/manholes with forebays. Use 50% pretreatment for upstream GPT in EnviroPod. Use 75%-90% system efficiency																																																																								
Area Cartridge Bay (Abay)	1.670 m ²																																																																									
Total area of Cartridges (Acarts)	0.362 m ²																																																																									
Area Lower Volume (Alow)	1.308 m ²																																																																									
Volume Lower Volume (Vlow)	903 L																																																																									
Area Upper Volume (Aupp)	1.670 m ²																																																																									
Volume Upper Volume (Vupp)	167 L																																																																									
Live storage volume at internal bypass (Vstor)	1070 L																																																																									
3.2 Catchment Sediment Loading		<table border="1"> <thead> <tr> <th rowspan="2">Std Manhole Dimensions</th> <th colspan="4">Without forebay</th> <th colspan="4">With forebay</th> </tr> <tr> <th>Cart Bay Length (m)</th> <th>Cart Bay Width (m)</th> <th>Cart Bay Area (m²)</th> <th>Max Number Carts</th> <th>Cart Bay Length (m)</th> <th>Cart Bay Width (m)</th> <th>Cart Bay Area (m²)</th> <th>Max Number Carts</th> </tr> </thead> <tbody> <tr> <td>1050</td> <td>1.00</td> <td>0.77</td> <td>0.77</td> <td>1</td> <td>1.00</td> <td>N/A</td> <td>N/A</td> <td>N/A</td> </tr> <tr> <td>1200</td> <td>1.00</td> <td>1.03</td> <td>1.03</td> <td>3</td> <td>1.00</td> <td>0.76</td> <td>0.76</td> <td>2</td> </tr> <tr> <td>1500</td> <td>1.00</td> <td>1.67</td> <td>1.67</td> <td>4</td> <td>1.00</td> <td>1.39</td> <td>1.39</td> <td>3</td> </tr> <tr> <td>1800</td> <td>1.00</td> <td>2.44</td> <td>2.44</td> <td>7</td> <td>1.00</td> <td>1.83</td> <td>1.83</td> <td>5</td> </tr> <tr> <td>2050</td> <td>1.00</td> <td>3.20</td> <td>3.20</td> <td>9</td> <td>1.00</td> <td>2.80</td> <td>2.80</td> <td>7</td> </tr> </tbody> </table> Table 2: Standard Stormwater360 Manhole Dimensions		Std Manhole Dimensions	Without forebay				With forebay				Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	1050	1.00	0.77	0.77	1	1.00	N/A	N/A	N/A	1200	1.00	1.03	1.03	3	1.00	0.76	0.76	2	1500	1.00	1.67	1.67	4	1.00	1.39	1.39	3	1800	1.00	2.44	2.44	7	1.00	1.83	1.83	5	2050	1.00	3.20	3.20	9	1.00	2.80	2.80	7									
Std Manhole Dimensions	Without forebay				With forebay																																																																					
	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts																																																																		
1050	1.00	0.77	0.77	1	1.00	N/A	N/A	N/A																																																																		
1200	1.00	1.03	1.03	3	1.00	0.76	0.76	2																																																																		
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2050	1.00	3.20	3.20	9	1.00	2.80	2.80	7																																																																		
Estimated TSS Concentration Impervious Roof (TSSroof)	100 kg/ha/year																																																																									
Estimated TSS Concentration Impervious Road (TSSroad)	300 kg/ha/year																																																																									
Estimated TSS Concentration Pervious Area (TSSper)	200 kg/ha/year																																																																									
Estimated Total TSS Load (TSSload)	28 kg/year																																																																									
3.3 Treatment Efficiencies		<table border="1"> <thead> <tr> <th rowspan="2">Std Vault Dimensions</th> <th colspan="4">Without forebay</th> <th colspan="4">With forebay</th> </tr> <tr> <th>Cart Bay Length (m)</th> <th>Cart Bay Width (m)</th> <th>Cart Bay Area (m²)</th> <th>Max Number Carts</th> <th>Cart Bay Length (m)</th> <th>Cart Bay Width (m)</th> <th>Cart Bay Area (m²)</th> <th>Max Number Carts</th> </tr> </thead> <tbody> <tr> <td>3.4 L x 1.5 W x 1.8 D</td> <td>2.85</td> <td>1.50</td> <td>4.28</td> <td>11</td> <td>2.30</td> <td>1.80</td> <td>4.14</td> <td>8</td> </tr> <tr> <td>4.5 L x 1.5 W x 1.8 D</td> <td>3.95</td> <td>1.50</td> <td>5.93</td> <td>17</td> <td>3.40</td> <td>1.50</td> <td>5.10</td> <td>14</td> </tr> <tr> <td>4.2 L x 2.0 W x 1.8 D</td> <td>3.95</td> <td>1.95</td> <td>7.70</td> <td>23</td> <td>3.40</td> <td>2.10</td> <td>7.14</td> <td>18</td> </tr> <tr> <td>5.6 L x 2.0 W x 1.8 D</td> <td>5.05</td> <td>1.95</td> <td>9.85</td> <td>31</td> <td>4.50</td> <td>2.10</td> <td>9.45</td> <td>26</td> </tr> <tr> <td>5.6 L x 2.4 W x 1.8 D</td> <td>5.05</td> <td>2.40</td> <td>12.12</td> <td>39</td> <td>4.50</td> <td>2.10</td> <td>9.45</td> <td>27</td> </tr> <tr> <td>6.2 L x 2.4 W x 1.8 D</td> <td>5.60</td> <td>2.40</td> <td>13.44</td> <td>44</td> <td>4.50</td> <td>2.40</td> <td>10.80</td> <td>33</td> </tr> </tbody> </table> Table 3: Standard Stormwater360 Vault Dimensions		Std Vault Dimensions	Without forebay				With forebay				Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	Cart Bay Length (m)	Cart Bay Width (m)	Cart Bay Area (m ²)	Max Number Carts	3.4 L x 1.5 W x 1.8 D	2.85	1.50	4.28	11	2.30	1.80	4.14	8	4.5 L x 1.5 W x 1.8 D	3.95	1.50	5.93	17	3.40	1.50	5.10	14	4.2 L x 2.0 W x 1.8 D	3.95	1.95	7.70	23	3.40	2.10	7.14	18	5.6 L x 2.0 W x 1.8 D	5.05	1.95	9.85	31	4.50	2.10	9.45	26	5.6 L x 2.4 W x 1.8 D	5.05	2.40	12.12	39	4.50	2.10	9.45	27	6.2 L x 2.4 W x 1.8 D	5.60	2.40	13.44	44	4.50	2.40	10.80	33
Std Vault Dimensions	Without forebay				With forebay																																																																					
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Pre-treatment Efficiency (EFFpre)	0 %																																																																									
System Efficiency (EFFsys)	75 %																																																																									
3.4 Maintenance Requirements		<table border="1"> <thead> <tr> <th>Land Use</th> <th>TSS (kg/ha/yr)</th> </tr> </thead> <tbody> <tr> <td>Road</td> <td>281 - 723</td> </tr> <tr> <td>Commercial</td> <td>242 - 1369</td> </tr> <tr> <td>Residential (low)</td> <td>60 - 340</td> </tr> <tr> <td>Residential (high)</td> <td>97 - 547</td> </tr> <tr> <td>Terraced</td> <td>133 - 755</td> </tr> <tr> <td>Bush</td> <td>26 - 146</td> </tr> <tr> <td>Grass</td> <td>80 - 588</td> </tr> <tr> <td>Roof</td> <td>50-110 (f)</td> </tr> <tr> <td>Pasture</td> <td>103 - 583</td> </tr> </tbody> </table> Table 4: Suggested TSS loads ⁴		Land Use	TSS (kg/ha/yr)	Road	281 - 723	Commercial	242 - 1369	Residential (low)	60 - 340	Residential (high)	97 - 547	Terraced	133 - 755	Bush	26 - 146	Grass	80 - 588	Roof	50-110 (f)	Pasture	103 - 583																																																			
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Estimated number of cleans per annum (nCleans)	0.66																																																																									
Estimated Maintenance Frequency (Mfreq)	18.182 months																																																																									
4.0 Design Summary		Figure 3: StormFilter Vault Cutaway																																																																								
Design Water Quality Treatment Flowrate (Qwq)	2.611 L/s																																																																									
StormFilter Design WQ Treatment flowrate (QSF)	2.840 L/s																																																																									
StormFilter Design flowrate at internal bypass (Qbypass)	3.057 L/s																																																																									
Number of StormFilter Cartridges required (nTOTAL)	2ea x 69cm Perlite cart(s)																																																																									
Treatment Flux per cartridge (FLUX)	1.4 L/s/m ²																																																																									
Restrictor Disc Size (d)	27.600 mm																																																																									
Maximum Hydraulic Effect (hmax)	0.930 m																																																																									
Estimated Maintenance Frequency (Mfreq)	18 months																																																																									



$$Q = 0.111d^{2.06} \Delta h^{0.5}$$

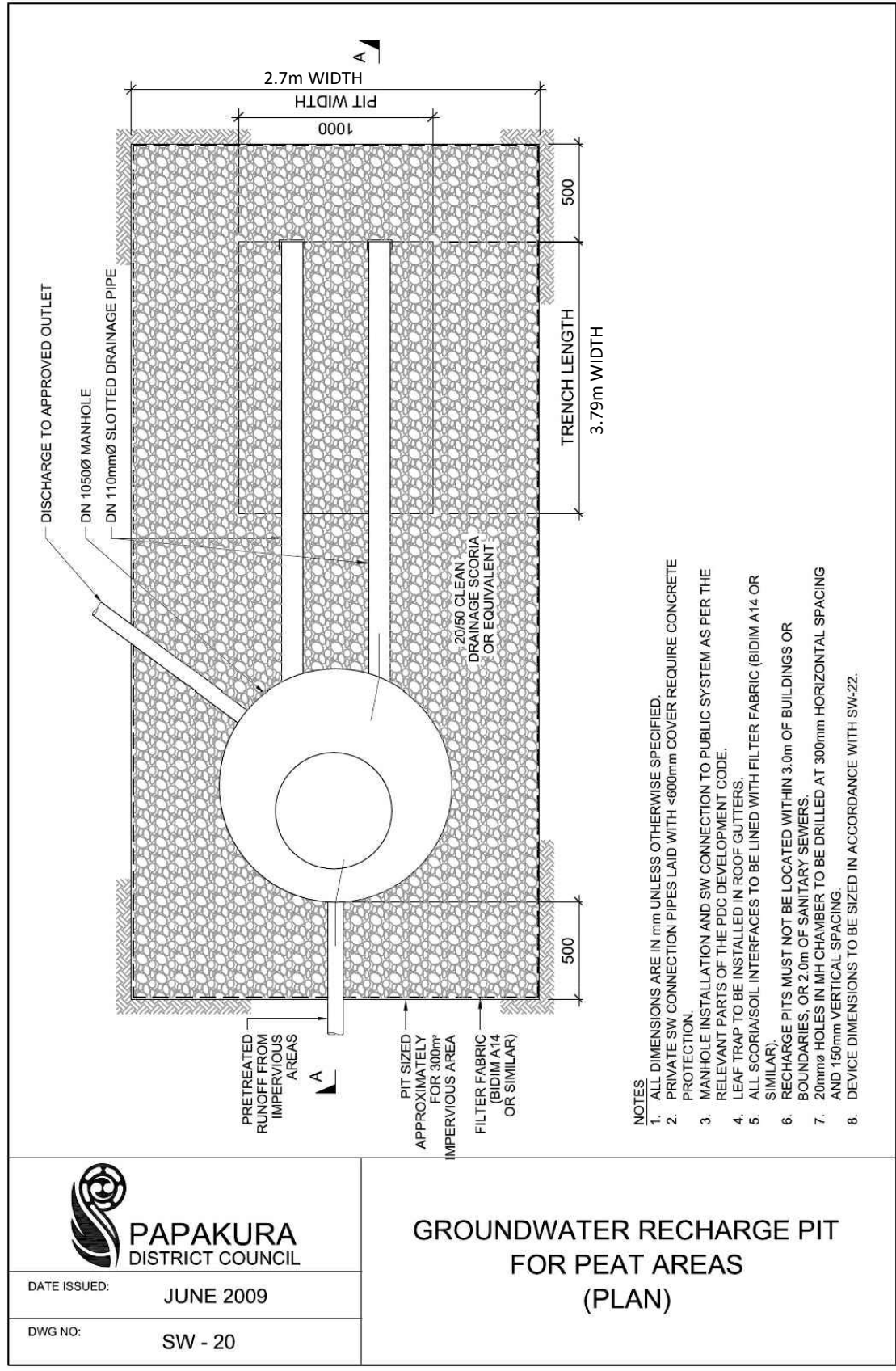
[Q]=L min⁻¹; [d]=mm; [h]=m



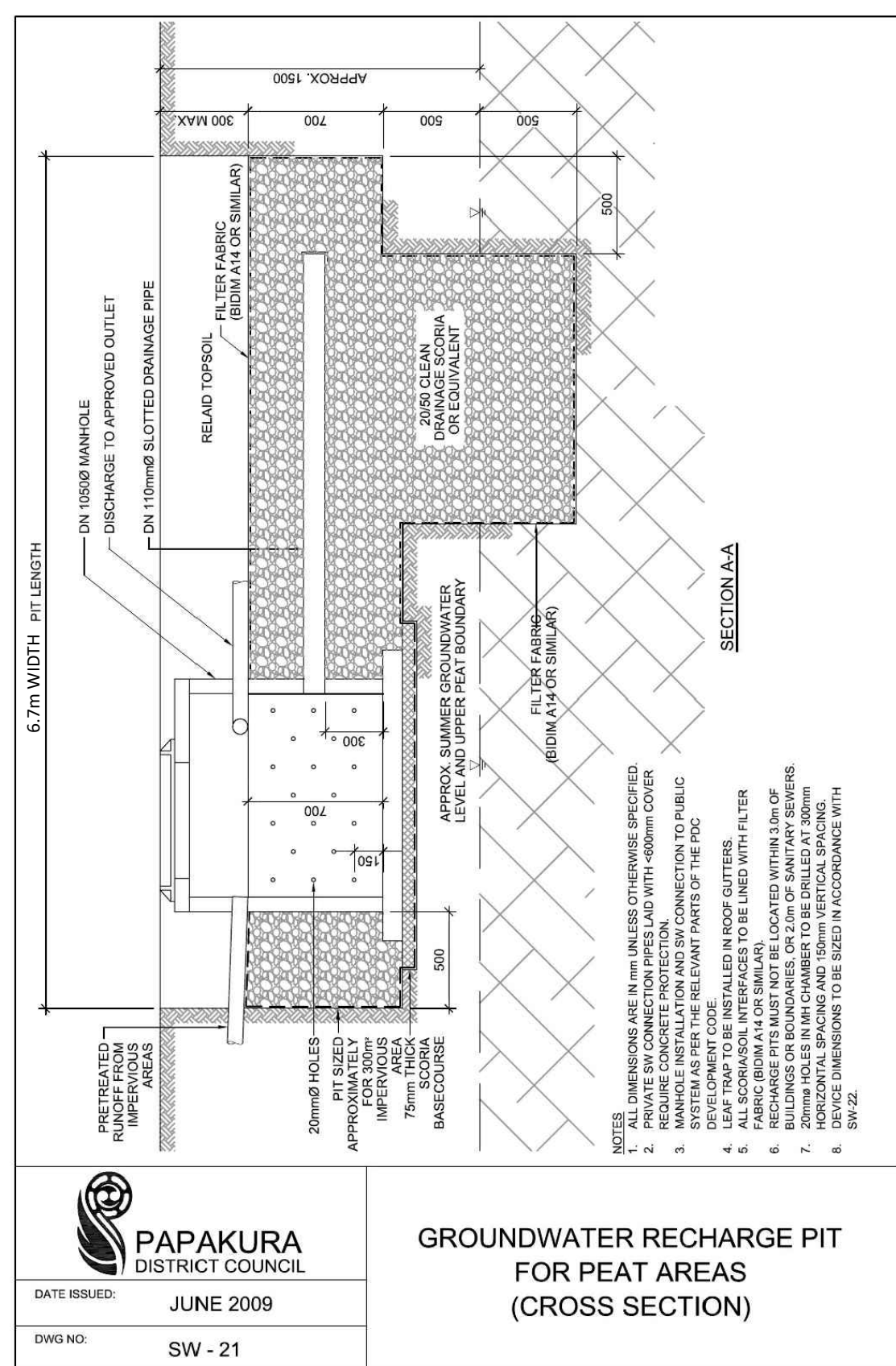
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 164-166 PORCHESTER ROAD
 TAKANINI, AUCKLAND**

SHEET TITLE:
**PROPOSED DRAINAGE
 STORMWATER360
 STORMFILTER CALCULATION**

ISSUE STATUS: **CONSENT**
 SCALE: (A1/A3) **NOT TO SCALE**
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 N.T.S.
 DRAWING NUMBER: **2094-014476** REV: **C2**



- NOTES**
1. ALL DIMENSIONS ARE IN mm UNLESS OTHERWISE SPECIFIED.
 2. PRIVATE SW CONNECTION PIPES LAID WITH <math><600\text{mm}</math> COVER REQUIRE CONCRETE PROTECTION.
 3. MANHOLE INSTALLATION AND SW CONNECTION TO PUBLIC SYSTEM AS PER THE RELEVANT PARTS OF THE PDC DEVELOPMENT CODE.
 4. LEAF TRAP TO BE INSTALLED IN ROOF GUTTERS.
 5. ALL SCORIA/SOIL INTERFACES TO BE LINED WITH FILTER FABRIC (BIDIM A14 OR SIMILAR).
 6. RECHARGE PITS MUST NOT BE LOCATED WITHIN 3.0m OF BUILDINGS OR BOUNDARIES, OR 2.0m OF SANITARY SEWERS.
 7. 20mm ϕ HOLES IN MH CHAMBER TO BE DRILLED AT 300mm HORIZONTAL SPACING AND 150mm VERTICAL SPACING.
 8. DEVICE DIMENSIONS TO BE SIZED IN ACCORDANCE WITH SW-22.



- NOTES**
1. ALL DIMENSIONS ARE IN mm UNLESS OTHERWISE SPECIFIED.
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 8. DEVICE DIMENSIONS TO BE SIZED IN ACCORDANCE WITH SW-22.

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MSS	05.06.19	DATE	
MSS	05.06.19	DATE	
MSS	05.06.19	DATE	
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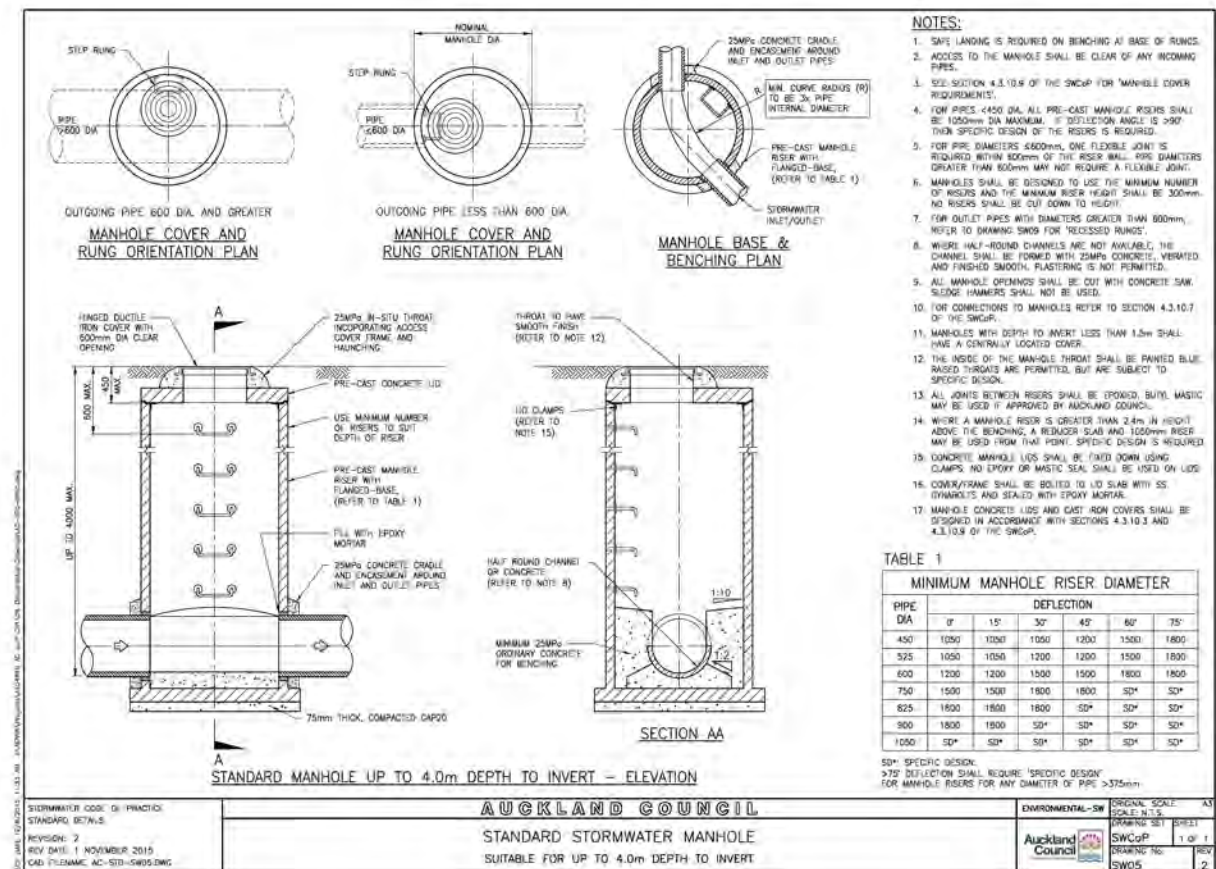
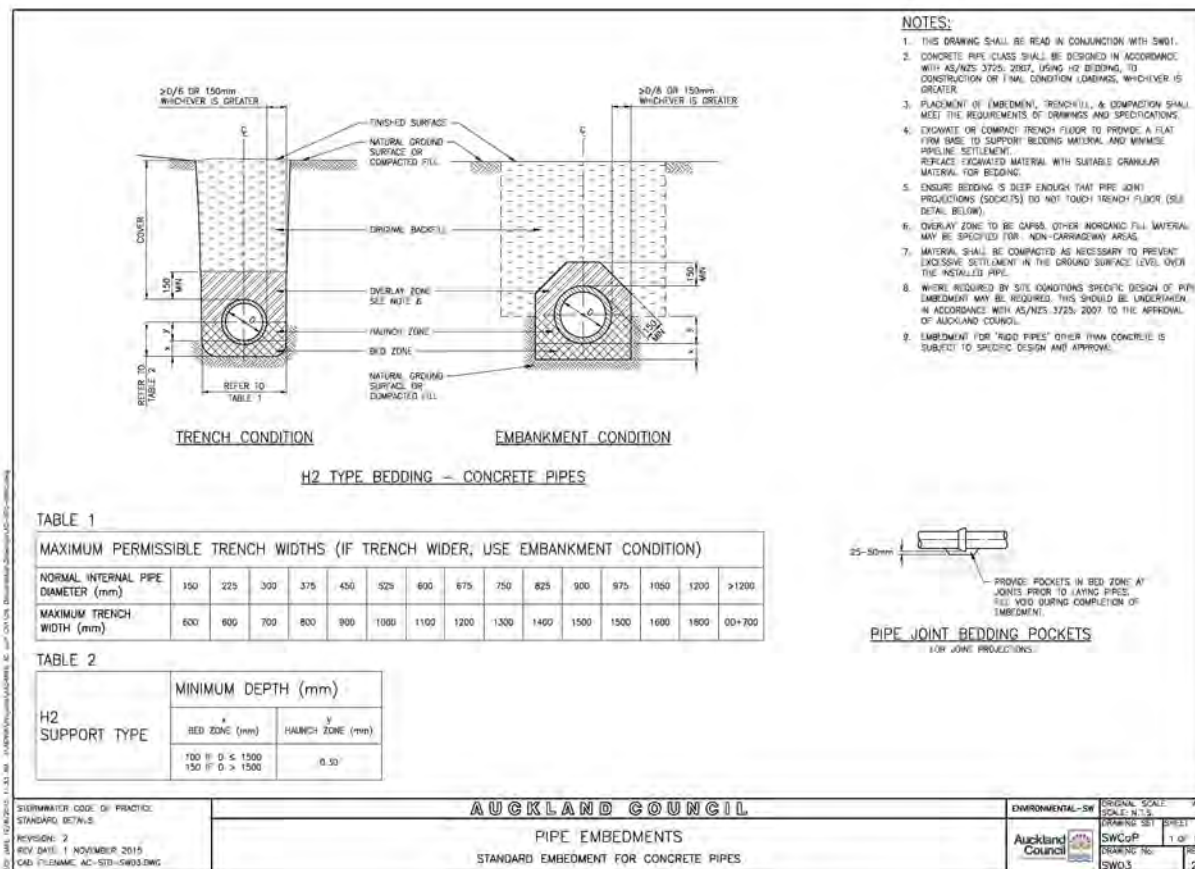
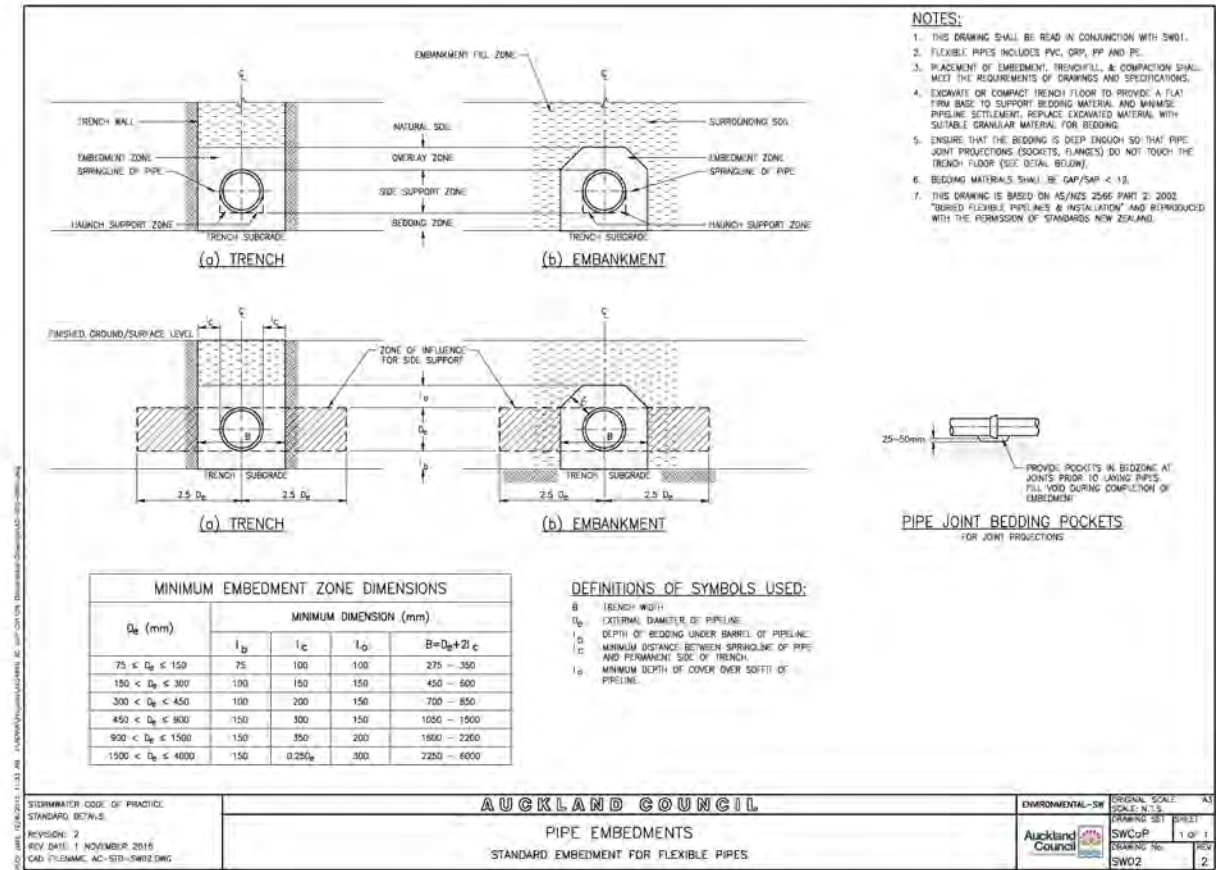
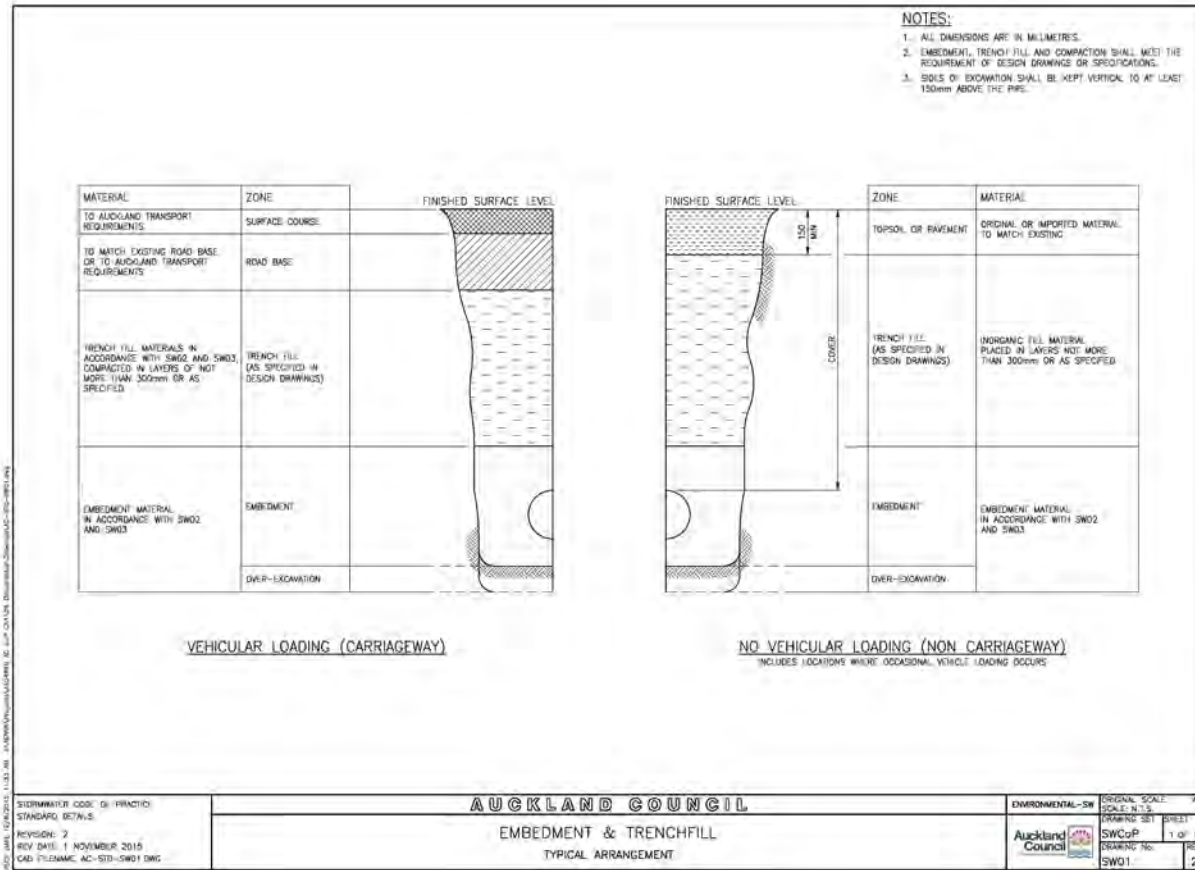
SHEET TITLE:
**PROPOSED DRAINAGE
RECHARGE PIT DETAILS**

ISSUE STATUS: **CONSENT**

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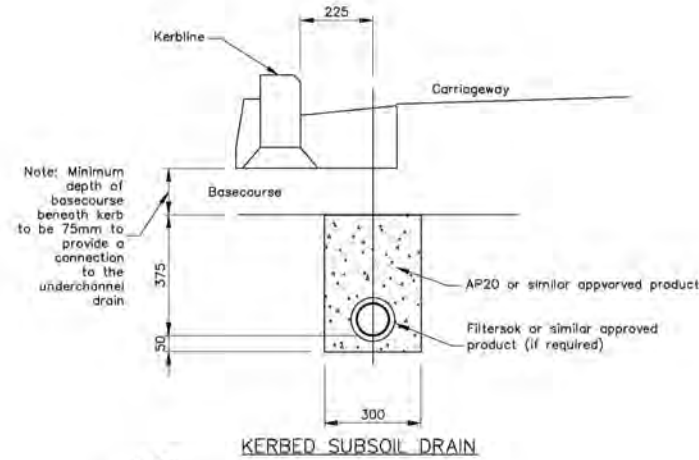
SHEET TITLE: PROPOSED DRAINAGE
STORMWATER STANDARD DETAILS
SHEET 1

ISSUE STATUS: **CONSENT**

SCALE: (A1/A3) NOT TO SCALE

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REV: C2

Drawing set for Chapter 17 - Road Drainage



KERBED SUBSOIL DRAIN

NOTE

1. Construct subsoil drain after stabilisation of subgrade.
2. **Underchannel Drains**
Shall be approved perforated drain pipe of 100mm internal diameter unless specified or scheduled otherwise. Subsoil drain pipes shall comply with the requirements of TNZ Specifications F/2. Trench backfill shall be approved AP20 material or similar. Trench backfill shall be approved 30/10 scoria or similar if a filter sock is provided. Depth below subgrade to be 375mm.

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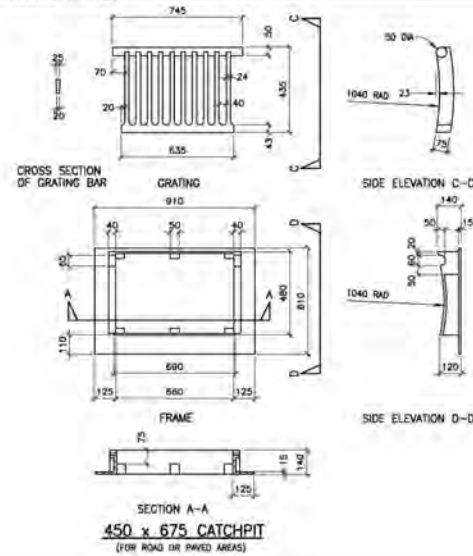
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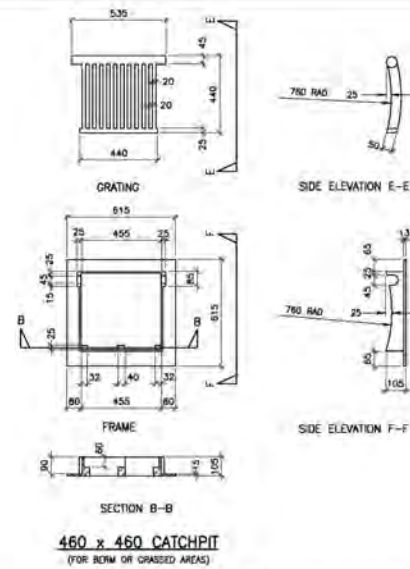
VERSION

TITLE: SUBSOIL DRAIN

Drawing set for Chapter 17 - Road Drainage



SECTION A-A
450 x 675 CATCHPIT
(FOR ROAD USE PAVED AREAS)



SECTION B-B
460 x 460 CATCHPIT
(FOR BORN OR GRASSED AREAS)

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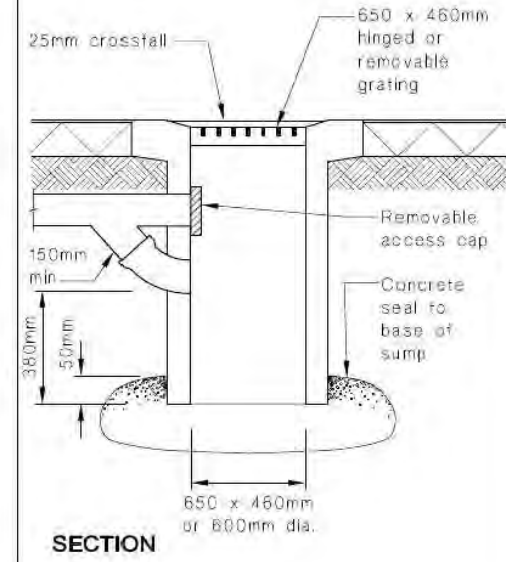
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DRAWING No. RDO30

VERSION

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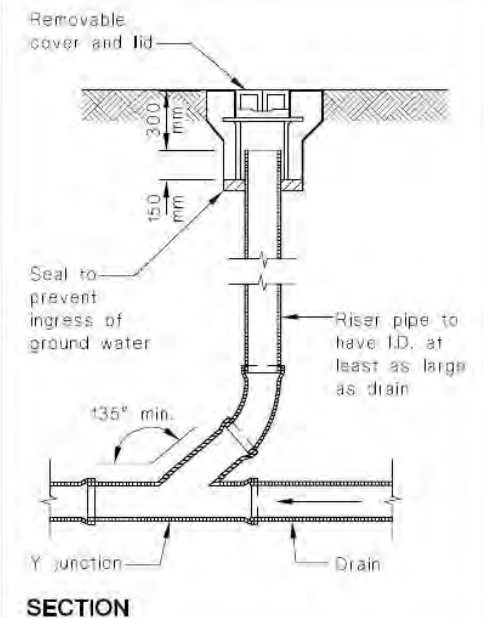
Figure 9: Type-two Surface Water Sump
Paragraph 3.6.2



SECTION

CATCHPIT DETAIL

Figure 10: Typical Rodding Point
Paragraph 3.7.1



SECTION

RODDING POINT DETAIL

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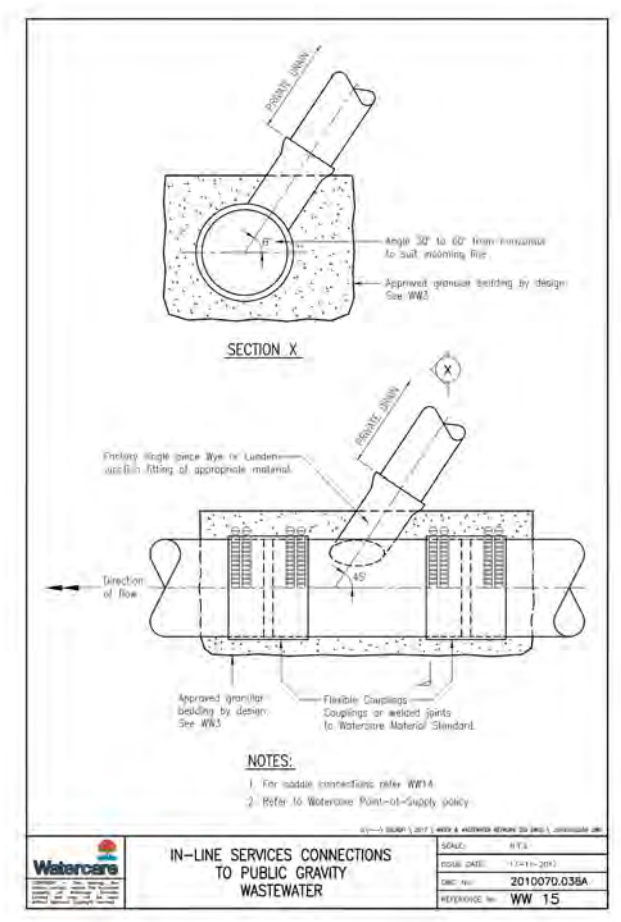
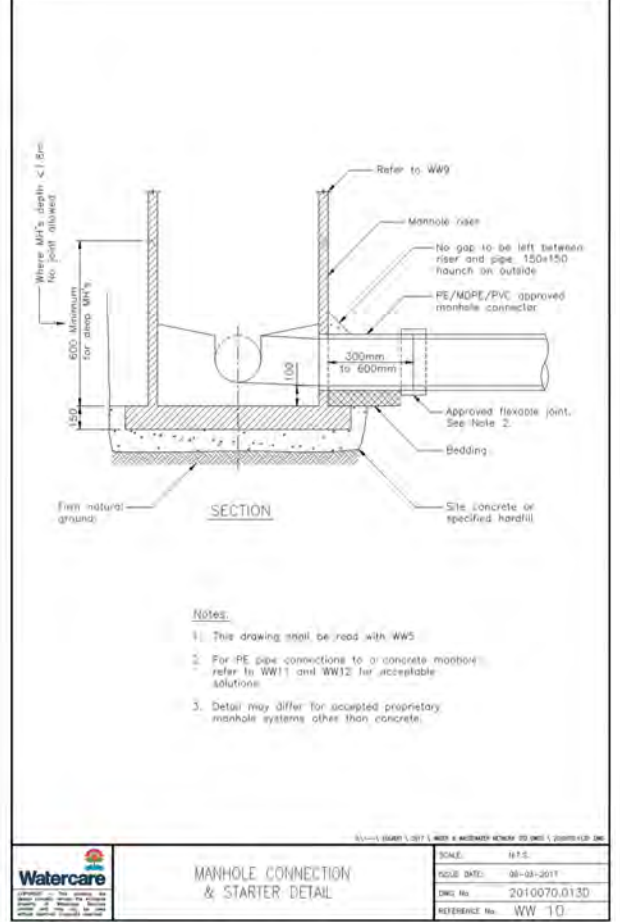
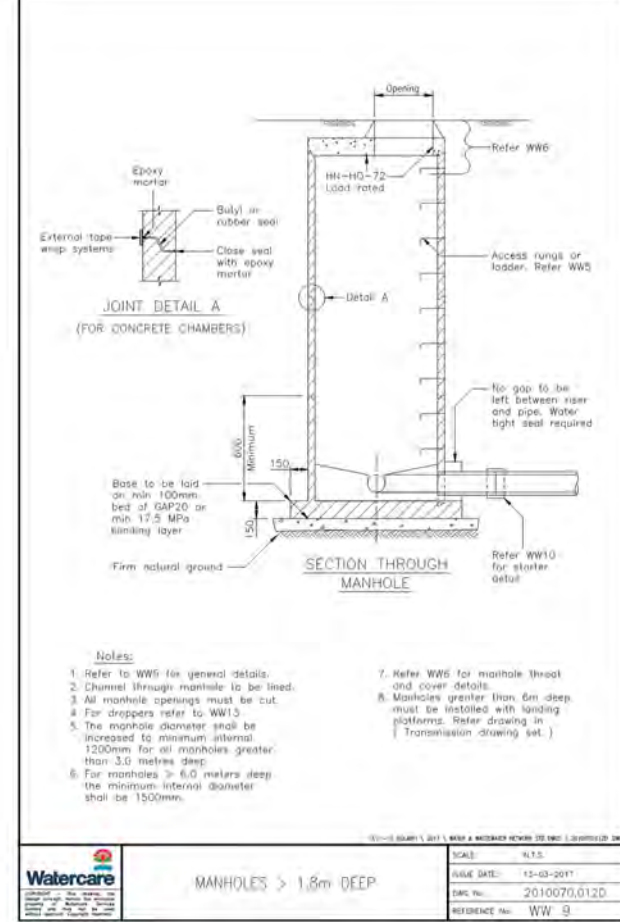
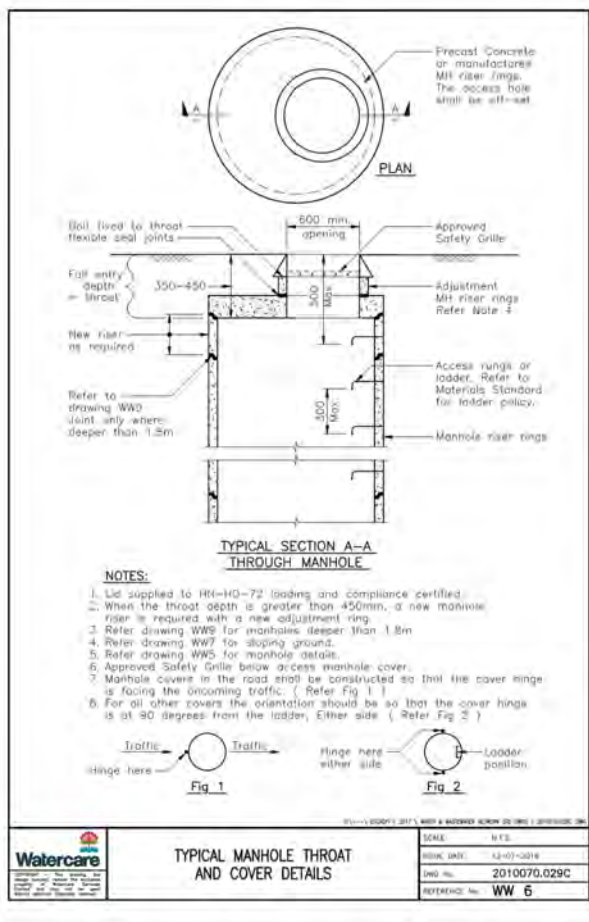
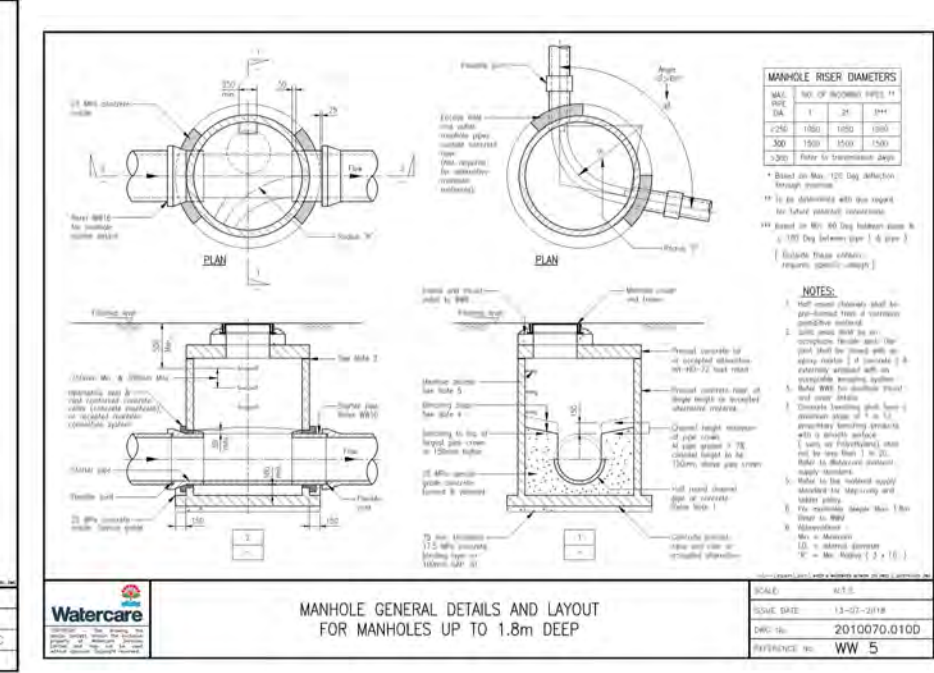
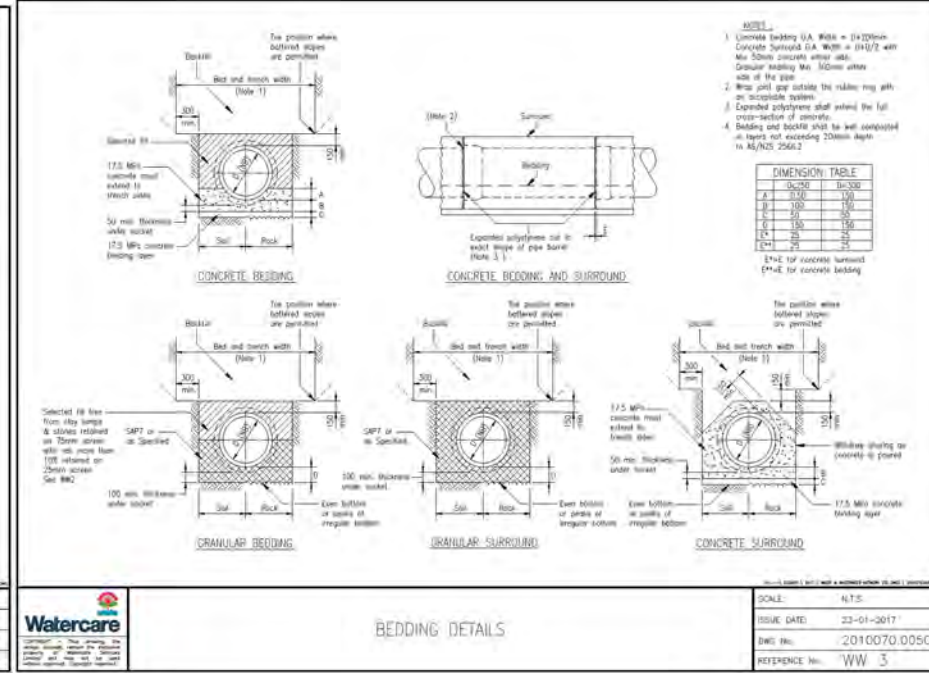
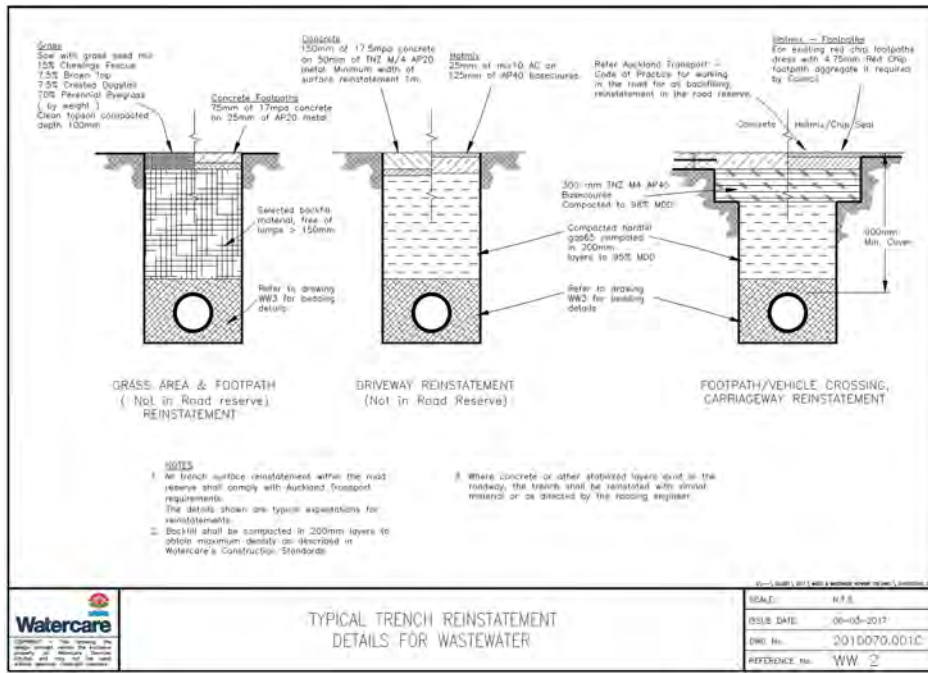
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SHEET TITLE:
PROPOSED DRAINAGE
STORMWATER STANDARD DETAILS
SHEET 2

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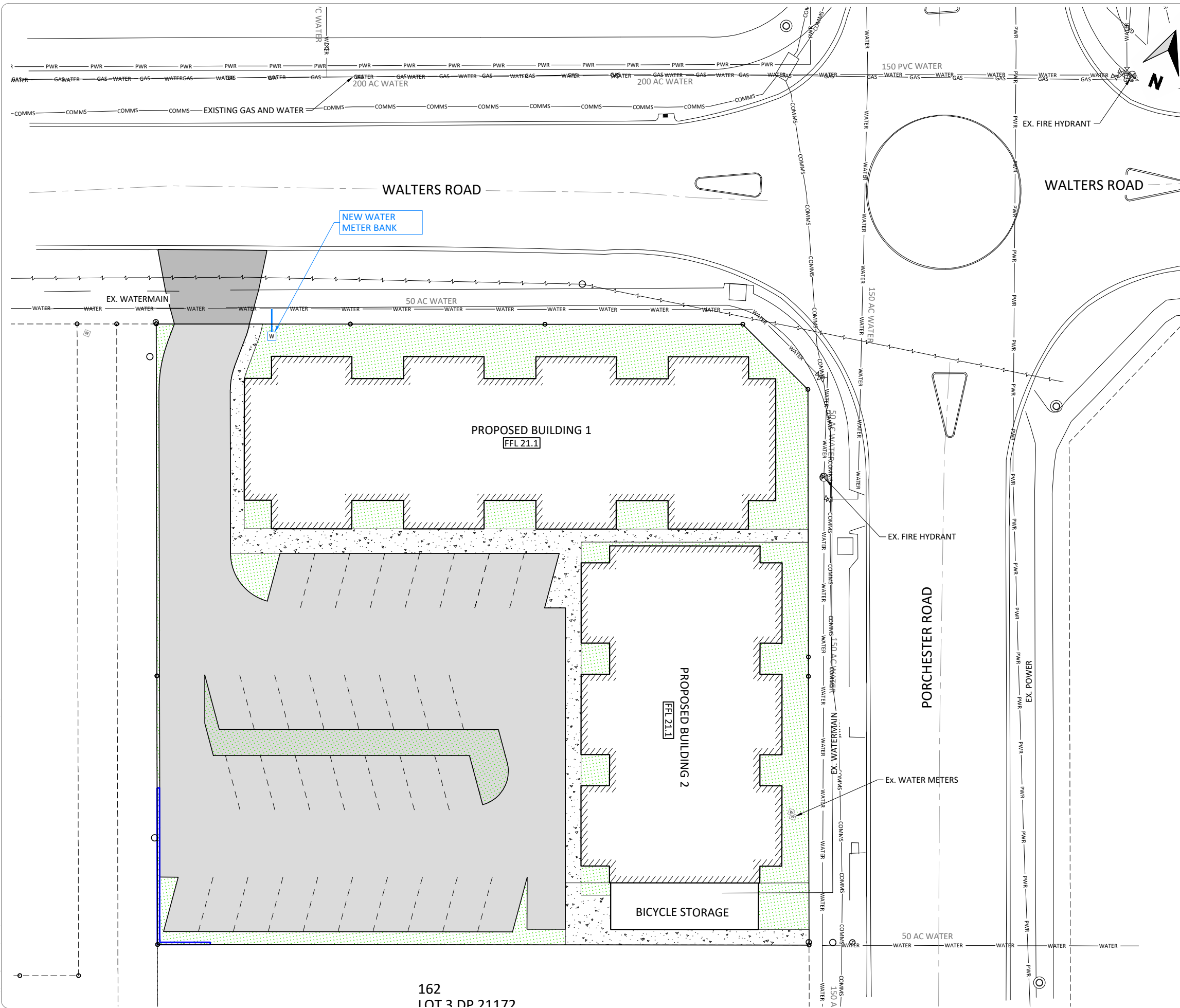
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**PROPOSED DRAINAGE
 WASTEWATER STANDARD DETAILS**

ISSUE STATUS: **CONSENT**
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- NOTES:**
1. ALL WATERMAINS TO COMPLY WITH WATERCARE'S 'WATER AND WASTEWATER CODE OF PRACTICE FOR LAND DEVELOPMENT AND SUBDIVISION' AND IN ACCORDANCE WITH THE REQUIREMENTS OF THE BUILDING CODE.
 2. ALL WATERMAINS TO BE LAID AND BACKFILLED IN ACCORDANCE WITH COUNCIL STANDARD SPECIFICATIONS.
 3. ALL WATERMAINS TO BE LAID 1.1m FROM AND PARALLEL TO THE BOUNDARY UNLESS SHOWN OTHERWISE WITH 600mm MIN. COVER (900mm UNDER CARRIAGEWAYS).
 4. ALL TRENCHES UNDER CARRIAGEWAY / VEHICLE CROSSINGS TO BE BACKFILLED WITH HARDFILL.
 5. ALL WORKS ON EXISTING WATERMAINS ARE TO BE CARRIED OUT BY AN APPROVED CONTRACTOR UNDER APPROVAL FROM WATERCARE AT DEVELOPERS EXPENSE.
 6. WHERE WATERMAINS ARE TO BE LAID IN AN ARC, THE PIPES SHALL BE LAID IN A CONSTANT RADIUS IN THE APPROPRIATE POSITION.
 7. CONTRACTOR TO SEARCH, LOCATE, PROTECT AND MAINTAIN ALL EXISTING SERVICES, POWER, COMMS ETC.
 8. SEE SHEET 2094-01-590 FOR STANDARD DETAILS.

- LEGEND**
- EXISTING**
- WATER — WATERMAIN
 - ⊗ FIRE HYDRANT
 - ⊗ SLUICE VALVE
 - PEET VALVE
 - ⊗ BLANK CAP
 - PWR — POWER
 - WATER — WATER
 - COMMS — COMMS
 - GAS — GAS
- PROPOSED**
- ⊗ WATER METER BANK

162
LOT 3 DP 21172

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MSS	05.06.19	APPROVED	
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SHEET TITLE:
**PROPOSED WATERMAIN
LAYOUT PLAN**

ISSUE STATUS: **CONSENT**

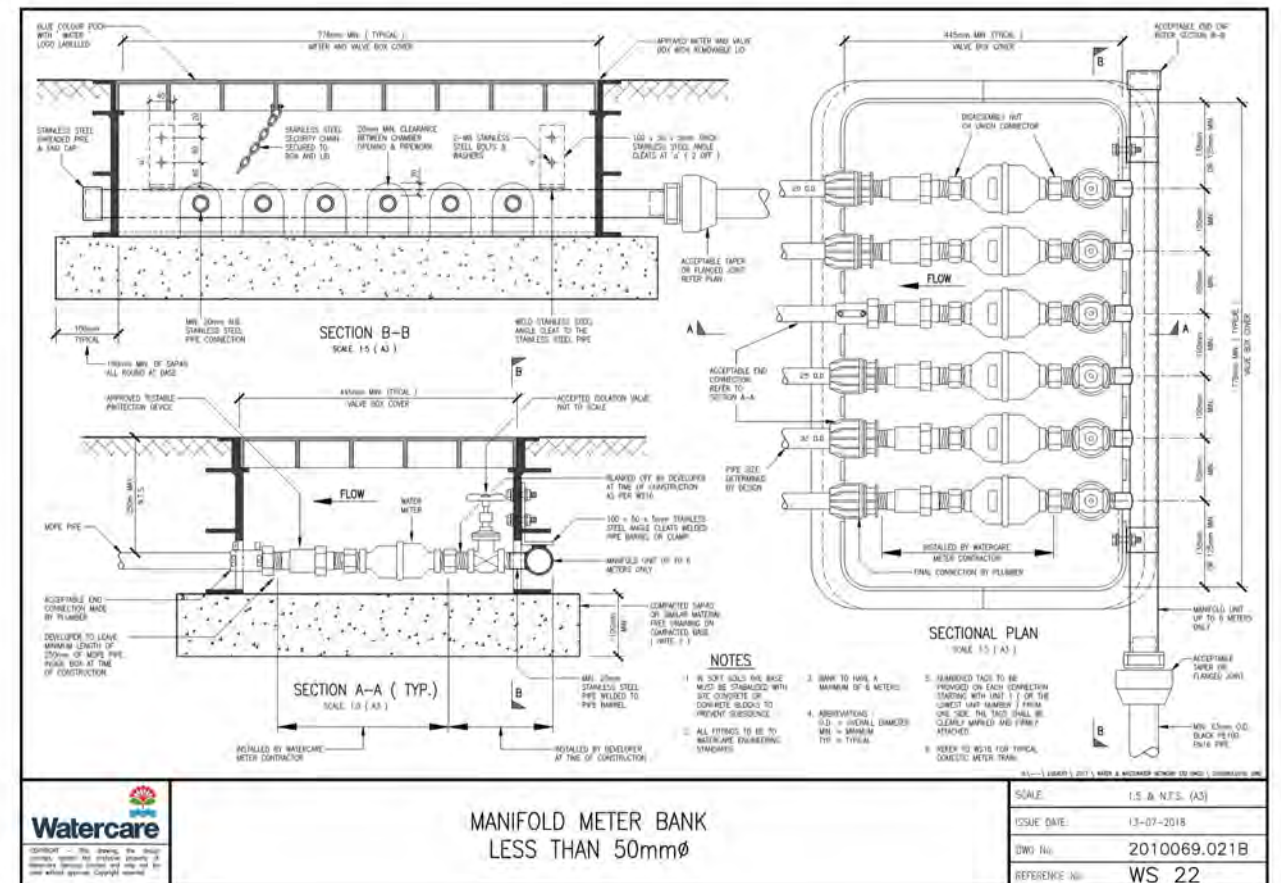
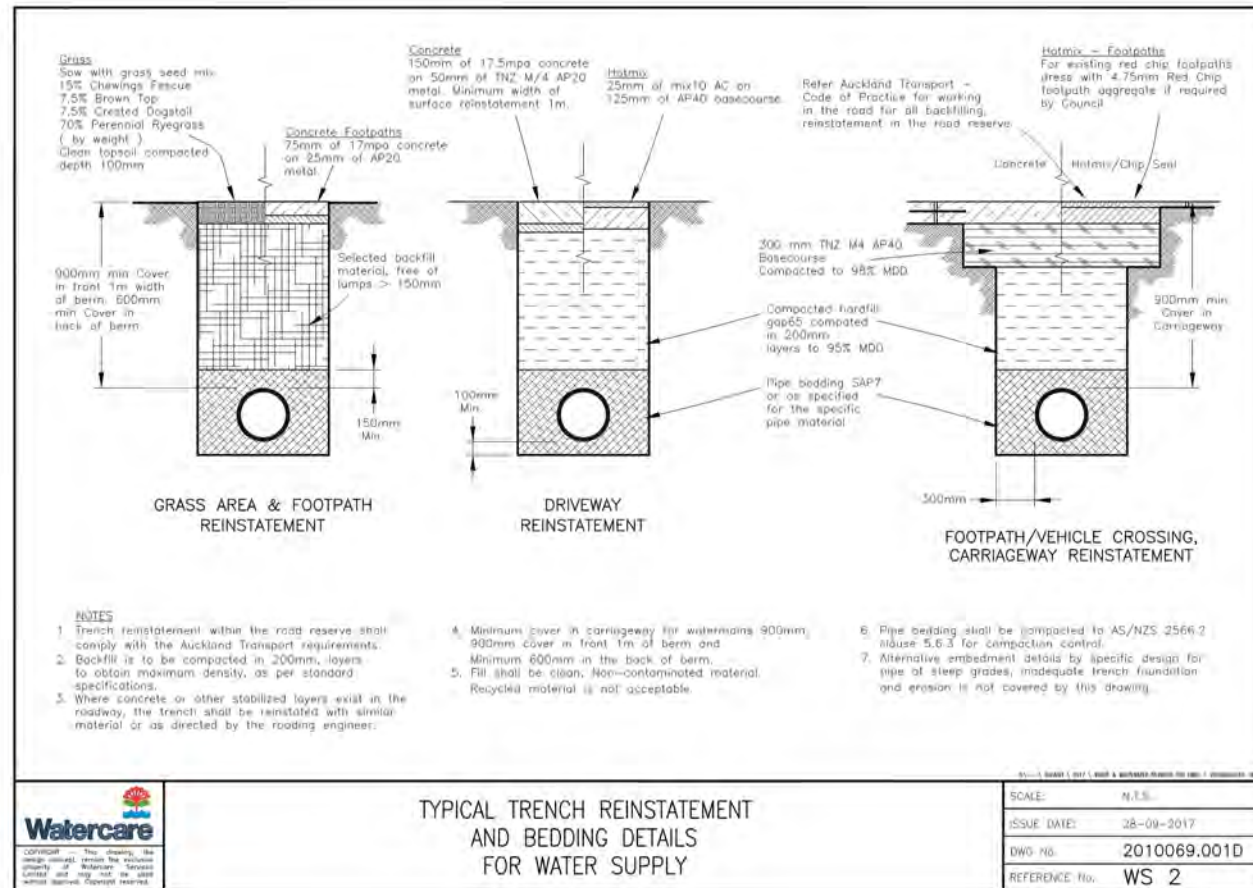
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SHEET TITLE:
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DRAWING NUMBER: 2094-01-590
REV: C2

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1251] Notice of Requirement online submission - Pam Butler Senior RMA Advisor KiwiRail
Date: Thursday, 14 December 2023 8:30:42 pm
Attachments: [KiwiRail reasons for submissions Takanini Level Crossings.pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Pam Butler Senior RMA Advisor KiwiRail

Organisation name: KiwiRail Holdings Limited

Full name of your agent:

Email address: Pam.butler@kiwirail.co.nz

Contact phone number: 0275708571

Postal address:
Private Bag 92138
Auckland 1142

Auckland 1142

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:

North Island Main Trunk Line designation ref 6302 in the AUP in the Takaanini area - at Spartan Road, Manuroa Road, Taka Street, and Walters Road

Do you support or oppose the Notice of Requirement? I or we support the Notice of Requirement.

The reason for my or our views are:
Please see attached "Reasons for Submission"

I or we seek the following recommendation or decision from Auckland Council:
Relief sought KiwiRail supports Takanini Level Crossing NoR's One and Two and seeks that the Notices of Requirement be recommended for approval subject to the comments provided and the applicant's proposed conditions.

Submission date: 14 December 2023

Supporting documents
KiwiRail reasons for submissions Takanini Level Crossings.pdf

Attend a hearing

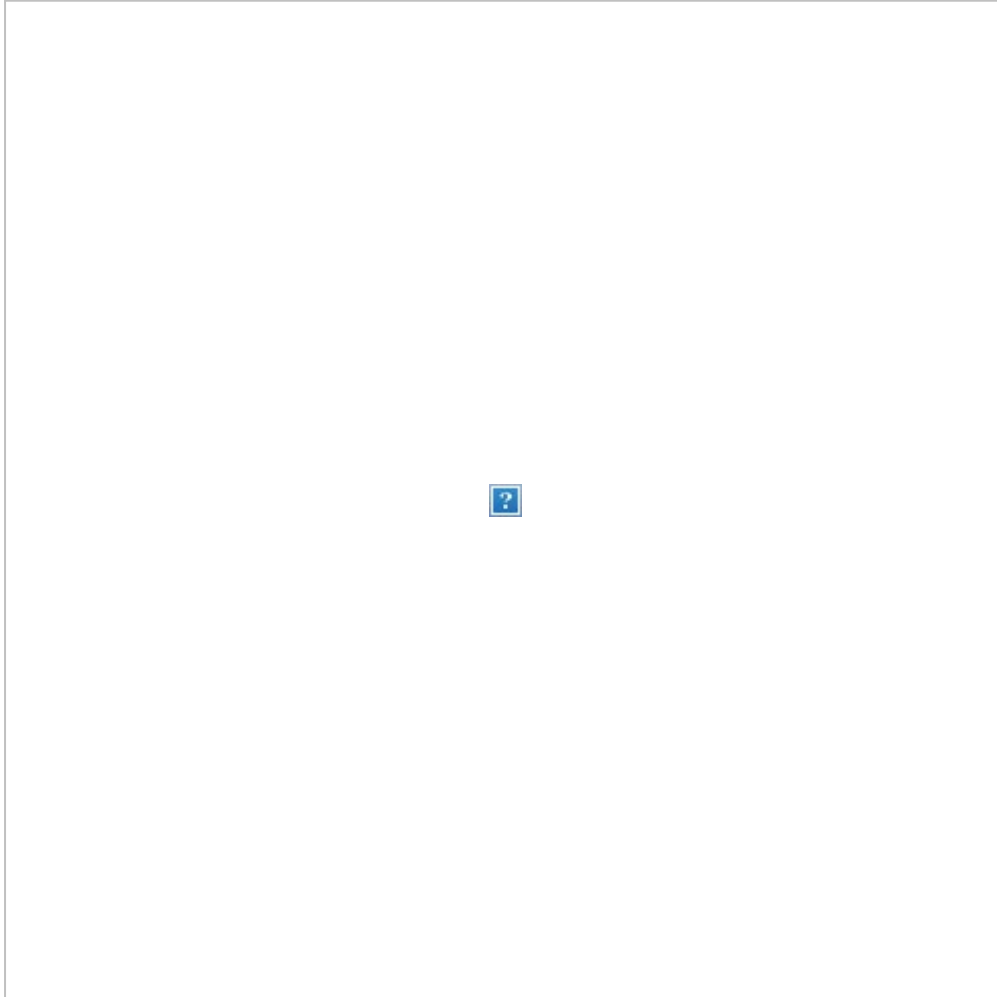
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

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- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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KiwiRail submission in support of Takanini Crossings NoRs 1 and 2

NoR1

Construction of a new bridge with general traffic lanes and/or walking and cycling facilities across the NIMT and associated works at Spartan, Manuia, Manuroa Roads and Taka Street and level crossing closures (except Manuia)

NoR 2

Construction, operation, maintenance and upgrade of transport infrastructure on Walters Road, Takaanini, which includes the closure of the existing level crossing on Walters Road, a new bridge with general traffic lanes and walking and cycling facilities across the North Island Main Trunk (NIMT) line, as well as local road connections and all associated works

Reason for submission

Level crossing safety

KiwiRail is part of Auckland's wider transport family and supports initiatives improving efficiency and safety on the rail corridor. Removing level crossings will enable a more efficient and productive freight network and improve safety for both users of the road network and the rail corridor. KiwiRail is committed to supporting level crossing safety, both in Auckland and across the network. This ranges from supporting closures to mitigating risks at existing private and public crossings, to support for TrackSafe and public education programmes.

International best practice has long established that physically separating rail, road, and pedestrian traffic creates optimum network outcomes from a safety and capacity perspective. Strategically, we need to move more people and goods on trains in the future than we do today. Working against this, rail capacity and spacing of the signalling system block sections is constrained by the level crossings, limiting the number of train movements on the existing network.

KiwiRail role

KiwiRail Holdings Limited (KiwiRail) is the State-Owned Enterprise responsible for the management and operation of the national railway and Interislander ferry services. In many places, the rail network has been in place for over 100 years and remains crucially important to the economic and social development of the areas it services. The rail network serves two functions as a metropolitan public transport service in Auckland and Wellington primarily, and a route for freight and other services nationally.

The land upon which the rail network operates is owned by the New Zealand Railways Corporation and leased to KiwiRail. KiwiRail owns the rail infrastructure (including rails, sleepers, sidings, and depots) and is a railway operator for the purposes of the Railways Act



2005. It is also the licensed Access Provider under the Railways Act, which provides KiwiRail with broad powers to safely control and restrict the use of railway assets and entry onto railway land.

While not formally part of Te Tupu Ngātahi/Supporting Growth (Te Tupu Ngātahi), KiwiRail is working closely with Auckland Transport (AT) and Waka Kotahi (WK) to develop the strategic transport network to support Auckland's growth areas, particularly in the south.

KiwiRail owns and maintains Auckland's Metro track network and is currently well into delivering major southern projects including electrification between Papakura and Pukekohe and, three new Drury stations (Drury, Ngākōroa and Paerātā), and will shortly embark on work to add capacity to the NIMT (North Island Main Trunk).

A functioning and efficient freight network is critical to the productivity of the nation's supply chain. KiwiRail also operates New Zealand's rail freight network and tourism passenger rail services between Auckland and Wellington and the Te Huia Hamilton – Auckland passenger service, which began in April 2021. Further interregional passenger growth is predicted. KiwiRail therefore has a significant interest in planning to enable the efficient flow of imports, exports, and domestic goods within and through the region. Freight tonnage is forecast to treble to, from and through the region over the next 25 years.

Aligned with its broader national role, KiwiRail and AT have developed a 30-year strategic plan to meet the needs of the network, its users and increasing demand. The Strategic Rail Programme plans to increase capacity, improve future passenger and freight levels of service to drive increases in rail mode share. This will be underpinned by greater network reliability and resilience through a step change in maintenance and renewals (without having to close lines for extended periods). The removal of level crossings is a key component of planned future investment outlined in the Strategic Rail Programme.

As KiwiRail is the Requiring Authority for the earlier designation/s, approval under s177 of the Resource Management Act 1991 (RMA) is required for any secondary requiring authority seeking to undertake works within the railway corridor.

KiwiRail acknowledges that the NoR(s) Assessment of Effects on the Environment (AEE) identifies that further engagement with KiwiRail will continue as the Project is developed and that bridge designs require particular attention. This future work will need to reference (and incorporate) the KiwiRail Engineering Principles and Standards applying at the time. Future construction methodologies will also need to prioritize the need to limit the operational impacts on the NIMT - which is acknowledged in the AEE.

Specific areas that are of greatest interest to KiwiRail and around which further detail will be required prior to granting any s177 approval, include:

That growing the capacity and resilience of the NIMT through the provision of additional tracks (as outlined in the Strategic Rail Programme) is acknowledged and accommodated as far as possible in the development and design of the Project

NoR alignments which restrict or challenge emerging rail corridor options are addressed in advance of starting detailed design

Evidence that the KiwiRail’s Engineering Principles and Standards are met

Construction methodologies that reduce the need for, or duration of, any proposed full closure of the NIMT

All safety and operational concerns arising from structures over and adjacent to the rail corridor are addressed, including but not limited to ongoing effects on corridor stability.

Table One below sets out several specific design areas for future discussions. KiwiRail considers that these can be managed to meet both parties’ objectives provided there is early and ongoing engagement.

Table One

NoR potential constraint at named level crossings and suggested approach

NoR	Issue	Resolution
<p>NoR 1</p> <p>Construction of a new bridge with general traffic lanes and/or walking and cycling facilities across the NIMT and associated works at Spartan, Manuia, Manuroa Roads and Taka Street and level crossing closures (except Manuia)</p>	<p>Allows for an increase of track capacity however potentially limits provision of maintenance access to improve resilience</p> <p>All bridge structure abutments and pier locations will need to be confirmed in future design.</p> <p>The proposed location of the cul de sac at the end of Takanui Road (Southwest corner) is very close to the existing mains so rail capacity implications need to be worked through with KiwiRail</p>	<p>Ongoing dialogue and engagement before detailed design starts and throughout design process (applies to all issues)</p>

NoR	Issue	Resolution
	<p>Station access from the bridge at Taka Street and from the footbridge at Manuroa Road needs to be well considered in detailed design.</p> <p>The proposed ramps at Manuroa and Spartan Roads need careful attention due to proximity to the rail corridor and overhead electric systems.</p> <p>Future swale and overland flow solutions will require a coordinated approach by AT and KiwiRail.</p> <p>At Spartan Road the major drainage swale/overland flow path in the rail corridor may conflict with footbridge. This needs to be considered in future design work including the reprovision of drainage infrastructure to prevent overland flow into the rail corridor.</p> <p>The location of the footbridge at Spartan Road needs to accommodate the potential capacity enhancement on the eastern side of the existing tracks.</p> <p>Provision required for on-track access from the eastern side</p>	<p>Identification of opportunities for future connection to the platform</p>
<p>NoR 2</p> <p>Construction, operation, maintenance and upgrade of transport infrastructure on Walters Road, Takaanini, which includes the closure of the existing level crossing on Walters Road, a new bridge with general traffic lanes and walking and cycling facilities across the North Island Main Trunk (NIMT) line, as well as local road connections and all associated works</p>	<p>Allowance for future capacity upgrades of rail system including access for construction, operation and maintenance needs to be considered.</p> <p>The bridge structure abutment and pier locations need to be confirmed in future design.</p>	<p>Ongoing dialogue and engagement before detailed design starts and during design process.</p>

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1259] Notice of Requirement online submission - Jayanta Bhaduri and Sudarshana Bhaduri
Date: Thursday, 14 December 2023 9:16:06 pm
Attachments: [Submission against building over bridge on Walters Road \(2\).pdf](#)

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Jayanta Bhaduri and Sudarshana Bhaduri

Organisation name: n/a

Full name of your agent: n/a

Email address: jayantabhaduri5@gmail.com

Contact phone number: 0212374181

Postal address:
3 ARION ROAD
TAKANINI
AUCKLAND 2112

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:

*Our Property will be affected in terms of: The Noise factor during building of the Bridge. The Noise factor post the bridge being built. The dust factors that will cause serious health issues to the residents We are anxious that the park besides our house will be destroyed. The substantial decrease in property value because of the proximity to the said bridge. Risk of Flooding.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

If Manuroa Road which is an arterial road for most of the industrial traffic, heavy trucks, is closed off all of it will diverted via Arion Road, which will create an absolute nightmare for the residents.

I or we seek the following recommendation or decision from Auckland Council:

We would like the Council to consider: All of the above factors mentioned including the following: The building of an underpass for the traffic under the railway line, which will take care of the safety issues as well as the traffic wait times that is being presumed to be a substantial hindrance, for the ease of future traffic flow. Of course keeping in mind the flooding factors. To consider the marring this well-organised neighbourhood and destroy the aesthetics of this place, which was the prime factor of our buying this property. Also building this bridge will create substantial traffic jam at the Great South Road crossing. Also there will be antisocial activities under the bridge.

Submission date: 14 December 2023

Supporting documents

Submission against building over bridge on Walters Road (2).pdf

Attend a hearing

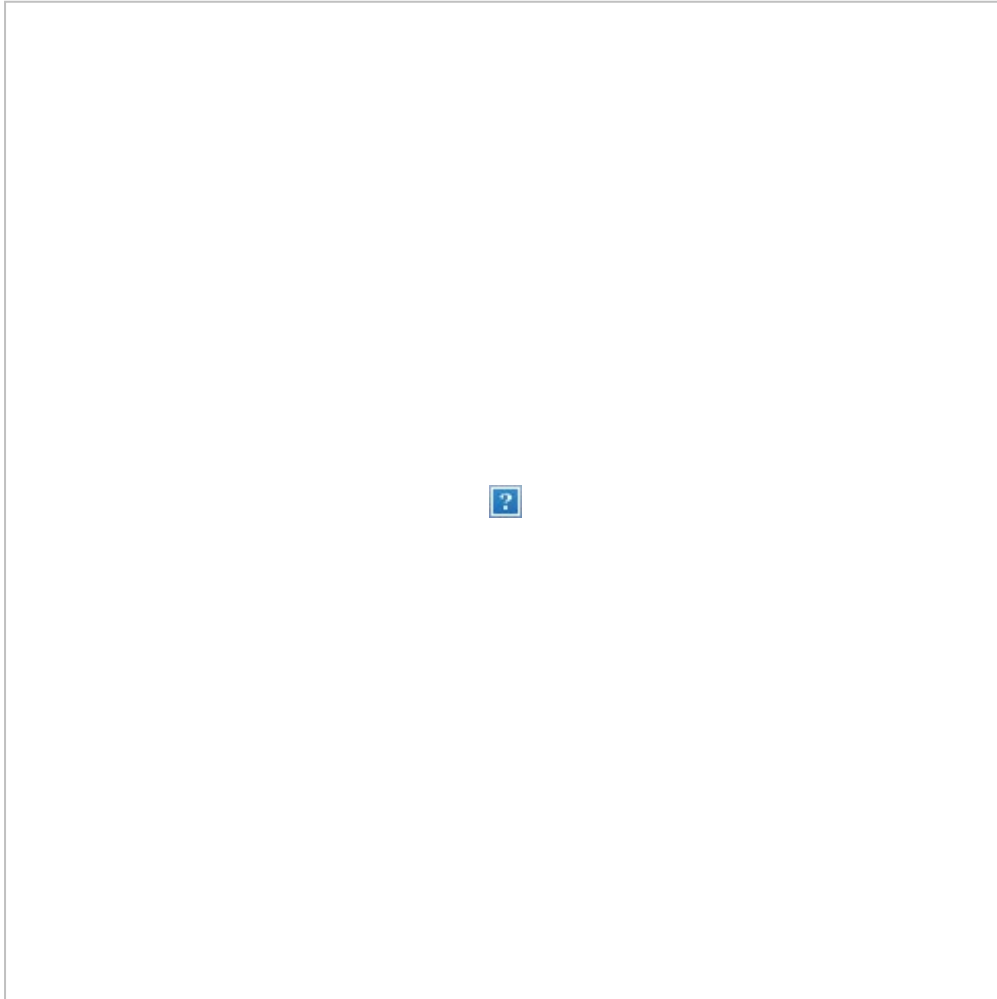
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

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Submission against building over bridge on Walters Road

This is to bring to your kind notice that we had come to New Zealand 20 years back to stay in a green and nice country. Initially we were staying at Papatoetoe and then we saw this place in Addison in Takanini and we loved it and have somehow purchased the house at 3 Arion Road and moved in 2014. We decided to purchase this house as it has a lush green reserve with lovely trees in front and we were told that this open space will never be blocked.

Me and my wife – we have been working very hard all these years to pay for this house and are still doing so that we can be mortgage free to stay in a beautiful house in a beautiful surrounding.

However, it is like a bombshell when we were informed that they are going to build a railway over bridge which will severely impact our house and surrounding area.

The logic of making an over bridge was given by Auckland Transport are:

1. To make it safe for people.
2. So that people do not have to wait for long at the level crossing when there will be more number of trains in future.

My replies to these are:

1. There is an electronic gate at the level crossing which does not allow anyone to cross the train line when it is closed when trains are plying. So why it is unsafe now?
2. During pick hours, there is not more that 10 cars which wait while trains pass by. This waiting time is not more than 2 to 3 minutes at the most. Spending such huge amount of tax payers money unnecessarily to make over bridge for such a reason is not justified at all. During normal hours there are hardly one or two cars which wait at the level crossing.
3. During pick hours at the motorway, people wait for hours. AT should rather think how to ease the congestion and waiting time at the motor way rather than wasting time, energy and tax payers' money on the project like this.
4. Also people wait at Middlemore hospital emergency for minimum 6 to 7 hours (if not more) to get urgent medical attention. Patients needing urgent surgery have to wait for months to get treated. Tax payers' hard earned money should be utilized there and not in this kind of silly project of over bridge at Porchester Road.
5. Also if AT regularizes train services in such a manner that both up and down trains crosses Takanini level crossing at the same time, people do not have to wait for up or down trains at the level crossing.
6. Also it is understood that the two existing level crossings will be closed. This would mean more traffic the other routes. This will mean very high level of traffic in the other routes. It is like closing a human artery which would mean more blood will flow through other arteries which will cause high blood pressure and my cause stroke. Same principle is applicable at this instance as well. The logic of closing these two level crossings is not understood.
7. Also heavy traffic will mean jam at Great South Road where the overbridge is supposed to end.

8. Building overbridge will mean lots of vibration which will damage our property.

9. It will have a very high level of noise and dust.

10. Pollution will increase many fold.

11. Nature and beauty of the place will be ruined.

12. So many trees will need to be chopped – destroying the nature.

13. Also we may need another motorway and not this over bridge.

14. Property value will be decreased.

15. People will not buy the property if over bridge is built.

In view of the above, our submission is please reconsider it and do not please we repeat **PLEASE DO NOT BUILD THE OVERBRIDGE.**

Thanks & Regards

Jayanta Bhaduri & Sudarshana Bhaduri

3 Arion Road

Takanini

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1270] Notice of Requirement online submission - Parks and Community Facilities
Date: Thursday, 14 December 2023 10:15:38 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: Parks and Community Facilities

Organisation name: Auckland Council

Full name of your agent: Bianka Griffiths

Email address: bianka.griffiths@aucklandcouncil.govt.nz

Contact phone number: 027 337 3218

Postal address:
Auckland House - Level 12
135 Albert Street
Auckland
Auckland 1010

Submission details

Name of requiring authority: Auckland Transport

The designation or alteration: Takanini Level Crossing: NOR 2 Walters Road level crossing closure and new multi-modal bridge

The specific provisions that my submission relates to are:
The direct effects of NoR 2 on 40R Walters Road, Takanini Auckland 2112.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:
The submitter is opposed to NoR 2 to the extent that part of 40R Walters Road will be acquired by the NoR. The submitter is concerned about the loss of public open space and its assets.

I or we seek the following recommendation or decision from Auckland Council:
If the extent of effects of NoR 2 on 40R Walters cannot be avoided, then the requiring authority must mitigate or remedy the loss of public open space and the submitter's assets caused by NoR 2 so that the same or more public open space is provided in a strategic location that is in proximity any area taken by NoR 2.

Submission date: 14 December 2023

Attend a hearing

Do you wish to be heard in support of your submission? Yes

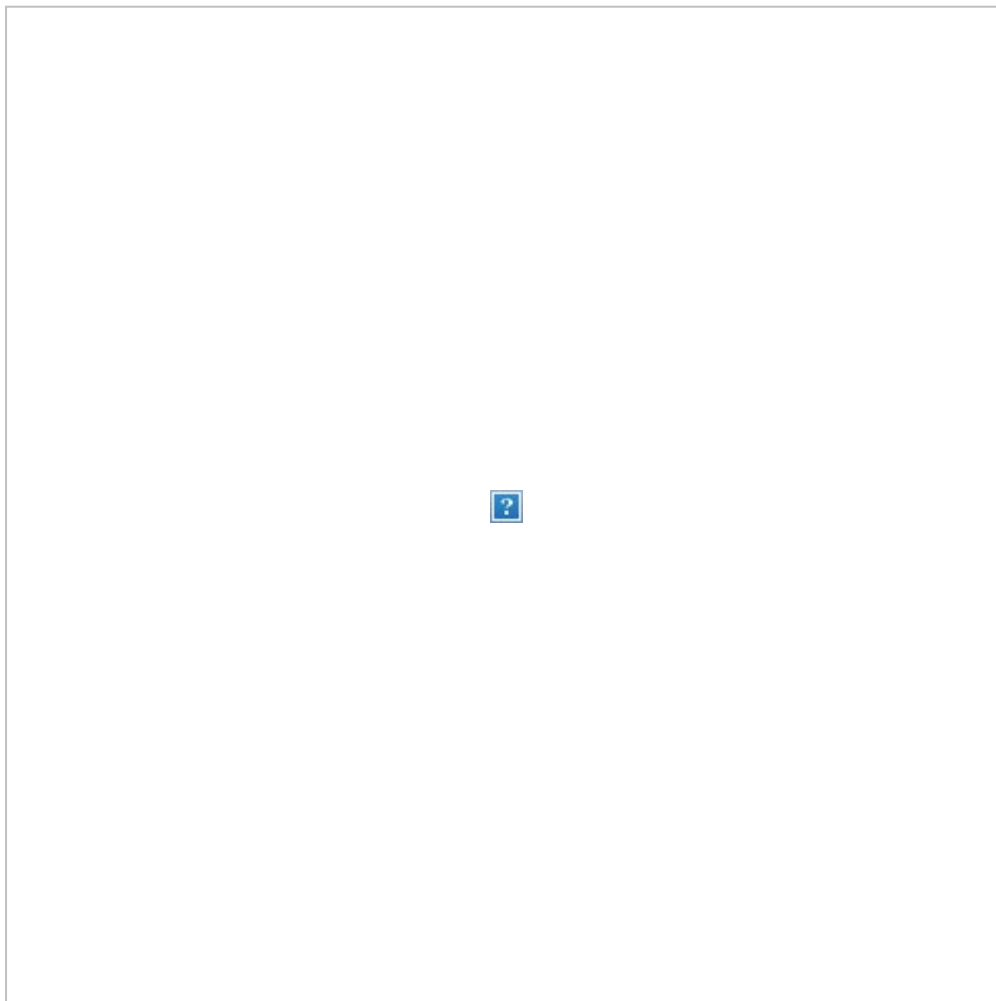
Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

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To:

Planning Technicians
Plans and Places
Auckland Council
PB 92300
Auckland 1142
By email: unitaryplan@aucklandcouncil.govt.nz

Supporting Growth - Notice of Requirement (NoR) 2 – Rail Crossing at Walters Rd – Takaanini**From:**

Steven Dogra
Takaanini Childcare Investments Ltd
20 Walters Rd
Takanini

This is a submission on a NoR:

By: Auckland Transport

For: Takaanini Level Crossing (NoR 2) – Walters Rd

The specific parts of NoR that our submission relates to are:

The affected property address is 20 Walters Rd Takaanini.

Our land is subject to the NoR to take the whole property to put an overbridge over the railway. Currently, we run a very successful purpose built childcare business on the land called “Go Bananas Childcare”.

Our submission is:

We oppose to the NoR.

1. The general reasons for our opposition are:

We understand that the main reason for the NoR is to find solutions to the current transport issues and remove the safety risks associated with rail crossings.

However, there are several issues that we have with the proposal.

In general terms, the NoR does not:

- meet the Purpose and Principles in Part 2 of Act;
- avoid, remedy and mitigate the significant adverse effects that the NoR will have on our social and economic wellbeing;

- meet the requirements of a NoR including the provisions in the Act, including s171, higher order national policy statements, and the AUP-RPS;
- adequately evaluate alternative sites, routes or methods, for undertaking the works;
- meet the threshold for being work that is reasonably necessary;
- place sufficient weight on the social and economic costs to landowners and businesses in the overall AEE assessment; and
- demonstrate sound resource management practice.

Without limiting the general scope of the issues raised above, the following specific concerns are also raised.

2. Economic and Social Impacts

As mentioned, my property at 20 Walters Rd is where we run our well established and successful childcare business, that serves the community. It is not easy to run a centre that complies with all the increasingly stringent regulatory requirements.

We run a purpose-built childcare facility to cater to the growing demand in Takanini. It is licensed by the Ministry of Education (MOE) for 100 children and can take up to 115 children. Demand for childcare is outstripping the supply of services as thousands of new houses and development have been planned in the area and is being built.

The high childcare demand from families saw the centre quickly grow to around 100 children within 5 months in the first year, despite COVID slowing down many centres in other areas.

Surplus land is available in Takanini on which more housing is planned and will keep increasing demand for childcare in future.

There are early childhood education (ECE) sector changes taking place and Network approval is required from 1st February 2023 before establishing a new childcare service. Barriers to building new childcare centres have become higher and a pre-approval is now required for new centres, while demand in the area is increasing with housing intensification.

Further barriers to entry are from competition with developers who are also buying for housing intensification, rendering it harder to build a childcare centre without a significantly higher rent as we are seeing for new builds. We are required to secure larger sites for an economic facility.

It takes a minimum of 3 years to build a new childcare of this scale from conception - Network Approval from MOE, resource consent, building consent from Council, engaging a builder and secure funding from the bank, etc.

Therefore, our licensed facility from MOE, is not easy to replace, if uprooted, and loss of the service would cause a childcare shortage in a high demand area—this can be an impediment for families to work and contribute to GDP.

Childcare centres such as ours, while privately run, are an essential service to the community.

Regarding the business itself, there will be significant economic losses and costs if the business is lost. It would also adversely affect the social and economic wellbeing, and livelihoods, of the approximately 20 staff that we provide employment for.

One of our biggest concerns is that we will not be able to get a Ministry of Education licence under new network management regulations after we lose our current one which is based on the current site. Not having a licence will significantly impact on our livelihood.

Therefore, it is not simply a matter of relocating to another physical site, even if a suitable site were available.

Also, the NoR will jeopardise the relationships we have been building with our children and their parents since opening. Many people in the community rely on us to provide quality and safe childcare, and understandably, earning the trust of parents is not easy and has taken a lot of hard work by our skilled management and staff. If the centre closes and there is no continuity with any new centre, it is inevitable that most of our team will be lost to other centres, and we will have to start again.

We are unsure about how the Supporting Growth project and will properly compensate us for the social and economic effects that our business, staff, parents, and children, will suffer with the taking of the land.

It is also unclear what form of assistance we may receive to secure another site, relocate to it, and very importantly, ensure that all of the necessary licencing and regulatory requirements are met.

3. Necessity of the Works

It is submitted that the works are not reasonably necessary, at least currently. It is noted that one of the main original strategic justifications for the works was to service the growth that was planned in the AUP for Future Urban Zoned (FUZ) land in Takanini and areas further south such as Drury.

However, while some parts of these areas are proceeding with plan changes from live zoning, the recently confirmed Auckland Council Future Development Strategy (FDS), removes some FUZ zoning in Takanini due to flooding risks, and defers the timing for the development of other Southern areas. Therefore, while some growth in the area is acknowledged (as above) it is not clear to us that these rail upgrade works are necessary to be undertaken at this time.

4. Timing and Uncertainty

Even at this relatively early stage of the NoR process, the uncertainty created by the proposed taking of our land risks affecting the childcare centre and the levels of service we

are offering. Because this is a public process, parents are aware that the Centre may be closing.

As to be expected, parents undertake a lot of investigations before committing to a childcare facility. This is particularly the case where they have, or are planning to have, more than one child and understandably want the continuity and convenience of attendance at one centre over many years. This provides more stability for the children and fosters deeper relationships between children and their parents and the staff.

The lapse time for designations under s 184 of the RMA is 5 years unless the designation provides a different period.

It is noted that a designation was deemed the most logical and effective method due to the certainty it provided all parties including affected landowners (e.g., on timing).

However, contrary to the reasoning above, for this project, the time frame has been increased to 15 years. This significant period, 3 times the statutory guidance, is not certain for us in any sense.

We do not consider it acceptable, and consistent with the overall purpose of the powers of designation, to, for example, confirm a designation, which effectively sterilises the land for sale and changing/maintaining the activities, when there is no planned capital budget to undertake the works. This means that the project is effectively “speculative”, and may in fact, never be implemented. Government and Council/AT priorities could change.

Therefore, if the Panel, were to grant such a long period of time as 15 years for the NoR before it lapses, the question is who should bear the effects of the uncertainty during this period, the landowners, or the requiring authority, that is exercising arguably draconian powers to take private property?

It is submitted that it is only fair and reasonable that it is the agency that should bear the consequences of the uncertainty created by the NoR/acquisition process, because they have control over the timing and outcomes.

For example, if our business was compromised for 12 years because we do not invest in it, and parents prefer other centres because ours could close at relatively short notice, but then the project is cancelled or changed at year 13, that would be very unfair. There are usually waitlists for the better centres, and parents would not want to compromise care at a lower quality facility, so are likely to try and avoid that risk, by not committing to our centre due to uncertainty.

The only reasonable approach is that if the full works are not funded in the next 5 years, at least the budget for land acquisition should be funded. If then the works are not undertaken at year 13, the agency can then resell the land that they had acquired. The designating authority is responsible for the timing, and it should bear the costs of any losses with reselling rather than the business and landowners bearing this social and economic burden for an unreasonable period.

Owners should at least have some influence over the timing of acquisition to try and accommodate the adverse effects on their businesses.

5. Alternatives Not Property Considered

Section 171(1)(b) of the RMA requires that adequate considerations should be given to alternative sites, routes, or methods when making a recommendation on a NoR when the requiring authority does not have interest in the land.

We do not believe that all the alternatives have been adequately considered in the process by AT to limit the amount of land being taken and used.

It is noted that Appendix A did assess the alternatives to a degree. However, several options did not progress past the initial considerations, and should have been more fully investigated.

Furthermore, options were not properly assessed against the impact that it would have on the surrounding people's property/land.

One option which could have limited the amount of land used and taken would have been rail-under-road trench. When comparing the rail-under-road trench and road-over-rail bridge summary they both had the same investment objectives, cost, and safety and design. A rail-under-road trench method of achieving the grade separation had one higher technical and two higher consentability requirements and was a bit of a longer time period.

Regardless, this option should have been explored more due to it potentially impacting the livelihoods of people in the surrounding housing areas less, for reasons including, avoiding, remedying, and mitigating:

- The adverse noise effects of the whole rail corridor on residents flanking the corridor.
- The adverse amenity effects of the unsightly gantry and cable infrastructure to provide power to the trains.
- The adverse visual amenity and dominance and shading effects of large concrete overbridge structures on the residential dwellings and public street and pedestrian spaces.

Further assessment of the rail-under-road trench could have found solutions to limit disruptions to the rail operations.

Even if the rail-under-road trench could not have been applied at Spartan Road (stated in Appendix A) there could have been several options considered to ensure that the right option was applied depending on the needs of the different areas.

6. Adverse Social and Economic Effects

If the NoR is confirmed, it is essential that the requirements of s5 of the Act, in terms of enabling people to provide for their social and economic wellbeing is achieved (s171(1)) is subject to Part 2).

We want fair compensation for our business, property, staff, and services to the community that are at risk. Once the Demand Notice will be approved by Auckland Council, then it will be listed on our title and after that our business and property prices will be greatly impacted. This will lead to lower confidence of parents in the centre and less revenue so less investment in the physical infrastructure and services, so amenity values drop, and these risks becoming a negative self-reinforcing decline.

While it is understood that the acquisition process is separate, it is requested that the Panel carefully consider what provisions should be put in place to ensure that landowners are properly compensated for adverse impacts on their social and economic wellbeing.

7. We seek the following recommendation or decision from the Council.

That the NoR be declined.

In the alternative, that the conditions of the NoR, including the duration of the approval, and process of acquisition, be shortened to 5 years to address the uncertainty for landowners and enable people to continue to provide for their social and economic wellbeing.

We wish to be heard in support of our submission.

Filed on behalf of:

Takanini Childcare Investments Ltd
20 Walters Rd
Takanini

By their Counsel:



Peter Fuller
Barrister
Quay Chambers

Address for Service:

PO Box 106215, Auckland City 1143
peter.fuller@quaychambers.co.nz
021 635 682

Date: 14 Dec 2023

Form 21

Submission on a requirement for a designation or an alteration to a designation subject to full or limited notification under Section 168A, 169, 181, 189A, 190 and 195A of the Resource Management Act 1991

Date: 14 December 2023

To: Auckland Council

Name of Submitter: Te Tāhuhu o te Mātauranga | Ministry of Education

Address for Service: Woods
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Submission on two Notices of Requirement for Te Tupu Ngātahi Supporting Growth for the Takaanini Level Crossing Project

SUMMARY

- 1) The Ministry of Education (“**the Ministry**”) is the Government’s lead advisor on the New Zealand education system, shaping direction for education agencies and providers and contributing to the Government’s goals for education.
- 2) Te Tupu Ngātahi Supporting Growth Alliance (“**Te Tupu Ngātahi**”) has lodged two Notices of Requirement (“**NoR**”) for the Takaanini Level Crossing Project (“**TLC**”):
 - NoR 1 – Takaanini Level Crossing: Spartan Road, Manuia Road, Manuroa Road and Taka Street - Auckland Transport
 - NoR 2 – Takaanini Level Crossing: Walters Road level crossing closure and new multi-modal bridge – Auckland Transport
- 3) This submission relates to both NoRs lodged by Te Tupu Ngātahi on behalf of Auckland Transport.
- 4) The Takaanini Level Crossings Project is located in proximity to a range of designated schools within Takaanini, Papakura, and Manurewa. There is potential for these existing schools, or any future schools developed in this area, to be affected by traffic, noise and other nuisance effects arising from future construction works of this transportation network. The Ministry is seeking to ensure that appropriate conditions are included in the designations to mitigate any adverse effects associated with the construction of the TLC.
- 5) The Ministry supports the provision of active transport modes (walking and cycling) as proposed through the TLC.
- 6) Overall, the Ministry’s submission is neutral on the NoRs subject to changes being made to the conditions as set out in this submission.
- 7) The Ministry wishes to be heard in support of its submission.

OVERVIEW OF THE MINISTRY’S RESPONSIBILITIES & LAND INTERESTS

- 8) The Ministry is the Government’s lead advisor on the New Zealand education system. The Education and Training Act 2020 sets out the obligations and responsibilities of the Ministry. The Ministry have responsibility for the education outcomes of students across the full spectrum of the education sector, including pre-school, primary and secondary levels.
- 9) The Ministry assesses population changes, school roll fluctuations and other trends and challenges impacting education provision at all levels of the education network to identify changing needs within the network so the Ministry can respond effectively.
- 10) The Minister of Education is a Requiring Authority under the Resource Management Act 1991 (“**RMA**”) and has over 400 education purposes designations in the Auckland Unitary Plan: Operative in Part (“**AUP:OP**”).
- 11) The Ministry has responsibility for all education property owned by the Crown. This involves managing the existing property portfolio, upgrading and improving the portfolio, purchasing and constructing new property to meet increased demand, identifying and disposing of surplus State school sector property and managing teacher and caretaker housing.
- 12) The Ministry is therefore a considerable stakeholder and social infrastructure provider in terms of activities that may impact existing and future educational facilities and assets in the Auckland region.
- 13) The Ministry has multiple education sites within the area (a 3km radius from NoR 1 or 2) including:
 - Alfriston College (4901)
 - Greenmeadows Intermediate School (4929)
 - Manurewa East School (4952)
 - Manurewa South School (4955)
 - Randwick Park School (4981)
 - Conifer Grove School (5013)
 - Cosgrove Primary School (5014)
 - Edmund Hillary School (5016)
 - Kelvin Road Primary School (5018)
 - Papakura Central School (5020)
 - Papakura High School (5021)
 - Papakura Intermediate School (5022)
 - Papakura Normal School (5023)
 - Takanini School (5028)
 - Kauri Flats School (5056)
- 14) The location of each NoR in relation to the Ministry’s existing assets is shown in **Figure 1**.

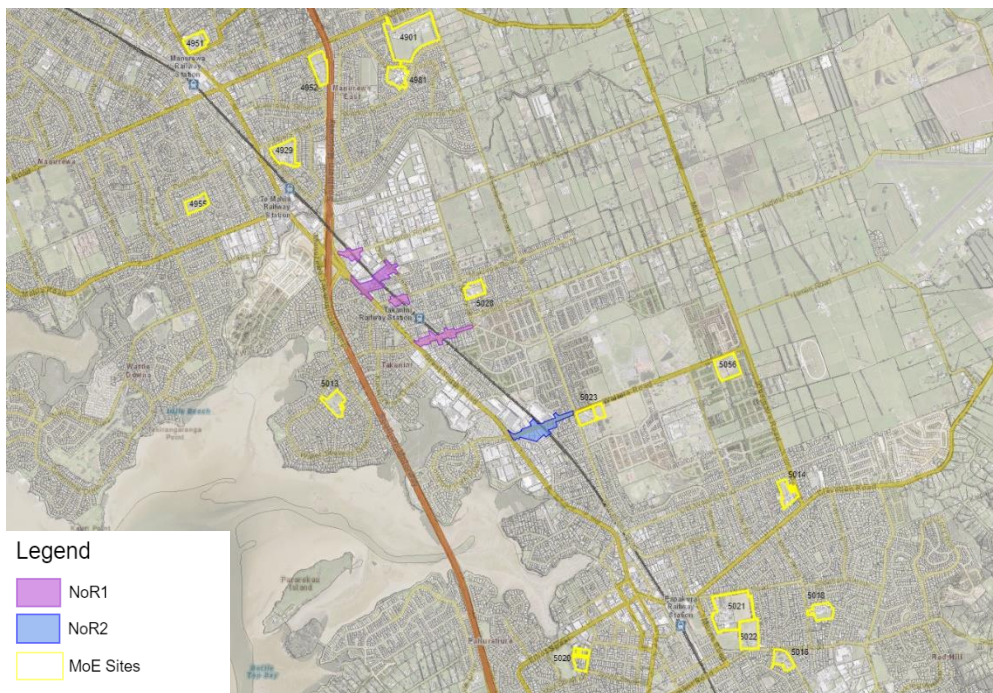


Figure 1: Project Overview – Location of two NoRs (identified in purple and blue) in relation to the Ministry of Education’s School Network (outlined in yellow)

MINISTRY OF EDUCATION'S SUBMISSION

- 15) Under the RMA, decision-makers must have regard to the health and safety of people and communities. Furthermore, there is a duty to avoid, remedy or mitigate actual and potential adverse effects on the environment.
- 16) The two NoRs will designate land to respond to both existing transport deficiencies as well as provide for the forecasted future growth pressures in the area. There are currently four public road level crossings along the North Island Main Trunk ("NIMT") line in the Takaanini area at Spartan Road, Manuroa Road, Taka Street, and Walters Road. Each of these currently experience congestion, severance, and an elevated level of safety risk stemming from the operation of the level crossings at grade. The project supports improved walking and cycling, public transport, and general traffic connections. The key reasons for this investment are to improve safety, better integrate transport and land use, improve accessibility, transport resilience, and promote travel choice. This is in the form of new bridges with general traffic lanes and walking and cycling facilities across the NIMT line.
- 17) The Ministry broadly supports the Project's aim to plan transport investment in Auckland's future urban zoned areas. The project will improve active mode facilities, enhancing the safety of students walking and cycling to and from school.
- 18) The Ministry supports the provision of construction, operation, maintenance and upgrade of transport infrastructure that will provide safe access to the current and future wider school network. Encouraging mode shift will provide significant health benefits for students and staff, reducing traffic generation at pick-up and drop-off times. Schools should be well serviced by safe and accessible pedestrian and cycling links as well as public transportation facilities, and it is considered that the proposed upgrades will provide adequate cycling and walking infrastructure to the surrounding area.
- 19) The Takaanini project is a large programme of work. The quantum of construction required to deliver the projects will likely have temporary adverse effects on the surrounding environment. There are several schools in proximity to the NoRs. There is potential for these schools to be affected by traffic, noise and other nuisance effects arising from future construction works. The construction timing and staffing is yet to be determined, so there is uncertainty regarding the construction methodology, including the routes for construction vehicles and the location of construction laydown areas.
- 20) The Ministry seeks to appropriately address and manage construction-related effects and the ongoing potential effects the project may have on the operation and management of the schools and any future schools for NoRs 1 and 2, as the project has a lapse date of 15 years.
- 21) The key issues that the Ministry has concerns about in relation to the NoRs include construction traffic effects and stakeholder engagement which are outlined below. Consequential changes are also sought to the acronyms/terms and conditions of the NoRs for consistency with other Te Tupu Ngātahi designations.

Construction traffic effects

- 22) The surrounding schools (and any future schools) will potentially be affected by an increased volume of heavy vehicles to access the construction area of the NoRs. This is a traffic safety concern for students walking and cycling to school at peak pick-up and drop-off times.
- 23) Condition 18 requires the preparation of a CTMP prior to the start of construction. The Ministry supports the inclusion of this condition but requests that specific reference is made to education facilities to address the estimated numbers, frequencies, routes and timing of traffic movements, including any specific non-working or non-movement hours (for example on roads servicing educational facilities during pick-up and drop-off times) to manage vehicular and pedestrian traffic near educational facilities or to manage traffic congestion.

Construction noise and vibration effects

- 24) The surrounding schools (and any future schools) will potentially be affected by an increased volume of construction noise and vibration due to the construction of these NoRs.
- 25) Conditions 21 and 22 requires the preparation of a CNVMP prior to the start of construction. The Ministry supports the inclusion of this condition but requests that specific reference is made to education facilities to ensure they are taken into consideration as part of the development of this plan as a key stakeholder. In addition, the Ministry requests that any construction activities that could be expected to significantly exceed the permitted noise and/or vibration levels are undertaken outside of study and exam periods to minimise disruptions to students' learning.

Stakeholder engagement

- 26) The Ministry supports the establishment of Stakeholder Communication and Engagement Management Plan (SCEMP) (condition 9). However, the Ministry considers that they are a key stakeholder in this Project, and specific engagement is required to manage construction effects on the schools.
- 27) Amendments made to conditions are requested to ensure consistency with the changes made to the Te Tupu Ngātahi Warkworth NoR conditions as included in the Strategic Planning & Conditions Rebuttal Evidence prior to the Council hearing. This includes the requirement that at least 6 months prior to construction, the requiring authority shall identify a list of stakeholders and properties and identify methods to engage with stakeholders and submit this record with any Outline Plan of Works for the relevant stage of work. The inclusion of a new condition that addresses this, is consistent with other conditions agreed through Te Tupu Ngātahi designations.

Acronym/Terms

- 28) The Ministry seeks that acronyms and terms used in the NoRs are consistent with those agreed through other Te Tupu Ngātahi NoRs. As these terms are continuously evolving through hearings on NoRs, a summary of the terms supported is provided below:
- Education facility
 - Stakeholder

RELIEF SOUGHT

- 29) In principle, based on the above, the Ministry supports the proposed walking and cycling facilities proposed in each NoR application providing improved active mode connectivity is essential to provide existing and future communities with a sustainable means of accessing education facilities in Takaanini area.
- 30) To ensure effects associated with the NoRs on the Ministry are appropriately managed, it is requested that amendments to conditions as set out above are adopted by Te Tupu Ngātahi. The amendments sought include:
- Inclusion of the Ministry as the primary contact for schools in the SCEMP
 - Amend the CNVMP condition
 - Amend the CTMP condition
 - Amendments to conditions, and acronym/terms to be consistent with other Te Tupu Ngātahi NoRs
- 31) Such other consequential amendments to the NoRs may be necessary to give effect to the relief sought through this submission.
- 32) Overall, the submission is neutral subject to the above changes being made to the designation conditions.
- 33) The Ministry wishes to be heard in support of its feedback.

SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Takanini Village Limited and Tonea Properties (NZ) Limited

1. Takanini Village Limited (**TVL**) and Tonea Properties (NZ) Limited (**TPL**) make this submission on the two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About TVL and TPL

2. TVL owns the leasehold interest in the land for Takanini Town Centre at 30 Walters Road, Takanini. TPL is the freehold landowner for Takanini Town Centre. Takanini Town Centre is situated on 5.4 hectares and has a gross floor area of over 19,000m². It is anchored by The Warehouse and integrates a diverse offering of more than 45 stores comprising retail, food and beverage, commercial offices, a (soon to be opened) Silky Otter cinema, medical centre and Te Paataka Koorero o Takaanini, the Takaanini Library and Community Hub.
3. The single landholding comprising Takanini Town Centre is owned and operated by TVL.
4. TVL and TPL (and therefore the tenants) will be significantly impacted by the Project, which includes works from NoR2 on Walters Road immediately south of the site, as well as tie-ins to portions of Arion Road, Braeburn Place, Tironui Road, the Porchester Road roundabout, and the Great South Road roundabout. The Project will have a direct impact on the one-way access into the town centre including the main access to the underground car park. The Project will also reduce the parking spaces in the southern carpark that serve the town centre and sever existing pedestrian connections both to the town centre and its surrounds.

5. The Submitters also have concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre.

Submission

5. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
6. TVL and TPL generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects. However, TVL and TPL oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on TVL and TPL and significant adverse effects on the surrounding environment including:
 - a. Adverse effects on the operation and safety of access from Walters Road to the Takanini Town Centre;
 - b. Potential adverse effects on the Walters Road/Arion Road intersection;
 - c. Parking effects during construction of the Project and following completion of the Project;

- d. Adverse effects on the interface with the Takanini Town Centre, including landscape effects and urban design considerations;
- e. Construction noise and vibration effects;
- f. Adverse geotechnical effects on existing buildings and infrastructure, including on the underground basement to the building in the southeast corner of the site;
- g. Adverse impacts on existing service connections to the site including water, fibre, gas, power and impacts on stormwater networks both piped and overland flows; and
- h. Adverse impacts on existing pedestrian and vehicular access and carparking including the loss of pylon signage, carparks, electric vehicle charging infrastructure, gardens and irrigation.

7. The Submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

- 8. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
- 9. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network

have not been given adequate consideration. Furthermore, the Requiring Authority's assessment of alternatives is cursory and falls short of "adequate".

10. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:

- a) Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
- b) Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
- c) Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
- d) Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
- e) The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
- f) Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
- g) Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
- h) A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase

and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

11. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:
 - a) The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
 - b) An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
 - c) There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
 - d) The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
 - e) There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on Takanini Town Centre and other surrounding properties and businesses.
 - f) There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for Takanini Town Centre and its surrounding sites.
 - g) An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Traffic Safety Effects

12. The Requiring Authority proposes to temporarily (for a period of 2.5 – 3 years) restrict access from Walters Road to the Takanini Town Centre. Critically this will place sole dependency on the remaining access into the site from Arion Road (a local residential road) during the

construction period and it will not be able to operate safely or efficiently. Based on the limited information provided it is not clear if the one-way in to the Town Centre from Walters Road will be lost permanently. This will result in significant adverse traffic effects on the surrounding transport network, including the signalised Walters Road / Arion Road intersection and on the safety of all transport users. This will also have an impact on the Arion Road entrance into the Town Centre and will adversely impact on traffic flows within the Town Centre itself. The Requiring Authority has not provided adequate information to support the design or its operation. The Submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

13. During the construction period the Requiring Authority also seeks to acquire carparking land which serves the Takanini Town Centre. The Project does not detail how adequate car parking will be maintained for the Town Centre during the construction period. The Project will result in a shortfall of carparking, and it will result in a direct contravention of a lease agreement to supply minimum carparking ratios to The Warehouse.
14. The NoR2 Project will also remove the electric vehicle charging stations that are located within the southern carpark.

Landscape and Urban Design Effects

15. The Project has not adequately demonstrated that an appropriate interface will be provided to the Takanini Town Centre. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Town Centre (and for nearby residential dwellings).

Construction Noise and Vibration Effects

16. The Project will result in an increase in noticeable noise levels and for some landowners it will result in significant adverse noise and vibration effects over long durations, and at high frequencies.
17. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

18. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
19. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
20. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

21. In the event that NoR1 and NoR2 are confirmed, the Submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.

Conditions should be imposed to ensure the minimum practicable impact on Takanini Town Centre especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

21. The Submitters seek that Auckland Council:
 - a) Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b) Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
22. The Submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

23. The Submitters could not gain an advantage in trade competition through this submission.

24. The Submitters wish to be heard in support of this submission. TVL and TPL would consider presenting a joint case at the hearing.

Date: 14 December 2023

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SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Sunlight Holdings Limited and South Auckland Marine Limited

1. Sunlight Holdings Limited (**SHL**) and South Auckland Marine Limited (**SAML**) make this submission on two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. SHL owns the land at 1-3 Walters Road, Takanini (**Site**). Originally SAML was established at 11 Walters Road before moving to the Site in 2002. SAML owns and operates the marine business undertaken on the Site. The Site is accessed from Walters Road and is located opposite the Southgate Shopping Centre.
3. SAML is a family owned and run business which has been operating in Takanini for almost 40 years. SAML provides goods and services to the marine industry, specialising in all major engine and boating brands. The Site includes a marine retail shop which specialises in boat products, trailer parts, fishing gear and bait supplies, stainless and rigging hardware.
4. In addition to marine retail, SAML are certified Yamaha and Mercury service agents providing engine diagnostic and servicing. SAML also provide services to repair trailers, boat wiring, battery setups, boat repairs and installations. Both the retail and the servicing undertaken on the Site require effective and safe access for vehicles towing boats or trailers. In addition, manoeuvring room on Site and display and/or storage room on Site for vessels for sale or being serviced are critical to the operation of the business.

5. SHL and SAML (as the tenant) will be significantly impacted by NoR2 as:
 - a. The proposed designation covers the entirety of the Site;
 - b. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (located between Walters Road and Tironui Road) cuts through a significant portion of the Site's existing building, making the operation unviable;
 - c. SAML's business requires a highly visible and exposed site with good connections to foot traffic and the public. The Site is well-equipped and is purpose-built for the goods and services provided by SAML;
 - d. The Project will have a direct impact on the one-way access and manoeuvrability into the Site from Walters Road. SAML has large boat and towing vehicles which require sufficient yard turning space; and
 - e. Together, the alignment of the new access lane and proposed extent of the designation boundary mean that continued operation of SAML's business would not be viable. Furthermore, post-construction the rump land will be an inefficient and unusual shape for development or use for the purposes for which it is zoned, have compromised access, and poor amenity due to the size, height and proximity of the proposed new bridge.
6. The Submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

7. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;

- c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
8. SHL and SAML generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
9. However, SHL and SAML oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on SHL and SAML and significant adverse effects on the surrounding environment including:
- a. SAML will not be able to continue its operation. The business will likely need to relocate;
 - b. Any rump land not used for the access road or for construction will be inefficient and unusually shaped which will make use of that land challenging;
 - c. Loss of yard space will severely impact SAML's ability to store and manoeuvre large boats and towing vehicles;
 - d. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;
 - e. Potential adverse effects on the Walters Road/Tironui Road intersection;
 - f. Parking effects during construction of the Project and following completion of the Project;
 - g. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and

- h. Construction noise and vibration effects.

10. The submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

- 11. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
- 12. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority’s assessment of alternatives is cursory and falls short of “adequate”.
- 13. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. This is particularly the case here where SHL’s entire landholding will be subject to the designation. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:

- a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
- b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
- c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
- d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
- e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
- f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
- g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
- h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

14. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.

- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

- 15. The Project will have significant and irreversible adverse effects on SHL and SAML. The location of the proposed overbridge and alignment of the access lanes in combination with construction setbacks will result in either the complete loss of the buildings on Site (such that the SAML operation is no longer viable) or an outcome where a small portion of the Site is retained with limited or no access.
- 16. To the extent that the rump of the Site is retained, the following adverse effects are noted.

Traffic Safety Effects

- 17. The Requiring Authority proposes to temporarily restrict access to sections of Walters Road. This is particularly relevant to the surrounding Takanini Village Centre and Southgate Shopping Centre. This will result in significant adverse traffic effects on the surrounding transport

network and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently. There is also a lack of detail as to how the Site can be accessed during construction.

Landscape and Urban Design Effects

18. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to tenants and customers.

Construction Noise and Vibration Effects

19. The Project will result in an increase in noticeable noise levels and for some landowners it will result in significant adverse noise and vibration effects over long durations, and at high frequencies.
20. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

21. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
22. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
23. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

24. In the event that NoR1 and NoR2 are confirmed, the submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini

Town Centre and the surrounding business and residential landowners including SHL and SAML.

25. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

26. The submitters seek that Auckland Council:
- a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
27. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

28. The submitters could not gain an advantage in trade competition through this submission.
29. The submitters wish to be heard in support of this submission. SHL and SAML would consider presenting a joint case at the hearing.

Date:	14 December 2023
Address for service:	Jeremy Brabant / Shannon Darroch Foundry Chambers Level 4, Vulcan Building Cnr Queen Street and Vulcan Lane PO Box 1502, Shortland St Auckland
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SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Mead Trusts Holdings Limited and Carters Buildings Supplies Limited

1. Mead Trusts Holdings Limited (**MTHL**) and Carters Buildings Supplies Limited (**Carters**) make this submission on two Notices of Requirement (**NoR**) for the Takaanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. MTHL owns the land at 12 Walters Road, Takanini (**Site**). Carters leases and occupies the Site from which it operates the Carters Papakura.
3. The Site has a long history in the timber industry. Carters has been operating as a timber merchant from the Site since the 1960s. Prior to that, timber was sold as part of a timber mill operation that was located on the Site.
4. Carters operates a “to trade” timber merchant business providing building materials to the greater South Auckland region. As part of this operation:
 - a. Carters stores timber and a wide variety of building products both in the yard and inside the warehouse/showroom.
 - b. Carters receive bulk deliveries of timber and construction materials transported via large, long-haul B train trucks.
 - c. Carters delivers products from the Site using small to medium sized trucks.
 - d. Customers can visit the Site to pick up materials.

- e. The Carters buildings are purpose-built, with the entirety of the Site covered in reinforced concrete to suit the business operations undertaken.
5. Currently, Carters Papakura has 26 staff members based in this location who are onsite 6 days a week.
6. MTHL and Carters (as the tenants) will be significantly impacted by the Project (particularly NoR2) as:
 - a. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (fronting Walters Road) cuts through a significant portion of the Site's yard, restricting access to the Site;
 - b. NoR2 will have a direct impact on the sole access to the Site from Walters Road. Deliveries of timber and construction materials by large truck and trailers will not be possible under NoR2;
 - c. Essential truck access will be restricted. Truck access is crucial for the operation of Carters' business;
 - d. NoR2 will reduce the 7,000m² Site by 1,500m². The reduction in site size removes a significant portion of the parking area used by staff and customers. It also results in the removal of adjacent roadside parking;
 - e. The Site has been designed and developed specifically for Carters' building supplies operation. The effects of NoR2's reduction in yard space and the removal of essential access and parking have adverse implications for the Site's fitness for purpose.
7. The Submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

8. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
9. MTHL and Carters generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
10. However, MTHL and Carters oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on MTHL and Carters and significant adverse effects on the surrounding environment including:
 - a. Significant reduction in the Site's yard size which has consequences for the continued operation of Carters on the Site;
 - b. NoR2 directly impacts and restricts the sole access to the Site from Walters Road. Deliveries of timber and construction materials by large truck and trailers will not be possible under NoR2. This will have critical adverse effects on Carters' operation and trade.
 - c. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;

- d. Potential adverse effects on the Walters Road/Great South Road intersection;
- e. Parking effects during construction of the Project and following completion of the Project;
- f. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and
- g. Construction noise and vibration effects; and
- h. Adverse geotechnical effects on existing buildings and infrastructure.

11. The Submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

- 12. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.
- 13. The Submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority’s assessment of alternatives is cursory and falls short of “adequate”.

14. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:
- a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
 - b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
 - c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
 - d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
 - e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
 - f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
 - g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and
 - h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

15. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

16. The Project will have significant effects on the Site's access from Walters Road and there are concerns that during the construction period the restricted access will not be able to operate safely or efficiently. The Site is accessed solely from Walters Road. Carters relies on that access for transport and deliveries of timber and building supplies from large truck and trailers. Removal, limitation or restriction of that access will have fatal consequences on Carters' operations.

17. Particularly when combined with the limitation of access on other businesses on Walters Road, this will result in significant adverse traffic effects on the surrounding transport network, including the roundabout at the Walters Road / Great South Road intersection and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The Submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

18. During the construction period the Requiring Authority also seeks to acquire carparking land which serves Carters (both staff and customer parking). The Project does not detail how adequate car parking will be maintained for the Site during the construction period. The Project will result in a shortfall of carparking, and it will impact staff and customers.

Landscape and Urban Design Effects

19. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Site.
20. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

21. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.
22. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
23. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

24. In the event that NoR1 and NoR2 are confirmed, the submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.
25. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

26. The submitters seek that Auckland Council:
- a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
27. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

28. The submitters could not gain an advantage in trade competition through this submission.
29. The Submitters wish to be heard in support of this submission. MTHL and Carters would consider presenting a joint case at the hearing.

Date: 14 December 2023

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SUBMISSION ON REQUIREMENT FOR DESIGNATION FOR TAKAANINI LEVEL CROSSINGS PROJECT

Section 169 of the Resource Management Act 1991

To Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1142

Name of Submitter: Arborfield Trust, Takanini Home and Trade Limited, and Mitre 10 Mega Takanini Limited

1. The Arborfield Trust (**Trust**), Takanini Home and Trade Limited (**THTL**), and Mitre 10 Mega Takanini Limited (**Mitre 10**) make this submission on two Notices of Requirement (**NoR**) for the Takanini Level Crossings Project (**TLC / the Project**) lodged by Auckland Transport (**Requiring Authority**). The NoRs comprise:
 - a. TLC: Spartan Road, Manuia Road, Manuroa Road and Taka Street (**NoR1**); and
 - b. TLC: Walters Road level crossing and new multi-modal bridge (**NoR2**).

About the Submitters

2. This submission relates to two adjoining parcels of land located at 230 Great South Road and 20A Walters Road (collectively, the **Site**). By way of context:
 - a. The land at 20A Walters Road is owned by the Trust and is leased to THTL trading as Mitre 10. 20A Walters Road stores timber overstock and receives container deliveries of timber. Mitre 10 customers are able to click and collect for retail bulk goods.
 - b. The land at 230 Great South Road Takanini (Southgate Shopping Centre) is owned by Centuria Group. Mitre 10 (the lessee) is the anchor tenant of the Southgate Shopping Centre. This site is sub-leased to THTL. Mitre 10 is a large-format hardware store which supplies customers (including trade customers) with building supplies, home improvement supplies, and garden retail.

3. The Mitre 10 Group is a co-operative that has been operating in New Zealand for almost 50 years. Mitre 10 (Takanini) has been operating from 230 Great South Road for the last 20 years. A measurable portion of Mitre 10's customer base is from the Addison/Botany/Flat Bush and Clevedon areas. Good site access and transport links are essential both from an operational and delivery perspective, and also to service the longer-distance customer base.
4. The submitters will be significantly impacted by the Project (particularly by NoR2 at 20A Walters Road) as:
 - a. The proposed Walters Road overbridge includes the construction of two new access lanes west of the NIMT (north and south of the road corridor and looping under the new Walters Road bridge). The proposed alignment of one of the access lanes (fronting Walters Road) results in the removal of the left turn option into the Site from Walters Road.
 - b. NoR2 will have a direct impact on the sole access into 20A from Walters Road;
 - c. 20A Walters Road is critical to the safe operation of Mitre 10's heavy vehicles, both for deliveries and the dispatching of goods from the Site.
 - d. As proposed, part of the NoR2 designation extends into a portion of the existing building at 20A Walters Road. The building is currently used for unloading containers and for bulk warehousing of timber and building supplies. It is an important component of Mitre 10's operation. The building will be rendered unusable if the land is required for construction of the Walters Road overbridge. 20A Walters Road is the submitters' MPI approved location for the loading and unloading of sea freight. Impacts on that site will have significant flow-on effects for distribution.
 - e. 20A Walters Road is specifically designed to enable deliveries (including container deliveries by large truck and trailers), unloading, and storage of timber. Use of the Site's front carpark for these activities is not a viable alternative as the carpark is not designed for large trucks for example, the trucks cannot turn around, the carpark cannot support tare weight, there is no storage facility, and there will be significant health and safety risks to staff and customers.

- f. The use of B train heavy vehicle combinations has become a regular vehicle of choice in logistics and supply chain. As proposed, NoR2 will limit Mitre 10's ability to use B trains.
5. The submitters also have general concerns about NoR1 which will impact on Spartan Road, Manuia Road, Manuroa Road and Taka Street, to the north of the Takanini Town Centre and result in cumulative adverse effects on the Takanini Town Centre, Southgate Shopping Centre, and surrounding businesses.

Submission

6. This submission relates to the Project being NoR1 and NoR2 in their entirety, but with a particular focus on:
 - a. The inadequacy of the consideration of alternative sites, routes and methods for the Project;
 - b. The adverse effects of the Project during the construction phase;
 - c. The adverse effects of the Project when completed and operational;
 - d. The inappropriate extended lapse period proposed of 15 years; and
 - e. The appropriate conditions imposed on NoR1 and NoR2 if they are confirmed.
7. The submitters generally understand and support the key outcome proposed by the Requiring Authority to enable safe east-west movements across the NIMT, which if undertaken using appropriate sites, routes and methods will bring benefits in providing for improved and safe walking and cycling facilities in the wider network and improved integration with existing and future public transport networks which support mode shift and travel choice without generating significant consequential adverse effects.
8. However, the submitters oppose the Project in its current form on the basis that the Project, as notified, has not given adequate consideration to alternative sites, routes and methods and as proposed will have unacceptable adverse effects on the submitters and significant adverse effects on the surrounding environment including:
 - a. Construction effects and the potential use of 20A Walters Road for construction purposes which will affect or remove part of the existing building on that site. This

will result in flow-on consequences for the operation and supply of materials for Mitre 10;

- b. Adverse effects on the operation and safety of access from Walters Road to surrounding business, particularly those with primary or sole access from Walters Road;
- c. Potential adverse effects on the Walters Road/Great South Road intersection;
- d. Parking effects during construction of the Project and following completion of the Project;
- e. Adverse effects on the interface with businesses along Walters Road including landscape effects and urban design considerations; and
- f. Construction noise and vibration effects; and
- g. Adverse geotechnical effects on existing buildings and infrastructure.

9. The submitters say:

- a. There is insufficient evidence for both NoR1 and NoR2 that the extent of designated land enabling the overpasses proposed is “reasonably necessary” to achieve the objectives of the NOR.
- b. There is insufficient evidence that the extent of designated land enabling the proposed pedestrian crossing access is “reasonably necessary” to achieve the objectives of NoR1.
- c. The objectives of the designations as sought can be achieved through an alternative method (i.e. underpass) which will significantly reduce the extent of private land required and reduce the level of adverse environmental effects.

Inadequate Consideration of Alternatives

10. Section 171(1)(b) of the RMA requires that when making a recommendation on an NoR, a territorial authority shall consider whether adequate consideration has been given to alternative sites, routes, or methods of undertaking the work in circumstances where the Requiring Authority does not have an interest in the land sufficient for undertaking the work; or where it is likely that the work will have significant adverse effects on the environment.

11. The submitters consider that both the physical form of the grade separation method proposed as well as the alignment and physical extent of each east-west crossing in the TLC network have not been given adequate consideration. Furthermore, the Requiring Authority's assessment of alternatives is cursory and falls short of "adequate".
12. The greater the impact on private land, the more careful the assessment of alternative sites, routes, and methods not affecting (or affecting to a lesser degree) private land will need to be. There are significant shortcomings in the assessment of alternatives, with other available methods resulting in a lesser extent of adverse environmental effects on private land. The Submitters consider that the assessment of these options explored has not been proportional to the potential effects of the options being considered. These adverse effects include:
 - a. Reduced access to a large number of properties which front the five project areas which may result in a number of businesses that are considered important to the community potentially being lost;
 - b. Disjointed connectivity and disruption of the existing urban form with a reduced amount of east-west connectivity across Takanini, which cumulatively will result in significant adverse traffic effects;
 - c. Creation of unsafe pedestrian access crossings (i.e. Spartan Road and Manuroa Road) which are not suitable for all people to utilise;
 - d. Loss of existing open space amenity for the community (i.e. Takaanini Reserve);
 - e. The creation of CPTED issues through the proposed undercroft spaces (i.e., spaces under the bridges e.g. at Taka Street) as there is potential for these areas to become unsafe walking environments, be poorly lit, be compromised by obscure wayfinding and have low amenity values;
 - f. Closure of the current over-dimension freight route along Manuroa Road and inadequate consideration of the effects of the alternative freight movement route;
 - g. Lack of adequate consideration for suitable alternative routes to facilitate traffic, pedestrian, and cyclist movement across Takanini during the construction period of the five grade separation areas; and

- h. A large number of residential and commercial properties will be subjected to significant adverse landscape and visual effects, both during the construction phase and from the cumulative effects resulting from five large structures in close proximity to each other within the Takanini landscape.

13. Furthermore, the Requiring Authority's consideration of alternatives, particularly with respect to an underpass alternative, was inadequate as:

- a. The Requiring Authority has failed to undertake a proper consideration of alternatives that use a lesser extent of land and/or have lesser environmental effects than its preferred option.
- b. An underpass alternative was considered only at a cursory level. There is insufficient assessment as to how an underpass option compares to the Requiring Authority's preferred option.
- c. There is insufficient assessment of effects on the environment resulting from the works enabled by the Requiring Authority's preferred option and how it could be reduced by pursuing an alternative method (i.e. an underpass).
- d. The consideration of alternatives contains significant shortcomings. The adequacy of alternative options was not considered in sufficient detail in supporting reports (including the absence of an appropriate comparative costing assessment).
- e. There is inadequate assessment to support a conclusion that the entire extent of the designation was "reasonably necessary", particularly in relation to the shortcomings in the evaluation of alternatives and the failure to properly assess effects on the Site and other surrounding properties and businesses.
- f. There is an absence of assessment as to whether an alternative route or method would result in reduced environmental effects, particularly for the Site, or the ability for the Site to continue to be used for its current operations.
- g. An underpass option is not suppositious or hypothetical and ought to have been adequately considered.

Adverse Effects

14. The Project will have significant effects on the Site's access from Walters Road and there are concerns that during the construction period the restricted access will not be able to operate safely or efficiently. Particularly when combined with the limitation of access on other businesses on Walters Road, this will result in significant adverse traffic effects on the surrounding transport network, including the roundabout at the Walters Road / Great South Road intersection and on the safety of all transport users. The Requiring Authority has not provided adequate information to support the design or its operation. The submitters therefore have no confidence that the surrounding transport network will continue to operate safely or efficiently.

Parking Effects

15. During the construction period the Requiring Authority also seeks to use part of 20A Walters Road which infringes on an existing building and potentially part of the yard. Additionally, the submitters are concerned that the limitations imposed on Walters Road will have flow-on effects for the Southgate Shopping Centre – particularly with respect to parking and access. The Project does not detail how adequate car parking will be maintained for the Site during the construction period. The Project will negatively impact staff and customers.

Landscape and Urban Design Effects

16. The Project has not adequately demonstrated that an appropriate interface will be provided to the Site. Furthermore, the bridge will be visually prominent and dominant at Walters Road to all tenants and users of the Site.
17. The Project will also involve night-time and long weekend construction noise across all five areas. Construction noise and vibration works such as bridge piling and installation will also require night/weekend works. These are unreasonable expectations and timing for the affected landowners to be subjected to.

Extended lapse date sought of 15 years

18. Under section 184 of the RMA the default lapse date for designations is five years. The Requiring Authority has sought an extended lapse date of 15 years for this Project but there is a lack of cogent evidence supporting the proposed extended lapse date.

19. Furthermore, to expect a landowner to endure a planning blight for a period of 15 years is unreasonable and unfair and it will create significant uncertainty for landowners subject to these designations.
20. This is further exacerbated by a lack of funding for the works and the absence of any proper assessment or commitment to a works timeframe. The Requiring Authority has no secured funding or interest in much of the designated route.

Conditions

21. In the event that NoR1 and NoR2 are confirmed, the Submitters are concerned that the recommended mitigation and condition response proposed by the Requiring Authority will not adequately mitigate the actual and potential adverse effects of the Project on the Takanini Town Centre and the surrounding business and residential landowners.
22. Conditions should be imposed to ensure the minimum practicable impact on the Site especially in terms of access, visual and landscape amenity, geotechnical risks, noise and vibration effects and impact on existing services and operations.

Decision Sought

23. The submitters seek that Auckland Council:
 - a. Recommend that the requirement is withdrawn; or (in the alternative as secondary relief)
 - b. Recommend that the requirement is modified or made subject to conditions to address all of the concerns raised in this submission.
24. The submitters seek such alternative, further or consequential relief as may be required to address the concerns raised in this submission.

Procedural Matters

25. The submitters could not gain an advantage in trade competition through this submission.
26. The submitters wish to be heard in support of this submission. The Trust, THTL, and Mitre 10 would consider presenting a joint case at the hearing.

Date: 14 December 2023

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**Submission on the Two Notices of Requirement for the Takanini Level Crossing lodged by
Auckland Transport as requiring authority under the
Resource Management Act 1991**

TO: Attn: Planning Technician Auckland Council Level 24, 135 Albert
Street Private Bag 92300 Auckland 1142

SUBMISSION ON: Notices of Requirement ("**NoRs**") for the Takanini Level Crossing
("**TLC**")

FROM: Watercare Services Limited ("**Watercare**")

ADDRESS FOR SERVICE: Mark Bishop
Regulatory & Policy Manager
Watercare Services Ltd
Private Bag 92 521
Wellesley Street
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Phone:022 010 6301
Email: Mark.Bishop@water.co.nz

DATE: 14 December 2023

1. INTRODUCTION

- 1.1 Watercare is pleased to have the opportunity to make a submission on the two NoRs for the Takanini Level Crossing TLC lodged by Auckland Transport as a requiring authority under the Resource Management Act 1991 ("**RMA**").
- 1.2 Watercare neither supports or opposes these NoRs (ie it is neutral as to whether the NoRs are confirmed or not). Watercare seeks to ensure that any decisions made on the NoRs respond to the issues raised in this submission and avoids, remedies, or mitigates potential adverse effects on Watercare's ability to provide water and wastewater services now and in the future.
- 1.3 Watercare could not gain an advantage in trade competition through this submission.

2. WATERCARE – OUR PURPOSE AND MISSION

- 2.1 Watercare is New Zealand's largest provider of water and wastewater services. We are a substantive council-controlled organisation under the Local Government Act 2002 ("LGA") and are wholly owned by Auckland Council ("Council"). Watercare has a significant role in helping the Council achieve its vision for the city. Our services are vital for life, keep people safe and help communities to flourish.
- 2.2 Watercare provides integrated water and wastewater services to approximately 1.7 million people in the Auckland region. Over the next 30 years, this could increase by another 520,800 people, potentially requiring another 200,000 dwellings along with associated drinking water, stormwater and wastewater infrastructure. The rate and speed of Auckland's population growth puts pressure on our communities, our environment, and our housing and infrastructure networks. It also means increasing demand for space, infrastructure, and services necessary to support this level of growth.
- 2.3 Under both the LGA and the Local Government (Auckland Council) Act 2009, Watercare has certain obligations. For example, Watercare must achieve its shareholder's objectives as specified in our statement of intent, be a good employer, and exhibit a sense of social and environmental responsibility.¹
- 2.4 Watercare must also give effect to relevant aspects of the Council's Long-Term Plan, and act consistently with other plans and strategies of the Council, including the Auckland Unitary Plan and the recently adopted Auckland Council Future Development Strategy.
- 2.5 Watercare is also required to manage our operations efficiently with a view to keeping overall costs of water supply and wastewater services to our customers (collectively) at minimum levels, consistent with effective conduct of the undertakings and maintenance of long-term integrity of our assets.²

3. PLANNED AND EXISTING WATERCARE ASSETS

- 3.1 The Assessment of Effects on the Environment for the NoRs does not identify any Watercare assets within either of the NoR project areas.³ However, some of the project areas for the NoRs are within areas where Watercare has planned for future infrastructure development, as detailed at paragraph [3.2].
- 3.2 Specific commentary regarding known projects within Watercare's Asset Management Plan to service growth at a bulk level is outlined below. Solutions and alignments/locations are subject to change as we learn more, progress our projects and the area develops. There is also potential for new needs to surface, necessitating further bulk infrastructure. Ongoing engagement is critical to maintain alignment.

a) Takanini Level Crossing (TLC): Spartan Road, Manuia Road, Manuroa Road and Taka Street (NoR 1)⁴

¹ LGA, s 59.

² Local Government (Auckland Council) Act 2009, s 57.

³ Assessment of Effects on the Environment for the Takaanini Level Crossings (dated October 2023) at Table 11-13.

⁴ For new multi-modal bridge crossings of the North Island Main Trunk (NIMT) at Manuia Road and Taka Street; and new active mode bridge crossings of the NIMT at Spartan Road and Manuroa Road with two consequential road closures.

- Watercare has no planned projects at this time that intersect with NoR 1, although it may have future needs may change due to influences outside its control.

b) NoR Takanini Level Crossing (TLC): Walters Road level crossing closure and new multi-modal bridge (NoR 2)⁵

- Watercare’s Takanini Cross-Connection project, which involves a new Bulk Supply Point and watermain, has the potential to intersect with proposed designation on Walters Road between Great South Road and Porchester Road.

4. SUBMISSION POINTS AND RELIEF SOUGHT

4.1 This is a submission on both NoRs (detailed above) that were publicly notified on 16 November 2023.

4.2 As noted previously, Watercare neither supports or opposes these NoRs (ie it is neutral as to whether the NoRs are confirmed or not). Watercare seeks to ensure that any decisions made on the NoRs responds to the issues raised in this submission and avoids, remedies, or mitigates potential adverse effects on Watercare’s ability to provide water and wastewater services now and in the future.

Early engagement

4.3 Watercare seeks to ensure that there is a live and continual process planned forward to recognise that asset management and construction plans are constantly updating and changing.

4.4 Watercare acknowledges the proactive approach to engagement shown by the requiring authority to date. Watercare has been in discussions with the Supporting Growth Alliance, and the preceding ‘future urban land use strategy’ project work, as well as independent engagement with AT during the development of these NoR’s.

4.5 Watercare supports in depth collaboration and consultation (including information, data sharing and identification of opportunistic works) across infrastructure providers on the development (or redevelopment) of urban environments and wishes to ensure that there is ongoing and timely engagement and collaboration as these projects develop.

4.6 As noted, Watercare seeks early engagement from the requiring authority for future planning and construction works including prior to detailed design and during implementation of construction works. Early and fulsome engagement with Watercare, along with other infrastructure providers, can enable opportunities to plan and future proof the delivery of assets to provide for well-functioning urban environments. For Watercare, this includes applying for, in a timely manner, “Works Over” Approvals, in compliance with Watercare’s “*Water Supply and Wastewater Network Bylaw 2015*” (updated 2021).

4.7 Watercare seeks to ensure the NoRs do not impact its wastewater and water services in the NoR project areas in the future (these planned projects are detailed in paragraph [3.2] above). Watercare wishes to ensure it maintains access to its assets 24 hours a day, 7

⁵ For a new multi-modal bridge crossing of the North Island Main Trunk (NIMT) railway at Walters Road.

days a week for maintenance, safety, and efficient operation of its services and that it is consulted on any works undertaken by the requiring authorities that may impact Watercare's services.

Specific amendments to conditions

- 4.8 Watercare has filed evidence, and attended, recent NoR hearings for other Supporting Growth Alliance projects (the North West Strategic Network, and the Airport to Botany Bus Rapid Transit Project). The conditions proposed for the NoRs by the requiring authorities for these NoRs are similar to those which have been proposed at the recent North West Strategic Network hearing (in rebuttal evidence).
- 4.9 Watercare supports the intention of conditions proposed by the requiring authority which seek to ensure that there is engagement with relevant stakeholders during the development of both NoRs (ie the conditions which require a Network Utility Management Plan ("**NUMP**"), Stakeholders Communication and Engagement Management Plan ("**SCEMP**"), and Land use Integration Process ("**LIP**")).
- 4.10 That said, Watercare considers further amendments to the conditions are required to address matters raised in this submission, so that the conditions for both NoRs adequately provide for engagement with network utilities, in particular during the feasibility and detailed design stage.
- 4.11 Watercare seeks that a new condition requiring the preparation of a "Network Utility Strategic Outcomes Plan" be added to both NoRs to futureproof assets in consultation with network utility operators such as Watercare:

Network Utility Strategic Outcomes Plan (NUSOP)

- (a) A NUSOP shall be prepared in the project feasibility stage or as early as practicable.
- (b) The objective of the NUSOP is to set out a strategic framework for asset resilience that includes consideration of growth, corridor protection, and asset renewals over time.
- (c) The NUSOP shall:
- i. consider expected asset life of existing assets;
 - ii. consider expected asset capacity increases or changes; and
 - iii. demonstrate how city and national strategic plans are considered.
- (d) The NUSOP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project, including Watercare.
- (e) The NUSOP shall describe how strategic plans from the Network Utility Operators in relation to its assets have been addressed.
- (f) Any comments received from the Network Utility Operator shall be considered when finalising the NUSOP.
- (g) Any amendments to the NUSOP related to the assets of a Network Utility Operator shall be prepared in consultation with that asset owner.

4.12 If the above condition is not included in the NoRs, Watercare seeks the following amendments (shown in underline) to the NUMP condition both of the NoRs:

(a) A NUMP shall be prepared after consultation with Network Utility Operator(s) including during the feasibility and detailed design phases, and prior to the lodgement of an Outline Plan of Works for a stage of construction Start of Construction for a Stage of Work.

...

(c) The NUMP shall be prepared in consultation with the relevant Network Utility Operator(s) who have existing assets that are directly affected by the Project and shall include any s177 consents required for works affecting prior Designations and Watercare 'Works Over Approvals'.

...

(h) The Requiring Authority shall consult with Network Utility Operators during the feasibility and detailed design phases to identify opportunities to enable, or not preclude, the development of new network utility facilities including access to power, water services and ducting within the Project, where practicable to do so. The consultation undertaken, opportunities considered, and whether or not they have been incorporated into the detailed design, shall be summarised in the Outline Plan or Plans prepared for the Project.


5. RECOMMENDATION SOUGHT

5.1 Watercare seeks that the Council recommend:

- (a) amendments to the NoRs, including by way of conditions to ensure any adverse effects on Watercare's assets and operations are avoided, remedied or mitigated and to address the concerns set out above; and / or
- (b) such further other relief or other consequential amendments as considered appropriate and necessary to address the concerns set out above.

5.2 Watercare wishes to be heard in support of this submission.

5.3 If others make a similar submission, consideration would be given to presenting a joint case with them at any hearing.



Steve Webster
Chief Infrastructure Officer
Watercare Services Limited

14 December 2023

Attn: Planning Technician
Submission on Notice of Requirements Lodged by Auckland
Transport: Takaanini Level Crossings Project (NoR 1 and NoR 2)

SLR Ref No.: Takaanini Level Crossings
NOR_Z Energy Submission_v1.0

14 December 2023

SLR Ref No.: Takaanini Level Crossings NOR_Z Energy Submission_v1.0

Attn: Planning Technician
Auckland Council
Level 16,
135 Albert Street
Private Bag 92300
Auckland 1142

By email: unitaryplan@aucklandcouncil.govt.nz

**RE: Submission on Notice of Requirements Lodged by Auckland Transport:
Takaanini Level Crossings Project (NoR 1 and NoR 2)**

Submitter:

Z Energy Limited
PO Box 2091
Wellington 6140

Address for Service

SLR Consulting New Zealand
PO Box 911310
Victoria St West
Auckland 1142

Attention: Phil Brown

Phone: 027 467 1566

Email: philip.brown@slrconsulting.com

A. Introduction

- 1 This is a submission on two notice of requirement (*NoRs*) applications which were lodged in association with the Takaanini Level Crossings (*TLC*) project. The Supporting Growth Alliance (*SGA*) which includes Auckland Transport and Waka Kotahi, as a requiring authority under Section 167 of the Resource Management Act 1991 (*RMA*), is seeking *NoR* approval to enable this work.
- 2 The *TLC* project involves the construction, operation, maintenance, and upgrade of five grade-separated bridge crossings of the North Island Main Trunk. *NoR* 1 comprises the section of works in Spartan Road, Manuia Road, Manuroa Road, and Taka Street. *NoR* 2 comprises the section of works in Walters Road.
- 3 Z Energy (*the Submitter*) has an interest in the following sites which are the subject of this submission:
 - a) *NoR* 1: The Z Takanini service station, located at 166 Great South Road, Takanini. (Note that the Z Takanini service station is also affected by the South Frequent Transit Network *NoR* project; this is addressed in a separate Z submission).
 - b) *NoR* 2: The Z Papakura North service station, located at 254 Great South Road, Takanini.
- 4 The Submitter could not gain an advantage in trade competition through this submission and the submission does not raise matters that relate to trade competition or the effects of trade competition.

B. Summary of Submission

- 5 The Submitter supports the principle of improving road safety at level crossings and encouraging multi-modal transport options but opposes the *NoRs* insofar as they relate to (a) the significant loss of site features and facilities, and (b) the consequential changes to the layout of the sites.
- 6 The *NoR* application drawings generally show a setback between the permanent road corridor changes (e.g. new footpaths, berms, cycleways) and the designation boundary. While Z understands that *SGA*'s intention is to 'pull back' the designation boundary at implementation stage so that it is aligned with the finalised permanent works corridor, this submission focuses on all potential effects, understanding that permanent road upgrade works could potentially be undertaken by *SGA* up to the designation boundary if it is confirmed at its current location during and following detailed design.
- 7 The Submitter's opposition is on the basis that:
 - a) The project does not promote the sustainable management of natural and physical resources as required by Part 2 of the *RMA*;
 - b) The project does not enable people and communities to provide for their social, economic, and cultural well-being and for their health and safety;
 - c) The project does not promote the efficient use and development of urban land and development infrastructure;
 - d) The Assessment of Environmental Effects is inadequate and does not address the significant adverse effects of the works in sufficient detail to address matters under section 171(1) of the *RMA*;
 - e) The potential adverse effects on the Submitter have been inadequately identified, considered, or avoided, remedied, or mitigated;

- f) The nature and extent of the benefits of the project have not been demonstrated to outweigh the potentially significant adverse effects of the project;
- g) The adverse effects of the project are not sufficiently mitigated, including managing the effects of the NORs on adjacent activities;
- h) The project will generate significant adverse social and economic impacts, including on the Submitter's business;
- i) The proposed conditions do not adequately address the potential for adverse effects, including significant adverse effects; and
- j) The proposed works are not reasonably necessary for achieving SGA's objectives for the project for which the designation is sought.

C. Characteristics of Service Stations

- 8 Service stations are complex land use activities. Their design and layout require careful consideration of a wide range of matters and adherence to industry standards to ensure that they can operate safely and efficiently. This includes in relation to:
- a) The transfer, storage and handling of hazardous substances. Key hazardous substances components of service stations, including underground and aboveground fuel storage, remote fill points, associated underground fuel lines, and above ground fuel pumps, must be carefully designed and sited in accordance with industry legislation to ensure that potential adverse environmental effects and health and safety risks are appropriately managed. This includes the Hazardous Substances and New Organisms Act 1996 (*HSNO*) and Health and Safety at Work Act 2015 (*HSWA*).
 - b) Stormwater. Stormwater discharges at service stations are managed in accordance with the 'Environmental guidelines for water discharges from petroleum industry sites in New Zealand', Ministry for the Environment, 1998 (*The MfE Guidelines*). 'At risk' areas (i.e. locations where fuel products are being transferred, such as the refuelling forecourt and remote fill points), to be Guideline compliant, must be graded and directed to treatment devices such as an oil-water separator and treated separately (usually) than the 'balance' stormwater.
 - c) Road frontages, including signage. Service station road frontages are critical to their safe and efficient operation. Frontages typically contain landscaping comprising low-level shrubs and grasses, which ensures clear visibility of the forecourt for motorists to safely enter / exit the site whilst also providing an amenity buffer between the site and streetscape. The frontage also contains signage, which provides for early identification of the site to promote safe access, including the prime sign which is required under the Fuel Industry Regulations 2021 to clearly display fuel pricing to road users.
 - d) Location. As service stations are heavily reliant on the road network for their customers and the agglomeration of business in the surrounds, they are predominantly located on key arterial routes. Accordingly, the relationship between a site and the road environment is critically important to the location and operation of a service station.
 - e) Traffic access and safety. Service stations are 24/7 vehicle orientated activities which accommodate customers as well as fuel tanker trucks (*tankers*) which frequently deliver fuel to their sites. Having a layout that ensures safe and convenient access and manoeuvring for tankers and customers entering, exiting and moving within the site is a critical component of service stations.

D. Reasons for Submission

Background

- 9 Z Energy and its consultants have had multiple meetings with SGA to discuss the impacts of the TLC project on Z Takanini:¹
 - a) During the initial meetings, Z expressed major concerns regarding the proposal to establish a wetland within the site (as shown in the lodged NoR application drawings).
 - b) SGA subsequently made a formal amendment to the lodged TLC NoR 1 application², which removed the proposed wetland and reduced the extent of the NoR boundary encroachment into the site. However, the amended drawings retain the proposal to establish a 'road corridor' through the site.
 - c) During the more recent meetings, Z expressed major concerns in relation to the proposed road through the Z site for the reasons addressed in this submission. SGA acknowledged these concerns, but as far as Z is aware, has not made any further amendments to the application.

For the avoidance of doubt, this submission relates to the revised NoR 1 layout (per SGA's letter to Auckland Council dated 9/11/23) which now forms part of the NoR application and supersedes the lodged drawing layout.

- 10 Z Energy and its consultants also discussed the TLC project's potential impact on Z Papakura North during their meeting with SGA on 12/12/23.

Z Takanini

- 11 The existing Z Takanini service station (Lot 1 DP 150630) is located on the south-eastern corner of the intersection between Great South Road and Taka Street. Key site features are depicted in **Figure 1** below and include the refuelling forecourt and canopy, convenience store, carwash and water tanks, landscaped frontages, signage, remote fill points, car and trailer parking, underground oil-water separator, underground fuel tanks³, aboveground LPG storage tank. The site also accommodates a Burger King premise, with its restaurant attached to the Z convenience store building and its drive-through facility wrapping around the site's eastern boundary. Access to / from the site is gained via Taka Street (one vehicle crossing) and Great South Road (two vehicle crossings). Tankers access the site via a left turn from Great South Road and exit via a left turn back onto Great South Road.
- 12 NoR 1 of the TLC proposes, per the amended NoR drawing, the following next to Z Takanini:
 - a) A 'proposed road corridor' through the north-eastern corner of the site. Z understands that SGA's intention is for this road corridor to provide access to Taka Street from the church at 7 Taka Street and the care centre at 9 Taka Street;

¹ Meetings between Z and SGA to discuss Z Takanini were held on 28/9/23, 25/10/23, 2/11/23, 15/11/23, 12/12/23.

² Letter from SGA to Auckland Council dated 9 November 2023: '*Minor Alteration to Notified Extent of Auckland Transport's Notice of Requirement – Takaanini Level Crossings NoR 1 – Taka Street Project Area (West)*'. https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/takaanini_level_crossing_te_tupu_nq%C4%81tahi_-_minor_alteration_to_notified_nor_1_extent.pdf

³ It is noted that the underground 50,000 litre diesel fuel tank located next to the site's Taka Street access point was recently removed by Z Energy.

- b) A new berm, footpath and cycleway on Taka Street and Great South Road (in part), most of which are within the Z site's existing boundaries;
 - c) A raised or flush median⁴ on Taka Street;
 - d) A proposed wetland on the opposite side of Taka Street along with a stormwater culvert immediately adjacent to the existing Z carwash;
 - e) A proposed bridge crossing in the road reserve outside the site.
- 13 Z understands that the Taka Street changes will tie into the South Frequent Transit (*South FTN*) project under a separate NoR. The impacts of the South FTN project on Z Takanini are addressed in a separate Z submission. However, there are cumulative effects on Z Takanini when considering the South FTN, which together result in further significant adverse effects beyond the impacts of the TLC discussed in this submission. It is therefore recommended that the two submissions are reviewed in tandem.
- 14 To facilitate the works under NoR 1, SGA proposes to designate 755m² of the Z site.⁵ The permanent road corridor upgrades and designation boundary encroach beyond a number of critical features within the Taka Street frontage including the vehicle crossing, the carwash entrance and exit, the carwash rainwater tanks, landscaping, signage, vehicle parking and the Burger King drive thru entrance. The designation boundary also runs along the eastern edge of the forecourt and canopy. The extent to which site features are impacted by the NoR is depicted in **Figure 1** below. It is noted that the designation boundary also extends beyond infrastructure including a transformer on Taka Street (next to the vehicle crossing), a stormwater line connecting to the oil-water interceptor, and a wastewater line that connects to the carwash interceptor.

⁴ The NoR drawing notes that these could either be flush or raised medians.

⁵ Refer to Table 2 of the Letter from SGA to Auckland Council dated 9 November 2023: '*Minor Alteration to Notified Extent of Auckland Transport's Notice of Requirement – Takaanini Level Crossings NoR 1 – Taka Street Project Area (West)*'.

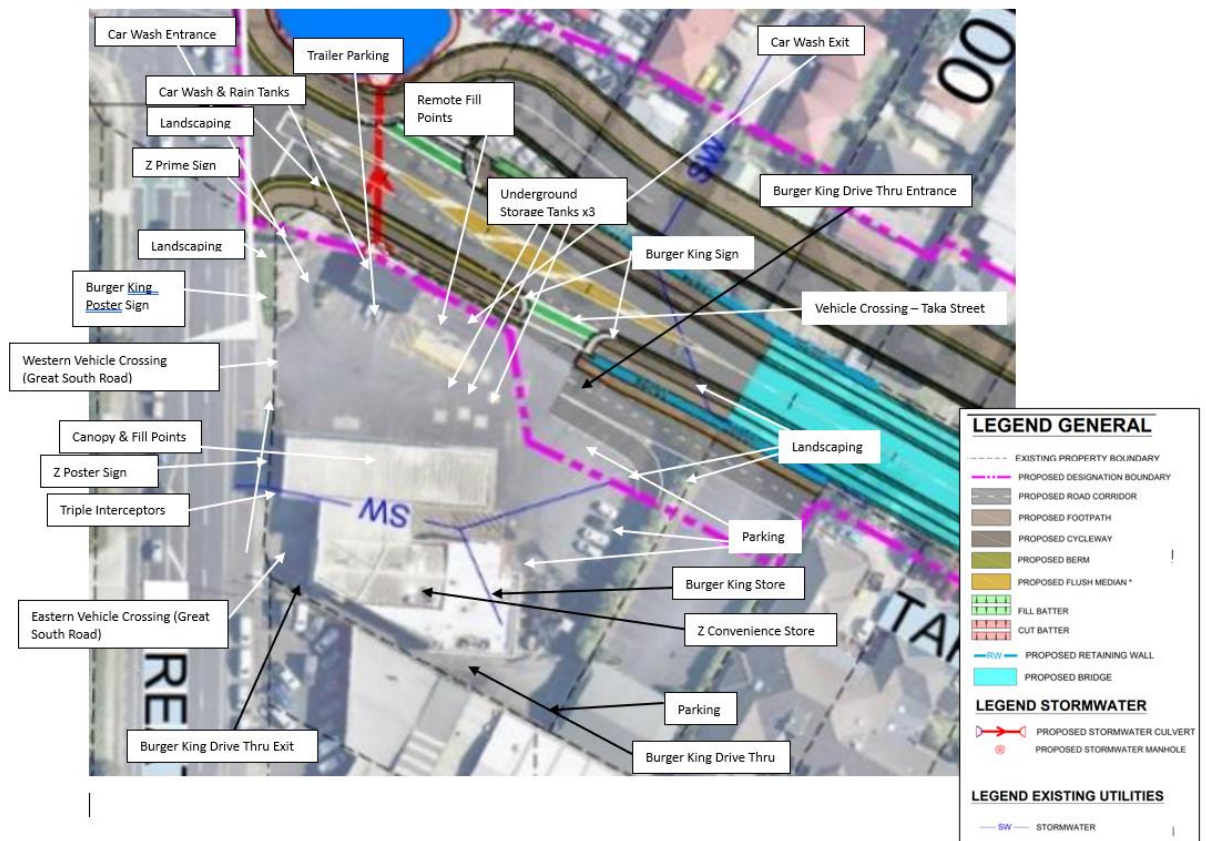


Figure 1: Z Takanini Site Features in Relation to Proposed TLC NoR 1 Concept Design Drawing (Source: Letter from SGA to Auckland Council dated 9 November 2023⁶)

- 15 The proposed changes will result in significant adverse effects on Z Takanini in terms of hazardous substances, traffic, stormwater, landscaping, signage, construction, social and economic considerations, and will almost certainly require a complete redesign of the site. Further, Z has concerns as to whether a redesign of the site will even be viable due to the NoR vastly reducing the site's footprint. While Z appreciates the challenges of assessing effects at a site-specific level for such a large-scale project, there is little to no evidence of the complexity and range of potentially significant adverse effects on the Z Site being adequately assessed.
- 16 Z is opposed to the proposal to establish a road corridor through its site as part of the TLC project. This is likely to result in significant adverse traffic effects, not only for the Z site but also for members of the public using this road. The proposed layout appears to essentially create an informal intersection where a number of vehicles will meet including service station customers, people entering / exiting the retirement village at 9 Taka St, people entering / exiting the church at 7 Taka St, trucks servicing these sites (e.g. rubbish trucks, service vehicles, emergency services accessing the retirement village), fuel tankers at the service station, Burger King drive thru customers, carwash customers, traffic travelling down Taka Street. Further, any queuing on Taka Street will lead to queuing at this informal intersection for exiting vehicles and will create major issues; not only in terms of congestion within the Z site and impeding tankers deliveries (discussed below), but also limited access to / from the church or retirement village in the event of an emergency within these sites. In

⁶ Website source:

https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/takaanini_level_crossing_te_tupu_nq%C4%81tahi_-_minor_alteration_to_notified_nor_1_extent.pdf

addition, the location of the “informal intersection” and the potential effects described above, could create serious safety concerns for users of the dedicated cycleway and footpaths immediately to the east of the informal road / intersection. In the absence of detailed design, Z considers that this appears to be an incredibly dangerous traffic arrangement, and such a layout has the potential to significantly implicate the ability of Z to safely operate its site.

- 17 The proximity of the proposed road corridor to the tanker deliver route is also a significant concern to Z. Refer to the tanker delivery route shown in **Figure 2** below. Tankers deliver up to 30,000 of petroleum product to the site on a regular basis. The site layout has been carefully designed to accommodate fuel tanker deliveries, including the location of fill points to accommodate their tracking curves within the site. When fuel tankers enter the site to deliver fuel, they park to the right of the remote fill points, noting that they must fill from the left side of the tanker. Tankers cannot be impeded by any vehicles and must be protected from any unnecessary movements / manoeuvres of other vehicles. Tankers cannot, under any circumstances, reverse manoeuvre. The proposed NoR layout does not appear to have factored in these safety issues, noting its proximity to the tanker route.



Figure 2: Z Takanini Tanker Delivery Route (Source: Z Energy)

- 18 Z considers that this informal intersection is likely to result in significant safety concerns, not just on tanker movements and traffic, but on the safety of customers, the tanker driver, and people in the immediate vicinity. For example, when vehicles are entering and exiting the access lane to / from the church and retirement village and entering and exiting the service station, at the same time as the tanker is refuelling the site, the intersection becomes incredibly busy and appears to be uncontrolled. Any unnecessary risks to health and safety in this regard are entirely unacceptable.
- 19 Notwithstanding the potentially significant adverse effects that the proposed road corridor arrangement poses, SGA is yet to articulate how this arrangement will actually work in practice. For instance, it is unclear as to how much of the land will be vested road, or whether traffic safety upgrades are proposed (e.g. traffic lights, stop

signs, blind spot mirrors). These additional components could further implicate operations within the Z site. Further, SGA does not appear to have presented any assessment to justify the appropriateness of this arrangement, for instance through trip generation modelling or tracking curves, nor has it demonstrated in its lodged documents any consideration of alternative options to provide access to these two neighbouring sites (7 and 9 Taka Street). Given the potentially significant adverse effects that this proposed road corridor results in, and the absence of an assessment of alternatives in relation to access options for 7 and 9 Taka Street, Z considers the information provided by SGA is inappropriate and does not meet the tests of s171 of the RMA. The taking of private land in this circumstance may be reasonably necessary for providing access to other private sites, but Z does not consider that this is reasonably necessary for the purpose of achieving the objectives of the designation project (per s171(c) RMA).

- 20 Z also has concerns regarding the proximity of the designation boundary and proposed road corridor to areas within the site where the handling, use of transfer of hazardous substances occurs, including the underground fuel storage tanks and remote fill points in the northern portion of the site proximal to Taka Street. HSNO and HSWA legislation dictates that these hazardous area setbacks cannot under any circumstances fall within the road reserve as this is a significant public safety risk.
- 21 The proposed designation boundary crosses into the Z site and encompasses landscaping and existing front yard signage (poster boards, directional signage, prime sign⁷). As noted previously, low lying frontage landscaping is an important element in service stations (and required under the AUP) as it provides a safety buffer, ensures the site is visible for motorists, and contributes to amenity values. Site signage is also located within the landscaped frontage, as there is limited space elsewhere in the site and noting that pricing must be clearly visible to motorists in accordance with industry requirements. If this signage is removed, this will necessitate a comprehensive re-evaluation of the location for necessary signage.
- 22 The proposed designation boundary encroaches into the carwash entrance, partially into the carwash itself and its water tanks, and partially into the carwash exit. These changes resulting from the NoR are highly likely to render the carwash inoperable; for instance the carwash exit cannot simply be reorientated to the south as this would result in collisions with tankers. Given that there is unlikely to be sufficient space within the new site boundaries to relocate the carwash, it will likely need to be closed.
- 23 The NoR is likely to result in the loss of a number of car parking spaces within the Z site, unless they can be accommodated elsewhere in the site (which is unlikely).
- 24 It is also likely that the proposed designation will impact on the site drainage layout (which is subject to the MfE Guidelines for stormwater discharges), including the catchment of 'at risk' discharges from parts of the site such as the remote fill points (noting that these may need to be relocated).
- 25 The NoR will also require the removal or relocation of other infrastructure and structures along the Taka Street frontage associated with the Z site, including wastewater drainage, fencing, and a shed next to the carwash. A transformer is also located next to the Burger King drive thru.
- 26 From the above discussion, it is clear that the NoR will not only significantly adversely affect the Z site, but it may also require relocation of various features, many of which are critical to the site's operation. Some of the features needing relocation would include the remote fill points (to accommodate tanker movements),

⁷ It is difficult to tell whether the prime sign falls within the TLC project NoR. Notwithstanding, the prime sign does fall within the proposed NoR boundary for the South FTN project (as addressed in a separate Z submission).

stormwater infrastructure, landscaping and signage, the carwash, the Burger King drive thru, vehicle parking. Relocation of the underground fuel tanks may also be required. Without a complete redesign of the entire site, it is highly unlikely that there will be sufficient space within the future site boundaries to accommodate these features, noting that SGA proposes to designate 755m² of the Z Takanini site and that the AUP requires a 2m landscaping frontage. A complete redesign of the site may even be difficult given the limited size of the site, and this is without accounting for the implications of the separate South FTN project.

- 27 The application anticipates a total construction duration of 2-3 years for Taka Street and seeks a lapse period of 15 years for NoR 1. These lengthy timeframes, coupled with the 'indicative design' approach of the NoR permanent works corridor in relation to the designation boundary, presents significant uncertainties for Z and its ability to plan for the site, with significant corresponding adverse social and economic effects. It is noted that Z Energy holds a long-term lease for the site.
- 28 Z also has concerns regarding significant adverse construction effects, noting the 2-3 year construction timeframe. As a 24/7 vehicle-oriented activity, maintenance of convenient and safe access for passing traffic to the service station is critical for the duration of the works. The proposed conditions rely heavily on a range of further information, including and via the submission of management plans, to address effects. While some management plan conditions appear effective, not all appear to clearly require meaningful consultation with affected parties or establish outcomes to be achieved. As such, the extent to which the works will affect and/or compromise the Submitter's business cannot be ascertained and the ability of the Submitter to influence the detail of how the works are managed to minimise effects as far as practicable are extremely limited.
- 29 *Z Papakura North*
- 30 The existing Z Papakura North service station is located on 1-3 Tironui Road (Lot 1-2 Deposited Plan 44494) and is located to the eastern corner of the roundabout, between Tironui Road and Great South Road. Key site features are depicted in **Figure 3** below. The site has two vehicle crossings on Great South Road.
- 31 NoR 2 of the TLC proposes to implement a designation boundary along the Z Papakura North site boundary. It also proposes permanent road upgrades including a new footpath in the road reserve. Refer to **Figure 4** below.



Figure 3: Z Papakura North in Relation to Proposed TLC NoR 2 Designation (Source: Auckland Council GeoMaps)

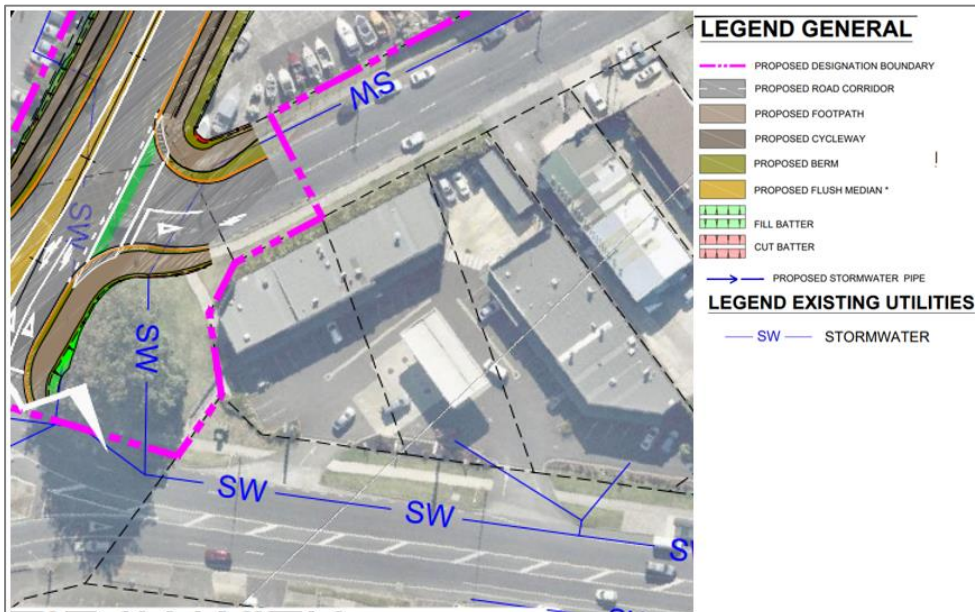


Figure 4: TLC NoR 2 Concept Design Next to Z Papakura North (Source: TLC NoR 2 General Arrangement Plan)

- 32 The designation boundary does not extend into the Z site boundaries and the concept plan shows the permanent works corridor to be reasonably setback from the Z site. Furthermore, the proposed designation area and works corridor does not extend down Great South Road in front of the site. As such, Z does not consider that the NoR in its current format is likely to result in any permanent operational impacts on Z Papakura North. However, this is subject to detailed design and ensuring that appropriate conditions are in place.
- 33 Z seeks to ensure:
- a) That the NoR does not propose permanent road changes that adversely affect traffic movements to / from and within the service station, including in relation to the Great South Road access points.
 - b) That the site's existing landscaping, signage, hazardous substance storage / transfer / use layout, and infrastructure (including stormwater) will not be affected by the NoR.
 - c) That the designation boundary does not encroach into the site, and that the designation boundary is not amended to extend along the site's Great South Road frontage.
- 34 Z does have a particular interest in ensuring that potential adverse construction effects on Z Papakura North are appropriately managed, noting that (a) a 2.5 to 3 year construction timeframe is proposed for the Walters Road section of road, and (b) the concept level design plans and designated area could change to suit SGA's future plans. As a 24/7 vehicle-oriented activity, maintenance of convenient and safe access for passing traffic to the service station is critical for the duration of the works. In line with the above comments relating to Z Takanini, Z considers that changes to the conditions are necessary to ensure that they achieve appropriate outcomes for stakeholders.

Submission on Conditions

- 35 Z Energy has reviewed the proposed conditions contained in Attachment C of Form 18 – NoR 1 - TLC. Failure to factor in the site-specific issues identified in this submission early on at the detailed design process has potentially significant adverse effects and may render the sites inoperable. Z Energy is therefore of the opinion that the conditions should provide more specific recognition of matters that need to be addressed at detailed design stage, rather than leaving this ambiguous and open-ended. To address this outcome, and broadly comment on the suite of conditions, Z Energy submits that:
- a) Not all management plan conditions require consultation with affected landowners and occupiers.
 - b) Some conditions refer only to landowners, where it may be the occupier who is just as adversely affected.
 - c) The extent to which feedback from consultation with affected landowners, leaseholders or occupiers is taken into account is unclear and should be acknowledged as a priority through conditions. i.e.: there is no clear requirement that feedback be considered and implemented to the extent practicable by the requiring authority.
 - d) The conditions do not go far enough to demonstrate that effects on the Z Energy sites will be avoided, remedied or mitigated, such that effects may be significant.

Relief Sought

- 36 The Submitter seeks that the NoR 1 in its current form is declined. However, in the event that NoR 1 is not declined, relief sought is set out below.

- 37 The Submitter is neutral regarding NoR 2.
- 38 The Submitter seeks that the designations are amended to avoid, remedy, or mitigate all matters of concern raised in this submission, including but not limited to the following:
- a) Minimising the encroachment of the designation boundary into the Z sites and ensuring that any temporary or permanent effects do not impact their ability to safely operate, including with regard to access/egress, all-vehicle manoeuvring, parking, stormwater treatment and drainage, the storage and use of hazardous substances (including tanks, remote fills, vents, dispensers), the forecourt canopy, signage, and landscaping.
 - b) Retaining safe and convenient entry and exit crossings, including the ability to right turn in and out of the site.
 - c) Retaining safe and convenient tanker access to and from the site and the remote fill points.
 - d) Retaining safe and convenient on-site manoeuvring.
 - e) Ensuring that any resultant changes will not result in Z Energy being unable to operate its sites lawfully.
 - f) Ensuring that works are appropriately managed through conditions to avoid, remedy, or mitigate adverse effects on the Submitter, per the below.
 - g) Amending the lapse period to 5 years, consistent with s184 of the RMA.
 - h) That the following condition changes are implemented (at a minimum):
 - i. **Proposed Condition 4 (Designation Review)** – The use of the phrase “or as soon as otherwise practicable” is unclear in the context of this condition and leaves the requirement to roll back too open. Amend the condition so that it states: *The Requiring Authority shall as soon as practicable, and otherwise within 12 months of Completion of Construction for each Stage of the Project...*
 - ii. **Proposed Condition 8 (Management Plans)** – As drafted, the condition does not require the requiring authority to incorporate feedback from stakeholders. Rather, it simply directs the requiring authority to summarise feedback and state whether the feedback has been incorporated or not. Amend the condition to require that, the summary of comments received (required by (8(a)(iv)) demonstrates how, as far as practicable, the feedback from stakeholders has been incorporated.
 - iii. **Proposed Condition 9 (SCEMP)** – The condition should be amended to include the requirement to prepare a schedule of sites affected and site-specific matters identified in the schedule to be addressed through consultation (refer to the Joint Witness Statement (Planning – Conditions) dated 20 September 2023 submitted to the Hearing Panel for the NW NORs). It is assumed that Z Energy will be a stakeholder to be engaged and listed under 9(b)(i)B.
 - iv. **Proposed Condition 12(d) (ULDMP)** – Z Energy supports this condition.
 - v. **Proposed Condition 14 (Existing property access)** – This condition should also refer to occupiers and leaseholders. It is not always just a landowner who may be affected.
 - vi. **Conditions 15 and 18 (CEMP and CTMP)** – These two conditions do not require consultation or engagement with any party in their preparation. Further, it is unclear from the SCEMP condition if these Management Plans

are to be prepared in consultation with affected parties. Z Energy seeks that AT either amends these conditions to require affected parties to be engaged with to participate in the drafting of these management plan OR amends the SCEMP condition so that this requirement is clear.

39 Z Energy would be pleased to meet with SGA to discuss this submission.

Signed on behalf of Z Energy Limited

Regards,

SLR Consulting New Zealand

A handwritten signature in black ink, appearing to read 'Phil Brown', written over a light grey rectangular background.

Phil Brown

Senior Planner

philip.brown@slrconsulting.com

**SUBMISSION ON AUCKLAND TRANSPORT'S NOTICES OF REQUIREMENT FOR THE
TAKAANINI LEVEL CROSSINGS PROJECT BY KĀINGA ORA - HOMES AND
COMMUNITIES**

TO: Auckland Council
Private Bag 92300
Victoria Street West
Auckland 1010

Submission via email: unitaryplan@aucklandcouncil.govt.nz

KĀINGA ORA - HOMES AND COMMUNITIES (“Kāinga Ora”) at the address for service set out below makes the following submission on the Notices of Requirement (“**NoRs**”) for the Takaanini Level Crossings Project (“**the Project**”) (Requiring Authority – Auckland Transport).

Background

1. Kāinga Ora - Homes and Communities (“Kāinga Ora”) is a Crown Entity and is required to give effect to Government policies. Kāinga Ora has a statutory objective that requires it to contribute to sustainable, inclusive, and thriving communities that:
 - (a) provide people with good quality, affordable housing choices that meet diverse needs;
 - (b) support good access to jobs, amenities and services; and
 - (c) otherwise sustain or enhance the overall economic, social, environmental and cultural well-being of current and future generations.
2. The Project sits within the Papakura Local Board area in Auckland. Within this area, Kāinga Ora manages 1702 rental properties¹ and there are 501 applicants on the public housing waitlist² as at September 2023. Kāinga Ora is therefore interested in this Project

¹ Kāinga Ora – Housing Statistics – Managed Stock – September 2023.
https://kaingaora.govt.nz/en_NZ/publications/oia-and-proactive-releases/housing-statistics/

² Ministry of Social Development – Housing Register – September 2023.
<https://www.msd.govt.nz/about-msd-and-our-work/publications-resources/statistics/housing/housing-register.html>

and its impacts for communities, noting that it has a number of properties and tenants which will be directly affected:

- (a) 6 Kāinga Ora properties are subject to full acquisition;
 - (b) 20 Kāinga Ora properties are subject to partial acquisition, with their front yard and/or access proposed to be partially removed; and
 - (c) 32 Kāinga Ora properties are considered '*Protected Premises and Facilities*' (**PPFs**), with 5 of these Kāinga Ora properties expected to receive a traffic noise level of over 55 dB $L_{Aeq(24h)}$.
3. However, as per the statutory objectives above, Kāinga Ora also has interests beyond its role as a public housing provider. This includes a role as a landowner and developer of residential housing and as an enabler of quality urban developments through increasing the availability of build-ready land across the Auckland region. Kāinga Ora is interested in all issues that may affect the supply and affordability of housing. This includes the provision of services and infrastructure.

Wider Context

4. In addition to the above, Kāinga Ora will play a greater role in urban development in New Zealand. The legislative functions of Kāinga Ora, as outlined in the Kāinga Ora Act, illustrate this broad mandate and outline two key roles of Kāinga Ora in that regard:
- a) initiating, facilitating and/or undertaking development not just for itself, but in partnership or on behalf of others; and
 - b) providing a leadership or coordination role more generally.
5. Notably, the statutory functions of Kāinga Ora in relation to urban development extend beyond the development of housing (which includes public housing, affordable housing, homes for first time buyers, and market housing) to the development and renewal of urban environments, as well as the development of related commercial, industrial, community, or other amenities, infrastructure, facilities, services or works.

The Government Policy Statement on Housing and Urban Development 2021 (“GPS-HUD”)

6. The GPS-HUD sets a direction for housing and urban development in New Zealand. Its overarching vision is that everyone in New Zealand lives in a home and a community that meets their needs and aspirations. The four main things it sets out to achieve are:
- (a) **Thriving and resilient communities** – The places where people live are accessible and connected to employment, education, social and cultural opportunities. They grow and change well within environmental limits, support our culture and heritage and are resilient.
 - (b) **Wellbeing through housing** – Everyone lives in a home, whether it’s rented or owned, that is warm, dry, safe, stable and affordable, with access to the support they need to live healthy, successful lives.
 - (c) **Māori housing through partnership** – Māori and the Crown work together in partnership so all whānau have safe, healthy, affordable and stable homes. Māori housing solutions are led by Māori and are delivered locally. Māori can use their own assets and whenua Māori to invest in and support housing solutions.
 - (d) **An adaptive and responsive system** – Land-use change, infrastructure and housing supply is responsive to demand, well planned and well regulated.

The National Policy Statement on Urban Development (“NPS-UD”) and the Resource Management (Enabling Housing Supply and Other Matters) Amendment Act 2021 (the “RMAA 2021”)

7. The NPS-UD aims to ensure councils better plan for growth and remove overly restrictive barriers to development to allow growth in locations that have good access to services, public transport networks and infrastructure. The NPS-UD’s intensification policies require councils to enable greater heights and densities in areas that are well-suited to growth, such as in and around urban centres and (existing and proposed) rapid transit stops. The RMAA 2021 introduced the Intensification Streamlined Planning Process for Tier 1 councils to implement the intensification policies and additionally required these councils to introduce the Medium Density Residential Standards.
8. Together, the NPS-UD and RMAA 2021 are intended to ensure New Zealand’s towns and cities are well-functioning urban environments that support housing supply and affordability, accessibility to jobs and services, and emissions reduction.

Scope of Submission

9. The submission relates to the two NoRs for the Takaanini Level Crossings Project in their entirety.

The Submission is:

10. Kāinga Ora **supports** the NoRs for the Project **in part**, subject to the relief Kāinga Ora seeks being granted and matters raised in its submission being addressed.
11. The NoRs seek to replace several at-grade level crossings of the North Island Main Trunk (NIMT) railway with bridges and undertake associated works. The purpose of the Project is to respond to network issues experienced by the corridors such as congestion, severance, safety risk from at-grade level crossings and inefficiency from the operation of barrier arms. The NoRs are broken down into the following sections with the key changes outlined:
 - (a) **NoR 1 – Spartan Road:** Closing the existing railway level crossing and replacing it with a walking and cycling bridge across the railway.
 - (b) **NoR 1 – Manuia Road:** Constructing a new multi-modal bridge across the railway.
 - (c) **NoR 1 – Manuroa Road:** Closing the existing railway level crossing and replacing it with a walking and cycling bridge across the railway.
 - (d) **NoR 1 – Taka Street:** Closing the existing railway level crossing and replacing it with a multi-modal bridge across the railway.
 - (e) **NoR 2 – Walters Road:** Closing the existing railway level crossing and replacing it with a multi-modal bridge across the railway.
12. In particular, but without limiting the generality of the above:
 - a) Kāinga Ora supports the outcomes to be derived from the Project particularly as they overall relate to the delivery of transportation infrastructure that improves the safety measures for walking and cycling as well as the reduction of traffic in some residential environments. Nevertheless, Kāinga Ora supports the proposed NoRs for the Project in part.
 - b) Kāinga Ora generally supports the proposed conditions of the designation and the use of the mechanisms outlined to avoid, remedy, or mitigate potential adverse

effects and to regularly communicate with the community, including but not limited to: the submission of an Outline Plan of Works, the conditions in relation to designation review and existing property access, Mana Whenua Kaitiaki Forum, Stakeholder Communication and Engagement Management Plan, Land Use Integration Process, Construction Environmental Management Plan, Cultural Monitoring Plan, Construction Traffic Management Plan, Construction Noise and Vibration Management Plan, Construction Noise and Vibration Management Schedule, Tree Management Plan, and the Network Utility Management Plan.

13. Notwithstanding the general support of the Project, Kāinga Ora seeks relief in the following aspects of the proposal:
- (a) Providing safer, more direct and more attractive connections for walking and cycling
 - (i) Consideration of alternative options including underpass design;
 - (ii) Provision of direct accessible routes including provision of an accessible elevator and steps where overbridges are provided; and
 - (iii) Connectivity and safety in and around the Takaanini rail station.
 - (b) Reducing the lapse period from 15 years to 10 years
 - (c) Operational noise and vibration
 - (i) Best Practical Option (BPO) for PPFs identification and assessment;
 - (ii) Low noise road surface to be implemented on all roads within the designation; and
 - (iii) Building modification mitigation for all PPFs to 40 dB $L_{Aeq(24h)}$ inside buildings.

In some cases, amendments to the designation conditions and/or the design of the Project are sought to address the concerns expressed in this submission. The concerns of Kāinga Ora are further discussed below.

Accessibility and safety for walking and cycling

Consideration of Alternative Options including underpasses design

14. Kāinga Ora acknowledges that the Project seeks to provide safer east-west active mode facilities for walking and cycling. There will also be a net gain of one walking and cycling crossing over the railway. However, Kāinga Ora requests consideration of more direct and accessible walking and cycling connections at Spartan Road and Manuroa Road. Justification is also sought as to why walking and cycling bridges have been proposed when active mode underpasses would confer the following advantages:
- (a) When designed properly, underpasses offer a better level of service for walking and cycling than overbridges. They are shorter, more direct, more intuitive and safer.
 - (b) Bridges that cross railway lines with overhead electrification need a minimum clearance of 5.5m above track level³. For underpasses designed for cycling and walking, a minimum height clearance of only 2.4m⁴ is required.
 - (c) A height difference of 5.5m or more will require long ramps. E.g. The designation for Manuroa Road shows a narrow arrangement for the ramps with repeated 180° turns and a total length of approximately 250m (assuming a 1:20 gradient). Kāinga Ora considers that a more direct crossing in the form of an underpass of approximately 20m would result in a better connectivity outcome.
 - (d) By minimising the height difference of a grade-separated foot and cycle crossing, the amount of effort required for a person to use the crossing is reduced. This is consistent with a universal design approach, which seeks to design infrastructure that can be used easily by as many people as possible.
 - (e) By providing direct in-line ramps, there is no deviation from the desire line, and the land requirements are also reduced.

³ Figure 3-2 of the Requiring Authority's Assessment of Environmental Effects.
https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/03_tlc_nor1_aee.pdf

⁴ Waka Kotahi – Grade Separation.

<https://www.nzta.govt.nz/walking-cycling-and-public-transport/walking/walking-standards-and-guidelines/pedestrian-network-guidance/design/crossings/grade-separation/>

Provision of accessible routes

15. The project proposes long ramp structures for walking and cycling bridges to enable accessibility for people with limited mobility; however, this will be harder to use than the existing level crossings, because of the gradients and increased distances.
16. If bridges are to be built, Kāinga Ora requests that elevators and staircases are provided to enable more direct routes over the railway. This is requested for Manuroa Road as a priority given its proximity to the Takaanini rail station.

Connectivity and Safety in and around the Takaanini Rail Station

17. It is a basic tenet of public transport planning that the walking and cycling level of service is prioritised at rail stations and bus stops. Kāinga Ora requests that Auckland Transport works with KiwiRail on the following aspects to improve pedestrian and cyclist connectivity in and around the Takaanini rail station.
 - (a) Manuroa Road – Kāinga Ora notes that the safety and connectivity from the proposed walking and cycling bridge to the Takaanini rail station platform in its current form is poor. Kāinga Ora is concerned that there is no segregated path to the platform on the eastern nor western side of the railway line. On the eastern side of the railway, people would need to go through the parking lot (approx. 300m in length) where vehicles would be manoeuvring, exposing active mode users to safety hazards and being at-risk of accidents. It is requested that a more direct, well-designed and safe route is provided.
 - (b) Taka Street – Kāinga Ora notes that active mode users would be able to reach the Takaanini rail station from the western side of the railway line via the existing walking and cycling path. However, it is requested that this path is widened for a more user-friendly shared path. Similar to Manuroa Road, it is noted that there is no segregated path to the platform on the eastern side of the railway line. It is requested that a more direct, well-designed and safe route is provided.

Changes to ULDMP conditions

18. In addition to the outcomes described in Paragraphs 14 to 16 above, Kāinga Ora also seeks changes to the Condition 12 Urban and Landscape Design Management Plan (f) as follows (Requiring Authority Notified Conditions in Black; Kāinga Ora Recommended Changes in **Red**, ~~strikethrough~~ and underline):

To achieve the objective, the ULDMP(s) shall provide details of how the project:

...

(ii) Provides ~~appropriate direct, efficient and high-quality~~ walking and cycling ~~connectivity connections~~ to, and interfaces with, existing or proposed adjacent land uses, public transport infrastructure and walking and cycling connections;

(iii) ~~Promotes~~ Provides inclusive access (~~where appropriate~~); and

...

Reducing the lapse period from 15 years to 10 years

19. Kāinga Ora recognises that the proposed lapse period for the NoRs need to balance flexibility, and acquisitions and construction time required by the Requiring Authority with the uncertainty and planning blight for the adjacent landowners and occupants. Kāinga Ora considers that the lapse date should be amended from 15 years to 10 years, noting that this would still be a longer period than the statutory 5 years lapse period.
20. Kāinga Ora seeks a lapse period of 10 years to encourage the project to be actioned with more urgency, noting that this is an important project for the surrounding communities in terms of better east-west connectivity and safety, particularly walking and cycling. The sooner the project is implemented, the sooner there will also be other positive impacts to communities such as alleviating congestion, reducing inefficiency from the operation of barrier arms and supporting both planned increases in train frequency and the expected urban growth in the Takaanini area.
21. For the reasons in Paragraphs 19-20 above, Kāinga Ora requests that Condition 5 Lapse be amended as follows (Requiring Authority Notified Conditions in Black; Kāinga Ora Recommended Changes in Red, ~~strikethrough~~ and underline):
 - (a) *In accordance with section 184(1)(c) of the RMA, this designation shall lapse if not given effect to within ~~15~~ 10 years from the date on which it is included in the AUP.*

Operational Noise and Vibration

Assessment of adverse effects

22. It is acknowledged that transport infrastructure support the enablement of a well-functioning urban environment, and that a degree of noise and vibration emissions are

expected. However, it must be recognised that significant noise emissions may have potential adverse effects on surrounding residential environments and the health and well-being of people living nearby. Therefore, Operational Noise and Vibration requires careful consideration to ensure that the effects are appropriately avoided, remediated or mitigated in accordance with Section 16 and 17 of the RMA.

23. Kāinga Ora is concerned that the Project does not fully assess the health effects associated with traffic noise of the Project. While the Project assesses the traffic noise effects in the context of NZS6806, Kāinga Ora is concerned that the standard does not fully capture the potential health effects of the proposal on its land and tenants.

Mitigation at-source to within 55 dB $L_{Aeq(24h)}$

24. Kāinga Ora notes that Auckland Transport identified as part of Private Plan Change 51 (PPC51) that activities subjected to an operational noise level of 55 dB $L_{Aeq(24h)}$ require mitigation to address potential adverse health effects. Kāinga Ora requests that operational noise levels for this project to not exceed 55 dB $L_{Aeq(24h)}$ beyond the boundaries of the designation or, where exceeded at a sensitive receiver, mitigation is provided.
25. This operational noise level was the baseline utilised within Auckland Transport's Acoustic Expert Evidence by Claire Drewery for **PPC51**⁵, who considered that there are adverse health effects in relation to road traffic, referencing both the World Health Organisation (WHO) Environmental Noise Guidelines for the European Region (2018) and enHealth's The Health Effects of Environmental Noise (2018). The WHO's guidelines are (in part) copied below:

WHO guidelines for Community Noise 1999 states the following in relation to dwellings

[page xiii]

... The effects of noise in dwellings, typically, are sleep disturbance, annoyance and speech interference. For bedrooms the critical effect is sleep disturbance. Indoor guideline values for bedrooms are 30 dB L_{Aeq} for continuous noise and 45 dB L_{Amax} for single sound events. Lower noise levels may be disturbing depending on the nature of the noise source. At night-time, outside sound

⁵ Paragraphs 6.7 and 6.9 of Statement of Evidence of Claire Drewery on behalf of Auckland Transport – Acoustic, dated 24 August 2021 for Private Plan Change 51 – Drury 2 Precinct.

levels about 1 metre from facades of living spaces should not exceed 45 dB LAeq, so that people may sleep with bedroom windows open. This value was obtained by assuming that the noise reduction from outside to inside with the window open is 15 dB. To enable casual conversation indoors during daytime, the sound level of interfering noise should not exceed 35 dB LAeq. To protect the majority of people from being seriously annoyed during the daytime, the outdoor sound level from steady, continuous noise should not exceed 55 dB LAeq on balconies, terraces and in outdoor living areas. To protect the majority of people from being moderately annoyed during the daytime, the outdoor sound level should not exceed 50 dB LAeq. Where it is practical and feasible, the lower outdoor sound level should be considered the maximum desirable sound level for new development.

WHO Environmental Noise Guidelines for the European Region (2018) states the following:

[page xiii]

Environmental noise is an important public health issue, featuring among the top environmental risks to health. It has negative impacts on human health and well-being and is a growing concern among both the general public and policy-makers in Europe.

[page xvi]

For average noise exposure, the Guideline Development Group (GDG) strongly recommends reducing noise levels produced by road traffic below 53 decibels (dB) Lden, as road traffic noise above this level is associated with adverse health effects.

Based on the above, Ms Drewery adopted 55 dB LAeq(24h) as the noise level above which potential health effects could occur and made subsequent recommendations for PPC51. Kāinga Ora considers that it is appropriate that a similar baseline is utilised for the Project.

26. Kāinga Ora considers that it is appropriate that the Requiring Authority is required to ensure that measures are undertaken to reduce noise and vibration at source, and where these can not be mitigated at source undertakes receiver mitigation to ensure infrastructure is not have adverse effects on the health and well being of existing communities.

Low noise road surface

27. Kāinga Ora requests that Condition 25 (Low Noise Road Surface) is amended to require the use of low noise and vibration road surfaces, for *all* road surfaces within this designation, unless further information confirms that this is not warranted from a health and safety perspective.

Mitigation inside buildings

28. Kāinga Ora submits that there may be circumstances whereby existing dwellings that experience increased exposure to noise and vibration require further mitigation in the form of building modifications, including but not limited to wall insulation, double glazing, forced ventilation and temperature controls.
29. In addition to the road surface, barrier and Category C mitigation proposed by the Project, Kāinga Ora considers that further mitigation is necessary to some non-Category C PPFs in order to address potential adverse health effects. It is sought that this applies where habitable spaces are likely to receive in excess of 40 dB $L_{Aeq(24h)}$ (equivalent to an external noise level of 55 dB $L_{Aeq(24h)}$) from operational noise with windows closed in the Design Year.
30. Kāinga Ora seeks that where mitigation is applicable for PPFs, that the offer for mitigation shall be extended, as per its recommended conditions in Appendix A. This is in the interests of natural justice in terms of giving sufficient time to consider and respond to the offer and mitigating adverse health effects for future occupiers.
31. Furthermore, the Requiring Authority's proposed conditions, which freeze the receiving environment to what exists currently, ignores the future receiving environment. Kāinga Ora considers that the Requiring Authority's proposed conditions should be revised to require a BPO assessment prior to construction in the future that recognises the receiving environment as it exists at the time.

Relief Sought

32. Kāinga Ora seeks the following further actions regarding the NoRs:
- (a) That the Requiring Authority considers other more direct and accessible walking and cycling crossing options for Spartan Road and Manuroa Road that maintain the existing directness and ease of use of the existing level crossings. Kāinga Ora

preference is for well designed safe and direct walking and cycling underpasses. In the event that overpasses are continued to be preferred by the Requiring Authority Kāinga Ora requests that accessible elevators and steps are added to the design to improve the connections.

- (b) That the Requiring Authority provides justification as to why ramped walking and cycling bridges have been proposed in preference to options which would deliver a higher level of service for people cycling or on foot.
 - (c) That the Requiring Authority investigates with KiwiRail: more direct, well-designed and safe walking and cycling opportunities in and around the Takaanini rail station that maximise pedestrian and cycle level of service particularly, at Manuroa Road and Taka Street.
33. Kāinga Ora seeks the following decisions from Auckland Council regarding the NoRs:
- (a) That Condition 12 (Urban and Landscape Design Management Plan) be amended as per Paragraph 17 above to provide safer, more direct and more attractive connections for walking and cycling.
 - (b) That Condition 5 (Lapse) be amended from 15 to 10 years as per Paragraph 21 above to provide greater certainty and for the project to benefit communities sooner.
 - (c) That the operational noise levels for this project shall not exceed 55 dB $L_{Aeq(24h)}$ beyond the boundaries of the designation or, where exceeded at a sensitive receiver, mitigation is provided.
 - (d) That Conditions 28-37 (Operational Conditions) be amended as per Appendix A to address:
 - (i) Best Practical Option (BPO) for PPFs identification and assessment;
 - (ii) Low noise road surface to be implemented on all roads within the designation; and
 - (iii) Building modification mitigation for all PPFs to 40dB $L_{Aeq(24h)}$ inside buildings.

- (e) Such further or other relief, or other consequential or other amendments, as are considered appropriate and necessary to address the concerns set out herein.
 - (f) Any other alternative or consequential relief to give effect to this submission.
34. In the absence of the relief sought, Kāinga Ora considers that the NoRs:
- (a) is contrary to the sustainable management of natural and physical resources and is otherwise inconsistent with Part 2 of the Act;
 - (b) will impact on the ability of people and communities to provide for their social, economic and cultural wellbeing.
35. Kāinga Ora does not consider it can gain an advantage in trade competition through this submission.
36. Kāinga Ora wishes to be heard in support of its submission.
37. If others make a similar submission, Kāinga Ora would be willing to consider presenting a joint case with them at hearing.

Dated this 14th day of December 2023



Brendon Liggett
Manager – Development Planning
Kāinga Ora - Homes and Communities

ADDRESS FOR SERVICE:

Kāinga Ora – Homes and Communities
PO Box 74598
Greenlane, Auckland
Attention: Jennifer Chivers
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Appendix 1

Requiring Authority Notified Conditions in Black
Kāinga Ora Recommended Changes in ~~Red strikethrough~~ and underline.

NoR No.	No.	Condition
Operational Conditions		
NoRs 1 and 2	25.	<p>Low Noise Road Surface</p> <p>(a) Asphaltic concrete surfacing (or equivalent low noise road surface) shall be implemented within 12 months of Completion of Construction of the project.</p> <p>(b) Any future resurfacing works of the Project shall be undertaken in accordance with <u>the BPO as determined in accordance with these conditions and</u> Auckland Transport Reseal Guidelines, Asset Management and Systems 2013 and asphaltic concrete surfacing (or equivalent low noise road surface) shall be implemented where:</p> <p>(i) The volume of traffic exceeds 10,000 vehicles per day; or</p> <p><u>(i) The road is located within the designation; or</u></p> <p>(ii) The road is subject to high wear and tear (such as cul de sac heads, roundabouts and main road intersections); or</p> <p>(iii) It is in an industrial or commercial area where there is a high concentration of truck traffic; or</p> <p>(iv) It is subject to high usage by pedestrians, such as town centres, hospitals, shopping centres and schools.</p> <p>(c) Prior to commencing any future resurfacing works, the Requiring Authority shall advise the Manager if any of the triggers in Condition 25(c)(i) – (iv) are not met by the road or a section of it and therefore where the application of asphaltic concrete surfacing (or equivalent low noise road surface) is no longer required on the road or a section of it. Such advice shall also indicate when any resealing is to occur.</p>

NoRs 1 and 2	<p>Traffic Noise</p> <ul style="list-style-type: none"> (a) For the purposes of Conditions 26 to 37; (b) Building-Modification Mitigation – has the same meaning as in NZS 6806; (c) Design year has the same meaning as in NZS 6806; (d) Detailed Mitigation Options – means the fully detailed design of the Selected Mitigation Options, with all practical issues addressed; (e) Habitable Space – has the same meaning as in NZS 6806; (f) Identified Noise Criteria Category – means the Noise Criteria Category for a PPF identified in <i>Schedule 2: Identified PPFs Noise Criteria Categories</i>; (g) Mitigation – has the same meaning as in NZS 6806:2010 Acoustics – Road-traffic noise – New and altered roads; (h) Noise Criteria Categories – means the groups of preference for sound levels established in accordance with NZS 6806 when determining the Best Practicable Option for noise mitigation (i.e. Categories A, B and C); (i) NZS 6806 – means New Zealand Standard NZS 6806:2010 Acoustics – Road-traffic noise – New and altered roads; (j) Protected Premises and Facilities (PPFs) – means only the premises and facilities identified in green, orange or red in <i>Schedule 2: PPFs Noise Criteria Categories</i>; (k) Selected Mitigation Options – means the preferred mitigation option resulting from a Best Practicable Option assessment undertaken in accordance with NZS 6806 taking into account any low noise road surface to be implemented in accordance with Condition 25; and (l) Structural Mitigation – has the same meaning as in NZS 6806.
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NoR No.	No.	Condition
NoRs 1 and 2	26.	<p>The Noise Criteria Categories identified in <i>Schedule 2: PPFs Noise Criteria Categories</i> at each of the PPFs shall be achieved where practicable and subject to Conditions 26 to 37 (all traffic noise conditions).</p> <p>The Noise Criteria Categories do not need to be complied with at a PPF where:</p> <p>(a) The PPF no longer exists; or</p> <p>(b) Agreement of the landowner has been obtained confirming that the Noise Criteria Category does not need to be met.</p> <p>Achievement of the Noise Criteria Categories for PPFs shall be by reference to a traffic forecast for a high growth scenario in a design year at least 10 years after the programmed opening of the Project.</p>
NoRs 1 and 2	27.	<p>As part of the detailed design of the Project, a Suitably Qualified Person shall determine the Selected Mitigation Options for the PPFs identified on <i>Schedule 2 PPFs Noise Criteria Categories</i>.</p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p> <p>For the avoidance of doubt, the low noise road surface implemented in accordance with Condition 25 may be (or be part of) the Selected Mitigation Options(s).</p>
NoRs 1 and 2	28.	<p>Prior to construction of the Project, a Suitably Qualified Person shall develop the Detailed Mitigation Options for the PPFs identified in <i>Schedule 2 PPFs Noise Criteria Categories</i>, taking into account the Selected Mitigation Options.</p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p>
NoRs 1 and 2	29.	<p>If the Detailed Mitigation Options would result in the Identified Noise Criteria Category changing to a less stringent Category, e.g. from Category A to B or Category B to C, at any relevant PPF, a Suitably Qualified Person shall provide confirmation to the Manager that the Detailed Mitigation Option would be consistent with adopting the Best Practicable Option <u>in accordance with NZS 6806 prior to implementation.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs.</u></p>
NoRs 1 and 2	30.	<p>The Detailed Mitigation Options shall be implemented prior to completion of construction of the Project, with the exception of any low-noise road surfaces, which shall be implemented within twelve months of completion of construction.</p>
NoRs 1 and 2	31.	<p>Prior to the Start of Construction, a Suitably Qualified Person shall identify those PPFs which, following implementation of all the Detailed Mitigation Options, will not be Noise Criteria Categories A or B and where <u>would still require</u> Building-Modification Mitigation might be required to achieve 40 dB L_{Aeq(24h)} inside Habitable Spaces (Category C Buildings).</p> <p><u>For those PPFs, following the process set out in Conditions 32 to 37, it shall be determined which Building Modification Mitigation is required to achieve 40 dB</u></p>

		<p><u>L_{Aeq}(24h) inside habitable spaces.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs. Any future residential or other PPFs provided for in the AUP:OP zonings shall have a BPO assessment undertaken, including mitigation measures within the Project design where practicable.</u></p>
NoRs 1 and 2	32.	<p>Prior to the Start of Construction in the vicinity of each Category C Building <u>requiring building-modification mitigation</u>, the Requiring Authority shall write to the owner of the Category C Building requesting entry to assess the noise reduction performance of the existing building envelope. If the building owner agrees to entry within six three months of the date of the Requiring Authority's letter, the Requiring Authority shall instruct a Suitably Qualified Person to visit the building and assess the noise reduction performance of the existing building envelope.</p> <p><u>If after 6 months following the date of the requiring authority's letter, no response has been received, the Requiring Authority shall again write to the owner of the Building requesting entry to assess the noise reduction performance of the existing building envelope. If the building owner agrees to entry within six months of the date of the Requiring Authority's letter, the Requiring Authority shall instruct a Suitably Qualified and Experienced Person to visit the building and assess the noise reduction performance of the existing building envelope.</u></p> <p><u>A new BPO assessment shall be undertaken to determine the BPO for the environment that is present prior to construction starting (time of lodging of OPW), and revision of the Schedule 2 PPFs and their classifications, to include future planned PPFs. Any future residential or other PPFs provided for in the AUP:OP zonings shall have a BPO assessment undertaken, including mitigation measures within the Project design where practicable.</u></p>
NoRs 1 and 2	33.	<p>For each Category C Building <u>requiring Building-Modification Mitigation</u> identified, the Requiring Authority is deemed to have complied with Condition 32 above if:</p> <ul style="list-style-type: none"> (a) The Requiring Authority's Suitably Qualified Person has visited the building and assessed the noise reduction performance of the building envelope; or (b) The building owner agreed to entry, but the Requiring Authority could not gain entry for some reason (such as entry denied by a tenant); or (c) The building owner did not agree to entry within six three months of the date of the Requiring Authority's <u>last</u> letter sent in accordance with Condition 32 above (including where the owner did not respond within that period); or

NoR No.	No.	Condition
		<p>(d) The building owner cannot, after reasonable enquiry, be found prior to completion of construction of the Project.</p> <p>If any of (b) to (d) above apply to a Category C-Building, the Requiring Authority is not required to implement Building-Modification Mitigation to that building.</p>
NoRs 1 and 2	34.	<p>Subject to Condition 33 above, within six months of the assessment undertaken in accordance with Conditions 32 and 33, the Requiring Authority shall write to the owner of each Category C-Building <u>requiring Building-Modification Mitigation</u> advising:</p> <p>(a) If Building-Modification Mitigation is required to achieve 40 dB LAeq(24h) inside habitable spaces; and</p> <p>(b) The options available, <u>at the cost of the Requiring Authority</u>, for Building-Modification Mitigation to the building, if required; and</p> <p>(c) That the owner has <u>24 three months from completion of construction of the relevant section of the project</u> to decide whether to accept Building-Modification Mitigation to the building and to advise which option for Building-Modification Mitigation the owner prefers, if the Requiring Authority has advised that more than one option is available.</p>
NoRs 1 and 2	35.	<p>Once an agreement on Building-Modification Mitigation is reached between the Requiring Authority and the owner of a Category C-Building <u>requiring Building-Modification Mitigation</u>, the mitigation shall be <u>implemented at the Requiring Authority's expense</u>, including any third party authorisations required, in a reasonable and practical timeframe agreed between the Requiring Authority and the owner.</p>
NoRs 1 and 2	36.	<p>Subject to Condition 33, where Building-Modification Mitigation is required, the Requiring Authority is deemed to have complied with Condition 35 if:</p> <p>(a) The Requiring Authority has completed Building Modification Mitigation to the building; or</p> <p>(b) An alternative agreement for mitigation is reached between the Requiring Authority and the building owner; or</p> <p>(c) The building owner did not accept the Requiring Authority's offer to implement Building-Modification Mitigation <u>within 24 months of completion of construction of the relevant section of the Project three months of the date of the Requiring Authority's letter sent in accordance with Condition 37 (including where the owner did not respond within that period)</u>; or</p> <p>(d) The building owner cannot, after reasonable enquiry, be found <u>within 24 months of the prior to</u> completion of construction of the Project.</p>
NoR 1 and 2	37.	<p>The Detailed Mitigation Options shall be maintained <u>by the Requiring Authority</u> so they retain their noise reduction performance as far as practicable.</p>

From: NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz
To: [Unitary Plan](#)
Subject: [ID:1203] Notice of Requirement online submission - MANPREET KAUR
Date: Thursday, 14 December 2023 12:45:44 pm

The following customer has submitted a Notice of Requirement online submission.

Contact details

Full name of submitter: MANPREET KAUR

Organisation name:

Full name of your agent:

Email address: emanpreet@hotmail.com

Contact phone number: 0220980032

Postal address:
6 PUAWAI STREET
KAIWAKA
KAIWAKA 0573

Submission details

Name of requiring authority: Waka Kotahi (NZTA)

The designation or alteration: North: NOR 4 State Highway 1 Improvements – Albany to Ōrewa and Alterations to Existing Designations 6751, 6760, 6759, 6761

The specific provisions that my submission relates to are:
33 WALTERS ROAD, TAKANINI

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

THE PROPERTY WAS PURCHASED AS A RENTAL INCOME WITH A VIEW TO DEVELOP IT IN THE FUTURE BY SUBDIVIDING IT. THIS IS WHY WE CHOSE A PROPERTY WITHIN THIS ZONE. THE PROPOSED DESIGNATION WILL TAKE AWAY ALMOST 20% OF THE TOTAL LAND AREA (89 OUT OF 470 SQM) AND MAKE IT PRETTY MUCH IMPOSSIBLE TO CARRY OUT ANY DEVELOPMENT WORK. AND THIS WILL RESULT IN US BEING SEVERELY DISADVANTAGED ECONOMICALLY.

I or we seek the following recommendation or decision from Auckland Council:
WE SEEK THE COUNCIL DO EITHER OF THE FOLLOWING: a) ABANDON THIS DESIGNATION PLAN SO THAT WE CAN REALISE THE POTENTIAL FOR WHICH THE PROPERTY WAS PURCHASED. b) PURCHASE THE ENTIRE PROPERTY FROM US (NOT JUST 20%) AND COMPENSATE US ACCORDINGLY.

Submission date: 14 December 2023

Attend a hearing

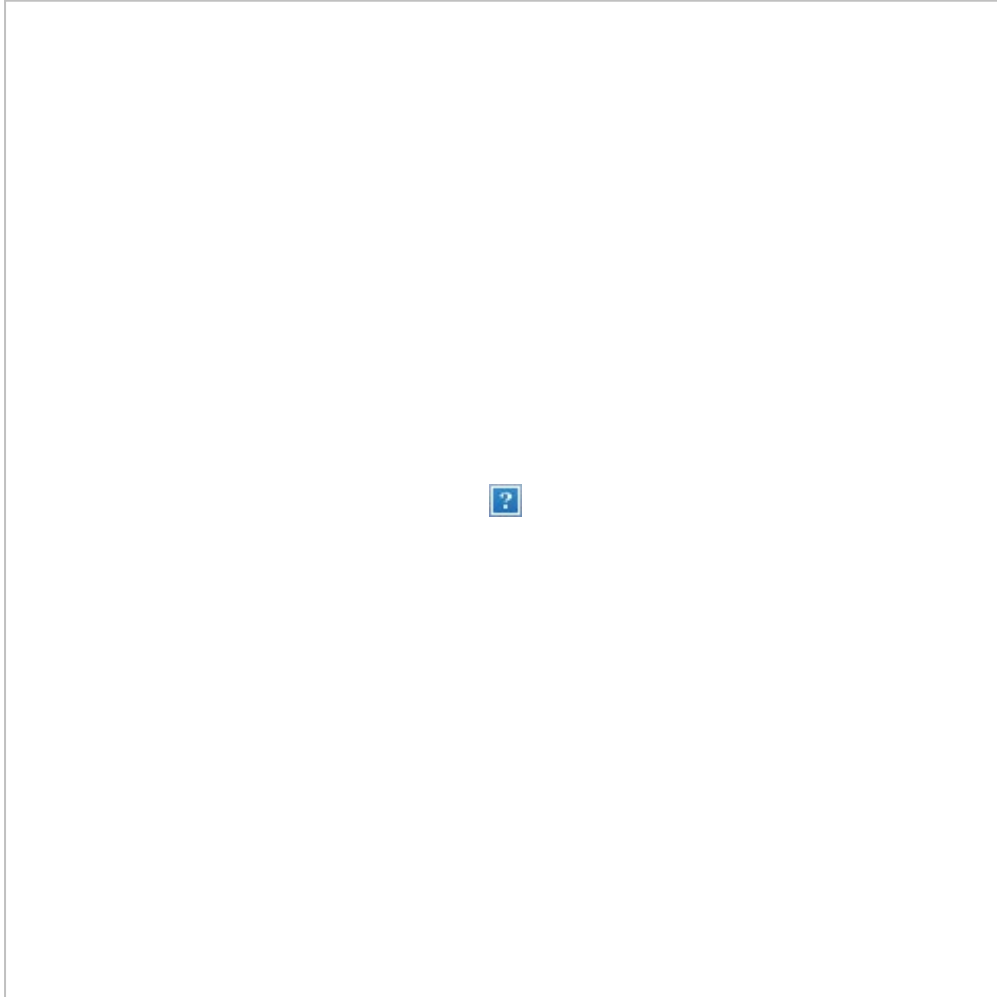
Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?
Yes

Declaration

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



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