

Heritage Evaluation

The Bridgens and Company Shoe Factory (former)

326 New North Road, Eden Terrace



Front elevation of the former factory building as seen from the northern footpath on New North Road

1.0 Purpose

The purpose of this document is to consider the place located at 326 New North Road, Epsom against the criteria for evaluation of historic heritage in the Unitary Plan Operative in Part.

The document has been prepared by Blair Hastings, Heritage Consultant on the specific instructions of the Heritage Unit, Auckland Council. It is solely for the use of Auckland Council for the purpose it is intended in accordance with the agreed scope of work.

2.0 Identification

Site address	326 New North Road, Eden Terrace
Legal description and Certificate of Title identifier	Lot 1 DP 205780 1,116m ² CT-134A/536
NZTM grid reference	1756113, 5918228 – to the centre of the site.
Ownership	WL Property Investments Limited
District/regional plan & zoning	Operative Auckland City - Isthmus Section Business Activity - Mixed Use
Unitary Plan Operative in Part zoning	Business - Mixed Use Zone
Existing scheduled item(s)	N/a
Additional overlays and controls	Unitary Plan Operative in Part: Built Environment: Identified Growth Corridor Overlay, Controls: Height Variation Control - Newton, 21m and Macroinvertebrate Community Index [rcp/dp] - Urban
Heritage New Zealand listing	N/a
Pre-1900 site (HPA Section 2a(i) and 2b)	No. The place is not recorded as an archaeological site and has not been assessed to determine if it has archaeological values, however it has some potential as a site of human activity before 1900 resulting from former rural activity in the vicinity.
Designations (UP):	Designations: Designations - ID 1609, Road Widening, Designations, Auckland Transport
CHI reference/s	N/a
NZAA site record number/s	N/a

3.0 Constraints

This evaluation is based on the information available at the time of assessment.

Site visits were made 26 October 2016 and 9 January 2017. The building was inspected from the footpath and from the carpark adjacent at 328 New North Road. The café was entered on the first visit. Both this space and a corporate foyer (visible from the café) were deemed a) 'private' and b) too small (in terms of square meterage compared to the whole) to be considered eligible for assessment. We note further that information to hand would suggest (and support our limited visual evidence) that the interior of the building has been very heavily modified in recent times. The physical evaluation is therefore limited to the exterior envelope only.

The evaluation does not include a structural assessment or a condition report of the building; and any comments on the structural integrity or the condition of the building are based on the visual inspection only.

The evaluation does not include an assessment of any archaeological values of the place and nor does it include an evaluation of the importance of the place to mana whenua.

4.0 Historical summary

Appendix 1 sets out additional historical research undertaken for the site.

4.1 Bridgens Shoe Company

The origins of Bridgens and Company Limited go back to Edward Bridgens (1846-1922). Born in London, by 1871 he was working as a shoemaker.¹ With his wife and children, Bridgens arrived in Auckland in 1875.² He may have started out in the employ of bootmaker James J Austin,³ but in 1890 he struck out on his own, setting up first at the top of Wellington Street⁴ then Greys Avenue shortly after.⁵ By 1892 he had his West End Boot Palace on Karangahape Road.⁶ For a time, in 1894, he had a branch store on Queen Street as well.⁷ Both shops were also used to manufacture the shoes and boots he sold. Despite a fire in the Karangahape Road shop in 1910,⁸ and the deaths of Edward Bridgens in 1922⁹ and his wife Martha in 1938,¹⁰ the Karangahape Road shop is thought to have remained open into the 1940s.¹¹

Bridgens' first factory opened at 54-56 Prospect Terrace, Mt Eden, on land purchased by Edward's son, Frederick (Fred) Henry Bridgens (1872-1938), in 1900.¹² Edward Bridgens died in 1922, aged 75.

¹ 1871 England census, district 12a, via Ancestry.com

² Norman Daniels, *The Bridgens Family of Auckland*, 1995, p. 26

³ In 1890, Bridgens advertised, once he started his own business, that he was "late Austin's". *Auckland Star*, 13 March 1890, p. 1(2). Austin's partnership dissolved in 1889, which appears to have led to the end of his business, and Bridgens may simply have moved on (*NZ Herald*, 12 January 1889, p. 1(4))

⁴ *Auckland Star*, 13 March 1890, p. 1(2)

⁵ *Auckland Star*, 2 July 1890, p. 1(1)

⁶ *Auckland Star*, 19 December 1892, p. 1(4)

⁷ *Auckland Star*, 3 May 1894, p. 1(5)

⁸ *NZ Herald*, 27 September 1910, p. 5

⁹ Death notice, *NZ Herald*, 21 February 1922, p. 1

¹⁰ *NZ Herald*, 6 December 1938, p. 14

¹¹ *Wises Directory*, 1958, p. 121

¹² Daniels, p. 36

Around 1923,¹³ Fred went into partnership with his two eldest sons, Edward George and Frederick Albert Bridgens,¹⁴ as Bridgens and Co,¹⁵ after working in the boot-making trade on his own for some time. In order to purchase the first part of their eventual property at New North Road in October 1929, they formally incorporated Bridgens and Co the month before.¹⁶

The company advertised in July 1930 that 100,000 pairs of shoes were made in the new factory on New North Road per year.¹⁷ In the same year, they boasted that “*the largest and most complete welted plant [for men’s boots] that has ever entered New Zealand was installed in our modern factory.*”¹⁸ That figure rose to 250,000 in 1933,¹⁹ 300,000 by 1935,²⁰ and 500,000 by 1938.²¹ On 10 June 1931, the Governor-General Lord Bledisloe and Lady Bledisloe visited the factory.²² In July that year, Bridgens and Co offered the Salvation Army a gift, which was accepted, of 500 pairs of boots and shoes, to be distributed among the poor of Auckland.

On Fred’s death in 1938, an obituary written in the *Auckland Star*, claimed that he, and his sons Edward [George] and Frederick [Albert] had, through adopting progressive and innovative manufacturing procedures, succeeded in building one of the largest New Zealand suppliers of the retail trade. With a capital of £80,000, and employment at times of up to 300 operatives, the company took an active part in developing the market for locally made footwear.²³

In that same year, the company introduced children’s sandals to their existing range of lady’s fashionable shoes and men’s footwear.²⁴ The Prime Minister, Michael J Savage, visited what was then described as Auckland’s largest shoe factory, in March 1939.²⁵

During World War II, the company held contracts to supply the military with footwear, and is said to have received commending letters from military authorities.²⁶

Noted athletics coach, Arthur Leslie Lydiard, worked at the factory from 1934 until circa 1958,²⁷ and was a foreman there towards the end in the 1950s.²⁸

Edward George Bridgens died in 1944. He was heavily involved with the footwear industry throughout his life. He was a member of the Footwear Industrial Plan Committee, which acted in an advisory capacity to the Government in all matters affecting the trade in New Zealand. At one time, he

¹³ Earliest employment advertisement found, *NZ Herald*, 4 May 1923, p. 1(6)

¹⁴ Daniels, p. 37

¹⁵ *Auckland Star*, 28 October 1927, p. 1(6)

¹⁶ BADZ A13 5181 R9095972, Archives New Zealand

¹⁷ *NZ Herald*, 10 July 1930, p. 20(3)

¹⁸ *Auckland Star*, 7 May 1930, p. 42 (supp)

¹⁹ *NZ Herald*, 13 November 1933, p. 2 (supp)

²⁰ *NZ Herald*, 21 September 1935, p. 19

²¹ *NZ Herald*, 6 August 1938, p. 11 (supp)

²² *NZ Herald*, 11 June 1931, p. 17

²³ *Auckland Star*, 31 August 1938, Volume LXIX, Issue 205

²⁴ *NZ Herald*, 5 May 1938, p. 26 (1)

²⁵ Caption to illustration, *NZ Herald*, 23 March 1939, p. 8

²⁶ Daniels, p. 103

²⁷ David Green. 'Lydiard, Arthur Leslie - Lydiard and his system', from the Dictionary of New Zealand Biography. Te Ara - the Encyclopaedia of New Zealand, updated 30-Oct-2012

URL: <http://www.TeAra.govt.nz/en/biographies/6l3/lydiard-arthur-leslie>

²⁸ Daniels, p. 117

was president of the New Zealand Footwear Manufacturers' Federation. He was considered a 'tower of strength' to trotting, as a past president of the Auckland Trotting Club. Frederick Albert Bridgens died in 1973.

Changing economic circumstances, accompanied by a relaxation on the control of imported goods in the late 1980s, allowed cheaper footwear imports on to the New Zealand market. This made it difficult for local industry to compete and in 1997 the Bridgens and Company factory closed down.

4.2 The Site and Building

The building is situated on part of what were a number of sections between New North Road and Dominion Road, spanning the railway line. In 1882, LINZ records indicate the land was owned by William and Eliza Baker, with the title in Eliza's name. The Bakers subdivided their land at this time, selling the individual lots. In 1894, George Herbert Baker obtained title just over quarter of an acre in to Lots 90 and 91, fronting onto New North Road and backing onto the railway line. When a later owner, Frank Edwin Holder, died in 1911, the property came under the administration of the Public Trustee. In 1929, Bridgens and Company purchased the property.²⁹

By late 1929, the construction of the new Bridgens factory was completed. Designed by architects Wade and Bartley, the factory was built by Fletcher Construction³⁰ at a cost of £9,000.³¹ Initially only the ground floor was used for production. Later the upper floor was used for manufacturing, and had a raised office from where the floor could be supervised.³²

In February 1934³³, a fire caused considerable damage to an old house used as a storeroom, at the rear of the factory (now 326B and 326C New North Road). Soon after Bridgens and Company commissioned T E Norman Wade³⁴ (formerly of Wade and Bartley) to prepare plans to replace the house with a rear addition to the factory building. This was completed by September of 1935.³⁵ The press room was relocated to the new section, a new staff entrance made at the rear of the building and a heavy capacity lift installed.³⁶

The rear portion of the adjoining lots 92 and 93, to the west, was formally acquired in 1941³⁷ (now 328 New North Road) and sealed as a company carpark soon afterward.³⁸ The remainder (the front of lots 92 and 93) was purchased in December 1945 (now 330 New North Road),³⁹ and the old Brodie house (on that site), was used by the company for storage. In 1974, part of the house was converted into a factory shop.⁴⁰

²⁹ NA 69/214, LINZ records

³⁰ *Auckland Star*, 3 December 1929, p. 24(2)

³¹ Daniels, p. 94

³² Daniels, p. 95

³³ *NZ Herald*, 27 February 1934, p. 8

³⁴ *NZ Herald*, 21 March 1935, p.18(4)

³⁵ Illustration showing the rear addition, advertisement, *NZ Herald*, 21 September 1935, p. 19

³⁶ Daniels, p. 99

³⁷ NA 173/238, LINZ records

³⁸ Daniels, p. 115

³⁹ NA 843/74, LINZ records

⁴⁰ Daniels p. 121

The company made extensions and additions to the factory in 1947, totalling £500 – again, designed by Norman Wade. The two-storey addition included a new inwards goods platform on New North Road. There were some internal alterations to the offices in the same year, and a storage platform was erected in 1950. Alterations were made to the factory worth \$6,250 in 1975, and a transformer station was built on the western side of the building in 1984.

Bridgens subdivided the property in 1998, and Parker Brothers purchased the factory building at auction in the same year. Unfortunately, a fire broke out in the warehouse and destroyed a third of the building, creating \$500,000 in damage, and resulting in complete replacement of the original flooring. In 2000, Parker Brothers subdivided the site (then being Lot 4, DP 191962 – originally lots 90 and 91) into Lots 1, 2 and 3, DP 205780. Lot 1, the current subject site, has undergone a number of changes in ownership since then.

Considerable internal renovations [within the subject site] have taken place since 2000, to enable the adaptive reuse of the building as offices and a café.

4.3 Wade and Bartley – Architect

The first part of the Bridgens factory (now the subject site, following subdivisions in 1998 and 2000) was designed by the prominent Auckland architectural firm of Wade and Bartley⁴¹ (Thomas Edward Norman Wade and Alva Martin Bartley).

Norman Wade received his training from his father's office and was for some years in partnership with his elder brother, Harry Wade, also a fellow of the institute.

Alva Bartley was the son of Edward Bartley, a prominent Auckland architect. He trained in his father's office until he enlisted for military service. After the war he remained in London to study at the Architectural Association before returning to New Zealand. He also became an Associate of the Royal Institute of British Architects.⁴²

Wade and Bartley practiced together from 1919 to circa 1935. Their major works include the Public Library and Borough Council offices, Dargaville (1923), the Commercial Hotel (now De Bretts, 1927), A and G Price Ltd, Quay Street (1927), Pascoes Jewellers, Karangahape Road, the former Auckland Electric Power Board building (1930) and the 1YA Studio building (now partly occupied by the Gus Fisher Gallery, 1934).⁴³

Norman Wade continued to work with Bridgens and Company in his own professional capacity as an architect after Wade and Bartley ceased to operate as a partnership.

⁴¹ Permit No. 8651, Auckland Council property file

⁴² <http://www.heritage.org.nz/the-list/details/0660>

⁴³ File W121, Shepherd Collection, School of Architecture Library, University of Auckland

5.0 Physical description

5.1 The Site



Figure 1: The site - in detail – outlined in blue. Auckland Council GIS – 2010/11 Aerial Photography – scale shown bottom left hand corner.

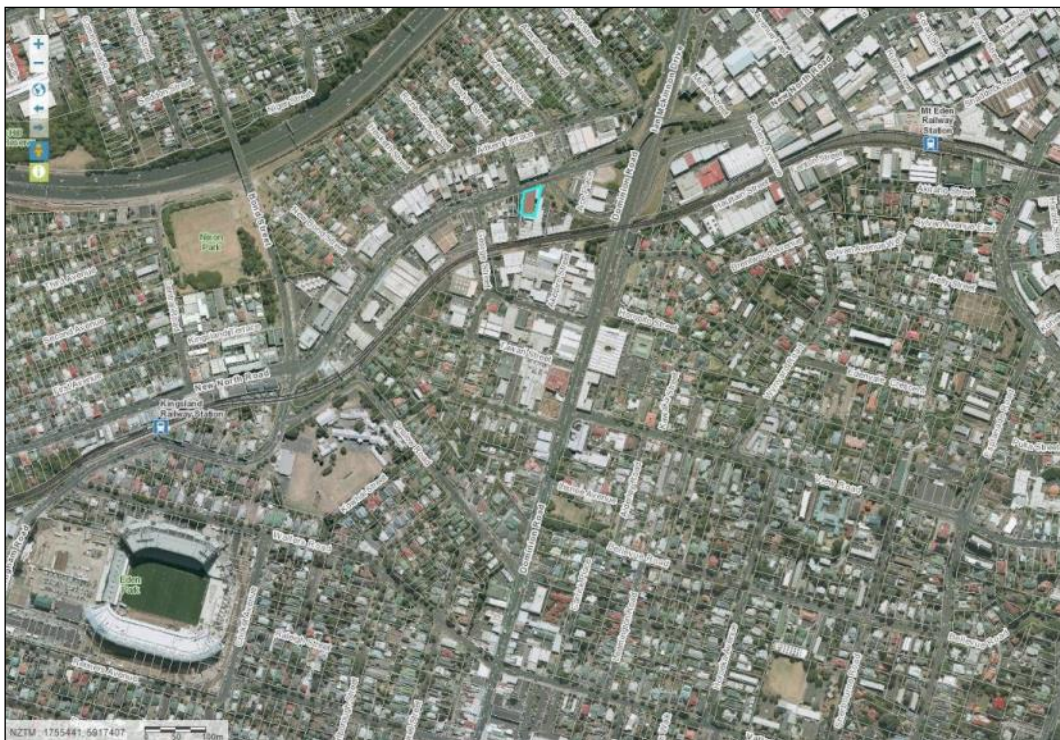


Figure 2: The site – in context - outlined in blue. Eden Park shown bottom left and the North-Western motorway (SH16) top left. Auckland Council GIS – 2010/11 Aerial Photography – scale shown bottom left hand corner.

The building at 326 New North Road occupies almost the entire 1,116m² trapezoidal shaped site, legally described as Lot 1 DP 205780. There is a courtyard in the southeast corner of the site, being in effect the leftover space between the original factory building (to the west), the 1946 extension to the street elevation of the building (to the north) and the building at 322 New North Road, built fully to its western boundary.

The site slopes gently downwards from the northeast corner of the building in both southerly and westerly directions. The building is built to the edge of the street or northern boundary and almost to the western boundary, shared with number 328 New North Road. The building is joined to a separately titled modern building to the rear – split between 326A and 326B New North Road. This building to the rear shares a similar footprint to the early extension to the original Bridgens Factory building, built in 1935. The 1935 extension was damaged in a fire in 1998. It was subsequently redeveloped and subdivided into these two titles, now independent of 326 New North Road.

There are a small number of carparks marked out to the immediate west of the building, south of the transformer station – that cross the boundary of the site. An arrangement may be in place whereby these parks are available for use of the occupants of 326 New North Road and likewise, parking may be available in the area to the rear of the villa to the west – at 328 New North Road. In 1941, Bridgens purchased these adjacent lots, partly for parking, and presumably for any other expansion of the business, selling them to Parker Brothers Limited in the late 1990s.

Delivery access was available to the east of the original building via a loading dock/bay. This access was in effect formalised in 1947 when the two-storeyed extension was built in this location, to provide covered access and further warehousing and storage facilities.

The principal pedestrian or visitor access to the site would have been through the main double doors on New North Road. Although the original 1929 plans do not clearly show other formal entrances, it is assumed staff would have entered through the rear of the building. It is known that a purpose built staff entrance was included within the 1935 extension, south of the main building.

It is unclear if the main front door now opens. If it does, it is assumed it is no longer commonly used. A new 'main' entrance has been developed in a rough opening of one of the original window bays, closest to the New North Road street boundary on the western façade .

5.2 The Building

On 5 October 1929 Norman Wade and A. M. Bartley placed a tender notification in the *New Zealand Herald*, calling for contractors, to build a two-storeyed factory in reinforced concrete, New North Road, for Messrs. Bridgens and Co. Ltd⁴⁴. In early December of the same year, Fletcher Construction Co. Ltd, having been awarded the main contract, tendered for painting

⁴⁴ *NZ Herald*, 5 October 1929 p. 5

contractors⁴⁵. In late January of 1930 'Bridgens' New North Road, were advertising for machinists for the boot trade⁴⁶. The new factory was finished and operating.

The original factory building measured approximately 39m along the west elevation, 18m across the rear and 44m along the east elevation⁴⁷. Including the two toilet blocks along the south elevation, the gross floor area (the GFA) of the building was approximately 800m². The irregularly shaped 'front of house' area, on New North Road, was presumably used as office space. To the south, the seven 4.3m deep x 18m wide rectangular bays form the 'back of house', or operations zone⁴⁸.

In 1947, Norman Wade designed an extension to the original building to facilitate a covered delivery bay, extra storage, warehousing and operations space. The extension measures approximately 8.5m across the New North Road street boundary. It returns approximately 18m along the east façade of the original building, to add approximately 125m² to the GFA of the overall factory building.

In isolation, the 1929 building is symmetrical with a relatively simple and flat gable-end. The parapet across the façade returns along both the west and east elevations. The parapets flank a single ridged roof (running north-south) which has lost its original cladding and distinctive ventilators. It now appears to be clad with an asphaltic coating. The membrane sits on timber roof trusses although it is unclear to what degree they (or any of the other internal decorative and structural elements) have survived the passage of time, and in particular the 1998 fire.

The remainder of the structural components comprises reinforced concrete walls, [decorative] cast iron posts (along the centre line of each of the seven 'back of house' structural bays), a fixed mid-floor spanning both storeys of the factory, a cross-braced timber framed first floor and a standard timber framed floor at ground level.

From a decorative and stylistic perspective, 'both' of the main street façades (if we are to include the 1947 extension in assessing the building's street elevation 'as one') exhibit restrained classical geometries, with 'stripped-back' but robust detailing. The style might otherwise be called Stripped Classicism, primarily a 20th-century classicist architectural style stripped of most or all ornamentation. The style embraces a 'simplified but recognisable' classicism in its overall massing and scale while eliminating traditional decorative detailing. The orders of architecture are only hinted at or are indirectly implicated in the form and structure.⁴⁹

It's not surprising that this typology was chosen for this 'new' factory. While the design was shy to embrace contemporary architectural thinking, it represented, in the timelessness and longevity of the aesthetic, the strength, solidity and

⁴⁵ *Auckland Star*, 3 December 1929 p. 24

⁴⁶ *Auckland Star*, 18 December 1929 p. 1

⁴⁷ Measurements taken from 2008 architectural drawings – Fig, 101, appendix 7

⁴⁸ Measurements taken from 2008 architectural drawings – Fig, 101, appendix 7

⁴⁹ https://en.wikipedia.org/wiki/Stripped_Classicism

long-term viability Bridgens and Company would hope to portray for years to come.

No surprise either that the original façade subtly dominates the entire New North Road composition. Its symmetry is a key feature with six double height window bays, four of which are flanked by five square fluted Ionic columns with voluted capitals, that provide the principal aesthetic character. The collective bays are delineated from the rest of the façade by a rendered geometric trim, which runs the perimeter of the building elevation. Painted in a contrasting colour, the trim highlights the transition between the original building and the later addition. The gold painted rosettes at each end of the triangular parapet originally flanked the company lettering. Now only evident in absentia, the former company lettering is now just a mismatched patch render making good the signs fixing holes. The steel windows are a striking feature of the original façade, providing both considerable amenity lighting and, through the size of the panes, human scale to both the user and viewer or visitor. The main door is interesting in that it shuns the symmetry otherwise constant across the façade design. Its location, within one of an even number of bays, meant that positioning it symmetrically would be impossible – other than through replicating a similar [perhaps decorative] recess about the apex of the building.

While the 1947 extension further upsets the symmetry of the original façade, it incorporates even more restraint in both its geometric patterning and detail. For example, the render has been ‘cut’ into horizontal banding, rather than the ashlar styled stone coursing of the earlier adjacent façade. Its aesthetic is, as a consequence, subservient to the earlier elevation. If, however, it recedes then from the main aesthetic, it also complements the original form by utilising a stepped parapet (to both the main and eastern façades), similar but smaller steel windows and painted architectural plasterwork to match the earlier façade.

The east elevation of the extension appears to maintain the original steel windows, although, there does appear to be one more bay in existence than was shown on the 1947 plans. Although very little of the ‘rear’ east façade of the 1929 building is visible⁵⁰, a photograph from 1963 (refer fig. 61, appendix 5) shows that all of the original windows had already been replaced in steel, but with larger and fewer panes of glass. It is reasonable to assume this was done contemporaneous with the change on the west façade, which we know (referring to figs. 56 (1957) and 59 (1963), appendix 5) was done sometime between 1957 and 1963.

Both the west and east façades are very plain, decorated with only an unrelenting solid plaster to relieve the structural concrete substrate. Sub-floor ventilation grilles offer the only other compositional relief. Even the sills were excluded from the return windows in the 1947 addition – either as a further nod to Stripped Classicism, or, just as likely, as a means by which to economise on the overall spend.

The rear toilet blocks have been modified on a number of occasions. These are situated on either side of the central rear face of the building. However, the block on the west façade is relatively intact, including original(?) windows and

⁵⁰ In effect the elevation that forms the west edge of the courtyard behind the 1947 extension.

plumbing; however, on the west façade at least, the face of the block, which includes the windows and some early or original plumbing, is relatively intact. The appearance of the façade, however, is highly compromised by the addition of another floor above the original roofline, strata titled to 326A New North Road.

5.3 Condition

The building is in generally good aesthetic condition. Some of the decorative render across the main façade has been chipped and/or otherwise damaged, but prima-facie, this represents superficial or visual issues only.

Slightly more problematic, may be the limited cracking evident in the original street façade, especially at the junction between the 1929 and 1935 buildings. However, again, this could most likely be attended to by implementing a standard repairs and maintenance programme. It is possible that in undertaking the extensive internal refit in the late 2000s, the developer attended to any decorative failings in any or all of the façades – as, to reiterate the commentary above, the overall cosmetic condition of the building is considered to be good.

5.4 Modifications

In 1935 Norman Wade designed a significant extension to the rear of the 1929 building, in order to facilitate growth and expansion. It was to increase the floor area by approximately 16,000 square feet, bringing the total area to about 40,000 square feet. The rear extension consisted of a two storeyed factory building with a saw-toothed roof. The cost of construction was approximately 9,000 pounds and made the factory one of the largest and most up-to-date in the Dominion. It included the press room, a new staff entrance and various spaces for lunch and other recreation purposes.

This 1935 extension was damaged in a fire in 1998. Subsequently redeveloped and subdivided into two lots now independent of 326 New North Road.

The 1947 extension to the main street façade has been outlined above. Built early in the building's life, 'in the style of', or 'in-keeping with' the architectural typology of the original factory building and designed by one of the two original architects, positions it as an integral element in the overall composition of the property. It is not a modification that negatively affects any of the heritage values associated with the place.

In 1975, various internal alterations were designed by Fletcher and Goad, in essence to recalibrate the office or front-of-house space. A new Auckland Electric Power Board transformer was built on site in 1984, attached to the northern end of the west elevation. It still exists today.

In 1997, Bridgens and Company Limited closed down. Subsequently they sold the property to Parker Brothers Limited. At or around this time (it is assumed) the three-dimensional lettering on the main façade, between the rosettes, above the double height window bays, was removed. There is remnant visual evidence of what may have been original fixing locations.

After a substantial fire in 1998, the redevelopment of the rear of the site (on or about on the footprint of the 1935 extension), and subdivision soon after, titles were split and the former Bridgens and Company Limited site was reduced to 1,116m², being the front lot subject to assessment in this evaluation.

The late 2000s saw further redevelopment as the interior of the original factory building (and of the 1947 extension) was refitted as office space. Assuming the approved and consented plans were built it is unlikely that much of the original interior survives today. Part of the works included establishing a new entranceway in the first or most northern window bay along the west façade of the original 1929 building. This involved the removal of the window. In 2015 plans were lodged with Auckland Council to convert much of the 1947 extension space into Greenleaf Organics Café. While again, the consented plans suggest further internal rearrangement took place, only minimal external changes were carried out.

There is signage on both the west and east parapet return ends of the street façade. Greenleaf Organics Café also has a three-dimensional sign face-fixed to the 1947 façade, to the right hand side of the original loading bay. While these signs do impact the visual presence of the building, the effect is considered both minor and 'reversible' so as to have little or no impact on the original fabric of the building.

5.5 Current Use

The site was transferred from Bridgens and Company Limited to Parker Brothers Limited in 1998. It was then transferred variously until it was purchased by WL Property Investments Limited in 2015, who remain the current owners at the time of writing. The building is variously tenanted.

5.6 Summary of Key Physical Features

Refer to appendix 6, figs. 67 - 87.

- Siting:
 - Trapezoidal shaped site – running north-south from New North Road;
 - Building built hard to north and south boundaries, virtually to the west boundary and to within 5m of the east boundary, creating a courtyard-type space adjacent to 322 New North Road;
 - No landscaping on site - although current aerial photography appears to show some vegetation within the courtyard space;
 - The site slopes gently downwards from both east to west (across the street façade) and north to south;
 - Three parking spaces are marked out adjacent to the west edge of the building, south of the transformer facility. They appear to bridge the site boundary.

- Building plan form:
 - Two trapezoidal shaped buildings (being the original 1929 factory and the 1947 extension) joined along the north or street boundary;
 - Original toilet block to the rear of (but built as part of) the 1929 building, now adjacent to (and below the second floor of) the redeveloped site at 326A and 326B New North Road;
 - The second floor of the rear of the building is strata titled to 326A New North Road;
 - According to the original plans:
 - West half of the 'Front of House' space (including the entrance door and internal corridor) given over to administrative operations;
 - East half of the 'Front of House' space given over to a delivery bay in the east elevation and general warehousing space;
 - Back of house 'factory' space divided into seven bays, the first or north most of which included vertical circulation;
 - Toilet blocks to the rear.

- Front elevation:
 - Two stylistically similar buildings (1929 and 1947), joined as one elevation;
 - Original 1929 building symmetrical about parapet gable end;
 - Cement rendered reinforced concrete;
 - Ashlar style stone coursing in the 1929 building; horizontally demarked coursing only on the 1947 extension;
 - Minimal classical detailing – including rosettes and ionic columns and capitals;
 - Six double height columns flank recessed window bays over two floors;
 - Decorative parapet across façade - returns to east and west elevations;
 - Large steel windows divided into multiple panes with opening sashes and concrete sills;
 - Internal brass window fittings may be early or original;
 - Entrance door in third bay from the west; asymmetrical about the original façade. Door surround and molded facing work original; door replaced or over-clad;

- High contrast paint scheme designed to highlight the order and geometry of the architectural composition of the façade;
 - 1947 extension building includes typologically similar but more restrained detailing; steel windows are smaller and without sills;
 - Parapet is stepped to its return along the east boundary;
 - Delivery door/opening in original location; café fit-out includes frameless glazing to this space; café signage on garage door and face-fixed above and to the right of the opening;
 - Rendered concrete skirting the only decorative element to run seamlessly across both façades – some evidence of cracking is apparent at the junction between the two buildings.
- Side elevation (west elevation – of 1929 building):
 - Cement rendered reinforced concrete;
 - Large steel windows divided into eight panes with opening sashes;
 - These replaced the original windows – installed sometime between 1957 and 1963 – the same is evident along the 1929 east elevation – through historic aerial photography;
 - Capped parapet walls;
 - Division of the elevation into seven structural bays beyond the north parapet return;
 - First window bay (from the north end) now converted at ground level into entranceway for current use as mixed office space;
 - Sub-floor ventilator grilles centred on each bay;
 - Signage high on façade return.
 - Side elevation (east elevation – of 1947 extension):
 - Cement rendered reinforced concrete;
 - Large steel windows divided into small panes with opening sashes and no sills;
 - Capped parapet walls;
 - Signage high on façade return.

- General:
 - Largely intact exterior;
 - Highly visible;
 - Opposite the former Kiwi Bacon Factory – with which the former shoe factory forms a gateway entrance to Kingsland, or in reverse a departure point to the city;
 - Very few recent external modifications.

6.0 Comparative analysis [other similar or related places]

The subject building is an example of factory architecture. It operated in this capacity for almost 70 years. A brief assessment of the early factory form, developed as 'mill buildings' to meet the needs of the industrial revolution, through to its early 20th century iteration, and the similarities between each and the Bridgens Shoe Factory, is considered a relevant arena for comparative analysis.

So too is a brief assessment of the building compared to others by aesthetic and not functional type. The Stripped Classicism employed by Wade and Bartley was less about the contemporary architectural thinking, developing in Germany, and more about representing, in terms of a familiar, robust and ordered aesthetic, the rooted ideals of an established Auckland company.

A brief comparison between the Bridgens Shoe Factory building and other footwear factories is a worthy area for further analysis, as too is a comparison between the subject building and other Wade and Bartley works.

6.1 Industrial Architecture – The Early Factory

The industrial revolution began in Britain. The first factories used water-power, so they were often called mills. Indeed the germ of the industrial aesthetic is seen in the simple mill buildings, dating from the late 18th century. As precursors of the modern factory, they were straightforward wooden or masonry buildings with repetitive forms and rhythmic openings, aimed both at encouraging an efficient work process and preventing fires.

The fear of fire was so prevalent, that insurance companies shaped much of the early architecture. They discouraged interior wall coverings as well as ornament on building exteriors. Instead they sought open, partition-free interiors to facilitate extinguishing fires, discouraged attics, encouraged large windows to facilitate fire suppression and recommended flat floor areas be separated from interior stairs. Factories were simple or plain in appearance because their utilitarian nature placed them low in the social and therefore aesthetic hierarchy.⁵¹

The Quarry Bank Mill built in 1784, in Styal, Cheshire is one of the early 'mill' factories. It is simple, robust, geometric and highly fenestrated. The Bridgens Factory includes many of these characteristics in its design, 145 years later. It was said of the Mill building that 'Its design was functional and unadorned, growing out of the pragmatism of the men who felt no need to make a bold architectural statement'.⁵²

While the aesthetic of the subject building designed by Wade and Bartley has considerably more architectural flair than the Quarry Bank Mill building, and at least some contemporary influence, it is clear that the comparisons between the building and the generic early factory are striking. In terms of plan layout, structural spans, fenestration and form, that Wade and Bartley's fundamental design thinking was in effect defined by tried and tested architectural prototyping.

⁵¹http://www.academia.edu/1977965/AESTHETICS_OF_INDUSTRIAL_ARCHITECTURE_IN_THE_CONTEXT_OF_IN_DUSTRIAL_BUILDINGS_CONVERSION

⁵² Calladine, Anthony; Fricker, Jean (1993). East Cheshire Textile Mills. London: Royal Commission on Historical Monuments of England. ISBN 1-873592-13-2 – p. 161



Figure 3: Quarry Bank Mill – 1784, Styal in Cheshire.⁵³ One of the early ‘mill’ factories – simple, robust, geometric and highly fenestrated – the Bridgens Factory includes many of these traits. ‘Its design was functional and unadorned, growing out of the pragmatism of the men who felt no need to make a bold architectural statement’.⁵⁴

6.2 Industrial Architecture – The Modern Factory

Around the turn of the 20th century new ways of producing energy enhanced the growth of both buildings and machinery, necessitating structures that could support more weight and span greater distances. Factories were expanding beyond the production of textiles, demanding more flexible and adaptable layouts. The combination of concrete and iron would pave the way for the larger, more efficient and safer modern factory.⁵⁵

Around this time also, as industrial needs grew in complexity and importance, schools of design and architectural theory emerged to respond to the challenges that this development posed. Around the early 1900s, the factory came to be seen as a building type deserving of architectural treatment in order to enhance the production of goods and dignify the workplace, as well as forge corporate identities.

Two influential industrial factories of note were designed by Peter Behrens (the AEG Turbine Factory) and Walter Gropius and Adolf Meyer (the Fagus Shoe Last Factory). Behrens’s AEG factory was regarded as a ‘temple to industrial power’, having a monumentality based upon neoclassical principles. For the Fagus factory, Gropius and Meyer eschewed this masking of structure, striving to clearly express its materials, an important hallmark of the aesthetic of Modern Movement.

⁵³ <http://www.performance-vision.com/england/07-quarrybankmill.htm>

⁵⁴ Calladine, Anthony; Fricker, Jean (1993). East Cheshire Textile Mills. London: Royal Commission on Historical Monuments of England. ISBN 1-873592-13-2 – p. 161

⁵⁵ http://www.academia.edu/1977965/AESTHETICS_OF_INDUSTRIAL_ARCHITECTURE_IN_THE_CONTEXT_OF_INDUSTRIAL_BUILDINGS_CONVERSION



Figure 4: AEG Turbine Factory (left) – Peter Behrens, 1909.⁵⁶ Figure 5: The Fagus Factory (right) – Walter Gropius and Adolf Meyer, 1911-1913.⁵⁷

Both of these revered examples of 20th century factory architecture have similarities with, or at least were perhaps influential on, the design of the Bridgens Shoe Factory in Eden Terrace. While Wade and Bartley utilised a considerable amount of timber in the building, (a consequence of the abundant availability of the material) they also used reinforced concrete in order to simplify construction, lighten the structural mass, and as a nod to both contemporary architectural and technological thinking. Window sizes in the Bridgens building better reflect the need to provide for the best possible internal occupant amenity rather than to principally provide for the fire suppression needs of the past. The façade aesthetic employed by Wade and Bartley however, notwithstanding Behrens' nod to classical principals, does not in any way reflect the 'modern' aesthetic.

6.3 Stripped Classicism

Stripped Classicism describes an architectural typology from which mouldings, ornament, and details have been elided, leaving visible only the structural and proportional systems.⁵⁸

While Wade and Bartley's building does include some recognisable classical detailing it is both scant in quantity and modest in form. If modernism was developing in Europe as a valid design style, the stripped classical form was still popular and commonly seen in New Zealand.

The Hewitt's building in Dunedin was extensively remodelled in 1929 by architect Leslie D. Coombs. The façade has strong similarities to the Bridgens Factory building. The large but shallow pilasters and capitals, an understated cornice, and mullioned steel-framed windows are all likewise, strong visual features of the latter. It could be said for both buildings that without the slender profiles of the joinery the character of the building would be very different, and the spare decoration of the masonry would not be as effective as it is.

⁵⁶ https://en.wikipedia.org/wiki/AEG_turbine_factory#/media/File:Berlin_AEG_Turbinenfabrik.jpg

⁵⁷ https://en.wikipedia.org/wiki/Fagus_Factory#/media/File:Fagus_Gropius_Hauptgebaeude_200705_wiki_front.jpg

⁵⁸ <http://www.encyclopedia.com/education/dictionaries-thesauruses-pictures-and-press-releases/stripped-classicism>



Figure 6: The Hewitt's Building, Dunedin - remodelled 1929 by architect Leslie D. Coombs.⁵⁹

The Barton's building, also in Dunedin, was built in 1928 and designed by architect William Henry Dunning. It exhibits the composition of a 'stripped classical' façade, with similar proportioning to the Bridgens building, albeit with a little less restraint in the decoration and detailing.



Figure 7: Barton's Building, Dunedin - 1928 by architect William Henry Dunning.⁶⁰

⁵⁹ <https://builtindunedin.com/tag/stripped-classical/>

⁶⁰ <https://builtindunedin.com/2013/08/14/bartons-buildings/>

While the Hewitt's, Barton's and Bridgens' buildings all share strong design similarities, it should be noted that the Bridgens building also exhibits a certain uniqueness in its compositional 'form', which appears to follow function. The gable-ended parapet façade seeks only to cover the ridged roof behind.

6.4 Footwear Factories

The Weinbrenner Shoe Factory in Wisconsin is a good comparative example of a contemporary shoe factory designed and built in a stripped classical architectural style. The large openings, the mullioned steel framed windows and the very ordered geometries, offer striking similarities with the Bridgens building – designed to provide both a contemporary or popular aesthetic, and light and spacious working conditions within.



Figures 8 and 9: Weinbrenner Shoe Factory (Top (8) and bottom (9)) – built 1935 - Marshfield, Wisconsin, USA.^{61, 62}

⁶¹ https://commons.wikimedia.org/wiki/File:Weinbrenner_Shoe_Factory.jpg

The Murray Shoe Company at 28 Crummer Road, Grey Lynn, Auckland, designed by Tole and Massey Architects, was built in the early 1930s. Original drawings suggest the exterior of the building has not changed much since it was first constructed, although the fluted columns evident in the plans have either been removed or were never built.

The similarities between this building and the factory in New North Road include the strong order geometries, the minimal decorative framing (albeit recessed in this case and not 'added' as adornments), and the large expansive steel framed windows.



Figure 10: The Murray Shoe Company at 28 Crummer Road, Grey Lynn, Auckland. Googlemaps, image capture October 2015.

6.5 Other Wade and Bartley works



Figure 11: Borough Council offices, Dargaville, 1923 – Wade and Bartley.⁶³

⁶² <http://www.businessinfocusmagazine.com/2015/06/a-solid-footing/>

⁶³ Photograph by Les Downey from the Walsh Memorial Library, MOTAT.
<http://www.nz museums.co.nz/account/3031/object/475544>

The Borough Council building in Dargaville was designed by Alva Bartley and Norman Wade in 1923. It is a rather elegant but restrained example of a neo-classical building exhibiting a decorative simplicity that may have held some influence over the later design of the Bridgens and Company Factory building.

Landmark House in Queen Street, Auckland, was designed by Alva Bartley and Norman Wade for the Auckland Electric Power Board, and opened in 1929. It is among the few examples of the Chicago commercial Gothic style of skyscraper architecture in New Zealand, where narrow windows and mullions are used to emphasise the building's height. While this building was designed contemporaneously with the Bridgens building, there are few similarities between the two, in form or function, other than to note the strong use of symmetry and order across both façades.



Figure 12: Landmark House in Queen Street, Auckland, was designed by Alva Bartley and Norman Wade for the Auckland Electric Power Board, and opened in 1929.⁶⁴

6.6 Comparative Analysis Conclusion

On a comparative basis then, the Bridgens building can be considered a direct descendant of the generic early factory, or Mill building, in respect of its simple, robust, geometric and highly fenestrated form. It is evident also that it has a contemporary (and popular, if not 'new') styling that was commonly employed across many commercial buildings (serving varying functions) in New Zealand at the time.

Despite these similarities, the former Bridgens Factory has sufficient aesthetic differences (principal among which are its gable-ended form) that it should be considered an important historic place locally and regionally within the framework of factory and stripped classical architecture.

⁶⁴ <http://truecommercial.nzherald.co.nz/insights/news/property-articles/floor-of-historic-landmark-on-market/>

7.0 Significance criteria

(a) Historical

The place reflects important or representative aspects of national, regional or local history, or is associated with an important event, person, group of people or idea or early period of settlement within the nation, region or locality.

Although Edward Bridgens had started his own footwear business in 1890, the first 'family' factory was not opened until 1900. By 1910, records show that 74 factories across New Zealand made 1.5 million pairs of shoes annually. Producing approximately 300,000 pairs annually by 1935, Bridgens and Company Limited was squarely among the leading rank of New Zealand shoe manufacturers. Operations only ceased in 1997, after a continuous span of business of approximately 67 years. The building has considerable local and regional heritage value through its association with the very active footwear industry that existed across New Zealand through the 20th century. That the on-shore industry is today so small, resulting from the lifting of import restrictions in the late 1980s, places more importance on this surviving association.

Less of an aspect of long-term heritage value but more in support of the importance of the family, the business and the building, are the number of visits to the factory made by New Zealand dignitaries. Lord and Lady Bledisloe visited in 1931 to inspect 'Model Footwear Made in a Model Factory'⁶⁵. Prime Minister M. J. Savage visited in 1939, shown the up-to-date methods employed in the manufacture of Bridgens footwear⁶⁶.

Various members of the Bridgens family held significant positions within the community over a period of many decades. Edward Bridgens was heavily involved with the footwear industry, and at one time held the position of president of the New Zealand Footwear Manufacturers Federation. He was also a past president of the Auckland Trotting Club – a position interestingly also held by one of the two architects of the original building, Norman Wade.

Globally renowned athletics coach Arthur Leslie Lydiard ONZ OBE worked in the factory for almost 25 years. He has been lauded as one of the outstanding athletics coaches of all time and is credited with popularising the sport of running and making it commonplace across the sporting world.⁶⁷

By 1930, Kingsland was well established as a thriving suburban community of Auckland. The location of the factory, especially in association with the Kiwi Bacon Factory opposite on New North Road, represents both the viability of commercial expansion along this main arterial route, and a physical 'gateway' both heading west into Kingsland, and east, into the central city business district.

In summary, the building and site at 326 New North Road are determined to be of **considerable local** and **regional** historical significance.

⁶⁵ *NZ Herald*, 9 July 1931 Pg. 20 (see also fig. 24 appendix 2)

⁶⁶ *NZ Herald*, 23 March 1939 (see also fig. 36 appendix 2)

⁶⁷ https://en.wikipedia.org/wiki/Arthur_Lydiard

(b) Social

The place has a strong or special association with, or is held in high esteem by, a particular community or cultural group for its symbolic, spiritual, commemorative, traditional or other cultural value.

Built in 1930, the presumably early success of the business paved the way for considerable expansion in 1935, from a floor area of 24,000 square feet to about 40,000 square feet. This growth during the years of the depression would have had a significant impact on local employment, and on local, even regional, social morale. It is indicated that the company may have reduced wages during the 1930s as an austerity measure. It was publically recorded that in 1936 the company restored salaries and wages for 242 employees to 1931 levels. The Auckland Bootmakers Industrial Union of Workers noted that the move 'was extremely appreciated by the company's employees'.⁶⁸

Large windows were often used in factory design for fire suppression. It is possible however that architects Wade and Bartley incorporated these large openings to provide a high level of internal occupant amenity. The resulting working conditions would have represented leading environmental and social design at the time. The company is also known to have included in the various expansions and alterations more spacious lunch rooms and recreation areas for employees.

In 1931 the company gave as a gift to the Salvation Army, 500 pairs of boots and shoes, to be distributed among the city's poor and less fortunate. Interestingly the gift was made jointly by the company and the employees, whereby the company donated the material, and the employees the labour. It is unlikely that such a collaborative undertaking would have occurred if the working relationships between the owners, the management and the staff were not strong.

In 1938 the government pledged to protect the local footwear industry by increasing tariffs on imported product. In response to what must have been significant concern that local pricing would rise, and be 'covered' as it were by the tariff, Bridgens advertised 'No need for alarm, Bridgens – New Zealand's leading shoe manufacturers definitely state that there will be no price increase'. If this was a savvy commercial strategy, it was also evidence of a social conscience that would have resonated with the local, regional and national consumers.

In summary, the building at 326 New North Road is determined to be of **considerable local** and **regional** social significance to the large number of people (and their families) employed by Bridgens and Company Limited, and to footwear retailers and consumers.

(c) Mana whenua

The place has a strong or special association with, or is held in high esteem by, mana whenua for its symbolic, spiritual, commemorative, traditional or other cultural value.

A Mana whenua values assessment has not been included as part of this evaluation.

⁶⁸ *Auckland Star*, Volume LXVII, Issue 114, 15 May 1936.

(d) Knowledge

The place has potential to provide knowledge through scientific or scholarly study or to contribute to an understanding of the cultural or natural history of the nation, region or locality.

As a result of the cessation of factory operations in 1997, the 1998 fire, the changing ownership in subsequent years and the redevelopment of the original building to suit reuse as office space, there is likely very little remaining within the building that can inform our knowledge of the shoe making industry. The exterior shell too, while largely intact, has little from a knowledge perspective to contribute to an understanding of the cultural history of the locality.

In summary, the building at 326 New North Road is determined to have **little** significance under this criterion.

(e) Technological

The place demonstrates technical accomplishment, innovation or achievement in its structure, construction, components or use of materials.

Both the structural and decorative materials used, and the construction methods employed in building the original factory building, and the 1947 extension are typical of commercial fabric and construction in the 1930s, and are unlikely to reveal any technical innovation.

In summary, the building at 326 New North Road is determined to have **little** significance under this criterion.

(f) Physical attributes

The place is a notable or representative example of a type, design or style, method of construction, craftsmanship or use of materials or the work of a notable architect, designer, engineer or builder.

The building was designed for Bridgens and Company Limited in 1929 by the notable Auckland architectural practice of Wade and Bartley. Norman Wade continued to work for the company in an individual capacity, after his partnership with Alva Bartley was dissolved circa 1935.

Wade and Bartley produced a number of significant buildings, including the Borough Council offices in Dargaville, and the Commercial Hotel, the former Auckland Electric Power Board building and the 1YA Studio building, all in Auckland. All of these works exhibit similarities with the factory building, in their general adherence to classical principles of order, symmetry and regular geometries. The Bridgens and Company building is different from other works in their portfolio. It is designed as a product of function, with its plan form, large structural spans, generous windows and spacious interiors, all key design elements of the early mill buildings of England, designed to meet the needs of the industrial revolution.

Regarded as an example of architectural 'Stripped Classicism', both the original 1929 and 1947 street façades are undeniably striking when viewed from New North Road. The six double height window bays within the original gable-ended façade provide the principal aesthetic character. A rendered trim in a high contrast paint scheme delineates the recess from the rest of the building

elevation. The gold painted rosettes, the voluted capitals, and the fluted columns form the limited decorative façade .

Given Wade and Bartley's portfolio of work, it is perhaps not surprising this typology was chosen for the factory. If they were in anyway shy to embrace a more 'modern' architecture, the stripped classicism was probably the perfect representation (in the timelessness and longevity of the aesthetic) of the strength, solidity and long-term viability Bridgens and Company would hope to portray for years to come.

The 1947 extension to the original façade, built to accommodate a covered delivery bay and other warehouse and factory facilities, is visually subservient to the 1929 building but, designed in a similar style, still a striking and relatively seamless element in the overall façade composition.

In summary, the building at 326 New North Road is determined to be of **considerable local** and **regional** significance for its physical attributes.

(g) Aesthetic

The place is notable or distinctive for its aesthetic, visual, or landmark qualities.

The building has been a local landmark for 87 years. It is still a conspicuous feature along New North Road and its historic form and fabric remains generally legible. Its physical presence was strongly associated with the Kiwi Bacon Factory, opposite. Notwithstanding the considerable modifications to the latter factory, the pair still acts as a strong visual gateway heading either west into Kingsland or east, to the city.

Surrounded by mainly residential properties or very small-scale commercial operations, the buildings would have been highly visible landmarks on this main arterial route. Indeed, they still are today, as a consequence of their striking physical attributes, despite the immediate stretch of New North Road west and east being now almost entirely commercial.

The building is an undeniably impressive structure with considerable streetscape value. The symmetrical, ordered and geometric design of the façade, with its impressive large mullioned steel windows, makes an attractive composition sought and admired by both the pedestrian and the motorist alike. The high contrast paint scheme, designed to highlight the otherwise subtle and restrained rendered architrave work, set against a simple background, further enhances the contextual value of the building while retaining the important 'cornerstone' virtues of the Bridgens Company – being permanence, stability and strength.

Indeed, as the Company only closed due to economic changes beyond their control, some 67 years after the factory first opened, it seems reasonable to conclude that the very striking aesthetic, well known to locals and many across the Auckland region, was aptly chosen.

The company's advertising strategy in the 1930s and 1940s was all about the distinctive landmark qualities of the building⁶⁹. They repeatedly included images of the building in the press – the 'bricks and mortar' of solidity and stability - over imagery of product. The factory would have been something of a landmark even to those who had not seen it.

⁶⁹ Refer figs.23, 29 and 32 in appendix 2

The building was highly valued by the company as being representative of important core values. The aesthetic was keenly guarded, as evidenced by the almost complete lack of alteration throughout its ownership by Bridgens.

In summary, the building at 326 New North Road is determined to be of **considerable local** significance, for its aesthetic, visual and landmark qualities.

(h) Context

The place contributes to or is associated with a wider historical or cultural context, streetscape, townscape, landscape or setting.

The landscape along New North Road between the intersection of Ian McKinnon Drive and Dominion Road to the east and Bond Street and Sandringham Road to the west has changed dramatically, shifting from being largely residential to almost entirely commercial in the last 50 years. When once the Bridgens [and Kiwi Bacon] factory would have had a significance presence contextually, in terms of function and scale, both buildings are now surrounded by similarly sized modern commercial operations. The aesthetic and physical attributes of the Bridgens factory however are so striking, that a contextual significance remains, through its architectural or visual presence.

Today almost all of the early residential buildings that lined the nearby vicinity of New North Road have gone. A few remain, but they are not in sufficient number to suggest the contextual impact is what it used to be. The nature of the building's intactness however does somewhat shift the contextual impact from function and scale, to appearance. In effect a new context has developed, within which the building still has heritage value.

In summary, the building at 326 New North Road is determined to be of **moderate local** contextual and visual significance, specifically to the New North Road streetscape.

8.0 Statement of significance

The former Bridgens Shoe Factory building has considerable historical significance locally and regionally through its association with the very active footwear industry that existed across New Zealand through the 20th century. The surviving association is today even more important as the on-shore industry has all but disappeared.

Members of the Bridgens family were highly active in the local and regional communities. Their reputation and that of the business was so highly regarded that the factory hosted various signatories for government and other official visits.

The place also has considerable social heritage value, as a successful and ethically minded employer, establishing its strong reputation during the early years of the Great Depression. The expansion to the rear of the subject site (now owned by others) included for larger and more spacious staff lunching and other recreation facilities. Soon after, in 1936, the company publically announced a return to pre-depression wages, for all 242 employees. All of these measures reflect a company whose social charter would have had a positive impact on a large number of people (and their families) employed by Bridgens and Company Limited.

The building also has considerable historic value as the work of notable architects Wade and Bartley, whose portfolio includes a number of high profile buildings largely within the Auckland landscape. The substantially intact exterior of the factory building is generally representative of their work, through incorporating classical principles of order and symmetry. However, because of its plan form, its function and location, stylistic differences are evident, that may make this unique in their portfolio.

The former factory has considerable significance for its physical attributes. The façade makes an attractive composition admired by both the pedestrian and the motorist alike. Designed in a stripped classical style, the original, front façade is a striking composition of bold geometric elements that form a significant visual presence about a parapeted gable-end. Decoration is at a minimum, and order and symmetry dominate, in order to reflect the 'cornerstone' virtues of the Bridgens Company – being permanence, stability and strength.

The building has considerable significance for its aesthetic value, as it remains a conspicuous historically intact, and generally legible, feature along New North Road.

Finally, the building is an undeniably impressive structure that supports contextual value. Over time, the residential landscape has given way to commerce. The building is however still of moderate contextual value, where its impact is now less about its function and scale, and more about its unique appearance, being so different from the modern commercial building. In effect a new context has developed, within which the building still has heritage value.

9.0 Extent of the place for scheduling

The identified extent of the former factory building at 326 New North Road for scheduling is the area considered integral to the function, meaning and relationships of the place, identified as the entire site, shown below outlined in blue, legally described as Lot 1 DP 205780. This area physically encompasses the original 1929 factory building and the 1947 extension to the New North Road façade. These substantially intact buildings collectively represent the identified heritage values described in section 7.0 above.

The extent of the place no longer includes all of the original site as it no longer contributes any heritage value, following later development and subdivision.

The extent of place should **also** include the area in front of the building to the kerb line, shown in red, as the building is considered to have a very strong interface and connection with New North Road.



Figure 13: The Extent of the Place - Identified as the entire site, shown above outlined in blue, including the strip of land along the northern boundary - legally described as Lot 1 DP 205780 and the area in front of the building to the kerb line, shown in red.

10.0 Recommendations

Based on the above evaluation, the former Bridgens Shoe Factory building meets the threshold for scheduling as a Historic Heritage Place: Category B.

11.0 Other matters

It is noted that 326 New North Road had been previously considered for scheduling under the legacy Auckland City Council scoring system (refer to Appendix 4.8). This report constitutes a re-evaluation using the Operative in Part Auckland Unitary Plan (UP) criteria, a values-based approach that reflects a broader suite of reasons why a place might be significant. The UP criteria builds upon the legacy system, allowing for additional focus on historical associations, context values, and significance to local communities.

It is also recommended that research be undertaken in relation to the former Kiwi Bacon Factory opposite, at 317-319 New North Road. This building has some prima-facie aesthetic and physical attribute values. In combination with the former Bridgens Shoe Factory, it presents a strong visual and contextual historic gateway, separating Kingsland from the Central Business District of Auckland. Notwithstanding recent modifications, the building may warrant its separate assessment, for historic heritage values.

12.0 Table of Historic Heritage Values

Significance Criteria (A-H)	Value* (None, Little, Moderate, Considerable, Exceptional)	Context (Local, Regional, National, International)
A- Historical	Considerable	Local, Regional
B- Social	Considerable	Local, Regional
C- Mana Whenua	Not evaluated	N/a
D- Knowledge	Little	N/a
E- Technological	Little	N/a
F- Physical Attributes	Considerable	Local, Regional
G- Aesthetic	Considerable	Local
H- Context	Moderate	Local

***Levels of significance or value:**

Exceptional: of outstanding importance and interest; retention of the identified value(s)/significance is essential.

Considerable: of great importance and interest; retention of the identified value(s)/significance is very important.

Moderate: of some importance and interest; retention of the identified value(s)/significance is desirable.

Little: of limited importance and interest.

NA/None: none identified

13.0 Overall Significance

Place Name and/or Description	The Bridgens and Company Shoe Factory (former)
Category	B
Primary Feature	The Bridgens and Company Shoe Factory (former) Building including the 1947 extension to the New North Road elevation
Known Heritage Values	A, B, F, G
Extent of Place	See 9.0 above - Lot 1 DP 205780, and the area in front of the building to the kerb line
Exclusions	Interior of building
Additional Controls for Archaeological Sites or Features	N/a
Place of Maori Interest or Significance	N/a

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Date: 28 March 2017

Reviewer: Megan Walker (Specialist Built Heritage – Policy)

Date: 10 April 2017

Appendices:

Appendix 1: Historic research

Appendix 2: Supplementary research

Appendix 3: Deposited Plans and Certificates of Title

Appendix 4: Auckland Unitary Plan Operative in Part zoning

Appendix 5: Historic photographs

Appendix 6: Site photographs

Appendix 7: Drawings

Appendix 8: Previous Evaluation and Streetscape Study Record

Appendix 1: Historic research

Summary: 326 New North Road Former Bridgens and Company factory

The building is situated on part of what were a number of sections between New North Road and Dominion Road, spanning the railway line, owned in 1882 by William and Eliza Baker, in Eliza's name.¹ The couple began subdivision and sale of the property from that point. George Herbert Baker obtained title to Lots 90 and 91, totalling just over quarter of an acre, fronting onto New North Road and backing onto the line of the railway, in 1894. When a later owner, Frank Edwin Holder, died in 1911, the property came under the administration of the Public Trustee, and was purchased by Bridgens and Company Limited in 1929.²

The origins of Bridgens and Company go back to Edward Bridgens (1846-1922). Born in London, by 1871 he was working as a shoemaker.³ With his wife and children, Bridgens arrived in Auckland in 1875.⁴ He may have started out in the employ of bootmaker James J Austin,⁵ but by 1890 he struck out on his own, setting up first at the top of Wellington Street⁶ then Greys Avenue shortly after.⁷ By 1892 he had his West End Boot Palace on Karangahape Road.⁸ For a time, in 1894, he had a branch store on Queen Street as well,⁹ both shops also used as premises to manufacture the shoes and boots he sold, but even with a fire in the Karangahape Road shop in 1910,¹⁰ and the deaths of Edward Bridgens in 1922¹¹ and his wife Martha in 1938,¹² the shop remained until at least the late 1950s.¹³

The first factory began at 54-56 Prospect Terrace, Mt Eden, on land purchased by Edward's son Frederick Henry Bridgens (1872-1938) in 1900.¹⁴ Around 1923,¹⁵ he went into partnership with his two eldest sons, Edward George and Frederick Albert Bridgens,¹⁶ as Bridgens and Co,¹⁷ after working in the bootmaking trade on his own for some time. In order to purchase the first part of their eventual property at New North Road in October 1929, they formally incorporated Bridgens and Co the month before.¹⁸ Lots 90 and 91 (the factory site) were purchased in 1929.¹⁹ The rear portion of the adjoining Lots 92 and 93 towards the west was formally acquired in 1941²⁰ (328 New

¹ NA 30/214, LINZ records

² NA 69/214, LINZ records

³ 1871 England census, district 12a, via Ancestry.com

⁴ Norman Daniels, *The Bridgens Family of Auckland*, 1995, p. 26

⁵ In 1890, Bridgens advertised, once he started his own business, that he was "late Austin's". *Auckland Star*, 13 March 1890, p. 1(2). Austin's partnership dissolved in 1889, which appears to have led to the end of his business, and Bridgens may simply have moved on (*NZ Herald* 12 January 1889, p. 1(4)).

⁶ *Auckland Star*, 13 March 1890, p. 1(2)

⁷ *Auckland Star*, 2 July 1890, p. 1(1)

⁸ *Auckland Star*, 19 December 1892, p. 1(4)

⁹ *Auckland Star*, 3 May 1894, p. 1(5)

¹⁰ *NZ Herald* 27 September 1910, p.5

¹¹ Death notice, *NZ Herald* 21 February 1922, p.1

¹² *NZ Herald*, 6 December 1938, p. 14

¹³ *Wises Directory* 1958, p. 121

¹⁴ Daniels, p. 36

¹⁵ Earliest employment advertisement found, *NZ Herald*, 4 May 1923, p. 1(6)

¹⁶ Daniels, p. 37

¹⁷ *Auckland Star* 28 October 1927, p. 1(6)

¹⁸ BADZ A13 5181 R9095972, Archives New Zealand

¹⁹ NA 69/214, LINZ records

²⁰ NA 173/238, LINZ records

Figure 14: Research Summary - 326 New North Road,
Eden Terrace, Lisa J Truttman 12 October 2016, p1.

North Road) and sealed as a company carpark soon afterward,²¹ and the remainder was purchased in December 1945 (330 New North Road),²² the old Brodie house, used by the company for storage. In 1974, part of the house was converted into a factory shop.²³ The first part of the Bridgens factory was designed by the architectural firm of Wade and Bartley²⁴ (Thomas Edward Norman Wade and Alva Martin Bartley).

Wade and Bartley practiced together from 1919-c.1935, and designed: Dargaville Borough Council offices and library, 1922; Grandstand, Auckland Trotting Club, 1924; the Bridgens factory on New North Road, 1929; Fountain of Friendship Lodge, 1931; and the 1YA Broadcasting Studio Building on Shortland Street, 1934.²⁵

The Bridgens factory was completed by late 1929; the builders were Fletcher Construction,²⁶ with a project cost of £9000.²⁷ Initially only the ground floor was used for production; the upper floor was used for manufacturing later, and had a raised office from where the floor could be supervised.²⁸ The company advertised in July 1930 that 100,000 pairs of shoes were made there per year,²⁹ and boasted that year that *“the largest and most complete welted plant [for men’s boots] that has ever entered New Zealand was installed in our modern factory.”*³⁰ That figure rose to 250,000 in 1933,³¹ 300,000 by 1935,³² and 500,000 by 1938.³³ On 10 June 1931, the Governor-general Lord Bledisloe and Lady Bledisloe visited the factory.³⁴ In July that year, Bridgens and Co offered the Salvation Army a gift, which was accepted, of 500 pairs of boots and shoes, to be distributed among the poor of Auckland.³⁵

After a fire in an old house at the rear of the original factory in 1934,³⁶ the company commissioned T E Norman Wade to prepare plans for a rear addition to the factory in 1935,³⁷ which was completed by September that year.³⁸ A plumber was injured by a fall during construction.³⁹ The press room was relocated to the new section, a new staff entrance made at the rear of the building and a heavy capacity lift installed.⁴⁰

A new company with the same name was incorporated in 1936, which was to last until the factory’s

²¹ Daniels, p. 115

²² NA 843/74, LINZ records

²³ Daniels p. 121

²⁴ Permit No. 8651, Auckland Council property file

²⁵ File W121, Shepherd Collection, School of Architecture Library, University of Auckland

²⁶ *Auckland Star*, 3 December 1929, p. 24(2)

²⁷ Daniels, p. 94

²⁸ Daniels, p. 95

²⁹ *NZ Herald*, 10 July 1930, p. 20(3)

³⁰ *Auckland Star*, 7 May 1930, p. 42 (supp)

³¹ *NZ Herald*, 13 November 1933, p.2 (supp)

³² *NZ Herald*, 21 September 1935, p. 19

³³ *NZ Herald*, 6 August 1938, p. 11 (supp)

³⁴ *NZ Herald*, 11 June 1931, p.17

³⁵ *Auckland Star*, 2 July 1931, p. 10

³⁶ *NZ Herald*, 27 February 1934, p. 8

³⁷ *NZ Herald*, 21 March p.18(4)

³⁸ Illustration showing the rear addition, advertisement, *NZ Herald*, 21 September 1935, p. 19

³⁹ *Auckland Star*, 9 September 1935, p. 8

⁴⁰ Daniels, p. 99

Figure 15: Research Summary - 326 New North Road,
Eden Terrace, Lisa J Truttman 12 October 2016, p2.

closure later in the century.⁴¹ In 1938, the company introduced children's sandals to their existing range of ladies fashionable shoes and men's footwear.⁴² The Prime Minister Michael J Savage visited what was then described as Auckland's largest shoe factory in March 1939.⁴³ During World War II, the company held contracts to supply the military with footwear, and are said to have received commending letters from military authorities.⁴⁴ In 1945, plans were announced for a branch manufacturing business in Hamilton, providing products to be finished off at the Auckland factory.⁴⁵

Noted athletics coach Arthur Leslie Lydiard worked at the Bridgens factory from 1934 until c.1958,⁴⁶ and was a foreman there towards the end in the 1950s.⁴⁷

The company made extensions and additions to the factory in 1946, totalling £500 and again designed by Norman Wade, including an extension of the ground floor under a new first floor, and forming a new inwards platform.⁴⁸ There were some internal alterations to the offices in 1947,⁴⁹ a storage platform was erected in 1950,⁵⁰ alterations were made to the factory worth \$6250 in 1975,⁵¹ and a transformer station built on the western side in 1984.⁵²

In December 1997, the factory closed down, due to changing economic circumstances, and a relaxation on control of imported goods which allowed cheaper footwear from overseas onto the market.⁵³ The company subdivided the property in 1998, and the factory building was purchased by Parker Brothers at auction in March 1998.⁵⁴ Unfortunately a fire broke out in the warehouse and destroyed a third of the building, creating \$500,000 in damage, and resulting in complete replacement of the original flooring.⁵⁵ There have been a number of changes of ownership since that time.

Internal renovations and removal of mezzanine floors was proposed in 2005,⁵⁶ and more internal alterations providing for a café fit out in 2015.⁵⁷

Lisa J Truttman
12 October 2016

⁴¹ BBNZ A1718 21462 R23414334, Archives New Zealand

⁴² *NZ Herald*, 5 May 1938, p. 26 (1)

⁴³ Caption to illustration, *NZ Herald*, 23 March 1939, p. 8

⁴⁴ Daniels, p. 103

⁴⁵ *NZ Herald*, 29 August 1945 p.8

⁴⁶ David Green. 'Lydiard, Arthur Leslie - Lydiard and his system', from the Dictionary of New Zealand Biography. Te Ara - the Encyclopedia of New Zealand, updated 30-Oct-2012

URL: <http://www.TeAra.govt.nz/en/biographies/613/lydiard-arthur-leslie>

⁴⁷ Daniels, p. 117

⁴⁸ Permit No. 92, Auckland Council property file

⁴⁹ Permit No. 831, Auckland Council property file

⁵⁰ Permit No. 1934, Auckland Council property file

⁵¹ Permit No. 472, Auckland Council property file

⁵² Permit No. 12942, Auckland Council property file

⁵³ *Central Leader*, 5 December 1997 p.5

⁵⁴ NA 56B/553, LINZ records; *Auckland City Harbour News*, 15 July 1998, p. 1

⁵⁵ *Auckland City Harbour News*, 15 July 1998, p. 1

⁵⁶ Auckland Council property file

⁵⁷ Auckland Council property file

Figure 16: Research Summary - 326 New North Road,
Eden Terrace, Lisa J Truttman 12 October 2016, p3.

Appendix 2: Supplementary research

BRIDGENS — On February 21 1922 (and duly), at his residence 275 Great North Road, Edward dearly beloved husband of Martha Elizabeth Bridgens aged 75 years funeral notice later

Figure 17: Paperspast – DEATHS., Auckland Star, Volume LIII, Issue 43, 21 **February 1922**. <http://paperspast.natlib.govt.nz/newspapers/AS19220221.2.4>.

Private.

Bridgens and Company, Ltd., footwear manufacturers. Capital: £12,000 in £1 shares. Subscribers: F. H. Bridgens, F. A. Bridgens and E. G. Bridgens, 3700 shares each; L. A. Bridgens and C. B. Bridgens, 450 shares each.

Figure 18: Paperspast – COMPANY AFFAIRS., Auckland Star, Volume LX, Issue 223, 20 **September 1929**. <http://paperspast.natlib.govt.nz/newspapers/AS19290920.2.19>.

T O C O N T R A C T O R S .

TWO-STOUREYED FACTORY in Reinforced Concrete, New North Road, for MESSRS. BRIDGENS AND CO., LTD.

Tenders for above will be received till Noon of MONDAY, 21st instant. Plans and specifications to be seen at our Offices, 61-63, Brunswick Buildings, Queen Street.

**NORMAN WADE, F.N.Z.I.A.,
A. M. BARTLEY, A.R.I.B.A.,
Architects.**

Figure 19: Paperspast – Page 5 Advertisements Column 7, New Zealand Herald, Volume LXVI, Issue 20378, 5 **October 1929**. <http://paperspast.natlib.govt.nz/newspapers/NZH19291005.2.7.7>.

X

TENDERS are invited for PAINTING
 WORK at Contract, New North Road
 for Messrs. **Bridgens** and Co., Ltd.

Particulars may be had from the Fore-
 man, and tenders close on **TUESDAY**
 10th inst., at Noon.

FLETCHER CONSTRUCTION CO., LTD
 Building Contractors. X

Figure 20: Paperspast – Page 24 Advertisements Column 2, Auckland Star, Volume LX, Issue 286, 3
 December 1929. <http://paperspast.natlib.govt.nz/newspapers/AS19291203.2.188.2>.

B O O T T R A D E.

WANTED FOR OUR NEW FACTORY,
 Situated in Eden Terrace,
RELIABLE OPERATORS.

MACHINE ROOM :
 Girls for Skivers, Beading Machines,
 Eyeletter, and First-class Machinists.
 Also Learners.

**CLICKERS, WELT OPERATORS,
 FINISHERS.**

Apply in first instance by letter only,
 stating age and experience, to **BRIDGENS**
AND COMPANY, LTD., 54, PROSPECT
TERRACE, DOMINION ROAD, AUCKLAND.
 Applications strictly confidential. 19

Figure 21: Paperspast – Page 1 Advertisements Column 6, Auckland Star, Volume LX, Issue 299, 18
 December 1929. <http://paperspast.natlib.govt.nz/newspapers/AS19291218.2.7.6>.

NEW ZEALAND'S MODEL BOOT FACTORY

Equipped with the world's latest machinery and appliances.




Bridgens & Co., Ltd., New Factory, New North Road.

Portion of Interior of Ladies' Shoe Department.

Our Ladies' Department . . .

We search the world for the best materials and machinery that will enable us to produce work of the finest quality. For ten years we have been manufacturing Westminster and Promenade shoes for ladies—shoes that are recognised everywhere as the scene of utility, the finest service shoes produced. A great combination of Quality, Style and Price, they will out-wear any imported shoes.

"Westminster" and "Promenade" Shoes are always available in the latest designs and shades—Stocked by all leading shoe retailers

Our Men's Department . . .

With the advent of 1930 came a new era in the manufacture of men's welted boots and shoes in this country. The largest and most complete welted plant that has ever entered New Zealand was installed in our modern factory. "Triumph" and "Pennant" brands were selected for these high grade boots and shoes. We have placed on the market these men's super-grade boots and shoes, made in the hand-sewn principle from the best materials procurable—and they sell at unusually low prices.

When you are choosing your NEXT Boots or Shoes ask to see "Triumph" or "Pennant" GENUINE WELTED.



Sectional View of Men's Welt Department.

BRIDGENS and COMPANY LTD.

BOOT and SHOE MANUFACTURERS
New North Road :: :: Auckland

Figure 22: Paperspast – Page 42 Advertisements Column 1, Auckland Star, Volume LXI, Issue 106, 7 **May** 1930, Supplement. <http://paperspast.natlib.govt.nz/newspapers/AS19300507.2.199.101.1>

100,000 PAIRS EVERY YEAR



**WE WANT TO TALK
WITH THE MOST PROGRESSIVE CITIZENS IN AUCKLAND**

Ladies—

Here in your own town are produced beautiful Shoes, exact replicas of the latest Paris and London models. "Westminster," the leading Fashion Shoe for Ladies. Faultless in style and quality, famous for value.

Gentlemen—

If you want comfort, combined with utility and price, wear "Pennant" Shoes, made on the hand-sewn principle, and equal to the world's best. New Zealand's wonder value footwear. Styles and shapes to suit all.

"Gloves for the Feet"

BRIDGENS & COY. Ltd., New North Road, AUCKLAND.

Figure 23: Paperspast – Page 42 Advertisements Column 1, Auckland Star, Volume LXI, Issue 106, 7 **May** 1930, Supplement. <http://paperspast.natlib.govt.nz/newspapers/AS19300507.2.199.101.1>

Vice-Regal Interest in the Dominion's Industries



Their Excellencies Lord and Lady Bledisloe were keenly interested in the various processes of footwear manufacturing during their recent visit to Messrs. **Bridgens & Co.**'s factory.

Model Footwear Made in a Model Factory

For Ladies—

*“Westminster”
“Bridco”*

By co-ordinating the latest ideas in factory architecture and the newest in plant, together with ideal working conditions for operations, **Bridgens & Co. Ltd.** have a decided advantage in the production of Quality Footwear. Ladies have learned to recognise the “Westminster” and “Bridco” Shoes as being up to the moment in fashionable style and to be equally famous for value, while the “Pennant” and “Triumph” brands in Men's Footwear symbolise a combination of utility and smartness at a reasonable price. New Zealanders will be proud to wear any of these brands—they certainly are

“GLOVES FOR THE FEET”

For Men—

*“Pennant”
“Triumph”*

Bridgens & Co., Ltd.

New North Road, Auckland

Figure 24: Paperspast – Page 20 Advertisements Column 2, New Zealand Herald, Volume LXVIII, Issue 20920, 9 July 1931. <http://paperspast.natlib.govt.nz/newspapers/NZH19310709.2.172.2>.

**GENEROUS GIFT OF FOOT-
WEAR.**

—o—
**AUCKLAND FACTORY AND
EMPLOYEES COMBINE.**

Auckland, July 4.

An offer to present Major Gordon, of the Salvation Army, with 500 pairs of boots and shoes, to be distributed among the poor of Auckland, was made and accepted this morning. The manufacture of the footwear is to be put in hand at once.

The gift is made by the firm of Messrs Bridgens and Co., boot manufacturers, and their employees. The firm will supply the material, and the employees the labour. This morning Mr Bridgen said that the firm was not looking for any kudos, but it was the employees of the firm who were keen to do something for the poor of the city. It was as a result of a joint effort between the heads of the firm and the employees that enabled the gift to be made.

This morning at the factory, New North Road, Major Gordon addressed the employees, and thanked them for their practical effort to assist less fortunate people. She mentioned that at no time in her long career as a social worker had she known there to be so much poverty and hardship, and the very generous gift which had been made would be greatly appreciated.

Figure 25: Paperspast – GENEROUS GIFT OF FOOTWEAR, Mt Benger Mail, 15 **July 1931**.
<http://paperspast.natlib.govt.nz/newspapers/MTBM19310715.2.10>.

OUTBREAK AT MOUNT ALBERT

HOUSE USED AS STOREROOM

Damage to the interior of a room in a wooden house behind the factory of **Bridgens** and Company, Limited, boot and shoe manufacturers, New North Road, Mount Albert, was caused by an outbreak of fire late yesterday afternoon. The Mount Albert and Mount Eden brigades were summoned from a street call box and soon had the outbreak under control.

The house was used as a storeroom by the company. Details of the loss caused by the fire and of insurance were not available last evening.

Figure 26: Paperspast – “Outbreak at Mt Albert – House used as Storeroom”. New Zealand Herald, Volume LXXI, Issue 21736, 27 **February 1934**. This fire at the rear of the site (then considerably larger than it is today) – led to the demolition of the house and the extension to the factory – see below, in 1935.
<http://paperspast.natlib.govt.nz/newspapers/NZH19340227.2.65>

T O **C O N T R A C T O R S.**

ADDITIONS TO FACTORY.
NEW NORTH ROAD, FOR
MESSRS. **BRIDGENS** AND CO., LTD.,
Boot and Shoe Manufacturers.

Tenders for above will be received until
Noon of WEDNESDAY, April 3.

Plans and Specifications to be seen at
my Office, Nos. 30-31 National Bank
Chambers, Fort Street, Auckland.

NORMAN WADE, F.N.Z.I.A.,
Architect.

Figure 27: Paperspast – 1935 Tender for extension to factory – to meet increasing demands. Also follows the fire in the house/storeroom to the rear of the original site. Page 22 Advertisements Column 7, New Zealand Herald, Volume LXXII, Issue 22065, 22 **March 1935**.
<http://paperspast.natlib.govt.nz/newspapers/NZH19350322.2.218.7>

ADDITIONS TO BUILDING BOOT MANUFACTURING FIRM

A contract for alterations which will increase the existing floor space of the premises of **Bridgens and Company, Limited**, boot and shoe manufacturers, New North Road, by about 16,000 square feet has been let to McKenzie Brothers, builders. The additions to the building, which will extend along the full length of the two storeys at the rear, will bring the total floor space to about 40,000 square feet.

The alterations, which have been rendered necessary by increasing orders, will cost about £9000, and will make the factory one of the largest and most up-to-date in the Dominion. The present lunch room for girls employed in the factory will be replaced by a new structure, on top of the building, and additional flat space for recreational purposes during the lunch hour also will be provided there.

Figure 28: Paperspast – ADDITIONS TO BUILDING, New Zealand Herald, Volume LXXII, Issue 22084, 13 **April 1935**. Major extension – to the rear of the original factory (subdivided and redeveloped as offices by others) – making the factory “one of the largest and most up-to-date in the Dominion”.

<http://paperspast.natlib.govt.nz/newspapers/NZH19350413.2.109>

"BRIDGENS"

15 YEARS OF PROGRESS!

A Record of
**PHENOMENAL
 GROWTH**
 1920 — 1935

WITH 15 years of fine shoe making behind its output, the firm of Bridgens & Co. Ltd., is to-day in the leading rank of New Zealand Shoe Manufacturers. The output of the firm's modern works approximates 300,000 pairs per annum. Every pair made up to standard, every pair enhancing a reputation; every pair ensuring a satisfied customer.

**"BRIDCO"
 LADIES' SHOES**

**"WESTMINSTER"
 LADIES' SHOES**

**"TRIUMPH"
 MEN'S SHOES**

**"PENNANT"
 MEN'S SHOES**



1920



1930

B

1935



A PORTION OF OPERATIVES EMPLOYED IN THE MANUFACTURE OF BRIDGENS' FINE SHOES

Ask for **BRIDGENS Shoes**
 FACTORY AND HEAD OFFICE: NEW NORTH ROAD, AUCKLAND

Figure 29: Paperspast – "Bridgens – 15 years of Progress". Bottom sketch shows the extension to the rear of the original factory, built circa 1935. Page 19 Advertisements Column 1, New Zealand Herald, Volume LXXII, Issue 22220, 21 **September 1935**. Major extension – to the rear of the original factory – making the factory "one of the largest and most up-to-date in the Dominion".
<http://paperspast.natlib.govt.nz/newspapers/NZH19350921.2.171.1>

WAGES RESTORED

AUCKLAND BOOT FACTORY.

The directors of **Bridgens** and Company, Ltd., one of the largest boot and shoe factories in the Dominion, with premises at New North Road, have decided to restore all salaries and wages to the 1931 level. There are 242 employees.

The secretary of the Auckland Boot-makers' Industrial Union of Workers, Mr. C. A. Watts, said to-day that the action of the company was greatly appreciated by the employees.

Figure 30: Paperspast – “Wages Restored”. Presumably after reductions in wages during the depression years. WAGES RESTORED., Auckland Star, Volume LXVII, Issue 114, 15 **May 1936**.
<http://paperspast.natlib.govt.nz/newspapers/AS19360515.2.108>

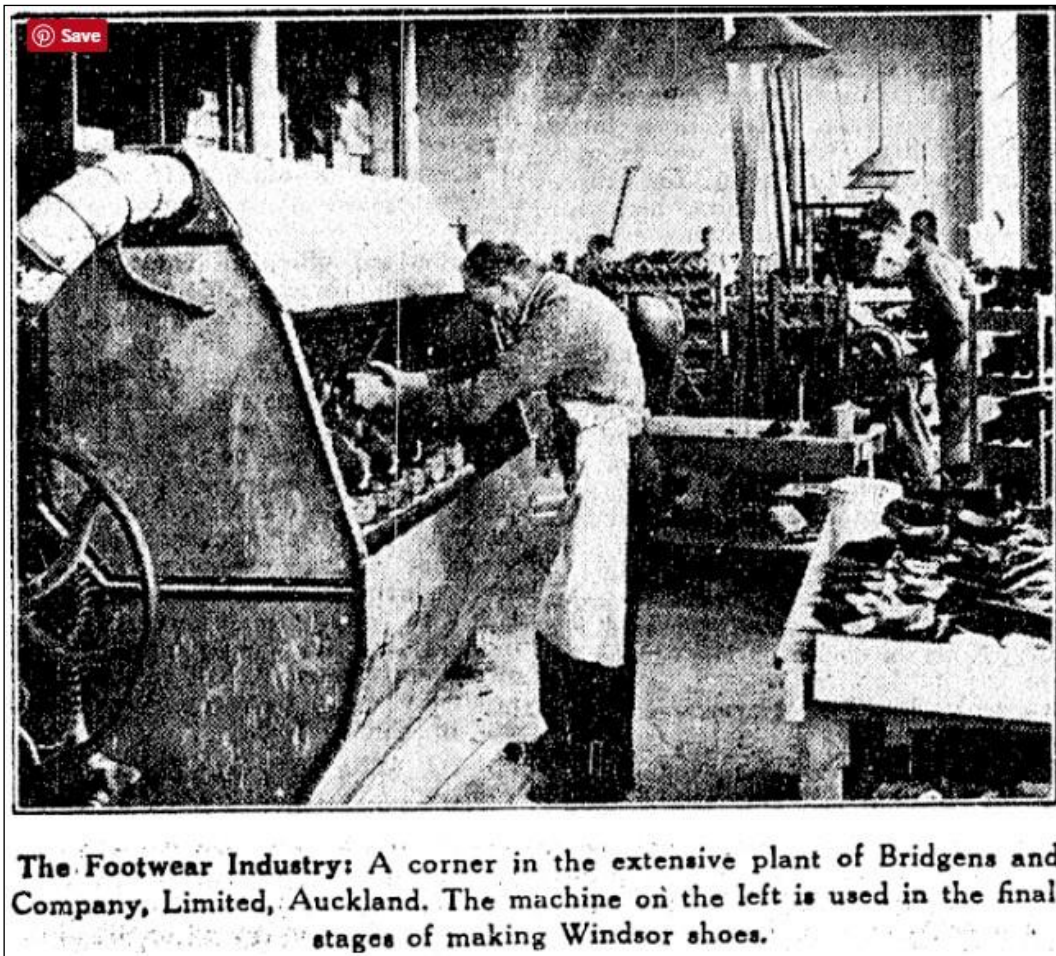


Figure 31: Paperspast – The Footwear Industry: A corner in the extensive plant of Bridgens and Company, Limited, Auckland. The machine on the left is used in the final stages of making Windsor shoes. New Zealand Herald, Volume LXXIII, Issue 22529, 21 **September 1936**.
<http://paperspast.natlib.govt.nz/newspapers/NZH19360921.2.190.1>

**NEW ZEALAND'S MOST
MODERN and PROGRESSIVE
SHOE FACTORY**

A rigid insistence on quality production in every detail has given Bridgens & Co. Ltd. a reputation throughout New Zealand for footwear of the finest type. Style-leadership has been maintained by original designs and skilful adaptation of the newest trends from abroad. Moderate price has been accomplished by the most progressive factory methods and the employment of a staff of over 300 skilled tradespeople. A Bridgens shoe is right in quality, style and price!

Manufacturers of
BRIDCO
Shoes for Women

★

PENNANT
and
TRIUMPH
Men's Weltped Shoes

BRIDGENS
and Company Ltd
238 NEW NORTH ROAD · AUCKLAND · NEW ZEALAND

Figure 32: 'New Zealand's Most Modern and Progressive Shoe Factory'. Christchurch Star **1937** – Being sold on EBAY: http://www.ebay.co.uk/itm/BRIDGENS-COMPANY-SHOE-FACTORY-NEW-NORTH-ROAD-AUCKLAND-NEW-ZEALAND-ADVERT-1937/222273172287?_trksid=p2047675.c100623.m-1&_trkparms=aid%3D222007%26algo%3DSIC.MBE%26ao%3D1%26asc%3D38661%26meid%3D1dc560c7d77c412d8fe0c805c346d339%26pid%3D100623%26rk%3D2%26rkt%3D6%26sd%3D221486991799.

THE PUBLIC AND THE TARIFF!

NO NEED FOR ALARM

BRIDGENS – New Zealand's Leading Shoe Manufacturers

DEFINITELY STATE

that there will be

NO PRICE INCREASE

in the Shoes They Manufacture

BRIDGENS & CO. LTD.

have served the Public of New Zealand faithfully for 18 years and can be depended upon to maintain their established policy of supplying the public with quality shoes at the lowest possible price

ONE AND A HALF MILLION PAIRS OF BRIDGENS SHOES
HAVE BEEN MADE AND SOLD IN THE LAST 5 YEARS
EVERY PAIR GAVE THE BEST VALUE FOR THE PRICE CHARGED

**ASK YOUR RETAILER TO SHOW YOU THE NEW
MODELS OF BRIDGENS FINE SHOES**

YOU OWE IT TO YOURSELF AND NEW ZEALAND TO BUY BRIDGENS!

Figure 33: Paperspast – Company advertisement that ‘Prices will not rise’ [despite the difficult economic times]. Page 17 Advertisements Column 1, New Zealand Herald, Volume LXXV, Issue 22977, 3 **March 1938**. <http://paperspast.natlib.govt.nz/newspapers/NZH19380303.2.193.1>

OBITUARY

MR. F. H. BRIDGENS

(By Telegraph—Press Association.)
AUCKLAND, This Day

The death has occurred of Mr. Frederick Henry Bridgens, one of the founders of Bridgens and Co., Ltd., footwear manufacturers, employing at times up to 300 employees. Born in London 66 years ago, he arrived in New Zealand with his parents at an early age. His mother is in her ninetieth year.

Mr. Bridgens leaves a widow and an adult family of seven sons and two daughters. One son, Edward, is president of the Auckland Trotting Club.

Figure 34: Paperspast – Obituary – Mr. F. H. Bridgens. Company founder – Father to Edward, Albert and Henry Bridgens. OBITUARY, Evening Post, Volume CXXVI, Issue 53, 31 August 1938.
<http://paperspast.natlib.govt.nz/newspapers/EP19380831.2.135>



Figure 35: Paperspast – PRIME MINISTER AND PARTY AT AFTERNOON TEA During the round of visits to Auckland factories yesterday, Mr. Savage and his party were entertained to afternoon tea by Mr. and Mrs. E. G. Bridgens (left), at the factory of Bridgens and Company, Limited. others in the photograph are, left to right, Mr. A. G. Osborne, M.P., W. T. Anderton, M.P., Mr. Savage and the Hon. O. G. Sullivan, New Zealand Herald, Volume LXXVI, Issue 23303, 23 **March 1939**.
<http://paperspast.natlib.govt.nz/newspapers/NZH19390323.2.114>.



Figure 36: Paperspast – PRIME MINISTER VISITS AUCKLAND'S LARGEST SHOE FACTORY. THE PRIME MINISTER VISITS AUCKLAND'S LARGEST SHOE FACTORY Yesterday the Rt. Hon. M. J. Savage, accompanied by the Hons. D. G. Sullivan (centre) and W. E. Parry (right), visited the factory of Bridgens and Company, Limited, New North Road. This photograph shows Mr. E. G. Bridgens explaining to the Prime Minister and members of his party the up-to-date methods employed in the manufacture of Bridgens footwear. New Zealand Herald, Volume LXXVI, Issue 23303, 23 **March 1939**.
<http://paperspast.natlib.govt.nz/newspapers/NZH19390323.2.26.5>

DEATH AT ROTORUA

MR. E. G. BRIDGENS

BUSINESS MAN - SPORTSMAN

The death occurred at Rotorua last night of Mr. Edward G. Bridgens, widely known Auckland business man and sportsman. He had been in ill-health for some time.

Born in Auckland in 1894, Mr. Bridgens formed the company of Bridgens and Co., Ltd., boot and shoe manufacturers, in 1922. Later he became managing director. He subsequently extended his business interests and became a director of a number of companies, but resigned these positions comparatively recently on account of ill-health.

Mr. Bridgens gave unselfishly of his time to the welfare of the boot and shoe industry. He was a member of the Footwear Industrial Plan Committee, which acts in an advisory capacity to the Government in all matters affecting the trade in New Zealand. At one time he was president of the New Zealand Footwear Manufacturers' Federation, and until his death was an executive member of the federation. At recent conciliation proceedings, which terminated in a satisfactory agreement, he represented the federation.

Figure 37: DEATH AT ROTORUA – the death of Mr Edward G. Bridgens – founder of the company, Auckland Star, Volume LXXV, Issue 207, 1 September 1944
<http://paperspast.natlib.govt.nz/newspapers/AS19440901.2.24>

OBITUARY

T. E. Norman Wade, Fellow.

The Auckland District Branch and the Institute have suffered another loss of one of the members of the Institute belonging to the older generation of members whose connection with the profession and with the Institute went back to the time before the Act in 1913, by the death of T. E. Norman Wade on 23rd of October, 1954, at the age of 74 years. He had been an invalid for 20 months.

Norman Wade received his training in his father's office and was for some years in partnership with his elder brother, the late Harry Wade, also a fellow of the Institute. He was in practice for over 50 years and was for a period in partnership with Mr. A. M. Bartley, (Fellow). He was responsible, either solely or in partnership, for many well known Auckland buildings, among which may be mentioned: The Auckland Electric Power Board Building in Queen Street; the IYA Building in Shortland Street; the Main Stand at the Trotting Course, Alexandra Park. He was Architect to the Auckland Harbour Board for many years and during that time was responsible for the new Port Building.

He was keen on several forms of sport, and was a past President of the Auckland Trotting Club, a member of the Auckland Racing Club, and was President of the N.Z. and Auckland Kennel Clubs until just before his death. He was internationally renowned as a judge of dogs and had been called upon to judge them at the Sydney Royal Show. He was also known as a breeder of race horses, having bred many gallopers of which the best known were Gay Rosette, Gay Lally, and Somerset Fair.

He is survived by his widow, a daughter and two sons.

22

NZIA Jnl 22:22 Feb '55

Figure 38: NZIA Journal 22:22 **February 1955** – Obituary T. E. Norman Wade, Fellow.



Figure 39: NZ Herald – 19 **October 1965** – Obituary of Henry Ernest Bridgens, Managing Director of the company at the time of his death.



Figure 40: "Clocking out for good" - Terai, Abigail - Auckland City Harbour News, 5 December 1997 p 1. ARC Serial 072 A8 ASK AT DESK. Abstract: "Bridgens & Company shoe manufacturers in Kingsland have been operating for 100 years (one of Auckland's oldest shoe manufacturers), but later this month they are to close, leaving 62 people out of work, including Brian McMillan who has worked for the company for 45 years. They blame government policies lowering tariffs on imported shoes."



Figure 41: "Victim of Changing Times", "How the Bridgens Family Empire Rose ... and Fell" - Terei, Abigail – Central Leader, 3 December 1997. Abstract: "It's been one of the landmark buildings around Kingsland and Central Auckland. Bridgens Shoes has operated its factory and shop near the old Kiwi Bacon Factory for years. Now it's going, undermined by cheap shoe imports."

YOUR PLACE YOUR PAPER



Auckland City Harbour News

24 JUL 1998

Your Favourite Community Newspaper delivered up to 19,600 homes in Poneke, Freemans Bay, Hare Bay, Westmere, Pt Chevalier, Moterue, Western Springs, Grey Lynn, Mt. Albert and Kingsland

Wednesday July 15, 1998

Old landmark ablaze

ARC

HALF PRICE

NO HALF PRICE

FE NOW

55.00

55.70

57.50

53.00

80 \$45.00

80 \$21.00

80 \$50.00

\$72.00

\$38.00

\$80.00

\$50.00

\$75.00

\$6.50

CEC TO CLEAR

\$39.00

\$49.00

\$5.00 \$60.00

SALE

ADDITIONAL VALUE

ADDITIONAL VALUE

\$14.00

\$30.00 set

FROM \$19.95 set

\$24.00 each

HALF PRICE

SAVILY REDUCED

SAVILY REDUCED

SAVE NOW

5.00 \$19.95

5.00 \$185.00

5.00 \$175.00

5.00 \$330.00

5.00 \$285.00

5.00 NOW

5.00 \$330.00

5.00 \$750.00

5.00 \$595.00

RUGS

FRONAL AND

AND COLLECT

SAVE NOW

\$1.00 \$1595.00

\$4.00 \$3775.00

\$3.00 \$3995.00

\$2.00 \$1995.00

\$1.00 \$1995.00

SEE THE

MARKET

RES



Winning smile and personality plus

You must have been a beautiful baby...or so goes the song.

When little Esita Fale gets older she'll be able to say she was — the most beautiful of all actually.

Esita, 10½ months, won a Baby of the Year contest last weekend out of 4000 entries and collected \$10,000 in prize money.

"It's her personality for sure," says Litarina, Esita's first-time mother. "She's full of it. She's a happy baby, she's smiling all the time."

"I was thrilled. I was really, really happy. I had a few tears."

Litarina and her husband Mislav, of Freemans Bay, had twice the reason to celebrate, as the weekend was also their wedding anniversary.

This weekend the family heads for a holiday in Western Samoa, where they will show off their special baby.

Mr and Mrs Fale didn't even know there was prize money when they entered the contest. "We just wanted to show her off."

They plan to put the money in trust for Esita's education.

\$50,000 raised from Johnson's Baby of the Year competition in New Zealand and Australia goes towards the Australian and New Zealand Children's Cancer Study Group.

Fire drill ahead of the real thing

Police and fire safety officers are to visit a fire that forced thousands of shoppers to be evacuated from St Lukes Shopping Centre on Monday.

Sixteen firefighters and four engines were called in to the fire, which started about 2.30pm in the men's staff locker rooms at Footscray.

Balanced station officer Clive Baker says the fire was deliberately lit in a handboard box, but was extinguished by sprinklers.

"It was a small fire but had the potential to be worse in a mall," he says.

"But the renovation was very good and everybody was out by the time we got there."

It was the second evacuation that day at St Lukes, after a fire drill in the morning.

St Lukes manager Bob Smith says the second evacuation was an unfortunate coincidence.

"It's bad for business," Mr Smith says. "People were getting in their cars and leaving."

"It's unfortunate that one person can put the whole centre out."

Footscray management wouldn't comment on the fire.

Office 849-6060 Delivery 525-2022 Classified 525-2100

Figure 42: "Old landmark ablaze" - Auckland City Harbour News, 15 July 1998 p 1. ARC Serial 072 A8 ASK AT DESK. Extract – "A suspicious fire almost destroyed one of Kingsland's oldest landmarks - the Bridgens and Company building. The building was built in the 1930s by the Bridgen family to house a shoe manufacturing company, which closed in December 1997."

Appendix 3: Deposited Plans and Certificates of Title

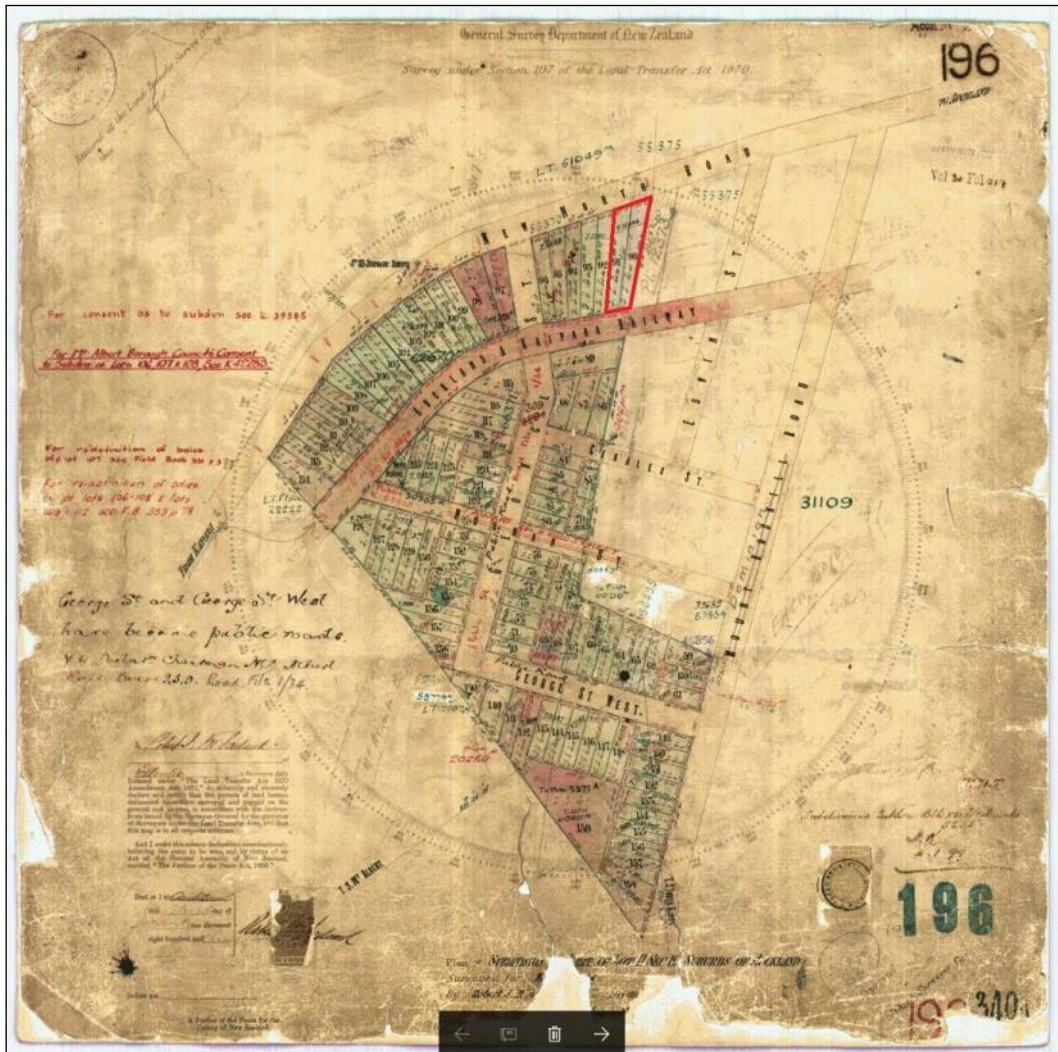


Figure 43: DP196 – date unknown – but presumed to predate NA69/214 (below) dating to 1894, showing lots 90 and 91 owned (as at 1894) by George Herbert Baker – Auckland Council files October 2016.

NEW ZEALAND.

[Form B.]



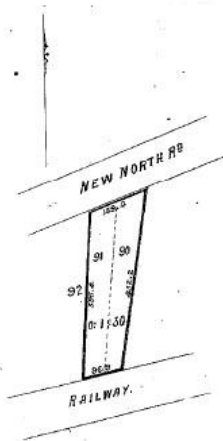
Reference: Vol. 30, folio 2114
Transfer No. 1574444

Register-book
Vol. 69, folio 2114

CERTIFICATE OF TITLE UNDER LAND TRANSFER ACT.

This Certificate, dated the seventh day of March, one thousand eight hundred and ninety-four, under the hand and seal of the District Land Registrar of the Land Registration District of Auckland Witnesseth that George Herbert Baker of Auckland, Clerk

is seized of an estate in fee-simple (subject to such reservations, restrictions, encumbrances, liens, and interests as are notified by memorial under-written or enforced hereon, subject also to any existing right of the Crown to take and lay off roads under the provisions of any Act of the General Assembly of New Zealand) in the land hereinafter described, as the same is delineated by the plan hereon bordered green, be the several admeasurements a little more or less, that is to say: All that parcel of land containing One rood and thirty perches more or less being Lots Ninety and Ninety one on deposited plan No. 196 of the subdivision of part of allotment Eleven Section Ten, Suburbs of Auckland. Provided however that William Baker of Auckland, Gentleman, the Vendor shall not be liable to contribute towards the erection or maintenance of any dividing or boundary fence between the within described land and any adjoining land of the said Vendor but this provision shall not extend to the benefit of any purchaser of such adjoining lands.



Scale - 2 Chains to an inch.

Geo. Stewart
Draftsman



Edwin Sampford
District Land Registrar

Mortgage No. 10572 from George Herbert Baker abandoned to Mary Jane Boylan of Auckland wife of Thomas Francis Boylan of Auckland, Gentlemen produced and entered seventh March 1894 at 11.2 a.m. o'clock.

Edwin Sampford
Dist Land Reg

Discharge of Mortgage No. 10572 produced and entered the 27th November 1895 at 12.32 p.m.

Discharge of Transfer No. 17397 from George Herbert Baker above named to John Hunter Harrison of Auckland, Sharebroker, produced and entered the 27th November 1895 at 12.34 p.m.

Discharge of Transfer No. 11115 John Hunter Harrison to Frank Edwin Holden of Auckland, Gentleman, produced the 27th November 1907 at 11.20 a.m.

CANCELLED.

W. H. Collier
Asst. L. Reg.

Figure 44: CT NA69/214, showing the ownership of lots 90 and 91 by George Herbert Baker – as at **March 1894** – see page two over, Auckland Council files October 2016.

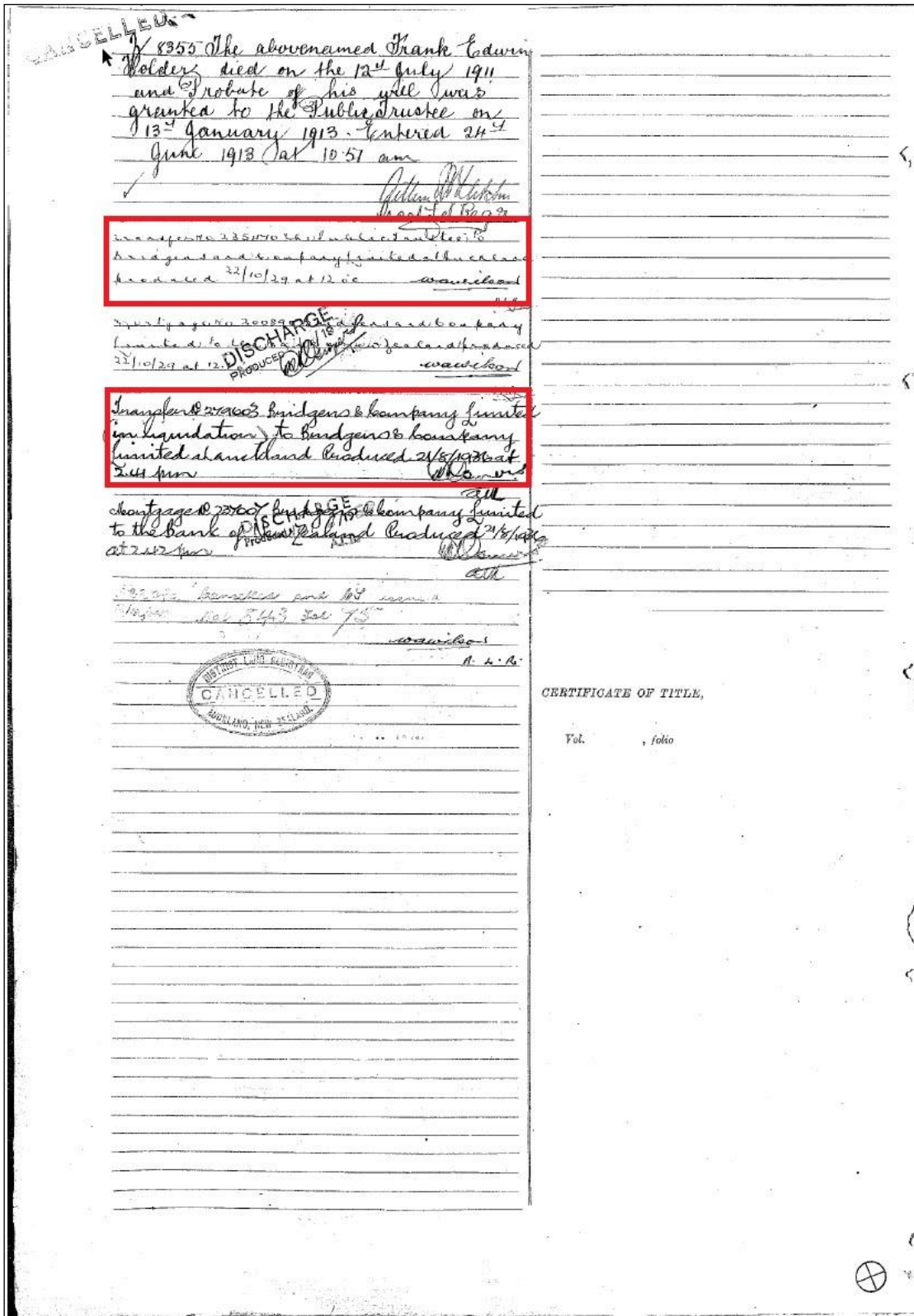


Figure 45: CT NA69/214 (pg. 2), showing the transfer of lots 90 and 91 to Bridgens and Company Limited in **1929**, and then again in **1936** to a variation on the company name (Bridgens and Company Limited at Auckland), after the liquidation of the earlier company – Auckland Council files October 2016.

NEW ZEALAND.

[Form B.

Reference: Vol. 30, folio 244.
Transfer No. 57371.

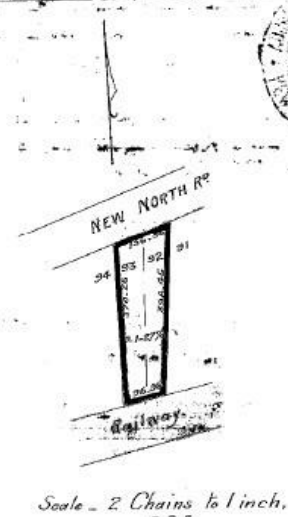


Register-book,
Vol. 173, folio 238

CERTIFICATE OF TITLE UNDER LAND TRANSFER ACT.

This Certificate, dated the twenty second day of December, one thousand nine hundred and four, under the hand and seal of the District Land Registrar of the Land Registration District of Auckland, testifies that Patrick Brodie of Auckland (New Zealand) Solicitor

is seized of an estate in fee-simple (subject to such reservations, restrictions, encumbrances, liens, and interests as are notified by memorial under written or indexed hereon, subject also to any existing right of the Crown to take and lay off roads under the provisions of any Act of the General Assembly of New Zealand) in the land hereinafter described, as the same is delineated by the plan hereon bordered green, be the several admeasurements a little more or less, that is to say: All that parcel of land containing one (1) foot and thirty two (32) inches and one quarter (1/4) thereof more or less being Lot 92 (subdivided) and Lot 93 (subdivided) and (being) shown as a plan deposited in the Land Registry Office at Auckland under C.P. 116 which said parcel of land is portion of Allotment C.P. 106 of Section 10, part of the Landward of Auckland.



Patrick Brodie
District Land Registrar

DISCHARGED

DISCHARGE
K 26671

2. 46013 transmission to Matthew Joseph Brodie, Roman Catholic Bishop of Christchurch and Patrick Brodie the younger of Auckland Civil Servant entered 21/11/1941 at 12.45 pm
Transfer 333994 Matthew Joseph Brodie and Patrick Brodie the younger to Bridgens and Company Limited at Auckland Produced 29/6/1941 at 1.06 pm and entered 5/6/1941 at 12.37 pm

More Quills due to condition of Original

CANCELLED

Figure 46: CT NA173/238 (1904), showing the transfer of the adjacent two lots to the west (92 and 93) to Bridgens and Company Limited at Auckland also, **June 1941** – Auckland Council files October 2016.

References
Prior C/T 843/74 & 843/75

Land and Deeds 69

Transfer No.
N/C. Order No. B.329551.1



REGISTER

No. 568 / 553

CERTIFICATE OF TITLE UNDER LAND TRANSFER ACT

This Certificate dated the 4th day of September one thousand nine hundred and eighty-four under the seal of the District Land Registrar of the Land Registration District of NORTH AUCKLAND

WITNESSETH that BRIDGENS & COMPANY LIMITED at Auckland

is seized of an estate in fee-simple (subject to such reservations, restrictions, encumbrances, liens, and interests as are notified by memorial underwritten or endorsed hereon) in the land hereinafter described, delineated with bold black lines on the plan hereon, be the several admeasurements a little more or less, that is to say: All that parcel of land containing 3484 square metres more or less being Lots 90, 91, 92 and 93 Deposited Plan 196 and being part Allotment 11 Section 10 Suburbs of Auckland

~~Appl. Sec. 27~~ C.T. file 15.4.85

J. Mc Murtrey

Assistant Land Registrar

Mt. Albert City

714861.1 Mortgage of part to the National Bank of New Zealand on 14/11/1977 at 10.35 o/c (formerly affects 00863/75)
D280009.1

J. Mc Murtrey
A.L.R.

Subject to the conditions specified in Certificate D.396196.1 pursuant to Section 643(2) of the Local Government Act 1974 (affects Lots 91 and 92 only) - 30.11.1984 at 2.43 o/c
see 0733535 26/11/84 to Shillia

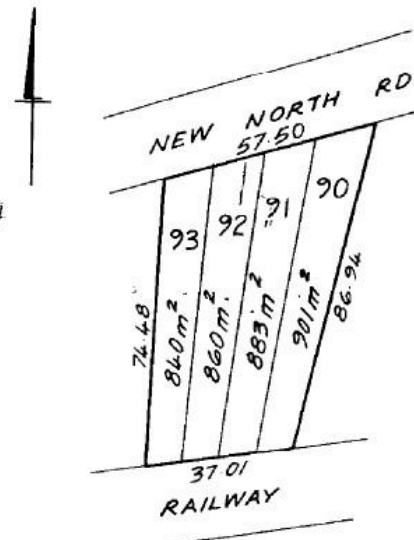
A.L.R.

B.437341.1 Transfer granting an electricity easement in gross over part - Affects Pt in favour of the Auckland Electric Power Board - 17.7.1985 at 2.41 o/c

J. Camp
A.L.R.

D260953.1 CAVEAT BY PARKER BROTHERS LIMITED 9.4.1998 AT 2.1 o/c

W. J. ...
26/11/1984
A.L.R.
FOLDER



Total Area 3484 m²
DP 196
51.11

Measurements are Metric

568 / 553

Figure 47: CT NA568/553 (1984), transferring an easement to the then AEPB – also showing the combined ownership of lots 90, 91, 92 and 93 by Bridgens and Company Limited – Auckland Council October 2016.

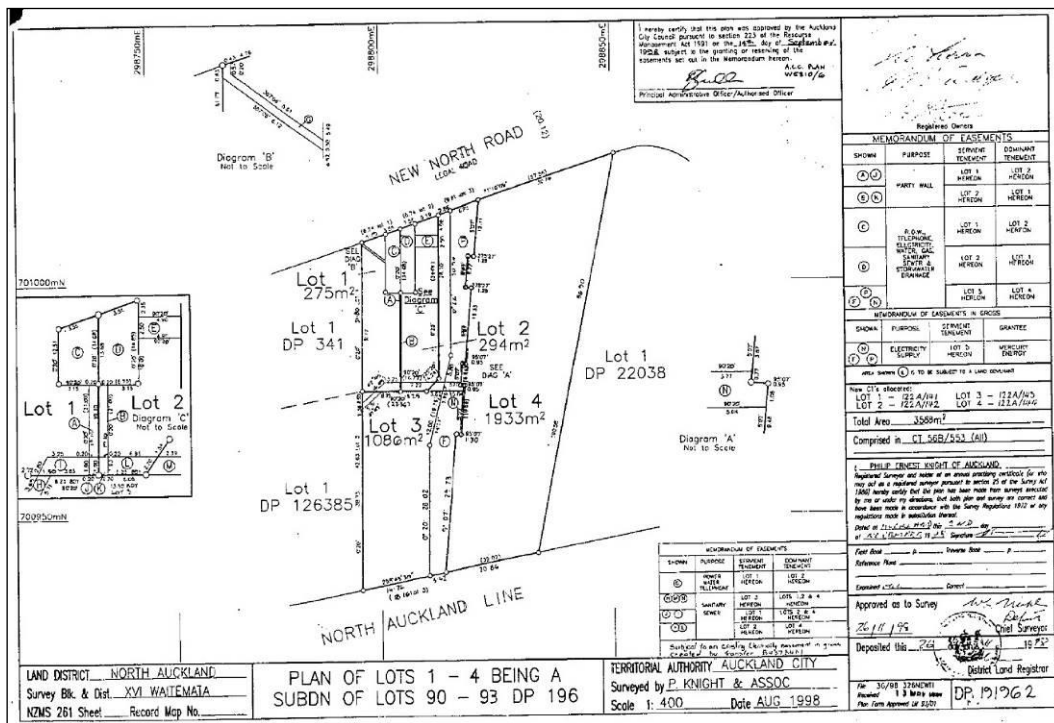


Figure 48: DP191962 (1998), showing the subdivision of lots 90, 91, 92 and 93 by Bridgens – to allow for the transfer of lots 1, 2 and 3 to others circa 1998. Lot 4 remained - Auckland Council files October 2016.

Reference:
Prior CT: 56B/553
Document No.: D330356.4



REGISTER^{LT69}

122A/144

CERTIFICATE OF TITLE UNDER LAND TRANSFER ACT 1952

This Certificate dated the 26th day of November One Thousand Nine Hundred and Ninety Eight under the seal of the District Land Registrar of the Land Registration District of NORTH AUCKLAND

WITNESSETH that **BRIDGENS & COMPANY LIMITED**

are seised of an estate in fee simple (subject to such reservations, restrictions, encumbrances and interests as are notified by memorial endorsed hereon) in the land hereinafter described, delineated on the plan hereon, be the several admeasurements a little more or less, that is to say: All that parcel of land containing 1933 square metres, more or less being **LOT 4**

DEPOSITED PLAN 191962



D330356.4 Easement certificate affecting Lots on DP 191962

NATURE	SERVIENT LAND	DOMINANT LAND
Right of Way, Telephone, Electricity, Water, Gas, Sanitary Sewer and Stormwater Drainage	3 - P, F & N CT 122A/143	4
Sanitary Sewer	3 - H, M & N	4
"	1 - J & I CT 122A/141	4
"	2 - K & L CT 122A/142	4

D376866.3 Transfer to Parker Brothers Limited

D376866.4 Mortgage to ASB Bank Limited

Both 13.4.1999 at 10.13.


for RGL

The above easements will be subject to Section 243(a) Resource Management Act 1991 when created

D330356.4 CAVEAT BY PARKER BROTHERS LIMITED

All - produced 13.11.1998 at 3.20 and entered 26.11.1998 at 9.00

WITHDRAWN FOR REG



For DI.R



122A/144

Figure 49: CT NA122A/144, showing the transfer of all lots (being the earlier historic lots 90, 91, 92 and 93) from Bridgens and Company Limited to Parker Brothers Limited **April 1999** - Auckland Council files October 2016.

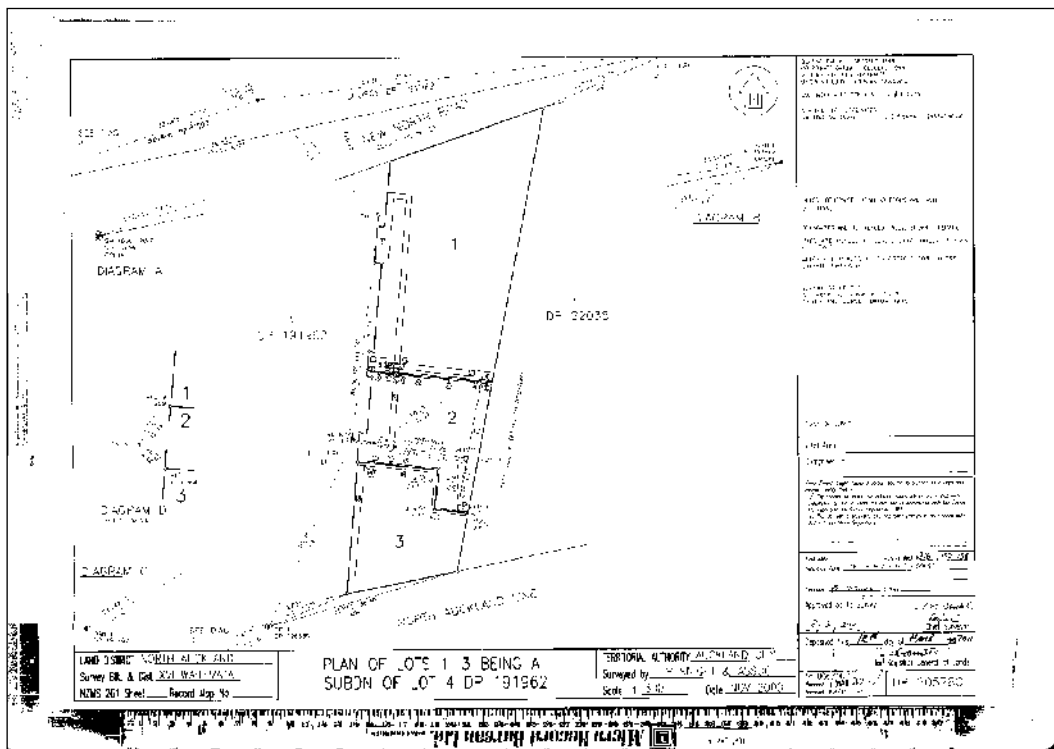


Figure 50: DP205780 (2000), showing the subdivision of lot 4 by Parker Brothers Limited – to allow for the eventual transfer of lot 1 (the subject site) to Sunningdale Properties Limited March 2001 - Auckland Council files October 2016.

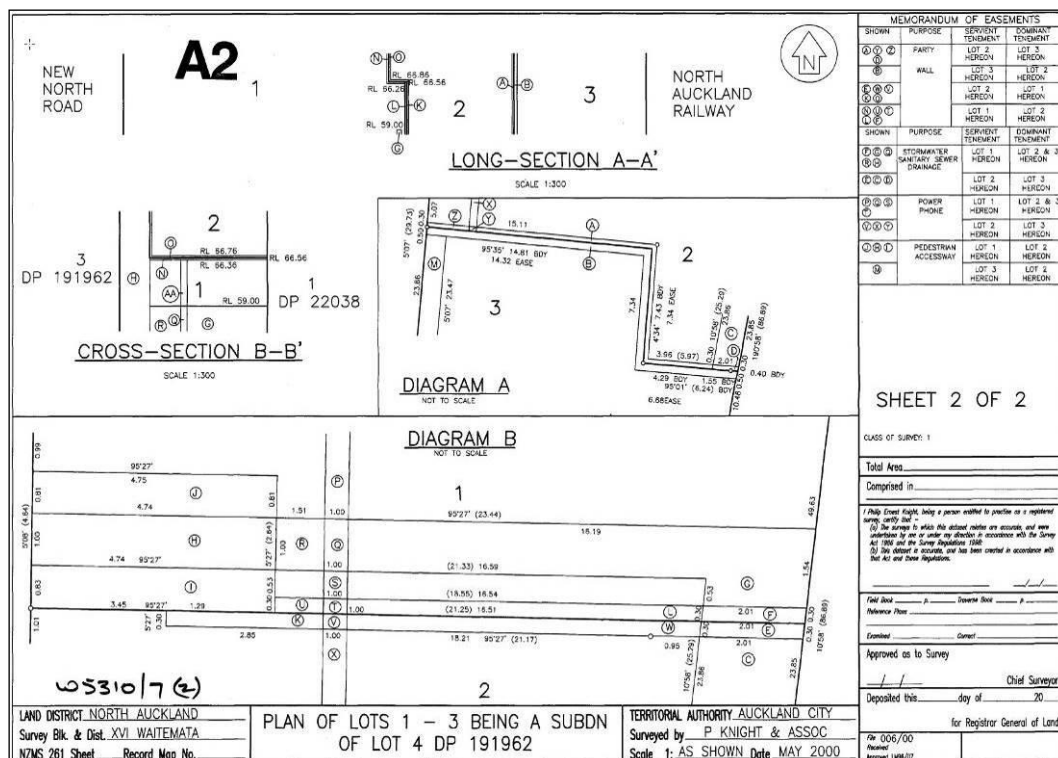


Figure 51: Subdivision survey - 2000, showing in Cross Section B-B that Lot 2 sits above Lot 1 (the subject site) on level two, above the toilet block at the rear of the original factory building - Auckland Council files October 2016.



COMPUTER FREEHOLD REGISTER UNDER LAND TRANSFER ACT 1952



Historical Search Copy

Identifier NA134A/536
Land Registration District North Auckland
Date Issued 12 March 2001

Prior References

NA122A/144

Estate Fee Simple
Area 1116 square metres more or less
Legal Description Lot 1 Deposited Plan 205780

Original Proprietors

Sunningdale Properties Limited

Interests

Appurtenant hereto are rights of way and telephone, electricity, water, gas, sanitary sewer and stormwater drainage rights specified in Easement Certificate D330356.4 - produced 13.11.1998 at 3.20 and entered 26.11.1998 at 9.00 am

The easements specified in Easement Certificate D330356.4 are subject to Section 243 (a) Resource Management Act 1991

Subject to a party wall right over parts marked N, U, T, L and F and stormwater, sanitary sewer drainage rights over parts marked F, G, Q, R and H and phone and power rights over parts marked P, Q, S, T and pedestrian accessway rights over parts marked J, H and I on DP 205780 specified in Easement Certificate D583645.4 - produced 1.3.2001 at 2.15 and entered 12.3.2001 at 9.00 am

Appurtenant hereto is a party wall right specified in Easement Certificate D583645.4 - produced 1.3.2001 at 2.15 and entered 12.3.2001 at 9.00 am

The easements specified in Easement Certificate D583645.4 are subject to Section 243 (a) Resource Management Act 1991

D612290.5 Mortgage to Johnston Prichard Nominees Limited - 12.6.2001 at 1.50 pm

5713496.1 Discharge of Mortgage D612290.5 - 2.9.2003 at 9:00 am

5713496.2 Transfer to Macklin Holdings Limited - 2.9.2003 at 9:00 am

6081166.1 Departmental dealing correcting the instrument number of Easement Certificate from D330356.4 to D330356.5 (incorrectly memorialised on historic paper title) - 15.7.2004 at 10:53 am

6113334.1 CAVEAT BY RAVITA HOLDINGS LIMITED - 12.8.2004 at 9:00 am

6145518.4 Withdrawal of Caveat 6113334.1 - 10.9.2004 at 9:00 am

6145518.5 Transfer to Vistaita Limited - 10.9.2004 at 9:00 am

6145518.6 Mortgage to Structured Finance (NZ) Limited - 10.9.2004 at 9:00 am

6145518.7 Mortgage to ANZ National Bank Limited - 10.9.2004 at 9:00 am

6173310.1 Departmental dealing correcting the priority of Mortgages by making Mortgage to ANZ National Bank Limited 6145518.6 and Mortgage to Structured Finance (NZ) Limited 6145518.7 - 6.10.2004 at 11:22 am

Transaction Id
Client Reference rbester001

Historical Search Copy Dated 8/11/16 1:19 pm, Page 1 of 4

Figure 52: CT NA134A/536 (2001, pg. 1) – the current CT – showing that the subject site sold to various parties - Auckland Council files October 2016.

Identifier**NA134A/536**

6487469.3 Variation of Mortgage 6145518.6 - 7.7.2005 at 9:00 am
7099192.1 Discharge of Mortgage 6145518.6 - 6.11.2006 at 9:00 am
7099192.3 Mortgage to Westpac New Zealand Limited - 6.11.2006 at 9:00 am
7099192.4 Mortgage Priority Instrument making Mortgages 7099192.3 and 6145518.7 first and second mortgages respectively - 6.11.2006 at 9:00 am
7956752.2 Discharge of Mortgage 6145518.7 - 15.10.2008 at 12:54 pm
7956752.3 Discharge of Mortgage 7099192.3 - 15.10.2008 at 12:54 pm
7956752.4 Transfer to Centro Investments Limited - 15.10.2008 at 12:54 pm
7956752.5 Mortgage to ANZ National Bank Limited - 15.10.2008 at 12:54 pm
10500782.1 Discharge of Mortgage 7956752.5 - 2.8.2016 at 6:59 pm
10500782.2 Transfer to WL Property Investments Limited - 2.8.2016 at 6:59 pm
10500782.3 Mortgage to Westpac New Zealand Limited - 2.8.2016 at 6:59 pm

Transaction Id
Client Reference rbester001

Historical Search Copy Dated 8/11/16 1:19 pm, Page 2 of 4

Figure 53: CT NA134A536 (2001, pg. 2) – the current CT – showing current ownership of the subject site (Lot 1 DP 205780) by WL Property Investments Limited – as at October 2016. Auckland Council files October 2016.

Appendix 4: Auckland Unitary Plan Operative in Part zoning

Property Summary

This summary lists searched address, legal description, Zone and other limitations that apply to the site.

Address
326 New North Road Eden Terrace Auckland 1021

Legal Description
Lot 1 DP 205780 1116m2

Appeals

Modification

Zone
Business - Mixed Use Zone

Precinct

Overlays
Built Environment: Identified Growth Corridor Overlay

Controls
Controls: Height Variation Control - Newton, 21m
Controls: Macroinvertebrate Community Index [rcp/dp] - Urban

Designations
Designations: Designations - ID 1609, Road Widening, Designations, Auckland Transport

Figure 54: Operative in part Unitary Plan zoning – Business – Mixed Use Zone - Auckland Council December 2016.

Appendix 5: Historic photographs



Figure 55: **April 1957**. Factories on the New North Road, Auckland, New Zealand. Whites Aviation Ltd. Shows both the Bridgens Factory and [opposite] the Kiwi Bacon Factory. Ref: WA-43959-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/23107313>. Full image. Close-up extract below.



Figure 56: **April 1957**. Factories on the New North Road, Auckland, New Zealand. Whites Aviation Ltd :Photographs. Ref: WA-43959-F. Alexander Turnbull Library, Wellington, New Zealand. Shows original windows along west façade and an early generator building close to the street boundary – superseded in 1984 by the AEPB transformer. <http://natlib.govt.nz/records/23107313>. Extract. Full image above.



Figure 57: **March 1963**. Bridgens and Company Ltd, New North Road, Auckland. Whites Aviation Ltd :Photographs. Ref: WA-59525-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/22325512>. Full image above. Close-up extracts over.



Figure 58: **March 1963.** Bridgens and Company Ltd, New North Road, Auckland. Whites Aviation Ltd :Photographs. Ref: WA-59525-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/22325512>. Full image previous - Extract above.



Figure 59: **March 1963.** Bridgens and Company Ltd, New North Road, Auckland. Whites Aviation Ltd :Photographs. Ref: WA-59525-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/22325512>. Note the 1957 photograph above – the windows along the west elevation have been changed. Full image previous - Extract above.



Figure 60: **March 1963**. Kiwi Bacon Company Ltd industrial premises, Auckland. Whites Aviation Ltd :Photographs. Ref: WA-59540-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/22860837>. Note the 1957 photograph above – the windows along the east elevation (south of the 1947 extension to the street façade) have been changed. Full image. Close-up extract below.



Figure 61: **March 1963**. Kiwi Bacon Company Ltd industrial premises, Auckland. Whites Aviation Ltd :Photographs. Ref: WA-59540-F. Alexander Turnbull Library, Wellington, New Zealand. <http://natlib.govt.nz/records/22860837>. Extract. Full image above.

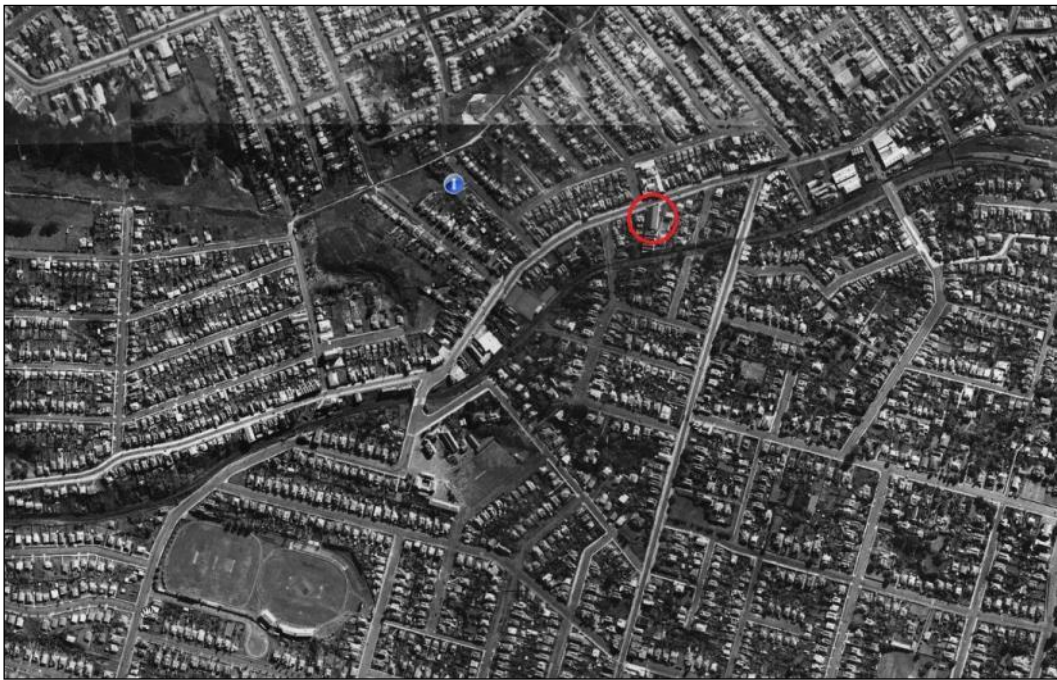


Figure 62: **1940** aerial photograph of the site – showing Eden Park bottom left. Full image top. Close-up extract below. Auckland Council GIS November 2016.



Figure 63: **1940** aerial photograph of the site – showing Eden Park bottom left. Full image above. Close-up extract. Auckland Council GIS November 2016.

RECENT DEALS for June 1

1:00 PM Wednesday Jun 1, 2016

Commercial Property

SHARE:



This character office building in Kingsland sold for \$8,228,888.

Kingsland character building

A 1754sq m fully leased two-level character office building on a 2202sq m Mixed Use under developed land holding at 326-328 New North Road, Kingsland, has sold for \$8,228,888 at a 6.45 per cent yield through Alan Haydock of Bayleys Auckland. The building, which is occupied by multiple tenants, is on an 1116sq m title, with a second 1086sq m title providing car parking for 40 vehicles.

Figure 64: 2016 'Recent Property Deals' – NZ Herald online – 1 June 2016.
http://www.nzherald.co.nz/property/news/article.cfm?c_id=8&objectid=11648911.

Appendix 6: Site photographs



Figure 65: Site context – Looking east along New North Road (towards the Ian McKinnon Drive and Dominion Roads overpass). The subject site is on the right – opposite the former Kiwi Bacon Company. Photo: Google Maps October 2015.



Figure 66: Site context – Looking west along New North Road (towards Kingsland). The subject site is on the left – opposite the former Kiwi Bacon Company. Photo: Google Maps October 2015.



Figure 67: The building façade October 2016 – Taken from the northern footpath on New North Road – looking due south. The façade is largely intact save for the original 'Bridgens & Company Ltd' 3-dimensional lettering. Note the façade of the 1947 addition – to the left (east) of the original factory building does not align with original plans – but does align with photos (above) from 1957 – the assumption is that the plans were modified prior to construction. Photograph: Blair Hastings.



Figure 68: The building's street and western façades October 2016 – Taken from the northern footpath on New North Road – looking southeast. The street façade is largely intact. Western façade contains steel window changes sometime between 1957 and 1963. The northern most window bay on the ground floor has been removed to accommodate the principal entrance into the shared office space. Photograph: Blair Hastings.



Figure 69: Close-up photograph of the junction formed by the 1947 extension to the original street façade. The stripped back classical detailing is in evidence either side of the junction – the later addition subtly in scale and form - Photograph: Blair Hastings.



Figure 70: Close-up photograph of the both the junction formed by the 1947 extension to the original street façade and part of the gable end of the original factory building. The restrained but high contrast paint scheme is well specified to highlight the strong geometric forms of the stripped back classical façade. Patching in the rendered façade is clearly evident where the lettering was removed between the decorative rosettes - Photograph: Blair Hastings.



Figure 71: The west elevation October 2016 –The current steel windows were changed from the original smaller sashes sometime between 1957 and 1963. The rough openings are assumed to be original in size. The recess to the right of the photograph is to the original two storey toilet block – the windows top-right may be original. The window in the rear or south facing ‘return’ into the recess does not appear on original plans. Photograph: Blair Hastings.



Figure 72: The west elevation October 2016. Shows the steel windows changed sometime between 1957 and 1963. The northern most window bay on the ground floor has been removed to accommodate the principal entrance into the shared office space – for which the building is now used. Photograph: Blair Hastings.



Figure 73: Close-up of the parapet return to the street elevation (forming part of the west elevation) - October 2016. Photograph: Blair Hastings.

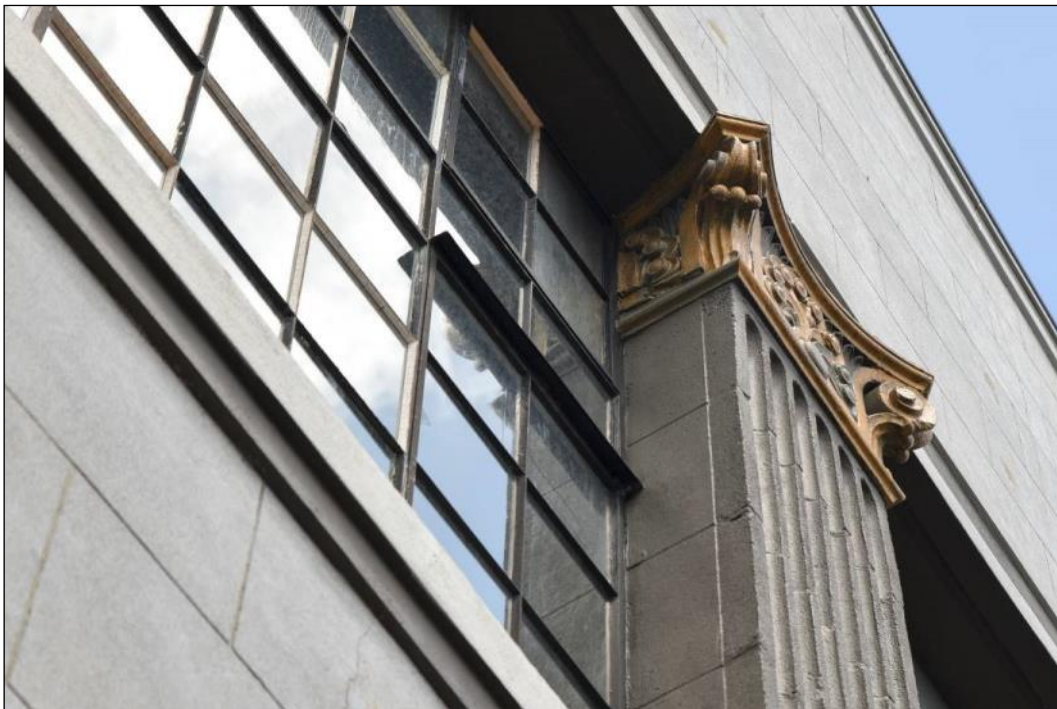


Figure 74: Close-up of one the square fluted columns on the main street elevation showing the painted ionic capital - October 2016. Photograph: Blair Hastings.



Figure 75: Close-up of the original entrance way / door frame (left) – the door has most likely been changed. Close-up of the base of one the square fluted columns on the main street elevation showing the 'restraint' or stripped back classical detailing (right) - October 2016. Photograph: Blair Hastings.



Figure 76: Close-up of the original steel window with opening top sash in the 1947 section of the main building façade (left) The entrance to Greenleaf Organics Café showing the new glazed 'shop-front' and street signage but generally otherwise intact original façade (right) - October 2016. Photograph: Blair Hastings.



Figure 77: Close-up of the entrance way / door frame on New North Road. The photograph evidences the generally good condition of the entire façade - October 2016. Photograph: Blair Hastings.



Figure 78: Close-up of one of the window bays – between the square fluted columns – above the original entrance door - October 2016. Photograph: Blair Hastings.



Figure 79: Close-up minor cosmetic deterioration in the main street façade – such ‘damage’ is relatively isolated and minor (left). East façade showing original steel window bays as seen on the 1947 plans (right) - October 2016. Photograph: Blair Hastings.



Figure 80: Close-up of the steel windows in the eastern façade of the 1947 extension (now Greenleaf Organics Café). Note the ‘simplified’ detailing to exclude proud sills when compared to the original 1929 building - October 2016. Photograph: Blair Hastings.



Figure 81: Close-up of the steel windows in the main street façade – showing early or original internal brass window fittings and stays - October 2016. Photograph: Blair Hastings.



Figure 82: Close-up of the original entranceway – the door sill tiling appears to have been modified and the door is new – or it is possible that the original has been over-clad - October 2016. Photograph: Blair Hastings.



Figure 83: Close-up of the toilet bay at the rear of the original factory building. The three first floor windows in the centre of the photograph may be original – there is a faint plaster line above them which may indicate the roofline of the original block. The window above is within the extension to the rear, subdivided and strata titled as a separate and independent property. The subject site / building ends at the underside of the structural ceiling to the first floor toilet. October 2016. Photograph: Blair Hastings.



Figure 84: Close-up of the ground floor of the recessed toilet bay at the rear of the original factory building, in the west elevation. The three windows are likely original – October 2016. Photograph: Blair Hastings.



Figure 85: The 'courtyard' to the rear of the 1947 extension showing the west façade of the original 1929 factory building. The escape stairs were part of the aforementioned extension built in the 1940s. The steel windows replaced the original windows sometime between 1957 and 1963 – October 2016. Photograph: Blair Hastings.



Figure 86: Both photographs show the restrained but strongly geometric detailing within the main street façade. The right hand photograph shows a modern cast iron railing / fence connected to a modern gatepost just to the west. Various other street amenity servicing is also evident – October 2016. Photograph: Blair Hastings.



Figure 87: Current aerial photograph of the site – Imagery Copyright Google, Map data 2016.

Appendix 7: Drawings

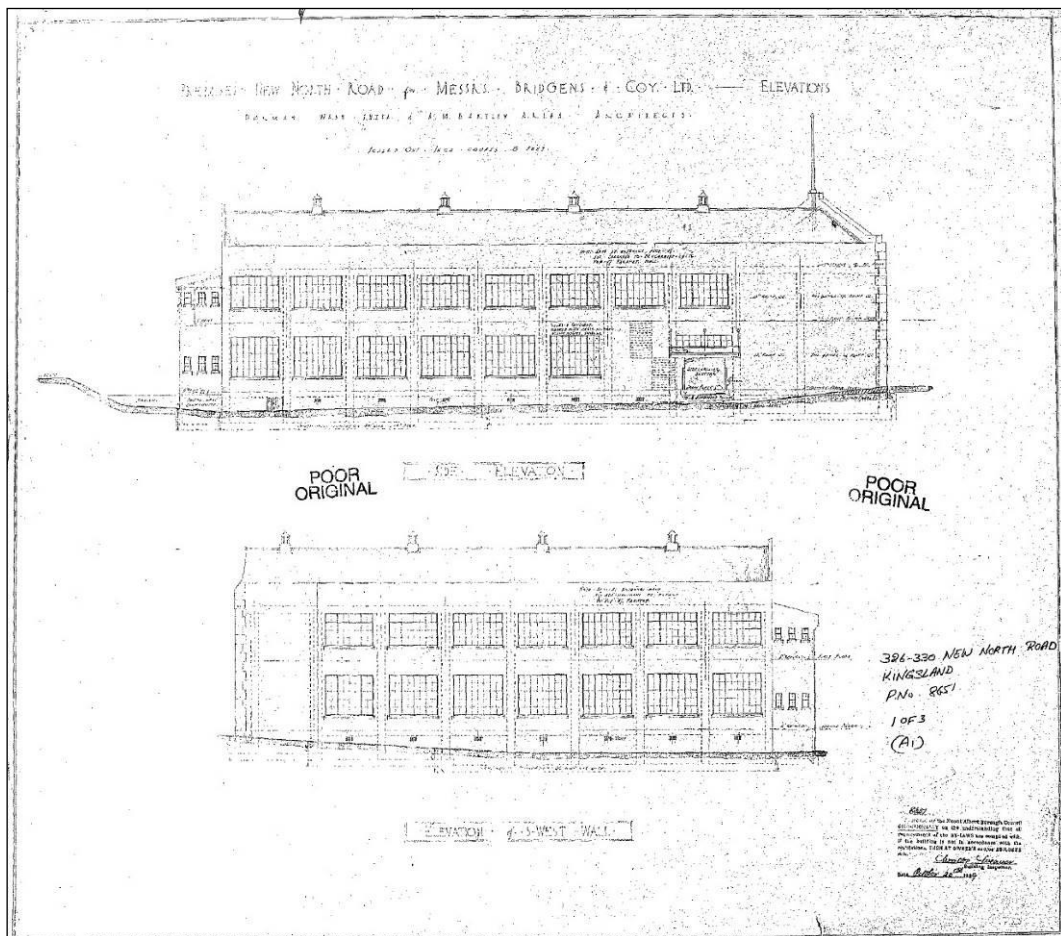


Figure 88: 1929 Drawings for 'Premises New North Road for Messrs Bridgens and Coy Ltd' - by Wade and Bartley Architects: West and East Elevations. Auckland Council Filenet. Accessed October 2016.

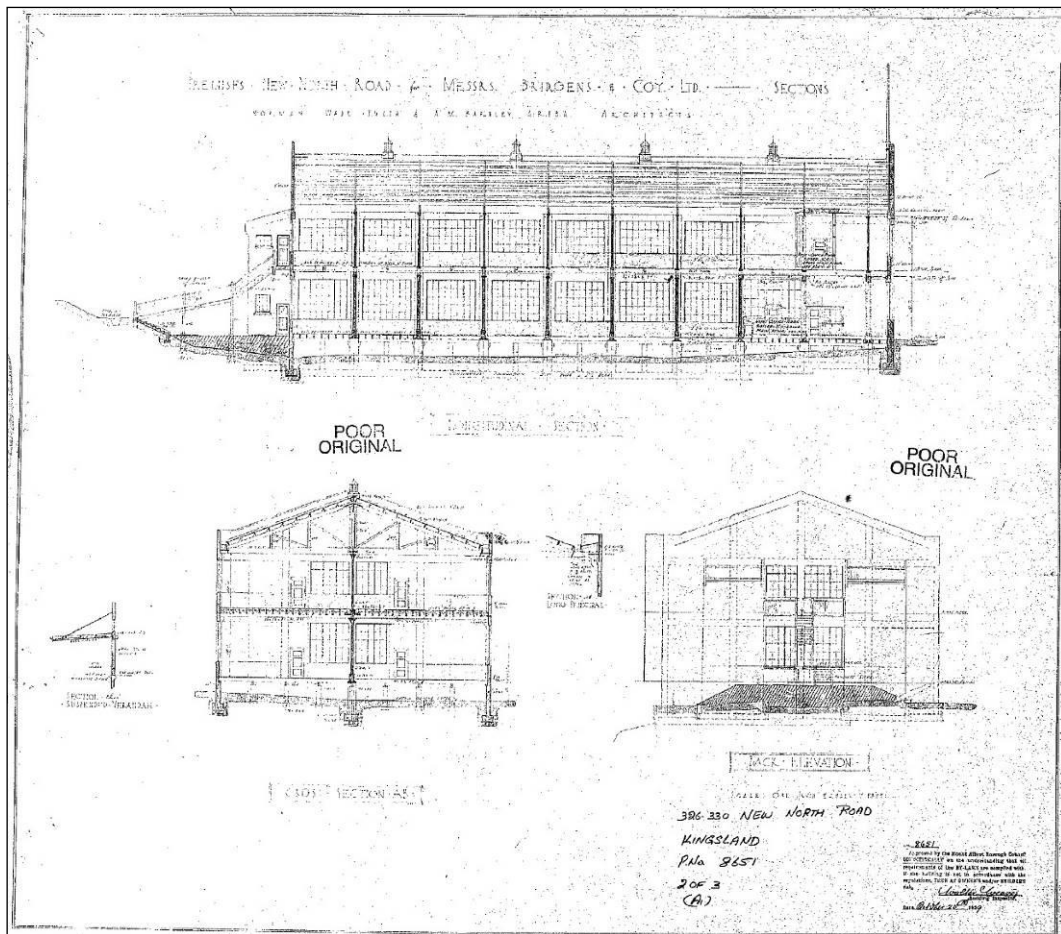


Figure 89: 1929 Drawings for 'Premises New North Road for Messrs Bridgens and Coy Ltd' - by Wade and Bartley Architects: Sections and Rear Elevation. Auckland Council Filenet. Accessed October 2016.

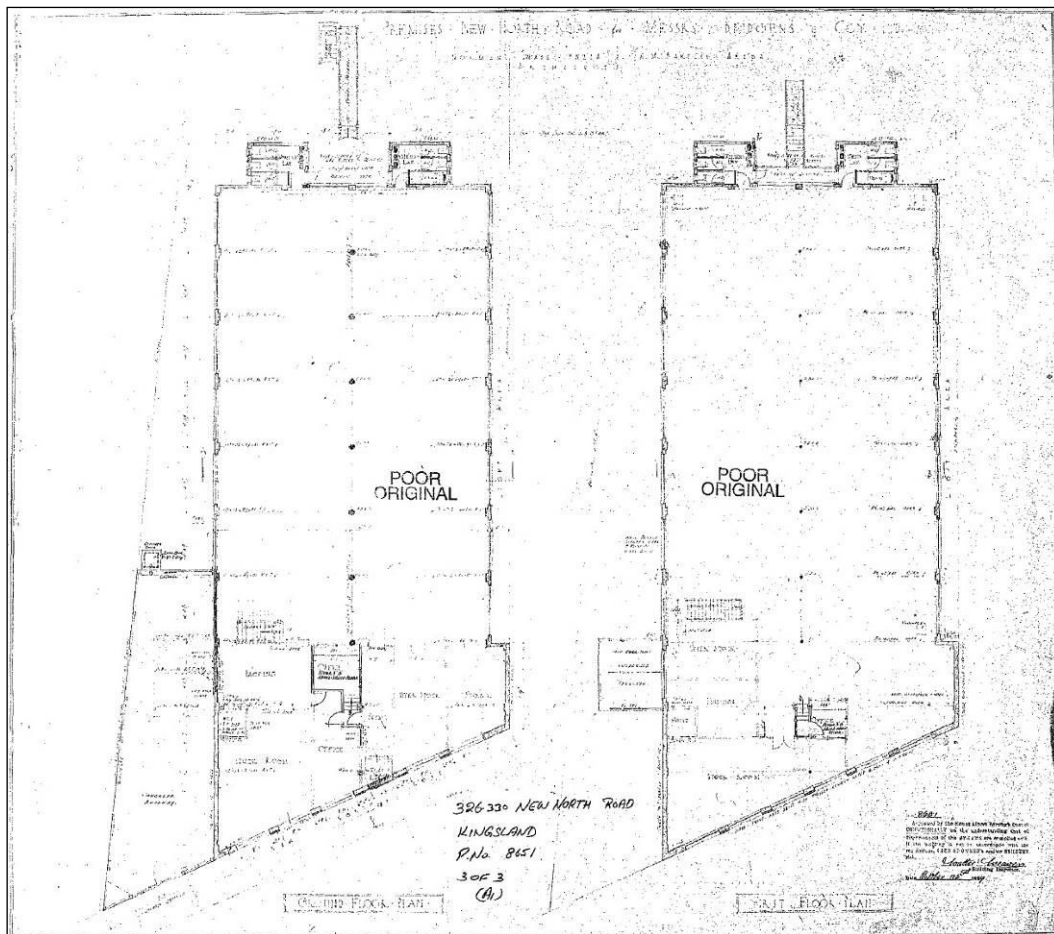


Figure 90: 1929 Drawings for 'Premises New North Road for Messrs Bridgens and Coy Ltd' - by Wade and Bartley Architects: Ground and First Floor Plans. Auckland Council Filenet. Accessed October 2016.

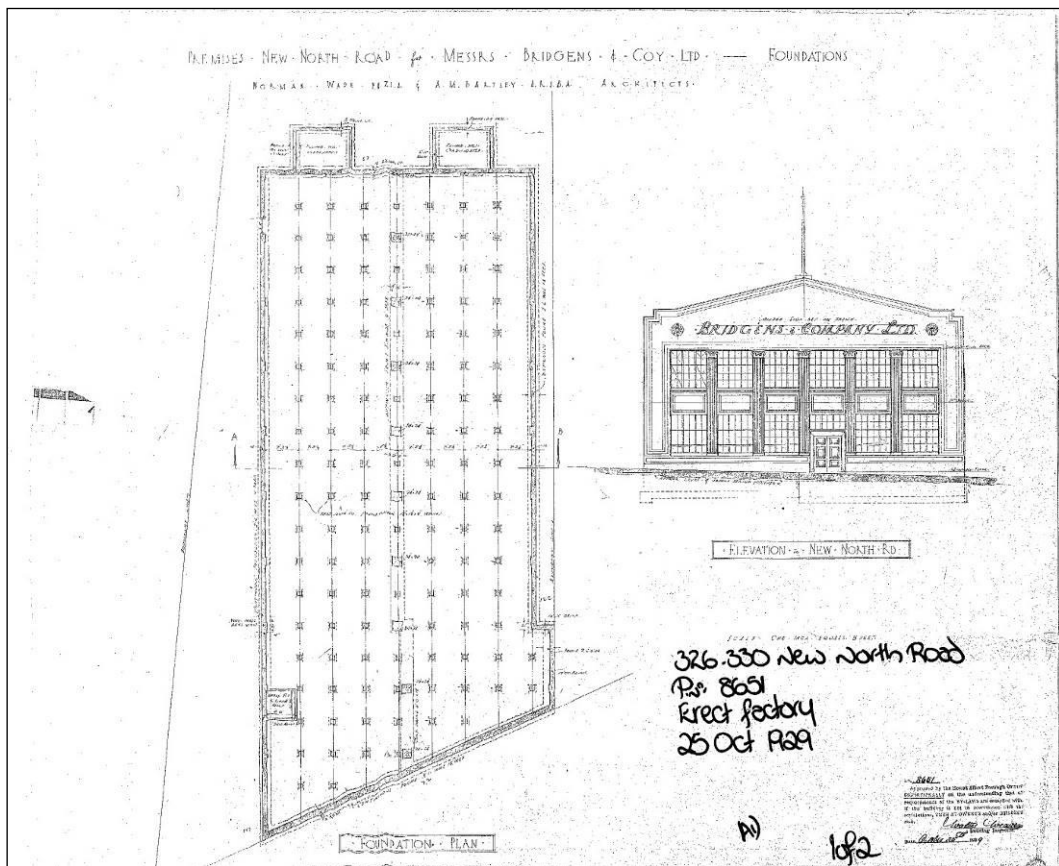


Figure 91: 1929 Drawings for 'Premises New North Road for Messrs Bridgens and Coy Ltd' - by Wade and Bartley Architects: Foundation Plan and Front (or street) Elevation. Auckland Council Filenet. Accessed October 2016.

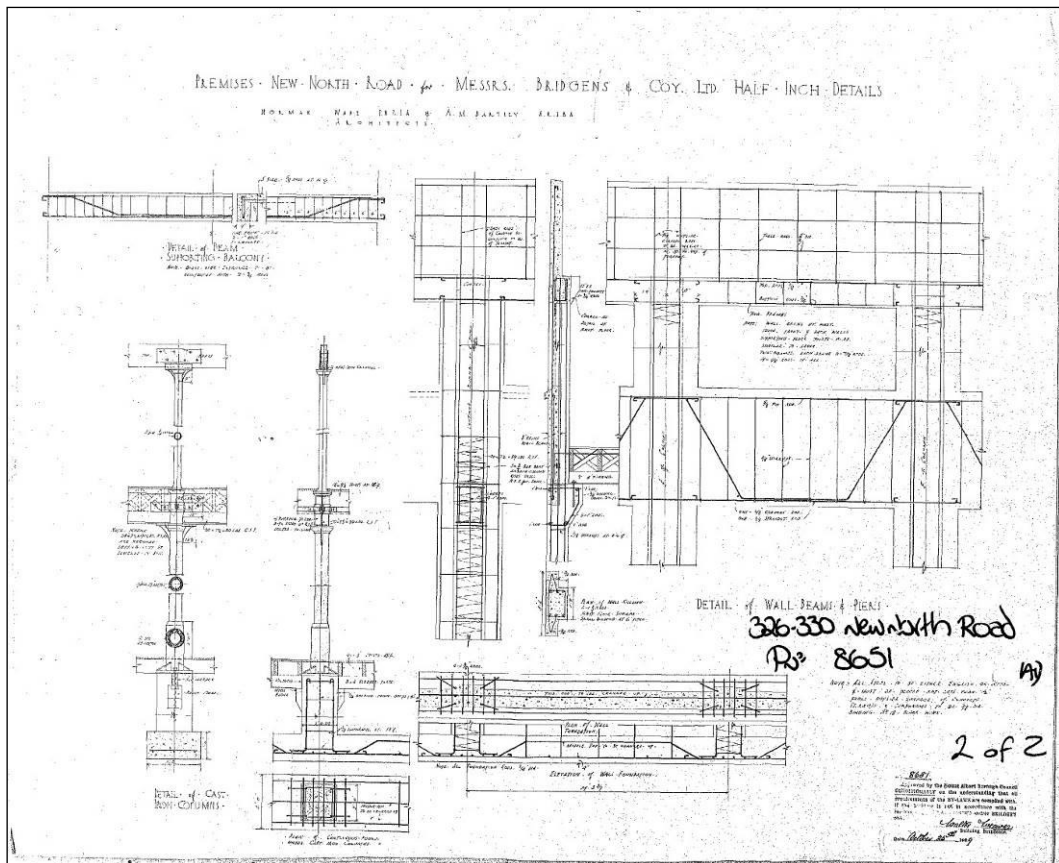


Figure 92: 1929 Drawings for 'Premises New North Road for Messrs Bridgens and Coy Ltd' - by Wade and Bartley Architects: Structural Details. Auckland Council Filenet. Accessed October 2016.

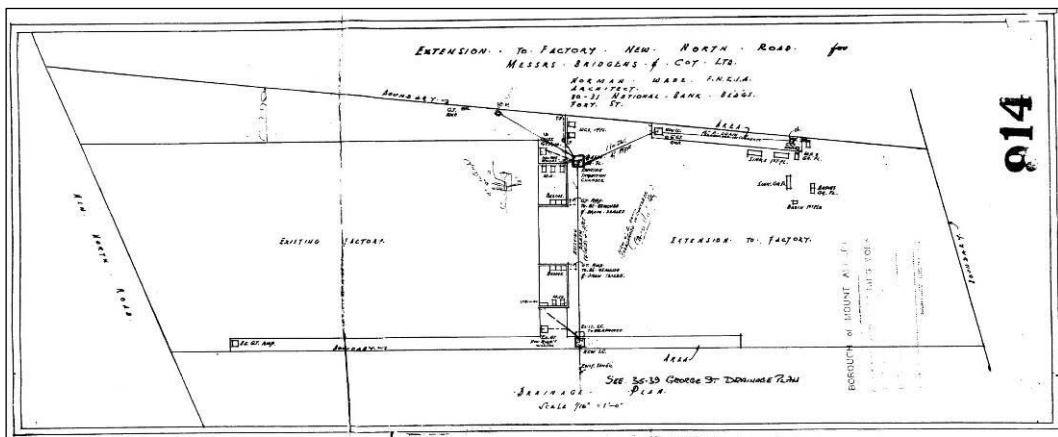


Figure 93: 1929 Drawings for 'Extensions to Factory New North Road for Messrs Bridgens and Coy Ltd' - by Norman Wade, Architect: Drainage plan. The extension suffered major fire damage in 1998. It was redeveloped by Parker Brothers Ltd. The new building was then subdivided from the original factory building in 2000 – It is now therefore independent of the subject site. Auckland Council Filenet. Accessed October 2016. Full image. Close-up extract below.

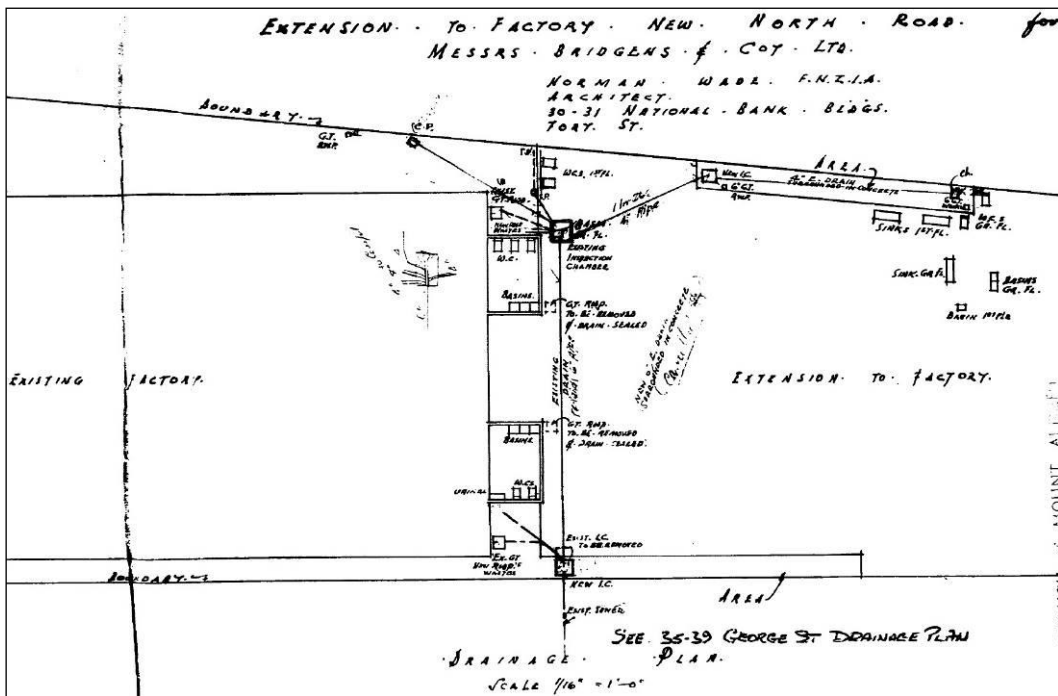


Figure 94: 1929 Drawings for 'Extensions to Factory New North Road for Messrs Bridgens and Coy Ltd' - by Norman Wade, Architect: Drainage plan. The extension suffered major fire damage in 1998. It was redeveloped by Parker Brothers Ltd. The new building was then subdivided from the original factory building in 2000 – It is now therefore independent of the subject site. Auckland Council Filenet. Accessed October 2016. Extract. Full image above.

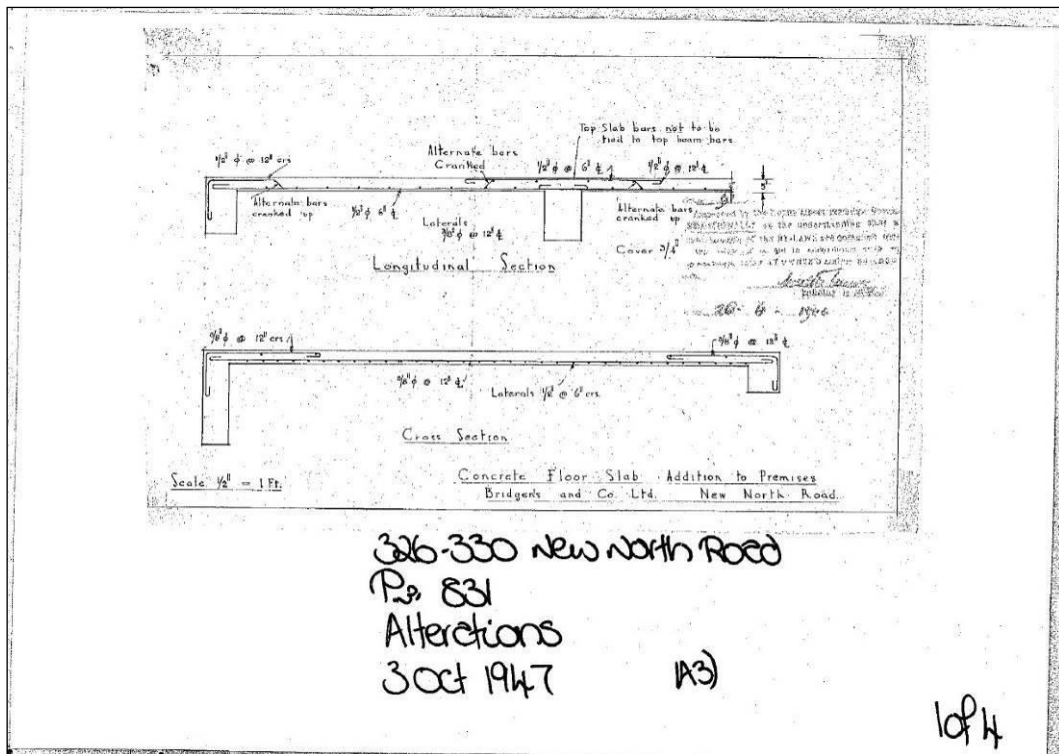


Figure 95: 1947 Drawings for 'Extensions to Factory New North Road for Messrs Bridgens and Coy Ltd' - by Norman Wade, Architect: Structural Slab Sections. Auckland Council Filenet. Accessed October 2016.

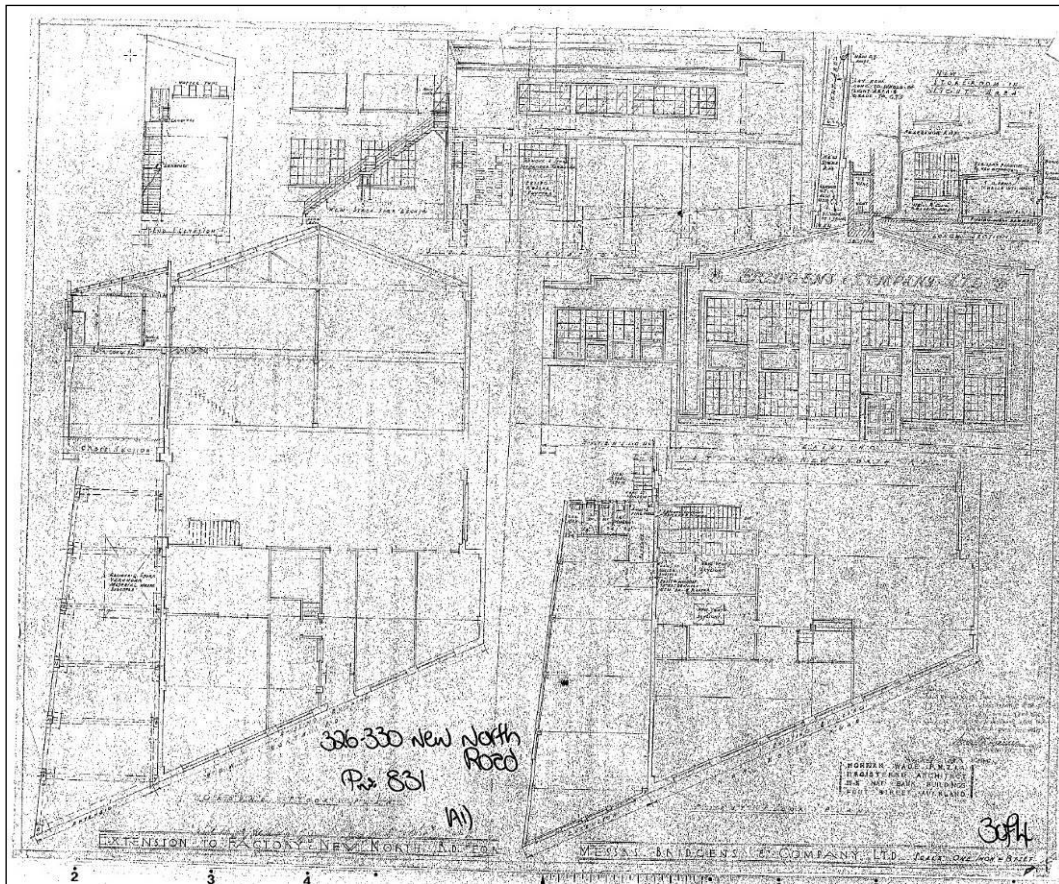


Figure 96: 1947 Drawings for 'Extensions to Factory New North Road for Messrs Bridgens and Coy Ltd' - by Norman Wade, Architect: Plans, Sections and Elevations. Auckland Council Filenet. Accessed October 2016.

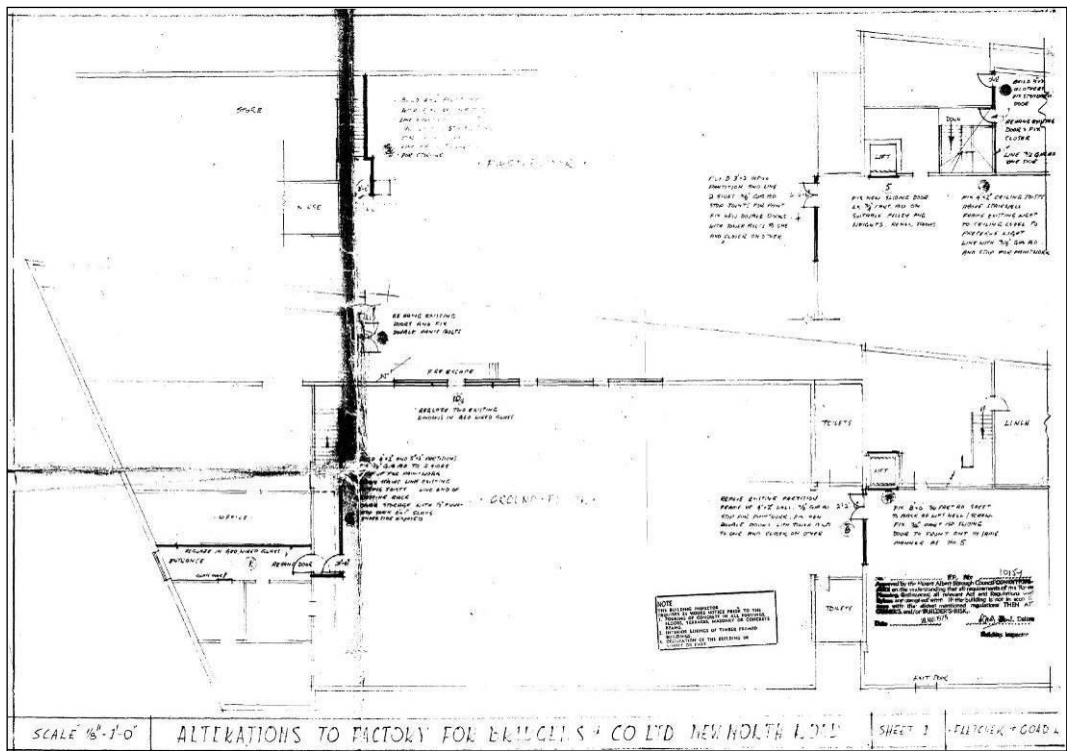


Figure 97: 1975 Drawings for 'Alterations to Factory for Bridgens and Co Ltd New North Road' - by Fletcher and Goad: Plans. Internal alterations only. Auckland Council Filenet. Accessed October 2016.

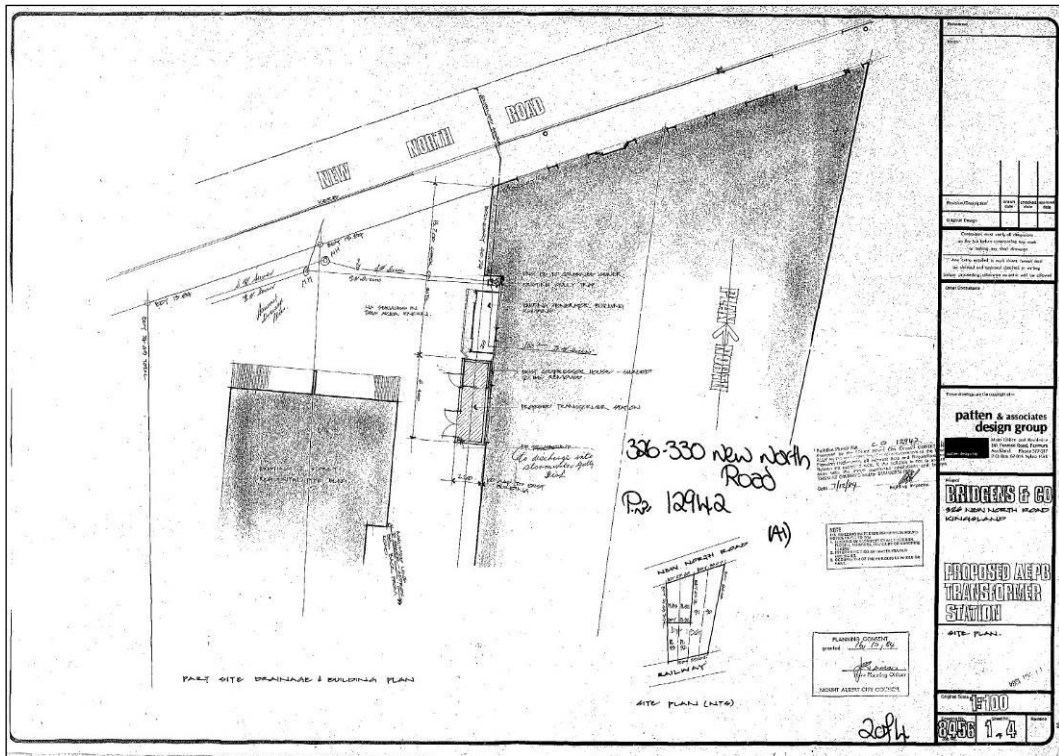


Figure 98: 1984 Drawings for 'Proposed AEPB Transformer Station for Bridgens and Co 326 New North Road' - by Patten & Associates Design Group: Plans. Auckland Council Filenet. Accessed October 2016.

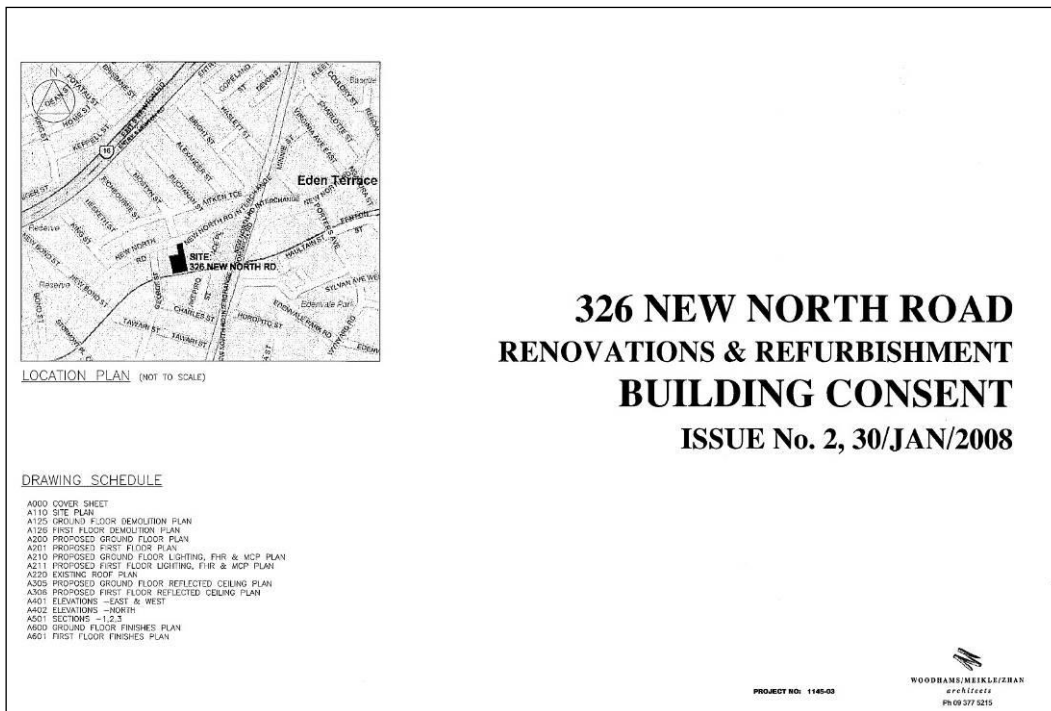


Figure 99: 2008 Drawings for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: Drawing Schedule. Mainly internal works for adaptive reuse as offices. Auckland Council Filenet. Accessed October 2016.

PART ONE - EXTERIOR WORKS

architects

1.00 General

1.01 External Walls – to North (New North Rd), East and West sides: Water blast all structure to remove damaged or unsound paint finish.

Plaster and make good any cracks, holes, blemishes etc, sand ready to access paint finish. Prime and seal all new works, damaged and make good work's ready for whole of exterior (except portions of north face as shown on plans) to accept sealer coat and two coats exterior acrylic paint finish.

1.02 Windows – Exterior Face: Water blast all ground and first floor exterior windows (to all north, east and west perimeter walls) and frames to remove damaged or unsound paint finish. Grind and sand back any blemishes, rust, cracks etc ready to access paint finish. Prime and seal all new and or damaged works, make good all windows ready for to accept sealer coat and two coats exterior acrylic paint finish. Replace any damaged or broken glazing to match adjoining glazing.

1.03 Roofing – no changes required

1.04 Gutters, down pipes, overflows - no changes required

1.05 Fire Egress Stairs - no changes required

1.60 Entry Landing – Uplift existing terracotta tiles to exterior landing and stairs. Prepare concrete substrate ready to accept new tiles. Provide and lay new tiles in accordance with specification.

1.07 Exterior Services and Plant - no changes required

1.08 Entry Stairs and Landing

Remove existing terracotta tiles and existing handrail. Make good existing slab and prepare ready to accept 600x 600 x 40mm paving tiles. Lay tiles over. Provide new balustrade and handrail to all of landing and stairs.

1.09 Signage – Remove all existing signage, supports, brackets etc from building exterior. Retain existing 326 letters on East and West sides. Also retain no 1 on West Wall and the tenant signs by the front doors.

1.10 Parking – no changes required

1.11 Landscaping – no changes required

1.12 Courtyard to East Side – no changes required

Figure 100: 2008 Extract from Specification for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: Calls for only limited exterior works – to clean, make good and otherwise prepare surfaces for new decorative paint finishes. Auckland Council Filenet. Accessed October 2016.

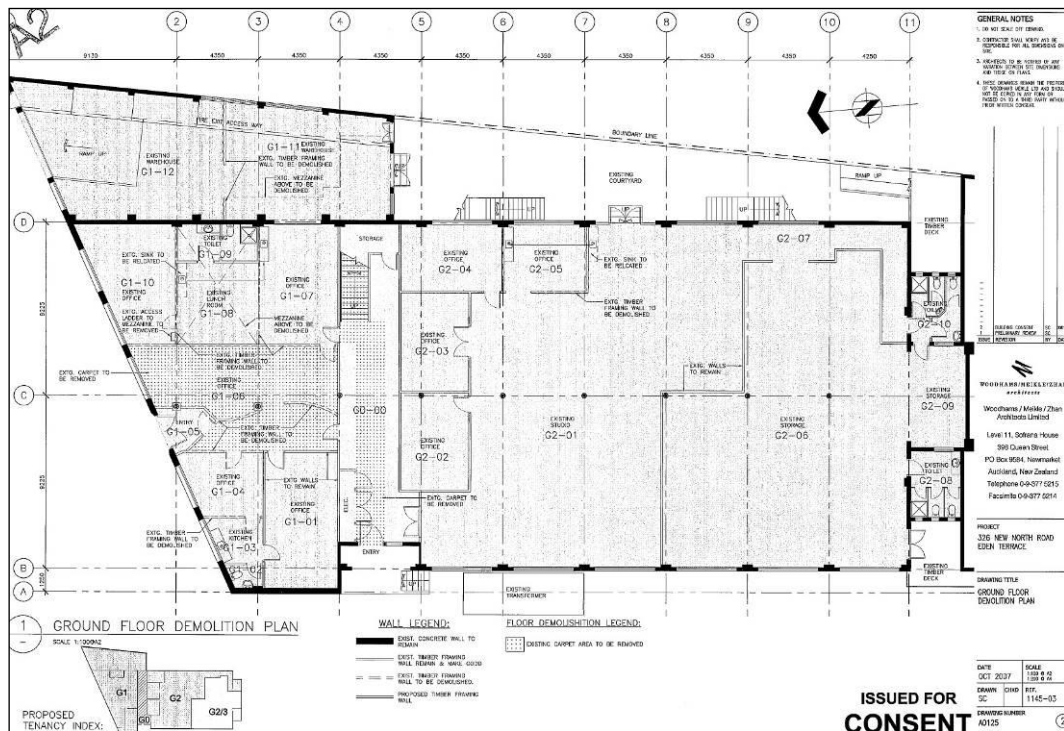


Figure 101: 2008 Drawings for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: Ground Floor Demolition Plan. Auckland Council Filenet. Accessed October 2016.

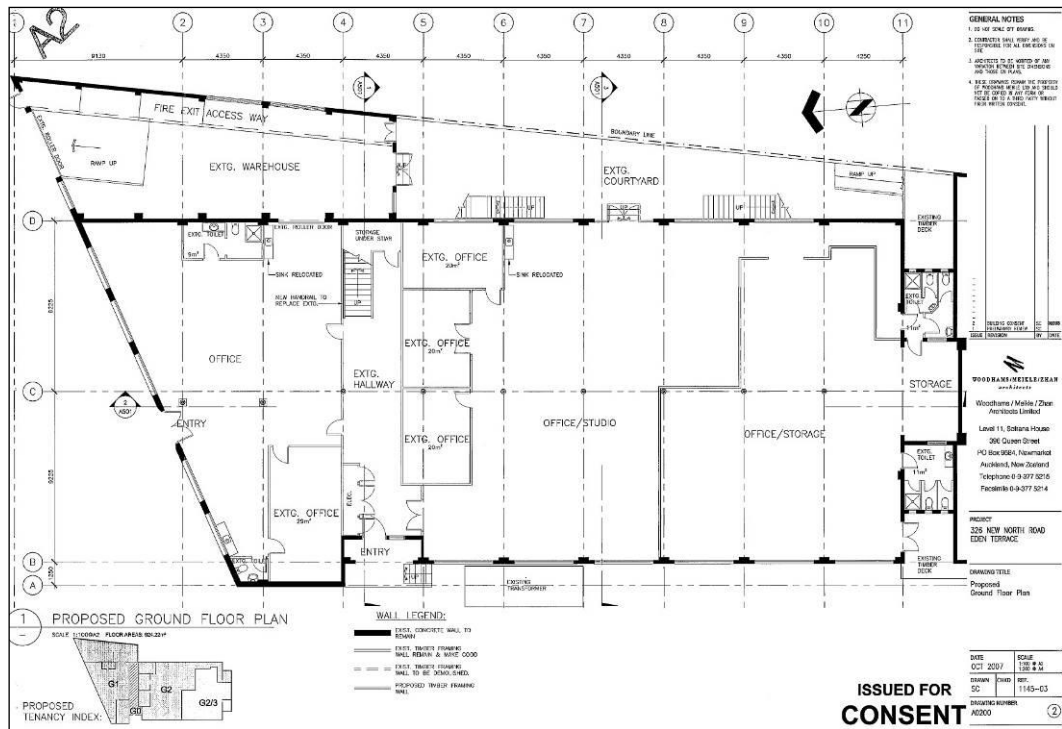


Figure 102: 2008 Drawings for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: Proposed Ground Floor Plan. Auckland Council Filenet. Accessed October 2016.

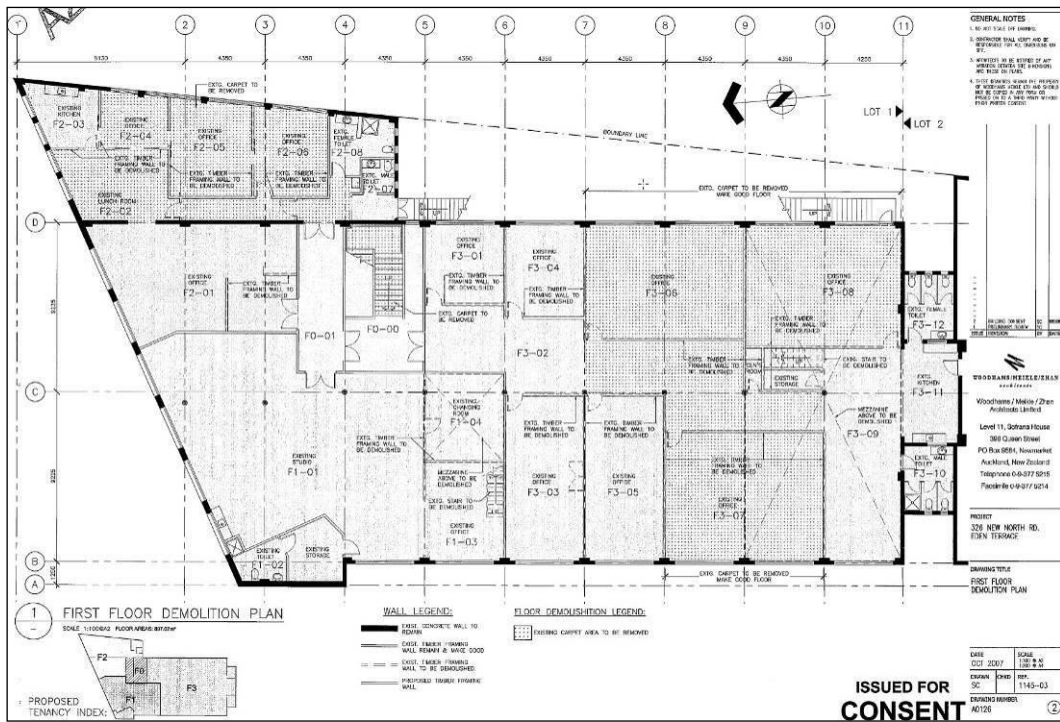


Figure 103: 2008 Drawings for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: First Floor Demolition Plan. Auckland Council Filenet. Accessed October 2016.

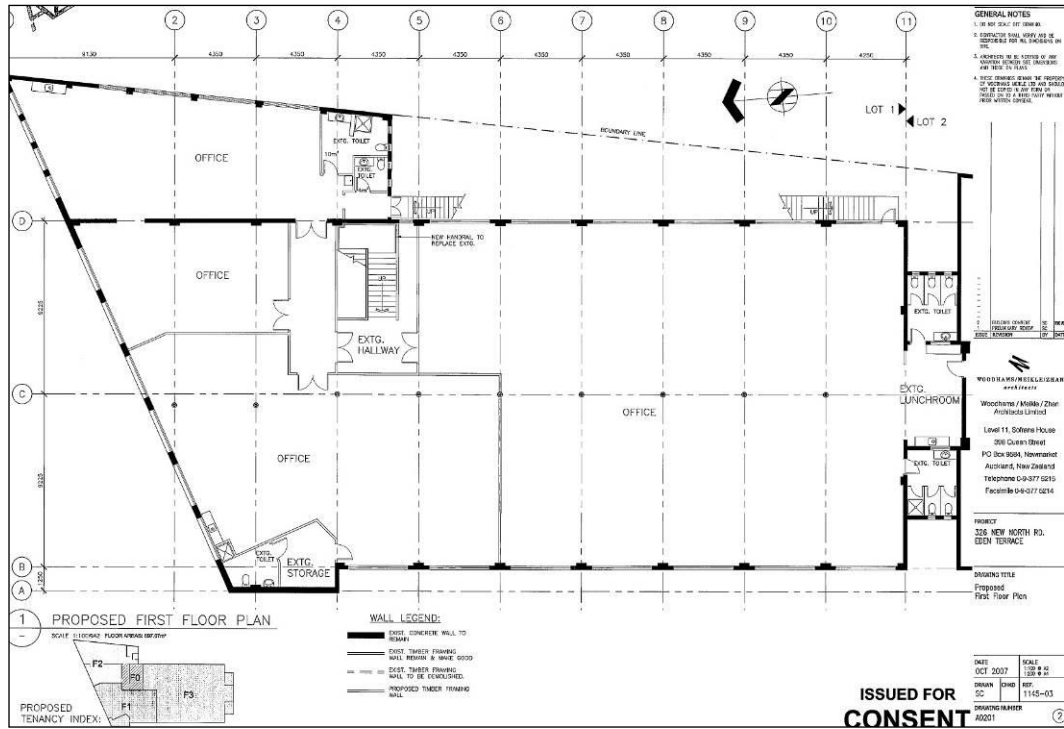


Figure 104: 2008 Drawings for '326 New North Road Renovations & Refurbishment Building Consent' - by Woodhams/Meikle/Zhan Architects: Proposed First Floor Plan. Auckland Council Filenet. Accessed October 2016.

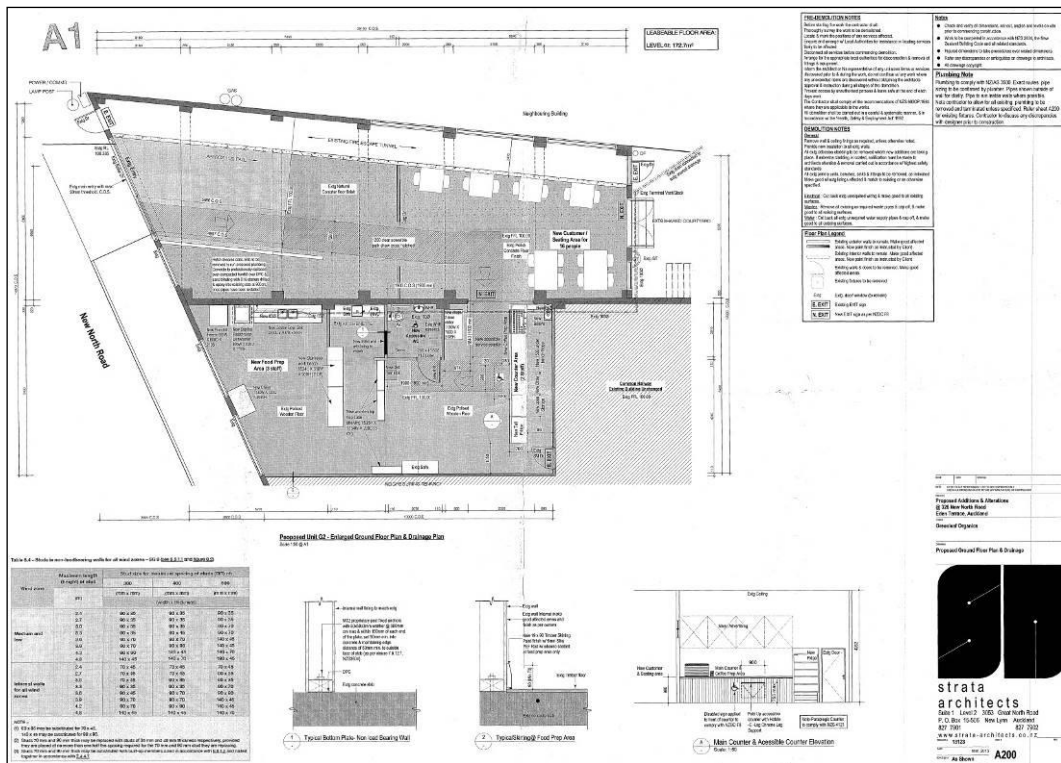


Figure 105: 2015 Drawings for 'Proposed Additions and Alterations @ 326 New North Road' for Greenleaf Organics [Café] - by Strata Architects: Proposed Ground Floor Plan and Drainage. Auckland Council Filenet. Accessed October 2016.

Appendix 8: Previous Evaluation and Streetscape Study Record

B3		DRAFT - EVALUATION				
AUCKLAND CITY ISTHMUS - HERITAGE OBJECT, FEATURE, OR PLACE						
Name(s)	Commercial (Bridgens Building)					
Address	New North Road, 0326, Eden Terrace					
Unique ID	00001109					
1 ARCHITECTURE		(Maximum 35)				TOTAL
A Style	1920's factory design, stripped classical style. Good example.	20 E	10 VG	5 G	0 F/P	5
B Construction	Brick construction, plastered, with timber truss roof.	15 E	8 VG	4 G	0 F/P	0
C Age	Year constructed C.1929	10 E	5 VG	2 G	0 F/P	0
D Architect	Wade & Bartley	8 E	4 VG	2 G	0 F/P	2
E Design	No significant features, although it does feature double-height columns and some plaster featurework.	8 E	4 VG	2 G	0 F/P	0
F Interior	The interior is apparently still largely intact, although there was damage in a fire 1998. Not currently sighted.	4 E	2 VG	1 G	0 F/P	0
						7
2 HISTORY		(Maximum 45)				
G Person	Edward George Bridgens - originator and owner until 1944, the family continuing to operate manufactory until 1997 (secondary/closely connected).	45 E	20 VG	10 G	0 F/P	20
H Event	No particular associations.	45 E	20 VG	10 G	0 F/P	0
I Context	Associated with development in Kingsland and closely associated with new Zealand footwear, prior to manufacture moving overseas.	40 E	20 VG	10 G	0 F/P	10
						30
3 ENVIRONMENT		(Maximum 5)				
J Continuity	Compatible with surrounding character of the area.	5 E	3 VG	2 G	0 F/P	2
K Setting	Building takes up most of site.	3 E	2 VG	1 G	0 F/P	0
L Landmark	Not a particularly conspicuous landmark.	5 E	3 VG	2 G	0 F/P	0
						2
4 USEABILITY		(Maximum 5)				
M Compatibility	Mixed Use. Compatible with zoning.	3 E	2 VG	1 G	0 F/P	3
N Adaptability	N/A	3 E	2 VG	1 G	0 F/P	0
O Public	N/A	3 E	2 VG	1 G	0 F/P	0
P Services	N/A	3 E	2 VG	1 G	0 F/P	0
Q Cost	Cost of retention about the same as comparable new construction.	3 E	2 VG	1 G	0 F/P	1
						4
5 INTEGRITY		(Maximum 10)				
R Site	Original.	3 E	2 VG	1 G	0 F/P	3
S Alterations	The building is generally original and overall form retained, although some alterations and additions in 1990's.	4 E	2 VG	1 G	0 F/P	1
T Condition	Appears good.	3 E	2 VG	1 G	0 F/P	1
						5
TOTAL						48
Evaluated:		Recommendation:		Category		
By: CAROLYN HILL	Date: 03/02/2006	Interior listed	N			
Reviewed:		Surrounds listed	N			
By:	Date:					
Approved:						
By:	Date:					
Signature: _____						

Figure 106: Draft Evaluation form – Auckland City Isthmus – Heritage Object, Feature, or Place – February 2006 - Council Filenet. Accessed October 2016.


AUCKLAND CITY STREETScape STUDY RECORD FORM		AUCKLAND CITY	
LOCATION / ADDRESS: 326-330 New North Rd, Kingsland		ITEM NUMBER	CATEGORY (IF SCHEDULED)
NAME (IF ANY) NUMBER, STREET, LOCALITY, WARD Bridgens building		LEGAL DESCRIPTION	
BUILDING / STRUCTURAL / OBJECT / SITE USE: <input type="checkbox"/> RESIDENTIAL <input checked="" type="checkbox"/> COMMERCIAL <input type="checkbox"/> OTHER <input type="checkbox"/> INDUSTRIAL <input type="checkbox"/> INSTITUTIONAL <input type="checkbox"/> <input type="checkbox"/> RECREATIONAL <input type="checkbox"/> AGRICULTURAL		OTHER LISTINGS	
HISTORY & KNOWN ASSOCIATIONS Constructed by Bridgens and Co (established 1922), footwear manufactures in 1929. Company formed by EG Bridgens, past president NZ Foot Manufacturers Federation and Footwear Industrial Plan Committee which advised the Government on matters related to footwear manufacturing. He was also President of the Auckland Trotting Club from 1937. Died 31 August 1944 The shoe factory closed down in late 1997. During its busiest periods it produced up to 350,000 pairs of shoes a year and was a major employer. A major fire occurred in the rear of the building during redevelopment in July 1998 Wade and Bartley were the architects for the building constructed circa late 1929 early 1930. Built by Fletcher Construction		ZONING	
REFERENCE SOURCES NZ National Review 15 September 1944, pg 49 Central Leader 6 September 1983, pg 5 NZ Building Record 22 November 1929, page 2 ACC Permit Records Permit No 8651 326-330 New North Road Auckland City Harbour News 15 July 1998, pg 1		REGISTERED OWNER	
PHOTOGRAPHS (HISTORICAL)		ARCHITECT / DESIGNER Wade and Bartley	
STATEMENT OF SIGNIFICANCE Associated with longstanding firm of Bridgens and Company, footwear manufacturers and major local employer. Associated with EG Bridgens local businessman. Designed by prominent architectural practice of Wade and Bartley. Stripped classical style featuring a double height columns, contrasting coloured decorative plasterwork. Interior retains original detailing and finishing including timber trusses, flooring, partitions and main stair.		PERIOD / DATE OF CONSTRUCTION 1929-1930	
		MATERIALS Plastered brick, timber trussed roof, timber framed floor and timber partitioning and stairs	
		CONDITION Good	
		INTEGRITY Generally original condition. Some alterations additions in late 1990s	
AREAS OF SIGNIFICANCE <input checked="" type="checkbox"/> HISTORICAL <input checked="" type="checkbox"/> ARCHITECTURAL <input type="checkbox"/> TECHNOLOGICAL <input checked="" type="checkbox"/> AESTHETIC <input type="checkbox"/> SCIENTIFIC <input type="checkbox"/> SPIRITUAL <input type="checkbox"/> SOCIAL <input type="checkbox"/> TRADITIONAL <input type="checkbox"/> SPECIAL <input type="checkbox"/> RARITY VALUE <input checked="" type="checkbox"/> GROUP VALUE <input type="checkbox"/> OTHER		SIGNIFICANT ELEMENTS New North Road frontage, steel framed joinery, blue and red coloured plaster finish to front façade only, interior original elements and finishes.,	
		CULTURAL HERITAGE VALUE	
		STREETScape VALUE Forms part of a group of historic structures, including the Kiwi Bacon Factory, and two storied timber house adjacent, north of Kingsland which define a gateway to the area.	
		DATE OF SURVEY November 2003 EXTERIOR: Yes INTERIOR: Yes BY: JM& AJ Matthews CHECKED: PHOTOGRAPHED: AJ Matthews	
PREPARED BY: Matthews & Matthews Architects Ltd FOR: AUCKLAND CITY COUNCIL			

Figure 107: Auckland City Streetscape Study Record Form – November 2003 - Council Filenet. Accessed October 2016.