



THE **PLANNING**
COLLECTIVE

**To establish a new Retirement Village, Childcare Centre with Associated
Car parking; and Construction works and to undertake a 2 Lot
Subdivision**

**Lot 9 DP 520205 and Sec 216 SO18072 Parish of Okura Small Road,
Silverdale**

September 2019

Updated February 2020

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1.0 THE APPLICANT AND PROPERTY DETAILS

To:	Auckland Council 50 Centreway Road, Orewa
Site Address:	Small Road Silverdale
Applicant's Name:	Matvin Developments Limited
Address for Service:	The Planning Collective Ltd PO Box 591 Warkworth Auckland 0941 Attention: Burnette O'Connor
Legal Description:	Sec 216 Psh of Okura SO 18072 and Lot 9 DP 520205
Site Area:	8.0937 ha
AUP Zoning:	Business – General Business Zone
AUP Precinct:	Silverdale 3 sub-precinct A, Sub-precinct Silverdale 3 sub-precinct C, Sub-precinct
AUP Overlays and Controls:	Controls: Macroinvertebrate Community Index – Urban Controls: Stormwater Management Area Control - SILVERDALE SOUTH, Flow 1
Locality Diagram:	Refer to Figure 1 .
Brief Description of Proposal:	To establish and operate an Integrated Residential Development (Retirement Village) involving the construction of 28 new buildings; including the operation of a childcare centre for a maximum of 100 children and 15 staff; food and beverage activities on the subject site, associated construction activities; car parking and to subdivide the site to create two lots.
Summary of Reasons for Consent:	Refer to section 4.0 .

Attached is an assessment of environmental effects and accompanying technical assessments that correspond with the scale and significance of the effects that the proposed activity may have on the environment.

AUTHOR

A handwritten signature in black ink that reads "Burnette O'Connor". The signature is written in a cursive style with a large, stylized 'B' and 'O'.

Burnette O'Connor

Director of *The Planning Collective Ltd*

Date: 19 February 2020

2.0 SITE CONTEXT

2.1 BACKGROUND

Matvin Group Limited has obtained resource consent for several land use activities for land they own or are involved in developing in the Painton Road / Small Road land area.

Of most relevance to this application is the existing resource consent for a childcare centre for 100 children and 15 staff members on Lot 9 DP 520205. This site also forms part of this proposal. It is proposed to slightly alter the location of the childcare as shown on the plan submitted. The scale and nature of the childcare activity will remain the same as consented. A copy of the granted resource consent is **Appendix 16**. This consent (LUC60332164) was granted on 14th May 2019 and is therefore current and able to be implemented. A snip of the approved site plan is set out below:



Figure 1: Approved resource consent site plan for proposed childcare centre.

In addition to the existing childcare centre consent, land use consent for the use of proposed Lot 1 of the subdivision proposed as part of this application, as an office precinct has been applied for. The consent has a Council Reference Number BUN60341937. The office complex will comprise of eight new buildings which are a mixture of two – three storeys high, parking and accessways.

2.2 SITE DESCRIPTION

The subject site comprises of two different sites legally described as Lot 9 DP 520205 and Section 216 SO 18072 as shown in **Figure 2** below.

The land use aspect of this proposal is limited to proposed Lot 2 which will have an area of 6.6974ha and is irregular in shape.

The parent site is cleared of vegetation, with the exception of the riparian margins. There are no buildings on the land which is pasture outside of the riparian areas. The site is located within close proximity, and to the east of the State Highway 1 north / Silverdale interchange.



Figure 2: Lot 9 DP 520205 and Section 216 SO 18072 with the property highlighted in yellow (Council GeoMaps)

John Creek flows alongside the western boundary of the site. An unnamed intermittent tributary of the John Creek flows through the eastern boundary of the property in a south to north direction. This stream flows to a large culvert at the edge of Small Road which discharges into the John Creek. There is a 10m riparian margin on either side of the stream which consists of both native and exotic species including kanuka, mapou, cabbage trees, brush wattle, crack willow, woolly nightshade, blackberry and gorse.

There is also an unnamed intermittent tributary which runs through the centre of the site which flows to a large culvert located at the northern boundary, which runs under Small Road and discharges into John Creek. Further details of the watercourses on the site are provided in the Watercourse classification assessment and Ecological Effects Assessment prepared by Bioresearches – see **Appendix 11**.

The site is of a gentle sloping nature. The eastern portion of the site slopes down from the eastern boundary towards the stream. On the western portion of the site, the land rises and has a ridge located between the gully and the western boundary. From the ridge the land slopes towards the north west. The landform has been highly modified by the previous owner with historic aerials showing tracks on the site which appear to be used for BMX or motor cross sport.

The site currently has no formed access from Small Road but there is a gateway opposite the vehicle entrance to the Park n Ride.

The site is zoned as Business – General Business. The site is within the Silverdale 3 Sub-precinct A and C areas as shown the precinct plan 1 below. The Small Road frontage is subject to the Building Frontage Control and Gateway Frontage control and a Stormwater Management Area runs through the middle of the site.

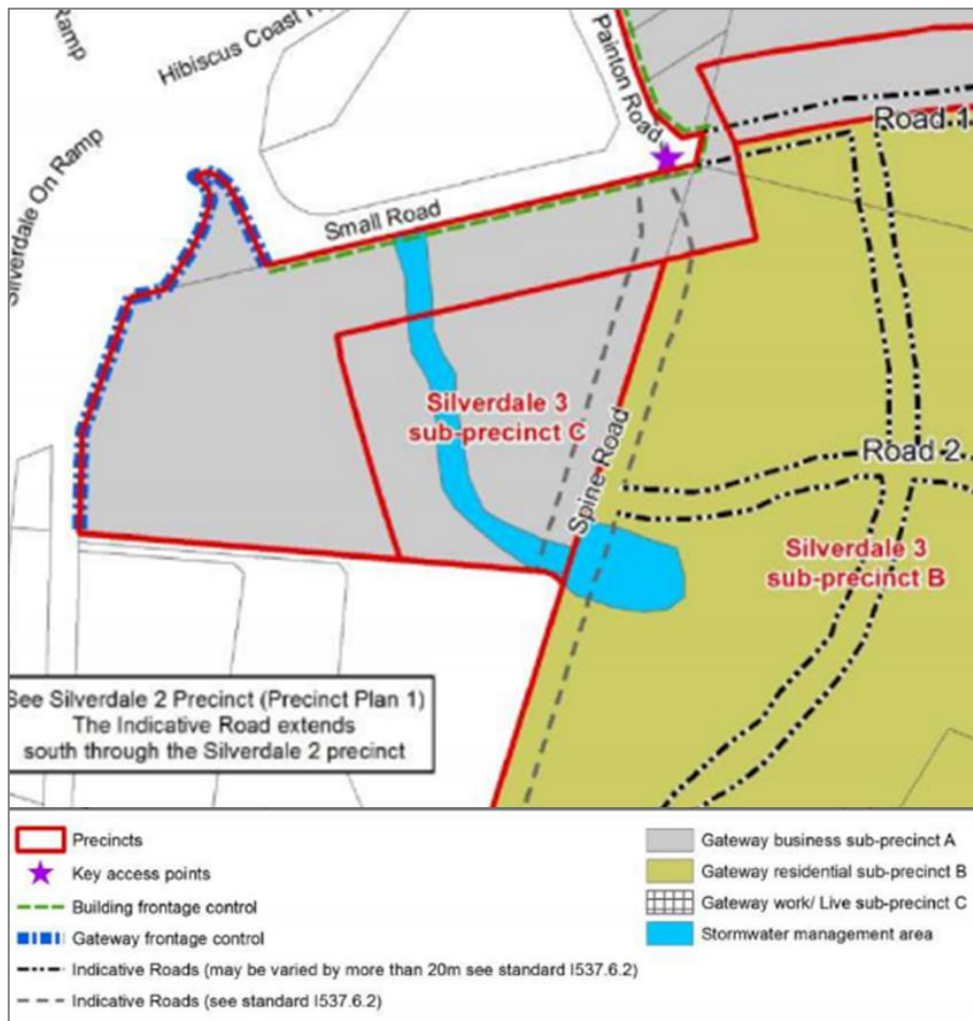


Figure 3: I537.10.1 Silverdale 3: Precinct Plan 1

2.3 SURROUNDING LOCALITY

A locality plan showing the land uses in the wider area has been included as **Appendix 2.2**. The site is located opposite the Silverdale Bus Station and Park and Ride to the north. Immediately to the east of the site is the proposed office development. To the north-east is a 'Z' petrol station and recently granted commercial development. Greenfield residential areas are currently being established to the east of the subject site zoned as Mixed Housing Urban and the earthworks to facilitate that development are readily apparent.

On 39 Small Road, the construction of a church has begun. In particular, the accessway and a flat building platform has been created.

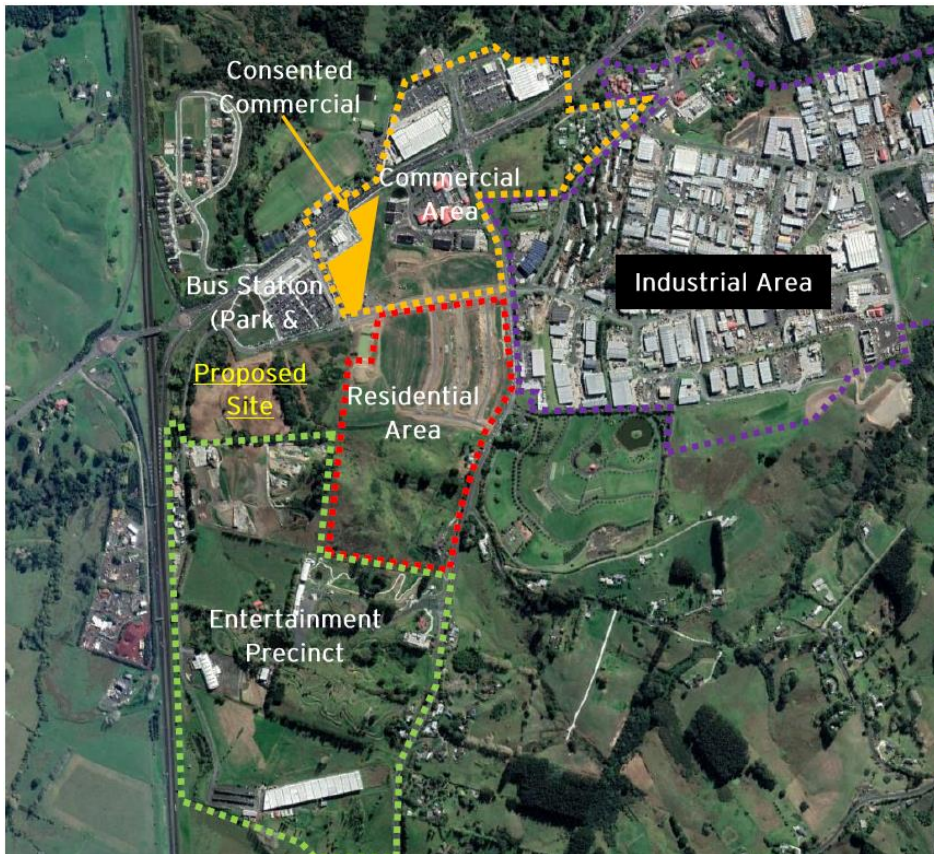


Figure 4: Land uses surrounding the subject site

Further north-east of the site are a range of large format retail shops such as Plant Barn, Pak n Save, Place Makers and Bunnings. Further east of the site, there is an established Light and Heavy Industrial Area. Approximately 1km to the north-east, is the Silverdale Town Centre which has a range of retail and commercial activities.

An assessment of the site and surrounding activities have been included in section 3 of the Economic Assessment in **Appendix 10**. This includes a summary of the total GFA of consented and existing commercial activities within the immediate area, being 115,620m².

3.0 PROPOSAL

A summary of the key elements of the proposal is set out below. More detailed description and assessment on specific aspects of the proposal are set out in the expert reports and plans accompanying the application.

3.1 OPERATION

3.1.1 Retirement Village

The main use of the site is for a retirement village, which is defined as an Integrated Residential Development¹ in the Auckland Unitary Plan (hereafter referred to as AUP(OP)). The village itself will have independent living units as well as care units to provide a range of care. In total there will be 501 retirement village units, which will comprise of the following units:

Table 1 – Summary of Retirement Village Units		
<i>Unit Type</i>	<i>Number of Units</i>	<i>Total</i>
Aged Care	49	49
One-bedroom Apartment	38	393
Two-bedroom Apartment	300	
Three Bedroom Apartment	55	
One-bedroom Villa	6	59
Two-bedroom Villa	47	
Three-bedroom Villa	6	
Total	501	

The apartments will be serviced by cleaners and medical staff; however the on-site medical facilities are all located within the main building. This will also include the main amenities areas which is for communal use. The ground floor will accommodate a commercial kitchen, dining area, staff room, library, cinema, lounges, and the main admin and reception area. Mail will also be received in the main foyer and distributed by staff to residents.

Guests will be able to pull into the port cochere and report to the reception area. It is anticipated that rubbish generated by residents will be lower than standard households. Private rubbish and recycling bins collection are proposed. A Waste Management Plan has been included as **Appendix 8**. These will be wheeled out from the rubbish storage area in the basement to an area accessible to the truck by staff.

The retirement village is to stay in a single ownership and units will be sold with rights to occupy. No subdivision of Lot 2 is proposed to ensure that the large landholding is held in one ownership to provide flexibility for a future use.

¹ Chapter J – Definitions, AUP (OP). Note that *Integrated Residential Development* is also included in the Residential Nesting Table J1.3.5.

3.1.2 Childcare Centre

The proposed childcare centre will be located within the retirement village but will be operated separately. As per the existing valid resource consent the centre will accommodate 100 children and 15 employees. The childcare centre building is shown as Block G on the plans and has a total area of approximately 700m².

The childcare centre will have four separate playrooms each with toilets and a sink, a sleeping area, staff room, office, reception and kitchen. There will also be a large outdoor area which will contain a sand pit and be enclosed by a 1.2m high balustrade and a 2-metre high acoustic wall on the northern side of the outdoor play area.

Given the site location, a high-quality architectural design is proposed to ensure an attractive streetscape will be achieved to the extent possible given the triangular nature of the site; that a positive interface with adjoining public space areas can be created. A range of materials on the building facades has been used to break up the overall bulk of the building as shown in **Figure 5**.



Figure 5: Perspectives of childcare centre in Block G

The shape of the building and variation in materials will provide visual interest and assist in establishing a strong visual street presence. Further detail of these elements has been provided in the Urban Design assessment included as **Appendix 4**.

The building is sited over the boundary line between Lot 9 and Sec216 SO 18072. It is acknowledged that either prior to Building Consent or as part of the Building Consent process; the sites will either need to be amalgamated or a notice registered on the titles that they cannot be separately from the other.

3.1.3 Commercial Services and Food & Beverage

Adjacent to Small Road, four commercial or food and beverage tenancies are proposed. These will be accessed directly from Small Road and have the following areas:

Table 2 – Summary of commercial tenancies	
<i>Tenancy Number</i>	<i>Area (m²)</i>
Tenancy 01	332.1
Tenancy 02	74.6
Tenancy 03	102.6

Tenancy 04	130.6
Gross Floor Area	639.9

All four tenancies have been designed to address Small Road and provide a canopy over the proposed footpath. This aligns with the Building Frontage Control overlay of the Silverdale 3 Precinct, which runs along this portion of the site.

Each of tenancy will be one level, apart from Tenancy 01 which has a mezzanine level. The loading and services are located at the rear of the building, in the basement car park of Block A.

3.2 BUILDINGS

The proposed buildings have been designed by Pacific Environments Architects, with the drawings of the buildings included in **Appendix 3**. The development is comprised of 24 buildings, which are labelled as Blocks A-Z, as shown in **Figure 6** below.

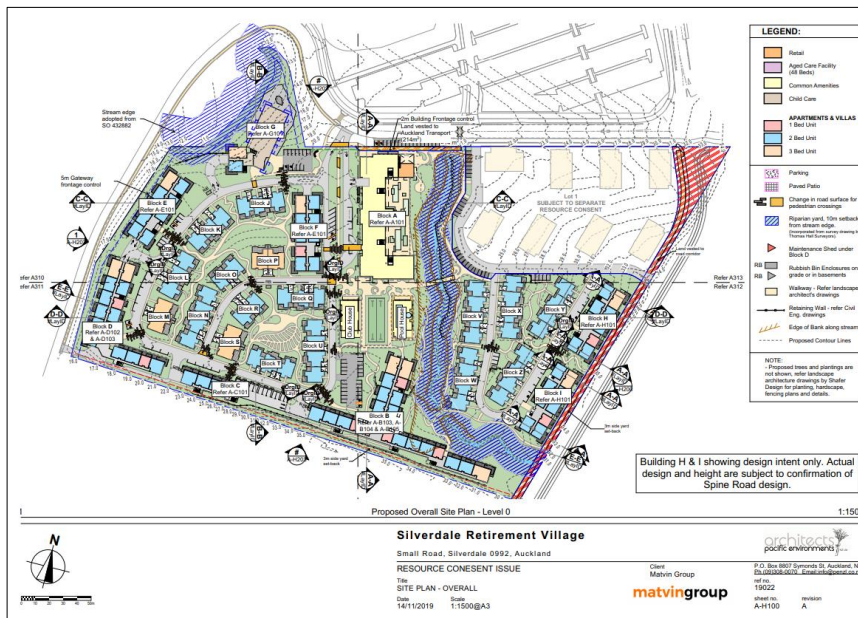


Figure 6: Site Plan with Block Names

Apartment blocks have been placed around the perimeter of the site and semi-detached villas within the middle of the site. The village is separated by the watercourse in the middle of the site, clearly demarking the eastern and western portion of the development.

Similar buildings and colour schemes will be used for consistency across the development. A network of pedestrian paths will also link the development together as pedestrian movement has been prioritised. Internal roads will also be constructed to provide access to parking in and around buildings. By way of summary, the numbers of units have been tabulated in section 3.1.1, and are described below:

3.2.1 Apartment Blocks and Communal Facilities

A total of nine apartment blocks are proposed all containing basement parking and some on street parking. These will be predominantly used as independent living units for the retirement village.

Block A

This block is located centrally on the site, immediately south of Small Road. The building has a coverage of 3,869m², is four storeys with a maximum height of 21.2m. In summary, the building will contain:

- four storeys high, which includes two levels of basement car parking. The building will be u-shape and overlook a centralised courtyard, as shown in **Figure 7** below.



Figure 7: View from the east looking at Block A

- Four commercial or food and beverage tenancies as described in section 3.1.3 above. These will be directly accessed from Small Road. To facilitate this outcome the scheme plan identifies a portion of land to be vested in Auckland Transport (214m²) to become part of Small Road. This will provide for car parking associated with the commercial tenancies. To the east, between Block A and the stream will be outdoor seating and a playground for public use.
- The main entrance to the retirement village is from the western side of the building. This is signalled by a large port cochere as shown in **Figure 8** below. The dominance of this architectural feature will assist with way-finding.



Figure 8: Perspective of Retirement Village Main Entrance

- Visitors and residents will enter the building on the ground floor, which will contain a reception area, activity rooms, lounges, bathrooms, cinema, library, and dining area for communal use. Some outdoor seating and a petanque court are located in the courtyard.
- The staff area will contain a commercial kitchen, meeting room, storage facilities and a separate bathroom.
- Level 1 will contain 49 care units, with three shared lounges, a therapy room and nurses' station. These are one-bedroom units which will have round the clock medical care. Each unit contains a bathroom, shower and wardrobe.
- Levels 2, 3 and 4 are independent living units. In total, there are 5 one-bedroom apartments, 30 two-bedroom apartments and 10 three-bedroom apartments. Each apartment will contain a balcony, an open kitchen/living/dining area and bathroom. Bedrooms will have in built wardrobes.
- The basement car park will contain a total of 13 visitor parks, 33 staff parks and 36 residents parking spaces. There will be one loading space, three mobility parks and 5 bicycle parks.
- 13 on street parking spaces will be established adjacent to Small Road. This 157m² area will be vested to Council.

Club and Pool House

Two buildings are proposed to the south of Block A which contain the club house and pool house. In the between the two buildings will be a 35m x 16m bowling green. These amenities are for communal use for residents of the retirement village.

The club house will be accessed form the northern façade and will have an area of 516m². The building has an open floor plan to allow for a range of uses and flexibility.

Two potential layouts have been provided in the drawing set, one with multi use activities, and the other a layout for event seating. There is also a sperate kitchen, bathrooms, outdoor seating and a dedicated room for a “blokes shed”.



Figure 9: Perspective of the club house looking south west

The 439m² pool house will contain a 16m x 4.5m lap pool. The building will also contain a sauna, bathrooms and plant room. The gym, spa and wellbeing room will look over the riparian reserve.

Both buildings will be single storey and feature pitched roofs as shown in **Figure 9** above. Vertical cladding, in a variety of colours will be used to articulate the building.

Block B

This block is located on the southern portion of the site, adjacent to the central waterway. This is a T-shaped block, with a total building coverage of 3,019m², and will have a maximum height of 19.0m. The building will contain the following features:

- A total of 102 apartment units, with 22 one-bedroom, 66 two bedroom and 14 three-bedroom apartments, across five levels.
- The apartments will have similar facilities as described above. Ground floor units will have a patio and upper levels will have balconies to the north where possible and/or overlooking the central watercourse.
- The basement car park will contain 71 parking spaces, 5 mobility scooter spaces and 4 bicycle parks.
- In front of Block B is an at grade parking area which contains 22 parking spaces.

- The main entrance of the building is the middle of the building which will have a centralised lift core. There is a secondary entrance on the eastern side of the building.
- There is a rubbish area in the western end of the block at basement which is to service nearby villas.

Block C

This block is located on the southern portion of the site and runs from east to west. The building is irregular in shape and has a total building coverage of 1,157m². The maximum building height will be 17.8m and will contain the following features:

- A total of 28 apartment units comprising of 24 two-bedroom apartments and 4 three-bedroom apartments,
- These will be oriented to the north and have a centralised lift core. The basement will contain 25 parking spaces, three mobility scooter parks and two bicycle parks.
- There is a rubbish area in the western end of the block at basement which is to service nearby villas.

Block D

Block D is a 2,145m² L-shaped building, located on the southern portion of the site. The maximum height of Block D will be 21.8m and will contain the following features:

- The units are accessed on grade in a centralised lobby which contains mobility scooter parking, lift entrance and a storage area.
- There is a total of 60 apartments, with 8 one-bedrooms, 44 two-bedrooms and 8 three bedrooms.
- The basement parking is accessed from the northern side of the building. The carpark has 54 spaces. In front of the building, five parallel parking spaces will be provided on the internal road and one mobility space near the entrance of the building.

Blocks E and F

Blocks E and F will have the same configuration and are both positioned on the northern section of the site. Each Block will have an area of 1,288m² and contain 32 units. This will comprise of 29 two-bedroom units and 3 three-bedroom units.



Figure 10: Perspectives of Block E and F

As shown in **Figure 10** above, a varied roof form is proposed and will have a range of materials which assist in differentiating the two buildings. Block E will have a maximum height of 20.6m and Block F will have a maximum height of 10.6m above the existing ground line.

28 car parking spaces will be provided for each Block in the basement. This will be accessed from the southern façade of Block E and the northern façade of Block F. There is a rubbish bin area located within the basement, which will have collection every three days, as outlined in the Waste Management Plan in **Attachment 8**.

There are diagonal parking spaces located to the east of Block F on the private accessway.

Block G

Block G will be located on the northern part of the site partly on Sec 216 and partly on Lot 9. As stated, these sites will either be amalgamated prior to Building Consent for this building; or a registration will be placed on those titles that they have to be held together. The relevant yard,

Block G includes the childcare described in section 3.1.2 on the ground floor, and two apartments. Levels one and two will also contain retirement units. In total there will be 20 retirement units, comprising 13 two-bedroom units and 7 three-bedroom units.

Basement parking will be accessed from the western façade. The basement can accommodate 26 parking spaces, which includes staff parks. To the east of the site entrance is an at grade parking space which can accommodate 12 parking spaces, including two mobility spaces.

The foundations of the building have been designed to minimise earthworks and retaining and manage the steep grade and potential flood hazards identified on the

GIS viewer by having a cantilevered foundation. The outdoor play area for the childcare centre is to be built on piles to address this as shown in **Figure 11** below.



Figure 11: Eastern elevation of Block G

Buildings Near Spine Road

The subject site is located to the east of the Future Spine Road. The road will be constructed in general accordance with I537.10.1 Silverdale 3 Precinct. The carriageway, footpath and berm on the northern side of the road will be constructed on the adjoining site, Lot 1 DP 200971, East Coast Road. The southern part of the footpath and berm is to be constructed on the subject site.

The applicant is working through the design details with the adjoining landowner. As the levels for this road have not yet been finalised, Blocks H and I are submitted as a separate drawing set acknowledging that some of the detail may need to be altered at a later date. It is important that the retirement units have a positive relationship to the Spine Road given the residential development that will occur on the land on the eastern side of the Spine Road.

3.2.2 Villa Blocks

Blocks J to Z contain semi-detached villas. There are located within centre of the site with a large separation distance from apartment blocks to ensure that they will receive adequate sunlight access. A total of 59 proposed in the following configuration:

Table 3 – Summary of Villa blocks			
<i>Block Name</i>	<i>Number of units</i>	<i>Unit type</i>	<i>Building coverage (m2)</i>
Block J	3	Two-bedroom	402
Block K	3	Two-bedroom	402
Block L	3	Two-bedroom	402
Block M	2	Three-bedroom	308
Block N	4	Two-bedroom	542
Block O	2	Two-bedroom	279
Block P	2	Three-bedroom	308

Block Q	3	Two-bedroom	402
Block R	2	Two-bedroom	279
Block S	2	Three-bedroom	308
Block T	3	Two-bedroom	411
Block U	4	Two-bedroom	543
Block V	4	Two-bedroom	543
Block W	4	Two-bedroom	543
Block X	4	Two-bedroom	543
Block Y	8	4 x one-bedroom 4 x two-bedroom	555
Block Z	6	2 x one-bedroom 4 x two-bedroom	550
Total	59		7,320

Nine villa typologies are proposed which vary in size from 73.8m² to 143.2m² with typical floor plans shown in Sheets A600 – A604 in **Appendix 3.5**. Each of the villas will contain an open living, dining and kitchen area. A storage cupboard, laundry and bathrooms will be provided as well as in built wardrobes.

Each of the two- and three-bedroom villas will have an internal garage and a single parking pad in front of the garage as shown in **Figure 12** below:



Figure 12: Proposed Block S when views from private road

Patios will be provided on ground floor units as private outdoor living space which will include clothes lines and paved areas. The villas will have a range of roof forms and cladding materials to provide visual interest and way finding.

3.2.3 Materials

Materials proposed for the buildings include the following materials:

- Cladding 1 - Vertical plank type texture in dark recessive colours

- Cladding 2 - Vertical plank type texture in warm colours;
- Cladding 3 - Roughsawn texture finish;
- Cladding 4 - Bagged brick texture finish;
- Cladding 5 – 70mm Brick veneer with matching colour group;
- Pre-painted metal garage doors;
- Glass balustrades;
- Aluminium louvres;
- Colour coated metal profiled roof, including skylights and sun tunnels.

The variation in building form and variation in materials will provide visual interest and avoid the repetition of the same building appearance within the retirement village. This will also assist in further articulating the building facades and establishing their street presence. Further detail of these elements has been provided in the Urban Design Assessment included as **Appendix 4**.

Details of the proposed buildings are illustrated in the drawings attached as **Appendix 3.5**. It is expected that a finalised landscape and materials plan will be provided to Council for approval and a condition requiring this will be a condition of consent.

3.3 SIGNAGE

Comprehensive development signage is proposed to be mounted on the building façades for tenant identification and way finding. A total of seven wall mounted signs are proposed on Blocks A and G and comprises of:

- One 2m x 3.6m sign for the retirement village;
- One 1.7m x 3.8m sign for the retirement village;
- Four 2.6m x 0.8m signs for the commercial / food and beverage tenancies; and
- One 3.5m x 0.8m sign for the childcare centre.

The wall mounted signs proposed will include the tenant name and logos and the location. Details for signage location is included as drawing number A-A200 and A-G201 in Appendix 2. No freestanding signs are proposed as part of this application.

All illuminated signage will comply with the AUP(OP) requirements and details of this will be confirmed at building consent stage.

3.4 LANDSCAPING

The proposed landscaping, paving and fencing details are shown on the landscaping plans prepared by Shafer Design Limited at **Appendix 5**. This includes a landscape

design statement. The overall landscape, paving and fencing strategy will feature the following:

- The design statement on page 01 summarises the key landscape area as:

“The landscape design strategy for the proposed Retirement Village Development is based on creating a garden environment, designed to bring a community together. There are several park-like spaces in the design which are dedicated to encourage the residents to interact with each other and gather together, utilising the shared garden spaces in a variety of ways.”

The planting strategy incorporates different species, which have been carefully considered in terms of their location throughout the site. Specimen trees will be planted along the southern boundary to provide visual screening from adjoining neighbours.

The landscaping scheme also proposes a range of shrub and groundcover species. These will screen the podium basement car park level and enhance views of the proposed building from the streetscape.

- **Access Design:** Pedestrian movement has been carefully considered within and around the site. In particular, the main arrival area as well as the creation of a pedestrian path within the riparian margin which runs through the middle of the site.

The roadways are divided into two zones; the main entrance with into the village which will cater for service vehicles and the residential roadways which are all shared pedestrian, vehicular areas.

The main entry/traffic area will be tar-seal and marked as necessary for various parking, drop off zones and truck/van access areas. This area will have concrete kerb and channel and limited pedestrian access points. Pedestrian paths in this area will be marked by a strong horizontal paving patterning and the boardwalk in the riparian margins which will be timber.

It is noted that the land to the south of the site is being developed for a church but also that there is a significant change in ground level in this location. The Spine Road that will be constructed in the near future provides good public access in a north / south direction and a path is shown on the northern side of the main stream that will enable public access through the site to the public space areas and retail units fronting Small Road; as well as the Park n Ride.

- **Street planting and traffic calming:** The internal accesses are designed as shared spaces, with traffic calming measures. This includes different coloured concrete to differentiate footpaths and identify key crossing areas as shown in **Figure 13** below.

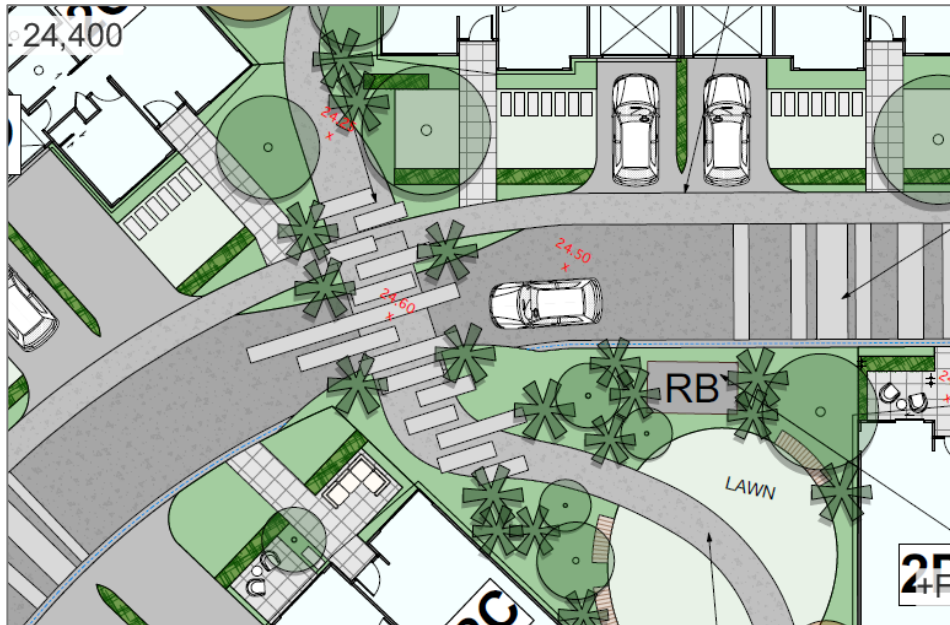


Figure 13: Typical street detail showing traffic calming

Around crossings, the road is also narrowed, with the use of planted beds, to encourage motorists to slow down. There kerbing and raised traffic humps have been avoided as they may be hazardous for the residence and may impede mobility scooters and walking frames.

Small trees with clear trunks and low-lying ground cover will be used around the streets to ensure that vehicles and pedestrians have good visibility around these spaces.

- **Amenity Spaces:** Different amenity spaces have been created around the retirement village to provide amenity for the residents. This includes:
 - Two plazas in Block A and near the club house. These include a range of amenities such as raised planters, pentanque court, seating, trees and mounded organic shapes.
 - Three main gardens are proposed including an edible garden with fruit and vegetable plants and a spring walkway with seating and blooming tree varieties. The summer lawn / autumn – winter garden has been designed to be seasonal with deciduous trees and an open summer lawn.
- **Villa landscape:** Each villa will have a 1.4m high hedge around the perimeter for the private outdoor space. These outdoor areas have been designed to be low maintenance by using pavers and artificial lawn. Raised planters will allow for personal gardening and clothes lines will be provided in patios.
- **Riparian Planting:** Native planting is proposed in the riparian area in accordance with the *Riparian Planting Plan* prepared by Bioresearches in **Appendix 12**. The timber boardwalk will weave through kanuka and totara trees providing an accessway across the tributary.

- **Boundary Treatment:** Along the western boundary, landscaping has been designed in accordance with the Gateway Frontage Control requirements as shown in **Figure 14** below.

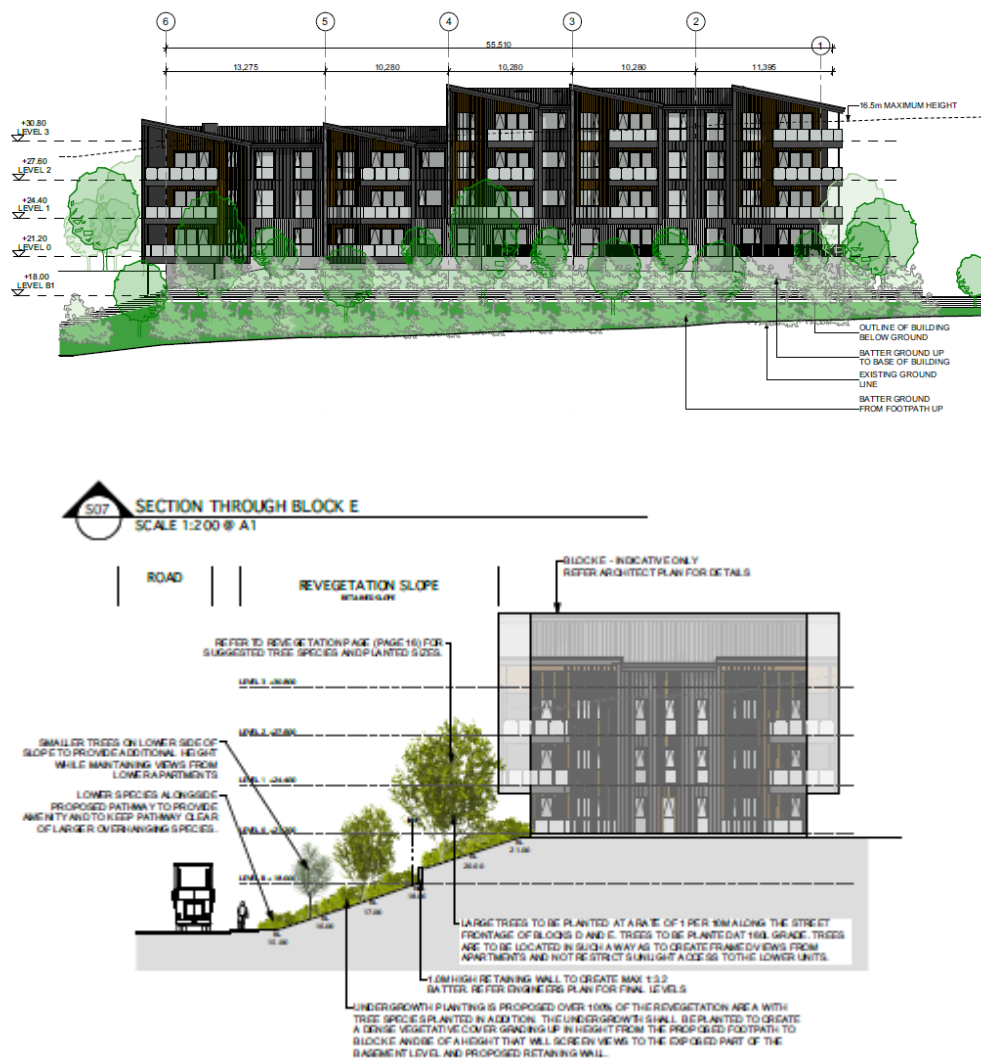


Figure 14: Section through Block E showing landscaping

Given that the site is located above Small Road, it is considered that fencing along the boundary is unnecessary. The landform will be battered where possible to conceal basement carparks and planed with ground cover, shrubs and specimen trees to provide “filtered views” in and out of the site.

3.5 SERVICING

The development will include the following servicing strategy, including stormwater drainage, wastewater drainage, water supply, power reticulation, and telecommunications, as set out in the Engineering Infrastructure Report by MSC Consulting Group Ltd at **Appendix 6**. Specifically, the infrastructure proposed will involve the following:

3.5.1 Stormwater

- There is an existing Network Discharge Consent for the Silverdale South Catchment (NDC 28908) which includes a stormwater management pond suggested by the relevant Integrated Catchment Management Plan (ICMP). This pond has been designed to provide stormwater quality treatment, attenuation and detention for 60% of impermeable surface coverage on site.
- The proposal will result in the increase of impermeable surface area with 36,698m² or 54.8% proposed. Stormwater runoff from the site will be provided via a connection to the existing stormwater manhole. Stormwater runoff from the car parking areas will be captured via cess pits and stormwater quality treatment is provided by the downstream stormwater pond at the AT Park and Ride.
- Given that the proposal does not exceed the 60% allowance, no additional detention or retention is proposed as they can be accommodated for by the downstream stormwater pond.
- There are two overland flow paths that enter the site as shown in **Figure 15** below. Block G will be built on piles and therefore the flow will not be impeded. No other structures are proposed within the overland flowpaths (noting that streams and rivers are excluded from the definition of an overland flowpath in the AUP(OP)).



Figure 15: Overland flow path affecting the site (Source: Auckland Council GIS)

3.5.2 Wastewater

- Watercare have indicated that the existing wastewater pipe located north of the site on Painton Road is close to capacity and it is understood that Watercare is planning a future connection to this area west of the SH1. A pump station is

proposed to be constructed to pump the wastewater west of the development to the new public wastewater pipe.

- The applicant is in discussion with Watercare regarding temporary and permanent design solutions for the disposal of wastewater from the proposed development. The permanent solution is a new pump station near Small Road that will have 8-hour storage and will connect into the new mains pump station west of the northern motorway. In the interim, i.e. until the pump station to the west of the motorway is constructed; a 24-hour storage will be provided on-site at a pump station and wastewater will be pumped to the existing network within Painton Road during times of low flow.

3.5.3 Water Supply

- It is proposed to utilise an existing water supply system. A new connection via a 150mm diameter watermain will be installed to service the development.
- In terms of firefighting demand, the development area will meet the required flows and the minimum required hydrant distances to service the new buildings. Overall, it is concluded that the proposed water supply network will be capable of servicing the development.

3.5.4 Utility Services

- The site is located within an area with established power and telecommunication infrastructure. It is proposed to utilise these existing services and extend them to the subject site.

3.6 SITE WORKS

Earthworks will be undertaken to prepare the land for development. The proposed earthworks for this development involve an area of 5.97 hectares (59,710m²). The works involve topsoil stripping of approximately 8,960m³; cut to fill earthworks of approximately 21,520m³ and cut to waste of approximately 117,310m³ that will be disposed of offsite at an approved cleanfill.

Retaining walls will also be installed. These range in height and location as shown on Proposed Contour Sheet 1 of 2 and Proposed Contour Sheet 2 of 2, as summarised below:

- Adjacent to Block E will be a wall which has a maximum height of 1.0m and a minimum height of 0.2m;
- In between Block B and the Club House is a 0.2m – 0.7m wall;
- Two 2.8m high walls to the east and west of Block B;
- Along the southern boundary, a 0.2m high to 4.3m high wall is proposed;

- To the north of Block X is a 0.5m high to 1.8m high wall;
- To the north of Block Y is a 0.2m high to 0.9m high wall and a 1.5m high wall;
- To support the walkway between Block Y and Z, two walls being 1.4m and 1.6m are proposed;
- Adjacent to Block Z is a 0.2m – 1.7m high wall and a 0.2m – 0.7m high wall.

These will be landscaped and will be designed by a structural engineer during the building consent process based on the specific recommendations in the Geotechnical Report included in **Appendix 13**.

The earthworks also result in the diversion of Channel 4 as detailed in the Ecological Assessment of Effects – **Appendix 11** and diversion of groundwater as detailed in the Geotechnical Assessment in **Appendix 13**.

3.7 CONSTRUCTION MANAGEMENT

The works will be carried out in accordance with Auckland Council's GD-05 and an Erosion and Sediment Control Plan (ESCP) has been provided in **Appendix 6** as prepared by MSC consultants. In particular, the use of a stabilised construction entrance and silt fences as described in section 5.2 of the report.

Works will be temporary in duration and will comply with standard construction noise limits and hours. Regular monitoring of the site by the contractor will ensure that alterations are made to the erosion and sediment control measures as required.

We propose a condition which requires a Construction Traffic Management Plan be prepared, prior to site works commencing by the appointed contractor. The Construction Traffic Management Plan should include:

- i. Construction dates and hours of operation including any specific non-working hours for traffic congestion/noise etc, aligned with normally accepted construction hours in the Auckland region;
- ii. Truck route diagrams both internal to the site and external to the local road network;
- iii. Temporary traffic management signage/details for both pedestrians and vehicles to appropriately manage the interaction of these road users with heavy construction traffic; and
- iv. Details of site access/egress over the entire construction period. All egress points are proposed to be located so that they achieve appropriate sight distances as per the RTS-6 Guide.

3.8 TRAFFIC, ACCESS AND PARKING

The proposed traffic, access and parking arrangement has been discussed in detail in the Traffic Impact Assessment, prepared by Team Traffic Engineering and Management, included as **Appendix 9**.

3.8.1 Parking

A total of 519 parking spaces including the Villa spaces (407 spaces excluding the Villas), including 12 disabled spaces; plus 25 spaces for mobility scooters and 28 bicycle parks will be provided on site, and distributed as follows:

- Block A – 93 spaces, 5 cycle spaces and 3 mobility scooter spaces
- Block B – 71 car parks, 4 cycle spaces and 5 mobility scooter spaces
- Block C - 25 car parks, plus 2 cycle spaces and 3 mobility scooter spaces
- Block D – 54 car park spaces, 2 cycle spaces and 3 mobility scooter spaces
- Block E – 35 carpark spaces plus 2 cycle spaces and 32 mobility scooter spaces
- Block F - 35 carpark spaces plus 2 cycle spaces and 32 mobility scooter spaces
- Block G – 41 car park spaces plus 7 cycle spaces and 3 mobility scooter spaces
- Block H – 31 car park spaces plus 2 cycle spaces and 2 mobility scooter spaces
- Block I - 31 car park spaces plus 2 cycle spaces and 2 mobility scooter spaces
- Blocks J – Z Villas – each 2 and 3-bedroom Villa is provided with a single car garage with an additional car park space in front of the garage. The one-bedroom Villas have a single car park spaces located on the adjacent street.

The details of the number and location of car park spaces are set out in the Traffic Impact Assessment contained in **Appendix 9**.

In relation to specific activities the above spaces are divided as follows:

- Villas: 112 parking spaces;
- Apartments and aged care: 370 spaces;
- Childcare parking: 20 spaces;
- Commercial / Food and beverage: 5 spaces in Block A basement and 23 spaces on the adjacent loop road;

The parking areas have been assessed by the traffic engineer. In particular, the manoeuvring aisle dimensions within the proposed basement have been assessed and the traffic report confirms that there are adequate proportions.

3.8.2 Loading

Two formal loading spaces are proposed on site one in the basement of Block A and other within the internal road across the road from the Block A entrance. The loading space within the basement will have a height restriction of 2.8m.

A Waste Management Plan has been attached as **Appendix 8**. Rubbish collection will be collected from bin storage areas around the village and within the loading spaces in and around Block A.

3.8.3 Vehicle Access

For the western portion of the village, there will be one main vehicular entry from Small Road. This is located in the middle of the site and will lead to a public portion of the site which is tar sealed. This contains a round a bout at the end of the accessway, which will assist in allowing people to turn around within the site to obtain access to the childcare centre in Block G. This portion of the accessway will be kerbed and the rest will be designed as a shared space.

All vehicular accessways will be privately owned and will have a formed width of 6m wide with a 1.2m deep wide foot path. To enter the basement carpark, this narrows to 5.5m. All roads within the Village have a maximum gradient of 1:20 making them also suitable for walking around the site.

For the eastern part of the village, access is gained from the private loop road as shown on the plans. This loop road will connect to the private road proposed to be constructed as part of resource consent BUN60341937 for the office development. In the event the office development is not consented by the time construction of this portion of the site commences then the required portion of this road can be constructed to provide connection to the Spine Road and Small Road if required. This scenario is considered unlikely given the timeframes to consent this notified application and the construction timeframes.



Figure 16: Pedestrian movement shown in pink and vehicle access shown in Blue

It is noted that the Spine Road does not form part of this application as most of the Spine Road is on the neighbouring site. Although indicative connections from the site to the future Spine Road are shown on the site plan, the development is not reliant on these connections with alternative access arrangements being available for these parts of the site as described above.

3.8.4 Pedestrian Access

The main pedestrian access to the site is from Small Road as shown on **Figure 16**. As outlined in the traffic assessment, it is also proposed to urbanise Small Road in the vicinity of the development site to provide for pedestrian and cycling connections to the bus station and the wider pedestrian and cycling network.

Pedestrian linkages have been enhanced to allow better internal circulation as well as better connections to the street frontages. This is achieved by increasing the number of connections within the site and constructing a boardwalk within the riparian margin to provide access to the eastern portion of the site, and along the creek which is identified as an area with high amenity. Cycle parking will be provided for within the site basement.

As described above the pedestrian connections to be provided adjacent to the river provide improved connectivity between existing and future public roads and through the proposed development. These connections also promote use and enjoyment of the riparian and stream environment in a manner that will not adversely affect the ecological or other environmental values associated with the area.

3.8.5 Small Road Improvements

The roading infrastructure on Small Road is proposed to be urbanised to support the development and integrate the development with the wider roading network. The following works, as summarised in the traffic assessment are proposed:

- *The provision of a 3-metre-wide amenity footpath along the frontage of the site;*
- *A 3-metre-wide pedestrian 'cut thru' within the existing traffic island on the Small Road approach to the roundabout to provide a connection between Small Road and the future Spine Road. This connection will include the provision of pram crossings and tactile pavers on both sides of the road;*
- *Pedestrian refuge islands on Small Road in the vicinity of the existing pedestrian path to the bus station. This connection between the development and the bus station will include the provision of pram crossings and tactile pavers on both sides of the road;*
- *A raised platform on Small Road on the western side of the vehicle access to the bus station. This raised platform will accommodate a formal pedestrian and cycle crossing that will connect the development with the existing path to*

the Hibiscus Coast Highway. The raised platform is also intended to promote lower vehicle speeds in the vicinity of the formal pedestrian/cyclist crossing and the vehicle access to the bus station;

- *A narrowed carriageway slow point on the western side of the main access to enhance the slow speed environment for vehicles approaching from the west;*
- *The provision of 17 perpendicular parking spaces on Small Road to replace on-street parking lost to the provision of property accesses along Small Road and to provide much needed additional street parking in the area – it is acknowledged that these parking spaces would be for public use and would need to be time restricted to prevent them being used by commuters;*
- *The provision of a 1.8-metre-wide footpath that extends along the site-side of Small Road as far as the southern end of the property to provide a public pedestrian connection to the south.*

The perpendicular parking spaces will be built within the subject site and this area is proposed to be vested to Auckland Transport upon completion to allow for public street parking. The area to be vested is as shown on the scheme plan contained in Appendix 3.

4.0 REASONS FOR THE APPLICATION

A rules assessment against the provisions of the Auckland Unitary Plan (Operative in Part version) (AUP (OP)) is attached as **Appendix 2**. The proposal requires consent for the matters outlined below.

4.1 AUCKLAND UNITARY PLAN (OPERATIVE IN PART)

Groundwater Diversion

- As detailed in the Geotechnical assessment in **Appendix 13** groundwater diversion consent is required in respect to the area of works and the depth of earthworks below natural ground level. The basement of Block B will also be drained. This outcome is permanent. In accordance with Rule E7.4.1 (A28) the diversion of groundwater caused by any excavation that does not meet the permitted activity standards or is not otherwise listed, is a Restricted Discretionary activity.

Stormwater Discharge and Diversion

- The discharge of 36,698m² of stormwater from an impervious area is a Discretionary Activity E8.4.1(A10).

Stormwater Management Area

- To establish more than 50m² or impermeable surface area with 36,698m² proposed is a Discretionary Activity E10.4.1(A4) as no retention and detention

is provided for onsite as it can be accommodated for within the downstream stormwater pond.

Land Disturbance

- The regional earthworks rules are contained in Chapter E11. The earthworks required for the Retirement Village involve more than 2,500m² on land with a slope greater than 10 degrees – Rule E11.4.1 (A8) and there will be more than 2,500m² of earthworks within the sediment control protection area i.e. 50 metres landward of the edge of the stream – Rule E11.4.1 (A9). Consents for a Restricted Discretionary activity are required under each of these rules.
- Chapter E12 contains the district rules for earthworks. The area and the volume of earthworks require consent because the area of earthworks is greater than 2,500m² (Rule E12.4.1. (A6); and the volume of earthworks exceeds 2,500m³ (Rule E12.4.1 (A10). Consents for a Restricted Discretionary activity are required under each of these rules.

Streams - Diversion

- The diversion of channel 4 in the southern portion of the site adjacent to Mainstem Stream requires consent as a Discretionary activity in accordance with Rule E3.4.1(A19). Consent under this Rules includes associated disturbance and sediment discharge.

Vegetation Management and Biodiversity

- Vegetation alteration or removal within 10m of an urban stream is a Restricted Discretionary Activity pursuant to E15.4.1(A19).

Signs

- Comprehensive development signage is a Restricted Discretionary Activity under E23.4.2(A53).

Transport

- The proposal involves accessory parking and access that does not meet the following parking and access standards and is a Restricted Discretionary Activity under rule E27.4.1(A2).
- The proposal involves Integrated Residential Development exceeding 500 units (501 units) and therefore technically Restricted Discretionary resource consent is required as per Rule E27.6.1 (T2). It should be noted however that the exemptions in Standard E27.6.1(1) apply because the Silverdale 3 Precinct provisions were approved on the basis of an Integrated Transport Assessment and the effects of the proposed land use in terms of trip generation and transport effects are the same or similar in character, intensity and scale to those identified in the previous assessment. In particular the land use traffic

effects are deemed lesser and construction traffic effects will be the same or similar;

- The proposal involves use and development that fails to meet the following core standards and is a Restricted Discretionary Activity under rule C.1.9(2):
 - Not meeting the minimum loading space requirements with three spaces required by E27.6.2.7 and two spaces proposed;
 - Parking spaces 50, 59 and 69, in the basement car park of Block A do not meet the dimensions set out in E27.6.3.1.1;
 - Block A garage door will have a 2.8m high vertical clearance whereas 3.8 metres for a loading space is required in accordance with Rule E27.6.3.5 – Vertical clearance. Because the standard is not met resource consent is required for a Restricted Discretionary activity in accordance with Rule E27.4.1 (A2) – parking loading and access not meeting the standards;
 - Not meeting the minimum 6m formed access widths as required by E27.6.4.2(T155) with 5.5m proposed at portions of the access ways for Blocks A-G.

Natural Hazards

- Any buildings or other structures, including retaining walls (but excluding permitted fences and walls) located within or over an overland flow path is a restricted discretionary activity pursuant to E36.4.1(A42).

Business – General Business Zone

- The establishment of an Integrated Residential Development is a Non-complying activity pursuant to rule H14.4.1(A4).
- The establishment of a care centre is a Discretionary activity pursuant to rule H14.4.1(A30).
- To construct new buildings is a Restricted Discretionary activity under rule H14.4.1(A42).
- The proposal involves use and development that fails to meet the following core standards and is a restricted discretionary activity under rule C.1.9(2):
 - The proposal does not meet Rule 14.6.1 maximum height control of 16.5m is breached by:
 - Block A – 20.4m high
 - Block B – 19m high
 - Block C – 17.8m high
 - Block D - 21.8m high
 - Block E – 20.6m high

- Block G – 20.4m high
 - Block W will be located within the riparian yard.
- In accordance with Rule H14.6.5 no more than 10% of the area of the riparian yard is to be covered in impervious surfaces. The Assessment of Ecological Effects details the extent of encroachment into the riparian yard in Section 4.4.4. The Rule is unclear as to whether the 10% applies to the riparian yard of a specific stream or whether it applies to all streams on a development site. I note that there is no more than 10% encroachment of impervious surfaces into any individual stream riparian yard; but that if the development site as a whole is considered then the total encroachment into all riparian yards is approximately 17 – 18%. For the sake of completeness consent is sought under this Rule.

Subdivision

- Restricted Discretionary consent is required for subdivision creating one new vacant site greater than 200m² in area. Lot 1 has a net site area of 1.3345 hectares and Lot 2 (retirement village site) has an area of 6.4.342 hectares.

Silverdale 3 Precinct

- Any land use activity, that does not meet the Vehicle Movement in PM Peak Permitted activity Rule (A6), as the collective total traffic generation will be more than 136 vehicle per hour in the PM peak is a Discretionary activity under Rule I537.4.1(A7).
- To construct new buildings is a restricted discretionary activity under rules I537.4.1(A1) and I137.4.2(A24).
- The establishment of a care centre is a Restricted Discretionary activity pursuant to rule I537.4.1(A21).
- Any development generally in accordance with 1537.10.1 Silverdale 3: Precinct plan 1 is a restricted discretionary activity under rule I537.4.2(A22).
- The proposal involves use and development that fails to meet the following core standards and is a restricted discretionary activity under rule C.1.9(2):
 - The proposal fails to meet I537.6.3 – Residential floor area as the proposed residential use on the ground floor on Blocks B, V, - Z is not ancillary to a business premise.
 - Blocks B, contain Unit types 2A – this is a two bedroom and option 1 only has 7.9m² private open space and Unit types 2B option 1 has 6.8m² private open space not meeting the H6.6.15;
 - The proposal fails to meet the landscaping requirements set out in I537.6.4(2) as no trees capable of reaching a minimum height greater

than 8m are proposed along the frontage to the future Spine Road with approximately 17 trees required. This will be developed in the future.

- The proposal fails to meet the Building Frontage Control I537.6.6.1(a)(i) as Blocks D, E and G are within 5m of the site frontage.
- The proposed veranda to the street will be 3m wide and therefore does not meet the 4m required by I537.6.6.2(b)(v).

4.2 NESCS

Resource consent is not required under the provisions of the National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health ('NESCS'), as this was addressed as part of the bulk earthworks consent.

4.3 ACTIVITY STATUS

Overall, this application is for a **Non-complying activity**.

5.0 PUBLIC NOTIFICATION ASSESSMENT (SECTIONS 95A, 95C TO 95D)

Section 95A gives a council discretion to decide whether to publicly notify an application or not. However, an application must be publicly notified if:

- (a) the activity will have, or is likely to have, adverse effects on the environment that are more than minor;
- (b) the applicant requests public notification of the application; or
- (c) a rule or national environment standard requires public notification.

Notification Rule H14.5(1)(b) states that any application for resource consent for integrated residential development² must be publicly notified. In this regard, public notification of this resource consent application is required in accordance with Section 95A(2)(c) of the Resource Management Act (RMA). Notwithstanding, the conclusion of this report and accompanying technical reports is that the effects of the proposal will not be more than minor and the proposal is not contrary to the objectives and policies of the AUP (OP).

² 'Integrated residential development' is defined as "a residential development on sites greater than 2,000m² which includes supporting communal facilities such as recreation and leisure facilities, supported residential care, welfare and medical facilities (inclusive of hospital care), and other non-residential activities accessory to the primary residential use. For the avoidance of doubt this would include a retirement village."

6.0 CONSIDERATION OF APPLICATIONS (SECTION 104)

6.1 STATUTORY MATTERS

Subject to Part 2 of the Act, when considering an application for resource consent and any submissions received, a council must, in accordance with section 104(1) of the Act have regard to:

- any actual and potential effects on the environment of allowing the activity;
- any relevant provisions of a national environmental standard, other regulations, national policy statement, a New Zealand coastal policy statement, a regional policy statement or proposed regional policy statement; a plan or proposed plan; and
- any other matter a council considers relevant and reasonably necessary to determine the application.

As a non-complying activity, section 104D of the Act states that a council:

- (a) the adverse effects of the activity on the environment will be minor; or
- (b) the application is for an activity that will not be contrary to the objectives and policies of the relevant plan and proposed plan.

Pursuant to section 104D of the Resource Management Act if a proposal is a non-complying activity then it must pass at least one of the tests of either section 104D(1)(a) or 104D(1)(b) before an application can be assessed to make a decision under 104B of the Act. If the application fails both tests of section 104D then the application must be declined.

6.1.1 Permitted Baseline and Existing Environment

In this case, the permitted baseline within each of the relevant zones in relevant

- Diversion and discharge of stormwater runoff from impervious areas up to 1,000m² within an urban area;
- An increase of 50m² of impervious surface area as it is within a Stormwater Management Flow Area;
- A total of 5m² and 5m³ of earthworks within a riparian margin for general earthworks.
- Accessory parking and access which complies with the standards in chapter E27.
- Trip generation – for integrated residential developments, the trip generation outlined in Table I537.4.1 (A3) and (A4).

There is no useful permitted which could be applied to new buildings in the Silverdale 3 zone and the General business zone, comprehensive development signage, the

removal of native vegetation within 10m of a watercourse and the placement of a building within a natural watercourse as these all require consent.

Although all new buildings within the Silverdale 3 Precinct and the General Business zone require consent, the relevant standards for the bulk and mass of buildings are compliance with a 2.5m + 45-degree recession plane along the eastern boundary and 16.5m high buildings. This is coupled with yard setback; landscape requirements and frontage controls indicate what scale of buildings can reasonably be anticipated within these zones.

In terms of activity, commercial services and food and beverage use are permitted within the General Business zone and the Silverdale 3 Precinct. Other permitted activities within both the underlying zone and the precinct are³:

- Commercial Services;
- Drive through restaurants;
- Food and beverage;
- Garden Centres;
- Marine Retail;
- Motor vehicle sales;
- Artworks;
- Industrial activities;
- Recreation Facility; and
- Marae complex.

6.1.2 Written Approval

No persons have given their written approval this application. The application has to be publicly notified and this is requested.

7.0 EFFECTS ON THE ENVIRONMENT (SECTION 104(1)(A))

Section 104(1)(a) of the Act requires that a council have regard to any adverse effects on the environment of allowing the activity.

Pursuant to section 104(2), when forming an opinion for the purposes of section 104(1)(a), a council may disregard an adverse effect of the activity on the environment if the plan or a national environmental standard permits an activity with

³ Note that General Rule C1.6(4) states that: *Where an activity is subject to a precinct rule and the activity status of that activity in the precinct is different to the activity status in the zone or in the Auckland-wide rules, then the activity status in the precinct takes precedence over the activity status in the zone or Auckland-wide rules, whether that activity status is more or less restrictive.*

that effect (i.e. the council may consider the “permitted baseline”). In this case, there are very limited activities that can be undertaken on this site as a permitted activity.

The permitted baseline can be used to assess the bulk and scale of buildings anticipated within this zone, and the effects associated with a permitted scheme within zone. In particular; a 16.5m building complying with yards and height in relation to boundary setbacks – noting that new buildings require resource consent for design matters.

Section 104(1)(ab) also requires that measures proposed, or agreed to by the applicant for the purposes of ensuring positive effects on the environment to offset or compensate for any adverse effect on the environment that will or may result from allowing the activity.

Having regard to the actual and potential effects on the environment of the activity resulting from the proposal, the following assessment of effects is made.

7.1.1 Positive Effects

Further, it is considered that the proposal will also result in positive effects including:

- Improved visual amenity of the site through introduction of architecturally designed buildings with extensive landscaping of a variety of species to integrate the development as a whole whilst ensuring that it integrates with surrounding land and achieves the objectives for the Silverdale 3 Precinct;
- The removal of pest species to enhance the ecological quality of riparian margins and improve ecological values;
- The addition of boardwalks within the riparian margin to improve internal circulation and enhance amenity by providing access to an area with ecological and landscape values;
- Works to Small Road to urbanise the road will provide pedestrian and cycle connectivity to the wider Silverdale area;
- Provision for commercial services including food and beverage activities for the local community in close proximity to the bus station;
- Provision of a childcare centre which is a beneficial community facility and is located in walking distance to the Park n Ride Bus Station;
- Provision of a retirement village, which is an activity identified as a having a shortage within the primary and secondary catchments of the area. This provides an important type of care within this area which is subject to extensive and rapid population growth.
- The operation of a retirement village is a commercial entity and will provide employment opportunities for the Silverdale community in a location where effects can be well managed.

Overall, it is considered that when taking into account the positive effects, any actual and potential adverse effects on the environment of allowing the activity are not greater than minor and are overall appropriate.

7.1.2 Neighbourhood Character and Streetscape

As described in Section 2.3 above, the surrounding locality is comprised of a bus station park and ride to the north, a consented commercial area, an entertainment precinct to the south – which has consent for a church and a residential area to the east. The site is adjacent to a motorway on ramp. Most of the sites within the immediate vicinity are vacant given the relatively recent zoning of the land for urban development and the time it takes to obtain consents and commence construction works.

In terms of the wider area, there is an established industrial area, commercial area and residential development across Hibiscus Coast Highway consequently, there is some variation in architectural style and character. Approximately 1.2km from the site is the Silverdale Town Centre which comprises of one to two storey retail spaces.

The proposal involves the construction of 28 buildings on a vacant site which will contain 501 retirement village units with shared amenities, a childcare centre and four food and beverage/commercial tenancies. The proposal has been assessed by the Urban Design Assessment prepared by Transurban (refer to **Appendix 4**). The proposed apartment units vary in size and have been designed to achieve a good level of external and internal amenity. The development positively addresses Small Road with living areas oriented to overlook the street providing passive surveillance and a high level of landscaping integrated with building design.

The commercial tenancies on Block A are subject to the Building Frontage Control. To achieve the outcomes sought by this Control Block A is built up to the boundary, with a high level of glazing on the ground floor, way finding and a veranda for weather protection.

As the levels of the future Spine Road are not known yet, as the majority of the road will be constructed on the adjoining site, a condition requiring a finalised landscape plan to address this interface is proposed. As acknowledged Blocks H and I are contained in a separate Drawing Set so that should change to these be required in the future in relation to the completed Spine Road it is simpler to alter a separate drawing set. This also reflects the fact that the design of the Spine Road is not yet finalised.

The proposed buildings have been designed to provide visual relief through an interesting and articulated built form that positively addresses and strengthens the built character in this locality to create the “gateway” experience sought by the Silverdale 3 precinct provisions.

The apartment buildings have been split into several buildings, with adequate separation distance to avoid excessive bulk. The overall design treatment of the apartment buildings, including a varied roof form, a combination of vertical and horizontal detailing, a variety of building materials and the use of upper level balconies will help to break up any perceived massing and create a highly articulated façade.

Whilst the proposal is for a greater density of development than what is currently prevalent in the surrounds, largely reflective of the developing nature of the location and the fact that much of the development underway is not yet completed; the development will feature a site layout that has been well considered with a building that is visually interesting.

A comprehensive landscape scheme that includes a 5m landscape strip along the Gateway Frontage has been developed which compliments the high-quality built form. This planted interface with the State Highway will provide filtered views to the development assisting in integrating the development into the wider landscape as sought by the Silverdale 3 Precinct provisions.

Given that the site is part of an undeveloped area, there is not yet an established character. Although the buildings are not commercial in nature, the high-quality architectural design, scale of the buildings and integrated landscaping is in keeping with the future character of the area envisaged by the AUP (OP). Specifically, that which is described in I537 for the Silverdale 3 precinct. Therefore, it is considered that any adverse effects with regard to streetscape and built character will be less than minor.

7.1.3 Visual Dominance and Shading

Consideration has been given to the extent to which the proposed development could give rise to adverse visual dominance and shading effects on the wider environment.

In the General Business zone, the yard setback and height in relation to boundary controls apply only to residential, open space and special purpose zones in order to manage visual dominance effects and to allow reasonable sunlight and daylight access in these zones where lower heights limits apply. This provides some flexibility to building placement within the site and has been taken into account when designing the proposed buildings. The proposal complies with all external boundary controls other than Block G which is constructed over the boundary of Sec 216 Parish of Okura and Lot 9 DP 520205. The proposed apartment buildings will not be out-of-scale with the intended level of development for the surrounding area.

In terms of the wider views, Mr Rae has completed an analysis of two key views into the site and makes the following comments:

- *The policy 1537.3(3) seeks that development contribute to a quality gateway experience with sensitively designed prominent buildings within a vegetative framework. This means that the buildings should be visible but integrated within vegetation. The proposed scale of the buildings will result in them being prominent visual elements and the height of the buildings help achieve this as the wider landform of the interchange is large and viewers are typically at levels higher than the site with views down. The open space surrounding these buildings will enable an attractive landscape outcome to be established.*
- *The height and scale proposed around the northern end of the stream is supported as it provides a vertical scale assisting with some enclosure to this open space enable many people to view and use this space adding to the quality of the amenity on site.*
- *Building A is approximately half a level taller than the permitted height standard as illustrated on drawing A-A200. This additional height will strengthen the visibility of this building and together with the others proposed will create an urban node.*
- *There is an expectation of buildings with height of 4 to 5 levels on the site and the proposal to include additional height to that provided by the standards is unlikely to cause adverse effects on the amenity of the street or dominance issues due to the function of that part of Small Road where the site is opposite the motorway land which includes a significant batter slope rising in height from the southern end of the site up to the interchange as illustrated in Image 22.*
- *There is potential for the buildings at the north western end of the site opposite the bus station and large car park to be taller than proposed due to the open large scale of the surrounding landscape and motorway interchange embankments. The proposal includes buildings taller than the height standard of 16.5m. This leads to interesting attractive roof forms and is appropriate in this context.*

Notwithstanding this, the architectural design of the development will assist with visually breaking down any perceived mass or bulk of the development when viewed from the wider environment. Together with the comprehensive landscaping design proposed, it is considered that the development will achieve an attractive and high quality-built form which will be within scale of the site's existing and planned context.

Based on the foregoing, it is considered that the bulk and massing of the proposed buildings are generally consistent with what might be expected in the Silverdale 3 Precinct, such that any adverse effects in terms of building dominance and shading will be less than minor.

7.1.4 Onsite Amenity

A detailed assessment of onsite amenity has been included in the Urban Design assessment at **Appendix 4**. The proposal involves a mixture of unit sizes to provide for different housing needs of the community and residents which have varying degrees of mobility. The dwellings and site have been designed to optimise the use of space both internally and externally and it is considered that the future residents will experience a high level of amenity and liveability.

The internal layouts are very efficient and all units are relatively generous in terms of their floor area, with an average of 100m² gross floor area of. The living areas of each dwelling open onto outdoor balconies, and the adjacency of the internal and external living areas provides a strong indoor-outdoor connection.

The architects have carefully laid out the site to provide space between the existing and proposed building. The units have several different outlook vantage points and orientations which are mainly oriented towards plazas, gardens, riparian planting and communal area. Generous windows are also provided for light and ventilation.

Visual privacy will be maintained through the careful selection of materials, placement of windows and the use of louvres and planting. Overall, the size, private open space, daylight access and ventilation of the proposed dwellings will provide quality living environments for residents.

The proposed landscaping internal to the development will incorporate a large variety of specimen trees, shrubbery and groundcovers within the communal internal access areas, parking area and along external site boundaries, which is considered to achieve a good level of amenity.

Overall, it is considered that the development will feature an appropriate level of residential amenity.

7.1.5 Economic Effects

An Economic Impact Assessment has been completed by Urban Economics which included as Appendix 10. The key findings of the report were:

- *The proposal site is within a substantial area of General Business zone land that has 29,700m² of existing activity, 12,500m³ of consented activity and 62,300m² of future potential activity. This is a large amount of business activity that is largely 'non-centre' and 'non-industrial'.*
- *The Sub-precincts A and C have a relatively narrow range of permitted and restricted discretionary activities when evaluated against unmet market demand in this location. Specifically, the majority of retail activities are not permitted in Sub-precinct A and C and industrial activities will be better served in Highgate Business part of the planned 300 – 400 hectare Dairy Flat area.*

- *In general, there is little demand for the permitted and restricted discretionary activities in Sub-precinct A and C and this is likely to have reduced its commercial potential.*
- *There are currently 3,410 retiree households aged 70 years plus in the Primary catchment. This is forecast to increase by 1,300 to 4,710 over the next decade, an increase of 40%. There are currently 9,310 retiree households aged 70 years plus in the Secondary catchments. This is forecast to increase by 4,050 to 13,360 over the next decade, an increase of 44%. These demographic trends underpin the strong demand for retirement villages.*
- *There are currently 4,704 retirement village units over all catchments, of which only 109 (2%) are vacant. This is a very low level of vacancy and indicates that there is a general shortage of units.*
- *There is a significant expansion in the supply of retirement units – 1,135 units currently in the pipeline supply over all catchments. This is however not sufficient to keep pace with demand growth.*
- *There is a demand for several additional retirement villages in the Hibiscus Coast over the next decade. However, the only location that has land of a suitable size and zone, to enable a new village, is Milldale.*

The economic assessment has identified that there is a shortage for retirement villages within the primary and secondary catchment, and there is only one other area which has suitable land and zoning to provide for a retirement village.

Therefore, even though the proposal is not an anticipated activity within this zone, it is uniquely positioned to cater for a shortage in this catchment area, whilst not undermining the strategy to provide growth in commercial activity in Auckland, as there is still 62,300m² available for future potential commercial activity and a new plan change for extensive industrial development on the opposite side of the motorway is being promoted by Auckland Council.

Although a retirement village is defined as a residential activity in the AUP(OP), it is run as a commercial entity and provides employment opportunities; specifically supporting staff to operate the aged care units and the maintenance of the communal grounds and independent living units.

A brief analysis of the existing and consented business has been undertaken in table 4 below. In terms of activities which are permitted on this site, the table below shows that these are catered for within immediate or local area:

Table 4: Business distribution within the surrounding area		
<i>Permitted activities on the subject site</i>	<i>Consented or existing businesses</i>	<i>Approximate distance from the site⁴</i>
Commercial Services	Silverdale Town Centre	1.5km

⁴ Note the distances were measured off GeoMaps along roads – not including future Spine Road.

	Lots 1-8 Painton Road	100m
Drive through restaurants	KFC & Burger Fuel – Silverdale Town Centre	1.5km
Food and beverage	Located on site Silverdale town centre Lots 1-8 Painton Road	0m 1.5km 100m
Garden Centres	Kings Plant Barn Bunnings Place Makers	550m 470m 950m
Marine Retail	Silverdale Town Centre Industrial Area	1.5km 2.0km
Motor vehicle sales	Silverdale Town Centre Industrial Area	1.5km 2.0km
Artworks	None	
Industrial activities	Industrial Area	1.5km
Recreation Facility	Silverdale United Rugby and Sports Club Coast Swim Club	230m 1.9km
Marae complex	Te Herenga Waka o Orewa Marae	1.3km

Therefore, it is considered that the distribution of established businesses within this area already provide goods, services, community facilities and opportunities for social interaction. In particular, the Silverdale Town Centre provides for convenience and is the focal point of the community. Whilst the proposed activity is not inherently commercial, it will still facilitate social interactions as sought by H14.2(4)(b) and provide employment and support the function of the town centre.

Overall the economic assessment shows that there is a demand for retirement village units within the primary and secondary catchments of the site. The proposal will address this shortage and will not undermine the growth of commercial activities as there is still 62,300m² available for future use within the catchment. Furthermore, the operation, maintenance and servicing of the village will provide employment opportunities.

7.1.6 Reserve Sensitivity Effects

The proposed site is unique that it only has two adjoining sites, being 39 Small Road to the South and Lot 1 DP 200971, East Coast Road to the east of the site. to the east, the plan allows for Live/Work units. The land to the south is subject to the Silverdale 2 precinct provisions which focus on entertainment facilities complimentary to the existing Snowplanet. We note that a consented church is currently being built on number 39 Small Road.

Given the proximity to the State Highway and considering the potential uses of the adjoining sites, noise attenuation and ventilation will be provided to ensure that the on-site amenity of habitable rooms will be acceptable and to minimise any potential reverse sensitivity effects. The design of noise attenuation and ventilation will be provided at building consent stage.

As set out above an acoustic fence is proposed as part of the Childcare centre to ensure that the noise levels in the proposed play area comply with the Ministry of Education requirements.

Coupled with the separation distances from adjoining neighbours and landscaping which provides filtered views in and out of the development, it is considered that any reverse sensitivity effects of the proposal will be less than minor.

7.1.7 Signage

The signage associated with the proposal includes wall mounted signage. The proposed signage constitutes a comprehensive development signage. Overall, the signage proposed is considered to have less than minor adverse effects on the surrounding environment for the following reasons:

- The signs which are most prominent within the surrounding streetscape will be on Block A on the northern elevation of the building. This will be positioned to address Small Road. This signage is considered to be appropriate in the context of the anticipated surrounding environment as naming/advertising signage will not be an uncommon feature within the wider Business area once it is all developed;
- Building naming signage is integrated into the building design and will have low levels of visual intrusion due to the flush nature of the signage with the building façade. The colour scheme of the building and building signage is relatively recessive whilst ensuring that the signage is easily legible and recognisable to vehicles driving past without compromising visual amenity;
- The proposed site is relatively large and able to accommodate the level of signage required without creating visual clutter that may otherwise occur on a smaller site;
- From a traffic safety perspective, the provision of clear signage is considered important to give motorists travelling on Small Road an early indication of the presence of the development; and
- The signage will also provide clear directions in terms of the vehicle entrance to the site and the traffic circulation routes within the car park and the accessways. It is considered that the signage will not have any detrimental impact on road safety in terms of undue driver distraction within the existing traffic environment, but rather will be a positive element in terms of road safety.

For the reasons outlined above, it is considered that the proposed signage will have less than minor visual and streetscape amenity effects and is appropriate and necessary as an integral part of the proposed development.

7.1.8 Transportation

The proposed development involves construction traffic; the formation of parking, loading and access infrastructure to service the new retirement village, commercial tenancies and childcare centre.

Vehicle accesses to the site is proposed from Small Road and indicative secondary accesses are provided from the future Spine Road; noting that the development is not reliant on these secondary accesses. Based on this proposal, Traffic Engineering and Management (“Team”) has assessed the potential adverse effects on the surrounding road network as a result of the development. The traffic impact assessment can be found at **Appendix 9**.

The TIA assess the proposal in relation to internal and external factors that may affect traffic. These include site access design, parking provision and demand, loading, safety impacts, and traffic generation volumes. It is not intended to repeat the assessment within the Traffic report; however, the conclusions with respect to traffic related matters are as follows:

- The Small Road improvements will provide an urban environment with good pedestrian and cycling connections from the proposed development to the wider transportation network. This includes a suitable connection to the Park and Ride, which is a viable alternative from private vehicles for the residents of the village given the close proximity to the bus station.
- The dimensions of roads are suitable for vehicles and service vehicles. The roads and paths will have a maximum grade of 1:20 making them suitable for the residents of the village, including those in mobility scooters.
- Design features, such as different materiality and landscaping will result in a low-speed environment and given the very low vehicle movements anticipated within the site, will be a suitable for the intended use.
- The basement accesses will be suitable for two-way traffic given that are most likely to be used by regular users and the proposal is for a residential activity;
- Adequate onsite parking has been provided and will be allocated and clearly marked out. Overall the basement parking will be reserved for residents and visitors can utilise on-street and outdoor parking. These parking spaces generally meet or exceed the dimensional requirements of the Unitary Plan for ‘regular users’ and ‘casual users’.
- There is also adequate parking for all aspects of the proposed activity and the provision of parking and cycle spaces; including disabled spaces complies with the Unitary Plan requirements.
- It is considered that the traffic generated by the development:

- *Can occur at the two connections that will be available to the surrounding network, with these being the Painton Road intersection with Hibiscus Coast Highway and at the new connection to East Coast Road.*
- *Will result in minimal increases to each of the existing turning movement at these intersections.*
- *Will be less than the daily fluctuations that currently exist during the peak hours and will be barely noticeable on this basis; specifically with regard to the increase in traffic on the wider network and in particular the Hibiscus Coast Highway and the Motorway.*

Having regard to the assessment provided in the transport assessment it is considered that the development is appropriate on the site and will have less than minor adverse transportation effects.

7.1.9 Ecological Values

The site is currently vegetated with exotic and native species, as described in the Ecological Assessment attached at **Appendix 11**. The proposal will require the removal of exotic and native vegetation within 10m riparian yard of the Mainstem stream. Some vegetation removal may result from the construction of the boardwalks and bridge. It is intended to manage these effects by way of a Vegetation Removal Methodology that will apply to the works within the riparian yard. The reason for this is that the removal of; or impacts on trees, is proposed to be avoided in the construction of boardwalks etc; however as detailed design has not yet been completed some minor vegetation removal may be required. This will also present an opportunity for weed control within the site. Minor vegetation removal could also occur in relation to the construction of deck structures and the cantilevered car parks etc.

Planting within riparian margins has an important role in the ecological processes and have a large influence on the function of waterways. Therefore, any vegetation that will be removed from this area will be replaced with appropriate native species to ensure that the buffer and filtration services are maintained. This will also present the opportunity for a weed control programme will also improve the ecological value of the overall site which is currently neglected and covered in weeds.

Overall it is considered that the proposal will have less than minor effects on the ecological value of the site. Positive effects will arise from the removal of weed species and also the proposed landscape planting that will provide better ecological connectivity than the existing pasture does.

7.1.10 Stormwater

The proposal will result in an increase in impermeable surface area by 36,698m² for the building, car park, access ways and footpath/paving. This will be diverted to the existing stormwater network via connection to the existing manhole. It is considered

that there is adequate capacity within the existing stormwater system to accommodate this increase in impermeable surface area as described in the Engineering Report attached as **Appendix 6**.

The subject site is within the Stormwater Management Area Flow 1 (hereafter referred to as "SMAF 1 area") area under the AUP (OP). The SMAF areas identify rivers and streams that are particularly susceptible to the effects of development or have relatively high values. Specifically, the SMAF 1 areas typically discharge to streams with moderate to high values and sensitivity to stormwater, but generally with higher levels of existing impervious areas within the catchment.

Attenuation (2 & 10yr ARI) & extended detention (first 34.5mm rainfall) will be provided by the existing stormwater pond, which has been designed to accommodate the wider catchment and the proposed detention tank. The pond also provides for quality treatment as detailed in **Appendix 6**.

Overall it is considered that there is adequate capacity within the existing stormwater network and retention proposed therefore the adverse effects of stormwater runoff within the SMAF1 will be less than minor.

7.1.11 Flooding

There are two overland flowpaths that are shown on Auckland GeoMaps, which have been assessed in the Engineering Report attached as **Appendix 6**. It is considered that the future Spine Road is likely to intercept this flow and therefore will not enter the site. This will be constructed prior to buildings on the eastern side of the village being constructed and therefore will not affect this development.

In terms of Block G, the building will have a suspended slab foundation to ensure that the building will not obstruct the overland flowpath and retaining has been designed to be outside of the overland flowpath area. Therefore, no structures will be located within the flowpath which would cause any diversion or reduction in the capacity of the flow.

The basement finish floor level will be 500mm above the floodplain and therefore the risk of property damage will be less than minor. Overall it is considered that the proposal will not exacerbate or create any new natural hazards.

7.1.12 Construction Activities

Construction activities associated with this proposal include the construction of retaining walls, buildings and formation of the accessways and car parking areas. It is anticipated that construction traffic will be managed by a Traffic Management Plan prepared by a contractor. It is acknowledged that waste fill will need to be removed from the site and will be disposed of at an authorised cleanfill site. This will result in traffic movements and it is expected that the route and associated effects will be

addressed through the Traffic Management Plan post consent. At this time the disposal site/s will be known.

As well as measures to mitigate sediment and erosion runoff effects as outlined in the MSC engineering report; measures to control dust generation and noise generation will also be implemented in accordance with standard good practice procedures and include limiting noise generating construction activities to appropriate times.

It is considered that the scale of the works is proportionate to the development and works will be temporary in duration. Construction will comply with standard construction noise restrictions.

It is therefore considered that any adverse noise, traffic and stormwater effects associated with the construction activities will not be greater than minor. Furthermore, there are no significant geotechnical constraints that would preclude the type of development proposed.

7.1.13 Summary

Having regard to the above, it is considered that any adverse effects associated with the proposal will be minor. It is therefore considered that the proposal will meet the test in s104D(1)(a) of the Act.

8.0 STATUTORY ASSESSMENT (SECTION 104(1)(b) AND SECTION 104D)

Sections 104 (1)(b) and 104D(1)(b) of the Act require an assessment of a proposal for a non-complying activity against the objectives and policies of the relevant planning documents. In this case both regional and district level provisions are contained in the Unitary Plan and are assessed below:

8.1 REGIONAL POLICY STATEMENT (RPS)

Chapter B2 of the AUP (OP) RPS provides the overarching policy framework to guide urban future growth and form in the region. This chapter recognises that growth needs to be provided for in a way that supports integrated planning of land use, infrastructure and development as well as optimising the efficient use of the existing urban area.

In general terms, the urban growth and form adopts a quality compact urban form around a hierarchy of centres. At a regional level the town centres function as the commercial, cultural and social focal points for the region and at a local level the local and neighbourhood centres provide for a range of activities and serve as focal points for the local communities.

Residential growth objectives and policies (B2.4) are focused on residential opportunities meeting current and future demands. Policy B2.4.2(2) enables

residential intensification on land within and adjacent to centres and corridors or in close proximity to public transport and social facilities (including open space) or employment opportunities. Policy B2.4.2(7) ensures that higher residential intensities can be adequately serviced by existing or planned infrastructure.

Commercial and industrial growth objectives and policies (B2.5) are focused on employment, commercial and industrial opportunities meeting current and future demands. B2.5.2(6) states that commercial activities are enabled, where appropriate, in business zones in locations other than town centres and identified growth corridors.

The Auckland Plan sets out Council's strategic direction on how growth will be accommodated within the next 30 years. According to the Auckland Plan 2050, around 62% of development is anticipated within the existing urban area and the remaining development is anticipated to occur in future urban areas (32%) and in rural areas (6%)⁵. There is an emphasis on the need to provide for integrated land use, development and the provision of infrastructure. The RPS emphasises the need to enable urban growth and intensification to achieve a 'quality compact' urban form that makes efficient use of land and existing infrastructure while responding to local character and sense of place.

The Council's Future Urban Land Supply Strategy, refreshed in July 2017, implements the Auckland Plan and gives effect to the NPS on Urban Development Capacity by identifying a programme to sequence future urban land over 30 years. The future urban zoning within the northern area include Warkworth, Wainui, Silverdale-Dairy Flat and around the existing urban areas of Hibiscus Coast⁶. Of relevance to the subject site, Silverdale West/Dairy Flat is sequenced to be *Development Ready* in the first half of the period 2018-2022 which could deliver about 9,000 jobs. The surrounding Wainui East and remainder of the Silverdale/Dairy Flat area is identified to be *Development Ready* in the second half of the period 2033-2037 with a total capacity for about 32,900 dwellings.

It is considered that the proposal is consistent with, and not contrary to the RPS and will support the dwelling growth anticipated in the Silverdale, Dairy Flat and Wainui area for the following reasons:

- A high quality connected urban environment is proposed that seeks to incorporate the natural stream environment. The development will feature a site layout that has been well considered with buildings that are visually interesting. A comprehensive landscape scheme has also been developed which compliments the high-quality built form.
- The proposal represents a new development which will cater for the anticipated urban growth in the area and will provide employment

⁵ Auckland Plan 2050, June 2018, page 217

⁶ Auckland Future Urban Land Supply Strategy, July 2017, page 26

opportunities through the supporting staff required for the operation of the aged care units and the maintenance of the communal grounds.

- For the considerations set out in the Economic Impact Assessment prepared by Urban Economics (**Appendix 10**), the proposal will address the shortage of retirement villages within the primary and secondary catchments of the site and will not undermine the growth of commercial activities as there is still 62,300m² business land available to potentially accommodate commercial activity in the future.
- For the reasons set out in the Traffic Impact Assessment prepared by TEAM (**Appendix 9**), the proposal will not undermine the safe and efficient operation of the transport network.
- The proposal can be adequately serviced in terms of stormwater, wastewater, water supply and other utilities.

8.2 OBJECTIVES AND POLICIES OF THE AUCKLAND UNITARY PLAN (OP)

8.2.1 Stormwater Discharge and Diversion and Stormwater Management Area – Flow 1

The objectives and policies for Stormwater Discharge are contained in sections E8.2 and E8.3, and aim to avoid as far as practicable, minimise or mitigate, the adverse effects of stormwater runoff on freshwater systems, freshwater and coastal water, and minimise the discharge and generation of contaminants and new adverse effects of stormwater runoff.

The site is within Stormwater Management Area – Flow 1. These rules seek to protect and enhance Auckland’s rivers, streams and aquatic biodiversity in urban areas. The objectives and policies of the SMAF areas seek to ensure that rivers, streams and aquatic biodiversity in urban areas in identified areas are protected from further adverse effects of stormwater runoff associated with development. Policies seek to manage stormwater runoff from impervious areas and requires stormwater mitigation where necessary.

Retention and detention are provided for within the existing stormwater pond and together with the treatment provided for within the pond, this will mitigate the effects of the increase in impermeable surface area to facilitate the proposed development. It is also considered that there is adequate capacity within the existing stormwater system to accommodate this increase in impermeable surface area as described in the Engineering Report attached as **Appendix 6**.

8.2.2 Land Disturbance – Regional and District

The objectives and policies for Land Disturbance – Regional are contained in – Chapter E11. The relevant objectives and policies are set out at E11.2 and E11.3. The District land disturbance provisions are contained in chapter E12 - E12.2 and E12.3 of the AUP (OP). The objectives seek to ensure that land disturbance is undertaken

in a manner that protects the safety of people and avoids, remedies and mitigates adverse effects on the environment; specifically, that sediment generation is minimised and land disturbance is controlled to achieve soil conservation. The policies support the objectives while also enabling land disturbance necessary for a range of activities undertaken to provide for people and communities social, economic and cultural well-being, and their health and safety. Further, the policies also seek to require that the earthworks are designed and undertaken in a manner that ensures the stability and safety of surrounding land buildings and structures. Of relevance to this proposal, avoid earthworks where practicable or mitigate where appropriate adverse effects of land disturbance on riparian yards and coastal protection yards.

It is considered that the proposal accords with the objectives and policies for land disturbance. In particular, the earthworks are required to enable urban development of the land and provide suitable roads, access and building platforms. The implementation of an erosion and sediment control plan designed and maintained in accordance with Auckland Council GD05 - Guidance for Erosion and Sediment Control will ensure that adverse sediment effects are appropriately managed and are not more than minor.

8.2.3 Vegetation Management and Biodiversity

The objectives and policies for Vegetation Management and Biodiversity are contained in chapter E15.2 and E15.3. These seek to ensure that ecosystem services and indigenous biological diversity values are maintained and enhanced while providing for appropriate subdivision, use and development. The objectives and policies also seek to ensure that indigenous biodiversity is restored and enhanced in areas where ecological values are degraded, or where development is occurring.

Whilst the proposal requires vegetation alteration and/or removal within the 10m riparian yard, this will be replaced with native species to ensure that the buffer and filtration services are maintained. A weed control programme will also be implemented to improve the ecological value of the site which is currently neglected and covered in weeds.

8.2.4 Streams

Chapter E3 of the Unitary Plan addresses Lakes, rivers, streams and wetlands. The introduction to this Chapter states the following:

E3.1. Introduction

The management of the beds of lakes, rivers, streams and wetlands is important for the protection of natural ecological and biodiversity values, for the efficient passage of flood flows, and the retention of high water quality. Retaining the natural profile and course of a river or stream, keeping riparian vegetation and fish passage and avoiding sediment generation from bed disturbance supports the retention of freshwater ecosystems.

While the Resource Management Act 1991 defines the term 'river' as including streams, the Plan refers to both 'rivers and streams'. Stream is a more common way of describing the smaller watercourses that are characteristic of Auckland.

In urban Auckland lakes, rivers, streams and wetlands provide an important component for the assimilation and conveyance of stormwater and form part of the overall stormwater network. Streams have also been piped and filled over to reclaim land for urban land development and have been modified to accommodate infrastructure such as roads, stormwater and wastewater networks and other utility services. Urban streams nevertheless continue to provide important ecosystem services and can provide meaningful ecological and biodiversity values.

There is a balance to be struck between the need to provide for the ongoing growth of urban Auckland, including the requirements of infrastructure, and the protection, maintenance and enhancement of lakes, rivers, streams and wetlands. It is important that development occurs in a sustainable manner which should involve, where practicable, the retention and enhancement of lakes, rivers, streams and wetlands.

The Plan identifies a number of areas where the natural values of lakes, rivers, streams and wetlands are higher than elsewhere. These areas are especially vulnerable to the adverse effects of inappropriate subdivision, use and development and require a greater level of protection. These areas are identified in the following overlays:

- [D4 Natural Stream Management Areas Overlay](#);
- [D5 Natural Lake Management Areas Overlay](#);
- [D6 Urban Lake Management Areas Overlay](#);
- [D7 Water Supply Management Areas Overlay](#)
- [D8 Wetland Management Areas Overlay](#); and
- [D9 Significant Ecological Areas Overlay](#).

This Plan requires that permanent loss is minimised and significant modification or diversion of lakes, rivers, streams and wetlands are avoided. Where adverse effects cannot be avoided, remedied or mitigated, it may be appropriate that the residual adverse effects be offset by providing environmental benefits either onsite or offsite. In

The portion of Channel 4 proposed to be diverted has been assessed to be negligible ecological value. As described in the ecological assessment the area is highly impacted and modified due to ponding and a blocked pipe. Rather than reclaim this portion of stream; which as noted in the Introduction above is an activity to be minimised; it is proposed to divert the stream into the Mainstem Stream with no net loss. Given the existing values of the area to be diverted, and the positive effects anticipated to arise from the diversion, it is considered that overall the adverse effects arising from diversion of this portion of stream are less than minor and potentially positive.

8.2.5 Signage

The signage objectives and policies seek to ensure that billboards and comprehensive development signage contributes to the social and economic well-being of communities through identifying places providing information, and advertising goods and services. The AUP manages signage to ensure signs are managed to maintain traffic and pedestrian safety, historic heritage values and the visual amenity values of buildings and the surrounding environment. The corresponding policies require that signage does not significantly detract from the profile or appearance of the building or cover any significant architectural features of a building and avoid signage clutter.

It is considered that the proposed signage is consistent with the signage objectives

and policies. In particular, the new signage will identify and advertise the retirement village and commercial units on the site whilst maintaining visual amenity values and will not result in any traffic safety effects with regards to the local road network.

8.2.6 Transport

The transportation objectives and policies seek to ensure that land use and all modes of transport are integrated in a manner that enables the benefits of an integrated transport network to be realised and the adverse effects of traffic generation on the transport network to be managed.

The corresponding policies seek to manage the number, location and type of parking and loading spaces to support the safe, efficient and effective operation of the transport network, the use of more sustainable transport options including public transport, walking and cycling, the efficient use of land and the functional and operational requirements of activities. Loading is required to support activities and minimise disruption on the adjacent transport network.

The traffic assessment prepared by TEAM concludes that any additional traffic generated by the proposal can be sufficiently accommodated without compromising the safe and efficient operation of the adjoining road network.

8.2.7 Natural Hazards

The objectives and policies for natural hazards are contained in sections E36.2 and E36.3 of the AUP (OP), and seek to ensure that natural hazard risks are identified and avoided where risk is significant, or remedied and mitigated where the risk is less significant or unable to be avoided.

The proposal is consistent with these objectives and policies as the buildings and retaining walls have been designed to ensure that the proposal will not cause any diversion or reduction in the capacity of the overland flow path. Further, floor levels will be 500mm above the floodplain ensure that the risk of property damage are minimised. Overall, the proposal will not exacerbate or create any new flooding hazards.

8.2.8 Business – General Business Zone

The objectives and policies of the General Business zone are contained in H14.2 and H14.3 of the AUP (OP). The Business zone objectives aim to provide a strong network of centres that promote commercial activity, with development that contributes towards planned future form and quality to create a sense of place. The objectives of the General Business zone seek to provide for a range of business activities outside centres, while ensuring function, role and amenity of centres is not compromised.

The Business zone policies reinforce its objectives and aim to increase the density, diversity and quality of the housing and encourage buildings at the ground floor level

to be adaptable to a range of uses to allow activities to change over time. Further the policies aim to recognise the functional and operational requirements of activities and development. The General Business zone policies aim to enable a range of business activities, including large format retail, trade suppliers, light industry and small service activities.

The objectives and policies that are relevant to this application are considered in detail in **Appendix 14**. Overall, it is considered that the proposed development is consistent with these objectives and policies as while the proposed retirement village is not envisaged within this zone, the retirement village runs as a commercial entity and provides for employment opportunities and support the function of the nearby Silverdale Town Centre. Further, the proposed development will not undermine commercial growth in the area as there is still 62,300m² available business land for future development within the catchment.

8.2.9 Silverdale 3 Precinct

The Silverdale sub-precinct has been applied to approximately 41 ha of land between East Coast Road and State Highway 1, known as the Hibiscus Coast Gateway. The objectives and policies are contained in I537.2 and I537.3 in the AUP(OP). The precinct aims to have a high-quality urban environment which contributes to a strong sense of arrival. A range of activities are enabled in this locality which can support the Hibiscus Coast Bus Station. There is an emphasis on the underlying landforms and planted interfaces to ensure that high quality visual landscape outcomes are met.

The objectives and policies that are relevant to this application are considered in detail in **Appendix 14**. Overall, the design of the buildings and landscaping will provide a high-quality urban environment and create the “gateway” experience sought by the Silverdale 3 precinct rules. This is discussed in detail in the Urban Design Assessment in **Appendix 4**. Notwithstanding that the retirement village is not anticipated within the precinct, the activities proposed will support the Hibiscus Coast Bus Station and will not generate adverse effects on the road network.

8.2.10 Summary

It is considered that the proposed development is generally in accordance with the objectives and policies of the Unitary Plan particularly when read as a whole.

8.3 RELEVANT RULES AND ASSESSMENT CRITERIA

The Unitary Plan specifies the relevant assessment criteria to be considered in assessing this application for each of the consent matters in the following sections:

- E7.8.1(6)(a)-(f) – diversion of groundwater;
- E8.7.2 Diversion and discharge of stormwater runoff from impervious areas greater than 1000m² and up to 5000m² within an urban area;

- E10.8.1 for development of more than 50m² of impervious surface area within the SMAF1;
- E23.8.2 for comprehensive development signage;
- E27.8.1(4) – trip generation;
- E27.8.1(8) any development providing fewer than the required minimum number of loading spaces;
- E27.8.1(9) development infringing the standards for design or parking and loading areas or access;
- E38.12.2(6) for subdivision in accordance with an approved land use resource consent;
- E27.8.2(7) for any activity or development which provides fewer than the minimum number of loading spaces required;
- E27.8.2(8) any activity or development which infringes the standards for access;
- H14.8.2(4) new buildings in the General Business zone;
- H14.8.2(7) for buildings that do not comply with the standards;
- I537.8.2(1) for development generally in accordance with I537.10.1 Silverdale 3: Precinct plan 1;
- I537.8.2(3) for new buildings in the Silverdale 3 Precinct; and
- I537.8.2(5) for development that does not comply with landscaping and frontage controls.

These criteria largely cover the same matters that have been discussed and assessed in the above report, pertaining to environmental effects and the objectives and policies of the Plan.

In particular, the proposal is generally compliant with the AUP (OP) standards for the General Business zone and Silverdale 3 Precinct, and where new buildings are being constructed, the purposes of these standards are met through the proposed landscaping and high-quality design.

Overall, it is considered that the proposal meets the assessment criteria of the AUP (OP) for the reasons described in sections 5, 6, 8, and 9 above.

9.0 SECTION 104 CONCLUSION

Section 104D of the Act states that consent cannot be granted to a consent for a non-complying activity unless it is either not contrary to the relevant objectives and policies of the Act; or if the adverse effects on the environment are not greater than minor. If an application is assessed as passing this “*Gateway Test*” then it can be considered for approval (or not) under the provisions of s104 of the Act.

From the above assessment it can be concluded that the application meets both Gateway Tests of s104D of the Act. Furthermore, the proposal is generally in keeping with, and is not contrary to the relevant objectives and policies and adverse are not greater than minor. There are also positive effects that may be considered in respect to s104(1) of the Act. Consequently it is determined that consent can be granted subject to conditions, including those proposed, which are relevant in respect of s104(1)(ab) of the Act.

10.0 PART 2 MATTERS

Section 5 of Part 2 identifies the purpose of the RMA as being the sustainable management of natural and physical resources. This means managing the use, development and protection of natural and physical resources in a way that enables people and communities to provide for their social, cultural and economic well-being and health and safety while sustaining those resources for future generations, protecting the life supporting capacity of ecosystems, and avoiding, remedying or mitigating adverse effects on the environment.

Section 6 of the Act sets out a number of matters of national importance including (but not limited to) the protection of outstanding natural features and landscapes and historic heritage from inappropriate subdivision, use and development. There are no relevant s6 matters requiring assessment.

Section 7 identifies a number of “other matters” to be given particular regard by Council and includes (but is not limited to) Kaitiakitanga, the efficient use of natural and physical resources, the maintenance and enhancement of amenity values, and maintenance and enhancement of the quality of the environment. The proposed development represents an efficient use of the land resource in a manner that contributes positively to the quality of the environment through building form and character, landscaping and building materials. Amenity values will be enhanced by these features as well as enhanced pedestrian connectivity.

Section 8 requires Council to take into account the principles of the Treaty of Waitangi.

Overall, as the effects of the proposal are considered to be less than minor, and the proposal accords with the relevant AUP (OP) objectives, policies, and assessment criteria, it is considered that the proposal will not offend against the general resource management principles set out in Part 2 of the Act.

11.0 OTHER MATTERS (SECTION 104(1)(c))

No other matters are considered relevant to the application, other than the proposed resource consents for activities in this area that have been addressed in the Background section.

12.0 CONCLUSION

The proposal involves the construction of 28 buildings for the purposes of establishing a retirement village as described in the application; earthworks and a subdivision of subdivision of Sec 216 Psh of Okura SO 18072 to create one additional lot.

Based on the above report it is considered that:

- Public notification is required as required by the Unitary Plan. An assessment of actual and potential effects has been undertaken and it is considered that effects associated with construction, built character and amenity, office activities, signage, traffic, access, and parking, ecology, stormwater and infrastructure and servicing are minor. There are also positive effects including providing an enhanced streetscape, employment opportunities and services to the local community.
- Limited notification is not required as no persons at adjacent properties are considered to be adversely affected by the proposal when taking into account the architectural design, separation distances, and that the buildings are within the planned built environment provided for under the AUP (OP);
- The proposal accords with the relevant AUP (OP) objectives and policies;
- The proposal meets the relevant AUP (OP) assessment criteria; and
- The proposal is consistent with Part 2 of the Act.

It is therefore concluded that the proposal satisfies all matters the consent authority is required to assess, and that it can be granted on a non-notified basis.

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