

Historic Heritage Evaluation

*Herne Bay Cruising Club
Sloanes Beach, Herne Bay*



Prepared by Auckland Council Heritage Unit

January 2017

Historic Heritage Evaluation

Herne Bay Cruising Club, Sloanes Beach, Herne Bay

Prepared by Auckland Council Heritage Unit

January 2017

Cover image: 15 November 2016, Anna Boyer

1.0 Purpose

The purpose of this document is to consider the Herne Bay Cruising Club building located at Sloanes Beach, Short Street, Herne Bay against the criteria for evaluation of historic heritage in the Auckland Unitary Plan.

The document has been prepared by Anna Boyer, Heritage Information Advisor, Heritage Unit, Auckland Council. It is solely for the use of Auckland Council for the purpose it is intended in accordance with the agreed scope of work.

2.0 Identification

Site address	Sloanes Beach, Short Street, Herne Bay
Legal description and Certificate of Title identifier	PART BED AUCKLAND HARBOUR SO 67209 AND SEC 1 SO 420557
NZTM grid reference	X: 1753786.27 Y: 5920880.88
Ownership	Crown (land below the MHWS) Herne Bay Cruising Club (building) Auckland Council (Herne Bay Reserve and Short Street)
Unitary zoning	Coastal - Coastal Transition Zone Coastal - General Coastal Marine Zone Open Space - Informal Recreation Zone
Existing scheduled item(s)	
Additional controls	Hauraki Gulf Marine Park Act 2000
Heritage Zealand Taonga details New Pouhere listing	Not listed
Pre-1900 site (HNZPT Act 2014 Section 6)	The place is the site of human activity before 1900 but is not recorded as an archaeological site and has not been assessed to determine if it has archaeological values.
CHI reference/s	18477
NZAA site record number/s	

3.0 Constraints

This evaluation does not include an evaluation of:

- archaeological values of the site and/or
- the importance of the place to mana whenua

The evaluation also does not address current structural integrity. Any comments regarding the condition of the building are based on a visual inspection only and this evaluation is not a condition report.

All fieldwork was conducted from Sloanes Beach. There was no inspection of the interior during the site visit to the building on 15 November 2016.

The evaluation is based on the availability of information provided or able to be sourced at this time, noting that additional research may yield new information.

4.0 Historical summary

See appendix 1 *Herne Bay Cruising Club Sloane's Beach, Auckland: Conservation Plan by Dave Pearson.*¹ The conservation plan includes a detailed history of the building, the following is a summary.

The Richmond Cruising Club was established in 1903 by a small group of keen yachting families. They were originally based at Rawene Avenue, Coxs Bay at the boatshed of James Donald. By 1908 the club had grown sufficiently to warrant a more permanent facility and the club sought to erect a boatshed at Sloanes Beach, Herne Bay.

A permit for a boatshed was granted by Auckland City Council to the Richmond Yacht Club, only to be refused by the Auckland Harbour Board. A few years later the club tried again; applied for a permit from the council and were granted consent provisional on paying an annual rent of one shilling. Construction started but was soon halted after objections from owners of three private sheds on Sloanes Beach caused the council to order works to stop. Negotiations with the owners of the sheds failed to reach a resolution. The club began discussions with the Harbour Board and agreed to move the building further towards the sea.



Figure 1: Opening of the Richmond Cruising Club clubhouse at Sloanes Beach, 8 November 1913. Auckland Council Archives. ACC439/2m 55 MARINE PARADE (FORMERLY RICHMOND YACHT CLUB BOATSHED SLOANES BEACH AT END OF BELLA VISTA ROAD), 1965-1966

Things were looking promising and works on the new boatshed started again and then promptly stopped when the Harbour Board changed their mind.

Eventually it was discovered that the permissions for building the boatshed was actually the responsibility of the Marine Department which did grant the consent for the boathouse in 1913. The boathouse was opened on 8 November 1913.

In their new home at Sloanes Beach the club continued to grow and prosper in the Auckland yachting scene. A second storey was added to the building in 1932 after successful fundraising efforts. The work was offered to unemployed members of the club at a rate of 12/- per day.



Figure 2: Richmond Cruising Club., Unknown, photographer, PH-NEG-C30482. Seek permissions to use Auckland Museum http://www.aucklandmuseum.com/collections-research/collections/record/am_library-photography-51332?k=richmond%20cruising&ordinal=1

By the end of the 1930s many Auckland yacht clubs were considering moving to the new reclamations at St Marys Bay (Westhaven). For Richmond Yacht Club they were increasingly isolated away from the action at Herne Bay and decided to move. In 1940 the Richmond Yacht Club opened their new building at the new reclamation at Westhaven. However the club retained the boatshed at Sloanes Beach and it was used as a locker room for club members.

The club retained ownership of the boatshed until 1965. At this point the Sea Ranges (a girl guide group) offered to buy the building for £500. The offer was accepted subject to the city council granting consent to alter the building. The consent was not granted. The building was then sold to the newly formed Herne Bay Cruising Club. The Herne Bay Cruising Club still have ownership and use of the building today as a clubroom and storage.

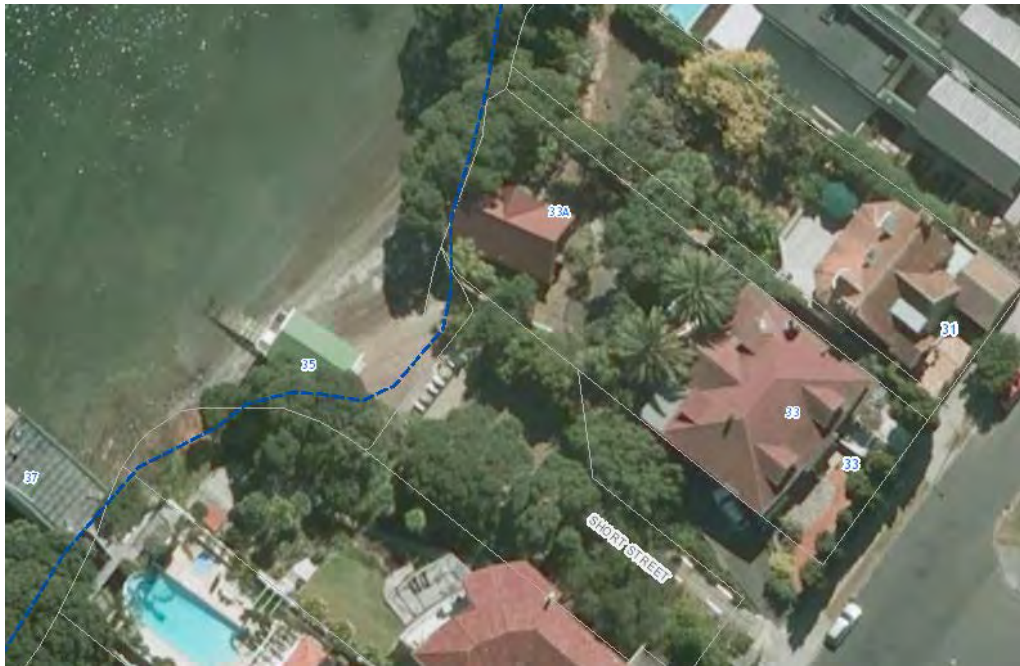


Figure 5: Site map showing the approximation of the Mean High Water Springs (MHWS) location (GeoMaps 2017)

Context

Herne Bay Cruising Club is located on Sloanes Beach, and can be accessed from Short Street, a dedicated but unformed road, off Marine Parade, Herne Bay. The building is open to the tidal beach, is below the mean high water spring (MHWS) (see Figure 4) and is approximately 50m from the low water mark.

The building is accessed by way of pedestrian walkway down Short Street, known as Herne Bay Reserve. A boardwalk leads from a stone abutment to the clubhouse. The building is on the beach facing North West into the harbour.

There are houses of varying ages and styles located on the slopes above and other boatsheds on the beach.

Alterations and additions

A second storey was added to the boatshed in 1932. It appears that the roof was lifted off and then reused. A join between storeys can be seen in the slight change of the width of the timbers used (see Figure 29). Apart from this major change to the building there have been various minor works done to upgrade and maintain the building. Including the replacement of some rotten boards, reinforcing of some of the struts, replacement of the wooden boardwalk, replacing the original single door with glazed French doors on the upper level, etc.



Figure 6: View from the bay looking back at the boatshed (15 November 2016. Anna Boyer) View from Marine Parade Reserve of the boatshed (15 November 2016. Anna Boyer)

Design

The boatshed is a small two storey building approximately 52m² with the ramp adding an additional 18m². The building is a simple rectangle with a gable roof. On the lower floor there is a single door leading into the storage area. Stairs on the same elevation lead to the upper level with a single door. The north and south elevations are similar with two single sash windows at the upper level.

The west elevation looks out to the harbour. The upper level has a deck with double French doors leading out and on the lower level a pair of doors lead from the storage area to the boat ramp. There is a flagpole supported by a bracket below the gable and a weather station pole secured to the deck.

The boatshed is a simple design with distinct bungalow influences; including the ventilator grill surrounded by wooden shingles located above the upper floor a detail repeated at the rear and the exposed rafters at the eaves.

Current use and condition

The building is used by the Herne Bay Cruising Club as clubrooms and storage for boaties and members of the community.

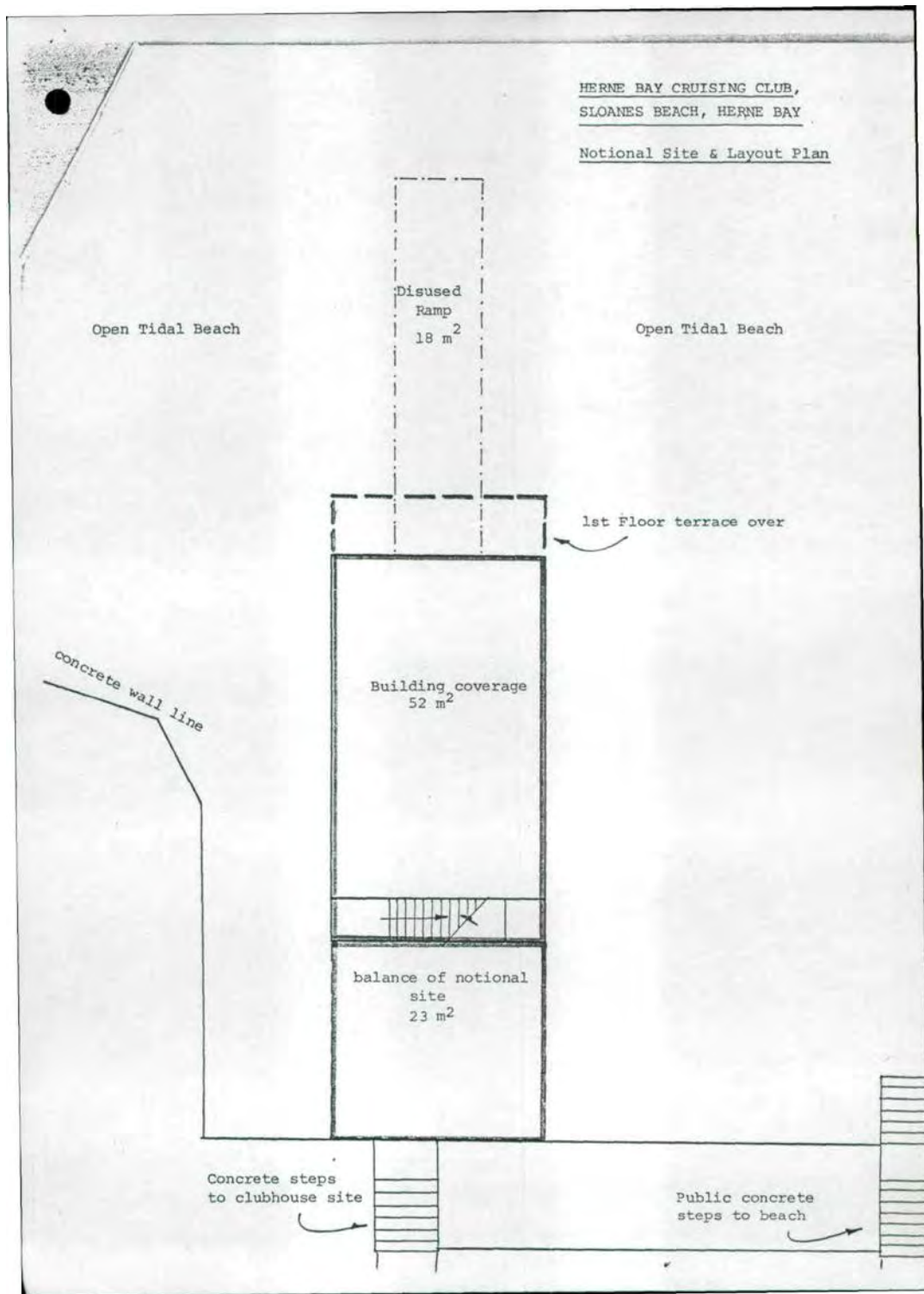


Figure 7: Plan of building from ARC site pack, 2004

Key physical features includes but is not limited to:

- Boat ramp
- Shingled gable ends
- Exposed rafters
- External window and door layout
- Flag pole
- Corrugated iron roof
- Horizontal timber weatherboards
- Piles, joists and bearers elevating the building above the beach

6.0 Comparative analysis

See appendix 2 Auckland regional boatshed study 2007 by Matthews & Matthews Architects Ltd for a more in depth analysis of boatsheds in the Auckland region.

Boatsheds are an important part of New Zealand's maritime history. Prior to the extensive development of road based transportation in the 1950s the harbours and waterways of Auckland provided both transportation and recreation. Boatsheds for both commercial use and personal use have been a common feature along Auckland's coastline since the late nineteenth century and early twentieth century.

Most of the boatsheds are associated with private recreational use. Although many boatsheds would have been constructed for use by yacht clubs, Herne Bay Cruising Club is one of the few of these remaining from the early twentieth century.²

There are three other comparable buildings listed in the Auckland Unitary Plan Schedule 14.1 Schedule of Historic Heritage which are worth mentioning, these are;

- Aotea Sea Scouts Hall, 1 Orpheus Drive, Onehunga (UPID 02598)
- Calliope Sea Scouts Hall, 25 King Edward Parade, Devonport (UPID 02125)
- Takapuna Boating Club, 17 Sir Peter Blake Parade, Bayswater (UPID 01103)

The Aotea Sea Scouts Hall (former Manukau Yacht & Motor Boat Club) was constructed in 1911. The building was used by the MY&MBC until reclamations and motorway developments from the 1960s made access difficult. The building is larger in proportions to the Herne Bay Cruising Club but shares the bungalow style, however the original design by John Park was for a more ornate arts & crafts design.³



Figure 8: Aotea Sea Scouts' Hall. 1 Orpheus Drive, Onehunga. Photo by M.Eaves, site visit 21 March 2013.



Figure 9: Herne Bay Cruising Club, 2016.

The Calliope Sea Scouts building was constructed in 1903-6 for the North Shore Rowing Club who occupied it until 1966, when it was leased to the Calliope Sea Scouts. The form and size of the building is similar to the Aotea Sea Scouts Hall. However the building has sash windows, while the Aotea Sea Scouts Hall and the Herne Bay Cruising Club have small rectangular windows.



Figure 10: North Shore Rowing Club (now known as Devonport Sea Scouts Hall). NZ Graphic, 13 February 1904, p39 from Sir George Grey Special Collections, Auckland Libraries, NZG-19040213-39-5.



Figure 11: Takapuna Boating Club, 16 February 1924, Sir George Grey Special Collections, Auckland Libraries, 1-W364

The third comparison is with the Takapuna Boating Club which was built in 1922/23. This building is much larger than the Herne Bay Cruising Club, Aotea Scouts Hall and the Calliope Sea Scouts Hall, spread over a generous three levels. Although it also incorporates bungalow features, the scale of the building makes it distinct. The club has retained use and ownership over the building.

The *Auckland regional boatshed study* by Matthews & Matthews Architects provides a regional context for the evaluation of the heritage significance of boatsheds in Auckland. One of the recommendations coming out of this report is the recommendation for the Herne Bay Cruising Club for heritage protection.

There are individual sheds that are significant in that they are representative of the boatshed type, are associated with prominent people or organisations or have high setting or landscape values. The most significant example would be the Herne Bay Cruising Club. This was originally constructed around 1913 and added to in 1932. It is associated with the Club which was established over 100 years ago in 1906.⁴

The analysis by Matthews & Matthews identifies the Herne Bay Cruising Club as an exemplar of the type in the Auckland Region. As well as identifying the Herne Bay boatsheds as some of the earliest in the region, an “iconic and highly visible group”.⁵ The study identified 21 sheds, of varying historical significance in Herne Bay.⁶ Although the study was commissioned 9 years ago the findings are still relevant.

7.0 Significance criteria

(a) Historical

The place reflects important or representative aspects of national, regional or local history, or is associated with an important event, person, group of people or idea or early period of settlement within the nation, region or locality.

Auckland is a coastal city with strong boating culture. Boatsheds are a common sight dotting the coastline of the harbour. Although there are fewer today, and most are associated with private residential use.

The Herne Bay Cruising Club was the home of the Richmond Yacht Club, one of the oldest and most prominent yacht clubs in Auckland. Richmond Yacht Club continues to make a significant contribution to yachting in Auckland.

The building is now owned by the Herne Bay Cruising Club and continues to be used by members for clubrooms and boat storage. A use that has changed little over the 103 years the building has been on Sloanes Beach.

The Herne Bay Cruising Club has **considerable regional** historical significance.

(b) Social

The place has a strong or special association with, or is held in high esteem by, a particular community or cultural group for its symbolic, spiritual, commemorative, traditional or other cultural value.

Richmond Yacht Club has made a significant contribution to the Auckland yachting scene since it was established in 1903. This building was their second base and although the club no longer owns the boatshed, it still has strong ties to the place. The Herne Bay Cruising Club has owned the building since 1965. The club's members have shown an interest in the building's history and significance.⁷

The building is a distinct feature on Sloanes Beach and is valued by the local Herne Bay residents for this visual appeal.

Based on what is known at this time the Herne Bay Cruising Club is of **moderate local** social significance.

(c) Mana Whenua

The place has a strong or special association with, or is held in high esteem by, Mana Whenua for its symbolic, spiritual, commemorative, traditional or other cultural value.

Not assessed.

(d) Knowledge

The place has potential to provide knowledge through archaeological or other scientific or scholarly study, or to contribute to an understanding of the cultural or natural history of New Zealand, the region, or locality.

Our knowledge about the place has been extended through this assessment, which contributes to our understanding of the cultural history of the place and area. However, the building does not have the ability to demonstrate or reveal important archaeological material.

Herne Bay Cruising Club is considered to be of **little** knowledge significance to the community in the **local** Herne Bay area.

(e) Technological

The place demonstrates technical accomplishment, innovation or achievement in its structure, construction, components or use of materials.

The building uses well-known and understood construction methods and materials for its period. The use of old railway lines and recycled telegraph poles on the ramp reflects the ingenuity of the club members using the recycled materials available to them. Although this is an interesting use of recycling, it is not a significant innovation under this criterion.

Herne Bay Cruising Club is considered to be of **little** technological significance to the community in the **local** Herne Bay area.

(f) Physical attributes

The place is a notable or representative example of a type, design or style, method of construction, craftsmanship or use of materials or the work of a notable architect, designer, engineer or builder.

The Herne Bay Cruising Club is not the notable work of an architect, designer, engineer, or builder but the method of construction is representative of the boatsheds built along Auckland's shorelines; built using the materials at hand by Richmond Yacht Club members. Club members built the second story during the depression of the 1930s with the work offered to unemployed club members first.⁸

The building has a bungalow style, evident in the exposed rafter beams and timber shingles at the gable ends. The building is a notable and rare example of an early twentieth century boatshed.

The Herne Bay Cruising Club has **moderate local** significance under this criterion.

(g) Aesthetic

The place is notable or distinctive for its aesthetic, visual, or landmark qualities.

The Herne Bay Cruising Club is a visual landmark on Sloanes Beach, Herne Bay. It can be viewed from Short Street, Herne Bay Reserve, Sloanes Beach and from the Waitemata harbour. The picturesque aesthetic qualities of the bungalow styled building are strongest when viewed from the harbour. Herne Bay Cruising Club is an exemplar within the larger grouping of the Herne Bay boatsheds.

The Herne Bay Cruising Club has **considerable** aesthetic significance to the **local** Herne Bay area.

(h) Context

The place contributes to or is associated with a wider historical or cultural context, streetscape, townscape, landscape or setting.

Auckland has a rich maritime heritage, the remnants of which dot the coastline. Boatsheds are a prominent features of the shoreline when viewed from the harbour. There are four identifiable geographical groups of boatsheds in Auckland as recognised by Matthews & Matthews.⁹ These are the groups at Hobson Bay, Rocky Bay Waiheke, Rangitoto and Herne Bay.

The Herne Bay Cruising Club has a prominent position on Sloanes Beach, Herne Bay since it was first constructed in 1913. It is the oldest of the Herne Bay boatsheds group and which comprises privately owned sheds owned by the adjoining properties. The boatshed contributes to the wider landscape value of the Herne Bay shoreline.

The Herne Bay Cruising Club has **considerable** context significance in the **local** context of Herne Bay.

8.0 Statement of significance

The Herne Bay Cruising Club building has considerable regional historical significance. Built in 1913 and extended in 1932 it is the former home of the Richmond Yacht Club one of the oldest and most prominent yacht clubs in Auckland. Richmond Yacht Club continues to make a significant contribution to yachting in Auckland.

The building has considerable aesthetic significance as a visual landmark on Sloanes Beach, Herne Bay. It is part of the larger grouping of the Herne Bay boatsheds. It is the oldest of the Herne Bay boatshed group and which comprises mainly privately owned sheds owned by the adjoining properties. The Herne Bay Cruising Club has considerable context significance in the local context of Herne Bay.

9.0 Extent of the place for scheduling

The identified extent of the place for scheduling is the area that is integral to the function, meaning and relationships of the place. The extent includes boatshed and the areas adjacent that are important in framing the place.



Figure 12: *Recommended extent of place (GeoMaps 2017)*

10.0 Recommendations

Based on the proceeding evaluation, Herne Bay Cruising Club at Sloanes Beach, Herne Bay meets the threshold for scheduled historic heritage places as a Category B Historic Heritage Place.

- The heritage values that meet the threshold of considerable include historical, aesthetic and context.
- Overall significance of the building is considerable.
- The recommended extent of place is defined in section 9.0 above.

11.0 Table of Historic Heritage Values

Significance (A-H)	Criteria	Value* (None, Little, Moderate, Considerable, Exceptional)	Context (Local, Regional, International)
A- Historical		Considerable	Regional
B- Social		Moderate	Local
C- Mana Whenua			
D- Knowledge		Little	Local
E- Technological		Little	Local
F- Physical Attributes		Moderate	Local
G- Aesthetic		Considerable	Local
H- Context		Considerable	Local

***Levels of significance or value:**

Exceptional: of outstanding importance and interest; retention of the identified value(s)/significance is essential.

Considerable: of great importance and interest; retention of the identified value(s)/significance is very important.

Moderate: of some importance and interest; retention of the identified value(s)/significance is desirable.

Little: of limited importance and interest.

NA/None: none identified

12.0 Overall Significance

Place Name and/or Description	Herne Bay Cruising Club
Verified Location	Sloanes Beach, Short Street, Herne Bay Reserve, Herne Bay
Verified Legal Description	PART BED AUCKLAND HARBOUR SO 67209 AND SEC 1 SO 420557
Category	B
Primary Feature	Building
Known Heritage Values	A, G, H
Extent of Place	Refer to diagram in section 9.0
Exclusions	Interior of building(s)
Additional Controls for Archaeological Sites or Features	
Place of Maori Interest or Significance	

13.0 Other recommendations

This evaluation and the *Auckland regional boatshed study 2007¹⁰* have identified the Herne Bay group of boatsheds as having potential heritage significance. It is recommended that the Herne Bay boatsheds are evaluated for their historic heritage value.

Author (and position)
Anna Boyer, Heritage Information Advisor

Date
31 January 2017

Reviewer
Cara Francesco, Principle Specialist Built Heritage

Date
January 2017

Appendices

Appendix 1 Historic research

673



HERNE BAY CRUISING
CLUB BUILDING
SLOANE'S BEACH,
AUCKLAND

A CONSERVATION PLAN

**HERNE BAY CRUISING CLUB BUILDING
SLOANE'S BEACH, AUCKLAND**

A CONSERVATION PLAN

Prepared for the
The Herne Bay Cruising Club

By
Dave Pearson Architects Limited



93 Victoria Road
Devonport
Auckland, New Zealand

This document and its contents are copyright.
Any unauthorised employment or reproduction in full or part is forbidden.

August 2004

TABLE OF CONTENTS

INFORMATION	7
SUBJECT AND AUTHORSHIP	7
FORMAT OF PLAN	7
STATUS OF THE PLACE	7
PHOTOGRAPHIC SOURCES	7
ACKNOWLEDGEMENTS	8
PART 1 – ASSESSMENT OF SIGNIFICANCE	
1 HISTORICAL ACCOUNT	10
HISTORY OF THE BUILDING	10
THE BUILDING IS EXTENDED	12
LATER HISTORY	14
CHRONOLOGY OF EVENTS	15
2 PHYSICAL DESCRIPTION	15
SITE & LOCATION	16
THE BOATHOUSE	16
PLANNING	16
ARCHITECTURAL FORM	16
ARCHITECTURAL INFLUENCES	17
CONSTRUCTION	19
CHANGES TO THE BUILDING	22
3 THE SIGNIFICANCE OF THE CRUISING CLUB BUILDING	23
CRITERIA FOR ASSESSMENT	23
SIGNIFICANCE OF ELEMENTS	24
STATEMENT OF SIGNIFICANCE	26
PART 2 – CONSERVATION POLICIES	
4 A FRAMEWORK FOR CONSERVATION POLICIES	29
CONSTRAINTS ARISING FROM THE STATEMENT OF SIGNIFICANCE	29
AIMS OF THE BUILDING OWNER	29
HERITAGE PROTECTION	29
LEGISLATION	29
THE RESOURCE MANAGEMENT ACT	29
THE BUILDING ACT 1991 AND THE NEW ZEALAND BUILDING CODE	30
PHYSICAL CONDITION OF THE BUILDING	31
5 CONSERVATION POLICIES	35
PROPOSED USES	35
RETENTION OF SIGNIFICANCE	35
INTERPRETATION	37
CONSERVATION STANDARDS	37
CONSULTATION AND REVIEW	37

DAVE PEARSON ARCHITECTS LTD

6	IMPLEMENTATION OF POLICIES	38
	REMEDIAL WORK	38
7	BIBLIOGRAPHY & INFORMATION SOURCES	40
	APPENDICES	41
	APPENDIX 1 Plans and Drawings	
	APPENDIX 2 Certificate of Incorporation	
	APPENDIX 3 Structural Engineer's report (Gary Chester Consultants)	
	APPENDIX 4 ICOMOS New Zealand Charter for Places of Cultural Heritage Value	

INFORMATION

DAVE PEARSON ARCHITECTS LIMITED

INFORMATION

SUBJECT AND AUTHORSHIP

This conservation plan concerns the boathouse at Sloane's Beach on Auckland's Waitemata Harbour to the west of the Harbour Bridge. It is currently owned by the Herne Bay Cruising Club. The club is currently applying for a Coastal Resource Consent to continue occupation of the coastal marine area as the original licence has expired. This conservation plan was commissioned to provide support for the application.

The conservation plan was written by Dave Pearson, principal of Dave Pearson Architects Limited. In compiling the historical account, extensive use was made of the book "Lee Rail" as written by Harold Kidd and Robin Elliott.

FORMAT OF PLAN

The document generally follows the format of a conservation plan as described in the publication *The Conservation Plan; a Guide to the Preparation of Conservation Plans for Places of Cultural Significance* (third edition) prepared by J S Kerr for the National Trust of Australia. Reference was also made to the Department of Conservation's *Standard Conservation Plan Contents Specification* and the *Guidelines for Preparing Conservation Plans*, prepared by the New Zealand Historic Places Trust.

Part 1 of the plan contains an historical account of the place. The significance of the building and its component parts is then assessed and a Statement of Significance prepared. Part 2 of the plan formulates a series of conservation policies to safeguard the heritage values of the place. The plan also includes a condition report and a schedule of remedial work.

The plan is also consistent with the *ICOMOS New Zealand Charter for the Conservation of Places of Cultural Heritage Value* (1993). The definitions in the conservation plan are as defined in the Charter.

ADDRESS

The boathouse is located in Short Street, otherwise known as Marine Parade Reserve off Marine Parade in Herne Bay, Auckland.

OWNERSHIP

The building, formerly owned by the Richmond Cruising Club, is now owned by the Herne Bay Cruising Club.

PHOTOGRAPHIC SOURCES

The source of the historical photographs is as noted. The contemporary colour photographs were taken by Dave Pearson.

ACKNOWLEDGEMENTS

The assistance of Christina Reinhart and Jim Mason is acknowledged.

PART ONE
ASSESSMENT OF SIGNIFICANCE

DAVE PEARSON ARCHITECTS LIMITED

8

1 HISTORICAL ACCOUNT

HISTORY OF THE BUILDING

In 1903, a group of enthusiastic Herne Bay yachting families came together to establish the Richmond Cruising Club in Cox's Bay on Auckland's Waitemata Harbour. The club was later to become the Richmond Yacht Club. It eventually relocated to Westhaven where it continues today as one of Auckland's oldest and best-known yacht clubs.



The club's beginnings were unremarkable with its first home comprising a boatshed belonging to James Donald in Rawene Avenue, Cox's Bay. In the following years, the club experienced extraordinary growth, expanding from seven members in 1906 to 106 in 1909.

James Donald's boatshed.

Throughout this period, however, the club was handicapped by the fact that it had no permanent facilities. Yachting races started and finished off the end of Ponsonby Wharf and temporary facilities had to be erected for every race day and then dismantled at the end of the programme. Club meetings were held in the Richmond Hall.

As early as 1908, the club recognised the need to have its own facilities and sought to erect a clubhouse at Sloane's Beach. A permit for a clubhouse was granted by the Auckland City Council after some difficulties. The Auckland Harbour Board, however, refused to allow its construction. After some years, the club again applied to the council for a permit and this was granted on condition that the club should pay the sum of one shilling as annual rental. The club was fortunate in having a number of carpenters as members and building began with the ordering of materials and the laying of the foundations.

There were, at the time, however, three other sheds on the beach, all owned by private individuals. These individuals objected to the new structure with the result that the council issued an order to the club to stop work. After negotiations with the owners of the other sheds failed, the club began discussions with the Auckland Harbour Board in an effort to resolve the dispute. After agreeing to a suggestion that the building be moved further towards the sea, the club again gained consent to build.

Work had barely started before the Board changed its mind and another letter arrived again ordering that work on the site cease. After assistance was sought from the local member of parliament, it was discovered that the board did not have the authority to adjudicate in the matter and that it was the responsibility of the Marine Department. However, it was only after the Minister for Marine received a deputation from the club and visited the site that progress was made. Finally, a letter arrived from the Department giving consent for the clubhouse.

Building again got under way and the clubhouse was finally opened on 8 November 1913 by the Hon Geo Fowlds, the club's patron, in the presence of between 150 and 200 guests.



The Clubhouse on opening day.

An article in the *New Zealand Yachtsman* of 19 July 1913 described the clubhouse as follows:

"The new clubhouse is 25ft x 18ft and has skids 22ft long leading from the front of the building. It is one storey high and is built after the bungalow type of house. The interior is not quite complete, but all the necessary work will be carried out before next season".

In 1919, the club was declared to be in sound financial position. But troubles were ahead for the Richmond Yacht Club. Competition for members between yacht clubs on the Waitemata was keen, new yachting classes were becoming established and other clubs were seen as having greater prestige. Richmond, by comparison, was seen as being conservative and continued to sail outmoded mullet boats. Consequently, with dwindling membership, the club decided not to take part in the 1919-20 season and went into recess for the next five years. Control of the Sloane's Beach clubhouse and club funds were retained by the club's trustees.

In September of 1925, Cook Thompson, a former commodore of the Richmond Club called a General Meeting of Richmond members and supporters at his home. At the meeting, it was unanimously decided that the club should again become active and that racing be commenced in the coming season. The President, Mr Donald offered a set of gold medals to be awarded to the boat winning the most points. He also donated timber to construct a grid to enable boats to be cleaned.

With the resurrection of the club, work was also carried out on the clubhouse with members holding working bees to paint the building. Electricity was installed and plumbing work carried out. The 1925-26 season was highly successful for the revitalized club, culminating in the winning of the prestigious Lipton Cup, donated by Sir Thomas Lipton.

The clubhouse, however, was seen as being of limited value. Committee meetings were held at the home of the Commodore, social functions were held at nearby halls and the Ponsonby Hall was the venue for the annual prize giving. At the 1926 AGM, the Commodore suggested that a boat harbour be formed at Cox's Creek and a new clubhouse constructed. However, the scheme turned out not to be feasible. In the following year, the Minister of Marine renewed the lease at Sloane's Beach at an annual rental of £1 for a further 14 years.

After some lean years and the prospect of amalgamation with the nearby Ponsonby Cruising Club, fortunes began again to improve for Richmond in the early 1930s. Committee meetings were held in the clubhouse and there was a new optimism. Fund raising activities and membership drives were held, along with an expanded racing programme. In June 1932, an "Art Union" raffle was run by Ivo Dennes and succeeded in raising, in Depression times, the huge sum of £222/12/-. The money was immediately set aside for additions to the clubhouse.

THE BUILDING IS EXTENDED

By 19 July, plans for a second storey had been completed. Approval was sought from the Marine Department and quotes obtained for the work. The timber was supplied by Leyland O'Brien at 10% discount and work was offered to unemployed members at the rate of 12/- per day. Work commenced on 27 July and was soon completed. James Donald presented a clock for the building and Gordon Bellve made an impressive key for the front door.



The crowd at the opening day of the second storey and the start of the 1932-33 season.

The 1930s also saw the establishment of the Herne Bay Junior Yacht Club which was formed to cater for young yachties under the age of 18. All racing was held off the Ponsonby Wharf to a mark off Sloane's Beach.

In the years following the depression, the Richmond club prospered. The enlarged clubhouse was constantly in use for committee meetings, ladies meetings, social events and card evenings.



Boats on the beach c 1939. The extended clubhouse with its upper storey can be seen in the background.

Towards the end of the 1930s, however, a number of clubs had moved from the upper reaches of the harbour to a new reclamation at St Mary's Bay that would become known as Westhaven. With the old clubhouse becoming increasingly isolated and inadequate, the Richmond Cruising Club began also to consider relocating to St Mary's Bay. In anticipation of a move to St Mary's Bay, the club was registered under the Incorporated Societies Act and its name was changed to the Richmond Yacht Club (Incorporated).

At first, consideration was given to relocating the old clubhouse. The Harbour Board, however, had no intention of allowing the old building on its new reclamation. The Board's idea was that any structures should be contemporary, partly in recognition of Auckland and the country's celebration of the centenary of European settlement in 1940.

The new clubhouse at St Mary's Bay was designed by Norman Wade in the Art Deco style with input from the Thorpe brothers who were both architects and club members. The new building was opened on 14 November 1940.

Richmond retained the lease of the old clubhouse at Sloane's Beach and used it as a locker room for club members. Also, a number of the top mullet boats in Auckland were moored in the area. However, by the mid 1940s, following the war, the building was becoming increasingly difficult to maintain with vandalism also a problem. The Rodney Sea Scouts used part of the building and their tenure was marked by complaints from the

neighbours and on-going damage to the skids and other parts of the building. Throughout the 1950s, the situation continued at Sloane's Beach. Locker holders held noisy parties to the annoyance of local residents and the vandalism continued.

The clubhouse briefly found a new use in the years prior to the opening of the Harbour Bridge. The Ponsonby Cruising club lost its facilities as a result of earthworks and took over the old Richmond building as a venue for youth training and committee meetings. By the 1950s, the Herne Bay Junior Yacht Club had become simply the Herne Bay Yacht Club. In 1952 that club was evicted from Westhaven by the Harbour Board and moved to the old clubhouse at Sloane's Beach. The Herne Bay Yacht Club finally disbanded in 1953.

In spite of continuing maintenance problems and noise caused by locker holders, the Richmond Club continued to retain ownership of the old clubhouse at Sloane's Beach. However, particularly high repair costs in 1964 caused the club to rethink its position. In 1965, the Sea Rangers offered Richmond £500 for the building, subject to consent being given by the City Council to alter the building. Council, however, declined the application and the bid faltered. Another bid was received, this time from the newly formed Herne Bay Cruising Club and this was enthusiastically accepted.

LATER HISTORY

The Herne Bay Cruising Club was subsequently incorporated as a Charitable Trust. Since they acquired the building, maintenance work such as painting and repair work has been carried out. More recently, the west deck has been reconstructed and the east stairs are currently under repair.

CHRONOLOGY OF EVENTS

Date	Event
1903	The Richmond Cruising Club is established in Cox's Bay.
1908	A permit is granted to erect a clubhouse at Sloane's Beach.
1913	8 November. The clubhouse is finally opened by the Hon Geo Fowlds.
1919	The club decides not to participate in the 1919/20 season and goes into recess.
1925	September. The club decides to again become active.
1925/26	The club has a successful season which culminates in the winning of the Lipton Cup.
1926	The idea of building a boat harbour at Cox's Creek is suggested.
1927	The lease is renewed at Sloane's Beach for another 14 years.
1932	June. An "Art Union" raffle raises the sum of £222/12/- for additions to the clubhouse.
1932	July. Plans are drawn up for an upper storey and work commences.
1930s	The Herne Bay Junior Yacht Club is established.
1930s	The Richmond Cruising Club contemplates a move to Westhaven.
1940	14 November. The new clubhouse is opened at Westhaven. The club retains control of the old clubhouse at Sloane's Beach.
1940s	The old building is occupied by the Rodney Sea Scouts. It is also subject to continuing vandalism.
1952	The Herne Bay Yacht Club (formerly the Herne Bay Junior Club) is evicted from Westhaven and occupies the old clubhouse.
1964	Extensive repairs are carried out to the old building.
1965	An offer of £500 by the Sea Rangers to purchase the building is rejected in favour of a second offer by the newly formed Herne Bay Cruising Club.
1998	The Herne Bay Cruising Club is incorporated under the Charitable Trusts Act 1957.
1998	July. Work is carried out on the building including some structural work, a new roof and work to the ramp.

2 PHYSICAL DESCRIPTION

SITE AND LOCATION

The Herne Bay Cruising Club building is located on Sloane's Beach on the Waitemata Harbour to the west of the Auckland Harbour Bridge. Access to Sloane's beach is by way of a flight of concrete steps leading from Marine Parade in Herne Bay.

At the bottom of the steps, a timber boardwalk leads from a stone abutment to the clubhouse itself. The building is located on the beach where the end wall faces west looking out to the harbour. From a pair of doors at the lower level of the west elevation, a ramp or skid extends out to meet deeper water. The ramp is approximately 1.99 metres wide and 10 metres long.

The slope above the beach is occupied by houses of varying ages and styles. Two other buildings are located on the beach at either side of the boathouse and these both appear to be used for residential purposes.

THE BOATHOUSE

PLANNING

The boathouse is rectangular in plan and has overall dimensions of 7.6 metres lengthwise and 4.5 metres across. It has two levels, the lower being used for boat and gear storage and the upper as a meeting and social room. Access to the lower level is by a single door in the east elevation near the southeast corner. This level consists of a single space and has lockers approximately 500 mm deep along the north and south walls. At the western end, a pair of doors, each 910 wide lead out onto the ramp.

The upper level is accessed by a flight of steps on the east elevation that leads to a single door near the northeast corner. This level also comprises a single space with a pair of triangular cupboards in the northwest and southwest corners. At the western end of the space, a pair of glazed French doors leads out onto a deck. Two single sash windows have been provided along the south and north elevations while two smaller windows are found at high level in the east and west walls.

ARCHITECTURAL FORM

The Herne Bay Cruising Club building has the simplest of architectural forms - a rectangular building with a gable roof running lengthwise. The roof has a pitch of 30 degrees.

On the east elevation the gable roof form is evident. Within the gable end are timber shingles and a small window. On the same elevation at the lower level is the single door leading to the boat storage area and the set of steps leading to the door at the upper level. The north elevation is again simple. Apart from two windows at the upper level, the wall is sheathed entirely with weatherboards. Exposed rafters at the eaves provide the only other architectural feature.



East elevation (left) and above the view from the northwest corner (DPAL)

On the west elevation facing the harbour, the end of the gable roof can again be seen. Like the east elevation, the gable end is sheathed with timber shingles and incorporates a small window. The most notable feature of this elevation is the handsome flagpole which is supported on a bracket below the gable. Also on this elevation, at the upper level, can be seen the pair of French doors that lead out onto the deck which overlooks the water. The deck is supported on a series of angled braces or struts which extend back to the wall. At the lower level are the pair of doors which lead from the boat storage area onto the ramp.

The remaining elevation, that facing south, is identical to the north elevation, being sheathed with weatherboards and having two windows at the upper level.

ARCHITECTURAL INFLUENCES

At the opening, the design of the boathouse was described as being based on the "bungalow type of house" and the form and details of the building conform to that style.

The bungalow originated in India where the word "bungalow" is a derivation of the Hindustani word "bangla" which was the name given to the bamboo and thatch houses of eastern India, near Bangladesh. In about 1870, the English colonial settlers took the style back to England where it was re-interpreted as a coastal holiday home. It was then exported to the West Coast of America about 1900 where, after various transformations it evolved into what became known as the Californian bungalow.

In New Zealand, following the end of World War I, the Californian bungalow quickly replaced the villa as the style of house most often constructed, reaching the height of its popularity during the 1920s and 1930s. The style was also not confined to the family home. Baches and holiday homes in many seaside locations, such as Rangitoto Island in the Hauraki Gulf had distinct bungalow influences. Other maritime structures such as the Takapuna Boating Club building at the end of the Bayswater Peninsula also exhibit characteristic bungalow detailing.

The New Zealand bungalow developed a well-defined architectural vocabulary. Roofs were generally pitched at about 22 degrees and the rafters were exposed at the eaves. Gable ends often featured slatted or trellised ventilators. The walls were usually weather boarded, although timber shingles could be fixed in gable ends or as verandah balustrading and below bow windows. Joinery generally consisted of side-hung casement sashes and projecting bay windows were common.

Bungalow details evident in the Herne Bay boathouse include the relatively low pitched roof, the exposed rafters at the eaves and the use of shingles in the gable ends and weatherboarding for the walls. Internally, the narrow tongue and groove wall lining at the upper level is typical of the bungalow period, as is the ceiling detail with its patterned plaster panels and dark stained battens.



West elevation. Note bungalow features including exposed rafters at the eaves and timber shingles in the gable end. The deck has been reconstructed but is similar to the original (DPAL).

The design and construction of the boathouse also reflects a "do-it-yourself" philosophy which is part of the New Zealand way of life. Funding for sporting clubhouses in New Zealand historically has always been limited. The building of a clubhouse often relied on the dedication of club members to raise funds from the local community and the use of working bees to provide labour.



Upper level interior. Note tongue and groove wall linings (DPAL).

A variety of small timber sheds and other buildings erected for rowing and sailing clubs in the Auckland area reflected this economical approach. Over the years, many have been demolished with the result that now relatively few remain. Two examples of such surviving buildings, other than the Herne Bay boathouse, are the Takapuna Boating clubhouse and the Sea Scout hall in Devonport.

CONSTRUCTION

Foundations and Subfloor

The boathouse is founded on timber piles. At the front of the building, it can be seen that the piles are set into 380 mm diameter steel drums filled with concrete. It is reasonable to assume the remainder of the piles are similarly set in concrete.

The original piles are approximately 150 x 100 mm in size, although some reduction in size has occurred due to the action of the sea and, in some cases, Toredoworm. The timber used is probably kauri. Some new 125 x 125 mm tanalised pine piles have been provided alongside old piles that have deteriorated. In various locations, ex 150 x 50 diagonal braces have been bolted to the side of the piles.

On top of the piles are set 100 x 75 mm bearers. Around the perimeter of the building, the bearers were originally connected to the piles with steel plates bolted to the timber members. A number of these have since corroded to the point where they are now missing completely. Some wire-tie connectors have been provided more recently. In some instances, the bearers are simply resting on the piles with no fixings having been provided.



Subfloor. Note diagonal braces (DPAL).

Ramp

The ramp has two runners consisting of inverted railway lines extending the length of the ramp and set in concrete foundations at the lower end. At approximately half way down the ramp, a pair of vertical stanchions, also made from railway lines, support the runners. The stanchions are also set in concrete.

The original planking comprises what appears to be recycled telegraph pole cross arms bolted to the runners. The planks are 75 x 75 mm in cross section and made of hardwood. In recent times, some of the original planks have been replaced with 100 x 75 mm tanalised planks set on the flat.



Ramp. Note railway line runners and supports (DPAL).

Floors

The framing to the lower floor comprises 150 x 50 floor joists which are overlaid with 230 mm wide rough sawn boards. In some instances the original floor boards have been replaced with plywood sheets.

The upper floor framing comprises ex 230 x 50 oregon joists spanning the width of the building. Herring bone strutting has been fixed between the joists to provide stability. Additional joists have been added to provide extra support for the west deck and the landing on the east wall. The floor to the upper level comprises ex 150 wide tongue and groove boards with a varnish finish.



Upper floor framing. Note herring bone strutting (DPAL).

Walls

The walls of the boathouse are entirely timber framed. The wall framing to the lower level comprises 100 x 50 studs. It is assumed that similar framing was used for the walls to the upper level.

The exterior is sheathed with ex 200 x 25 bevel backed weatherboards. The timber appears to be rimu. Corner boxes formed from ex 100 x 25 mm timber have been provided at each external corner. Within the gable ends, timber shingles have been fixed. These are likely to be cedar.

Gable Ends and Soffits

At the gable ends are ex 250 x 25 mm bargeboards. Cover boards of a similar dimension have been fixed over the bargeboards at the end of the roof planes. At the eaves can be seen the ends of the rafters. These are overlaid with tongue and groove boarding to form the soffits.

Roof

The roof is typical of the bungalow style, being of relatively low pitch, in this case 30 degrees. The roofing material consists of corrugated galvanised steel sheets. The ridging is also galvanised steel with lead edgings.

Joinery

The joinery throughout is timber. The windows to the upper floor in the north and south elevations are pivoting type and were originally divided into six lights or panes. The glazing bars have since been removed and the windows reglazed with a single sheet of glass. Smaller windows are also found at high level within the gable ends of the building.

At the time the boathouse had the upper storey added, a single door was provided to the deck on the west elevation. From historic photographs the upper section of the door appears to have been also divided into six lights. It is likely that the lower part of the door was sheathed with tongue and groove boarding. This single door has since been replaced with a pair of doors, the upper panels of which are glazed and the lower panels timber.

The doors to the sea from the lower level boat storage area are probably the original doors dating from 1913. They are sheathed with 140 mm wide tongue, groove and reeded boards. The door in the east wall is sheathed with smaller 85 mm wide boards, suggesting that it dates from a later period.

Externally, the windows and doors generally have ex 150 x 25 mm plain timber facings. The window in the gable at the east end has an additional decorative facing below the sill.

Internal Linings

The ceilings to the upper level comprise fibrous plaster sheets with a pattern based on a star motif. Dark stained timber battens have been fixed over the joints. The walls are lined with vertically fixed tongue, grooved and "veed" rimu boarding with a varnish finish. Ex 150 mm wide bevelled architraves have been fixed around the windows.

The walls to the lower level are unlined and the wall framing can be seen behind the storage lockers.

Fixtures and Fittings

There is little in the way of built-in fittings in the building. At the upper level, are two small triangular cupboards with tongue and groove doors in the northwest and southwest corners. At the lower level, the north and south walls are lined with lockers, the doors and facings of which are lined with tongue and groove boarding.

CHANGES TO THE BUILDING

The boathouse underwent a major change in 1932 when an upper level was added. This area became a place where meetings and other activities could be held and the higher standard of finish reflects this.

The plan dimensions of the upper storey were identical to those of the lower level and the exterior was sheathed with weatherboards of a similar size. From a comparison of photographs before and after the upper storey was added, it appears that the original roof structure was probably dismantled and re-erected at the higher level (refer to photographs on pages 11 and 13).

Various other changes of a relatively minor nature have occurred subsequently. These include:

Exterior

- Construction of boardwalk access on the south side of the building.
- Replacement of some original planks to the ramp.
- Reconstruction of the deck on the west elevation.
- Replacement of original single door at the upper level with glazed French doors.
- Removal of glazing bars from the windows in the north and south elevations.
- Provision of additional foundation piles.

Interior

- Replacement of some decorative plaster ceiling panels at the upper level with plain plaster board.
- The addition of strap bracing at the ceiling level of the lower storey.

The steps and landing on the east side of the building are currently being replaced because of decay in various members.

3 THE SIGNIFICANCE OF THE CRUISING CLUB BUILDING

CRITERIA FOR ASSESSMENT

In any historic building or structure, the various elements or fabric that make up the building have their own intrinsic value and the contribution they make to the overall significance of the building can be assessed. In addition, the cultural significance of the building as a whole can also be assessed and the building given an overall rating of significance.

In the following section the significance of the elements or fabric that make up the boathouse is assessed. The overall significance of the structure is then assessed and expressed as a "Statement of Significance".

Origin of Elements

In the assessment an indication is given of the assumed period from which each element originates.

Heritage Fabric

Original fabric (OF) Original fabric is that fabric that which dates from 1913 when the boathouse was first constructed. Some original fabric appears to have been reused when the upper storey was added.

Later fabric (LF) Later fabric is that which has been added subsequent to the initial period of construction. For the most part, it dates from the time of the upper storey in 1932.

Non-Heritage Fabric

Recent fabric (RF) Recent fabric is that which has probably been placed within the last 10-15 years. It generally has no heritage value.

Degree of Significance

The degree of significance of each element is assessed in accordance with the following scale:

Considerable This fabric is considered to make an important contribution to the overall significance of the building.

Some Fabric having some significance makes a comparatively minor contribution to overall significance.

Not Relevant Fabric in this category may not have any particular significance, however, it allows the building to function.

Intrusive Intrusive fabric consists of accretions that detract from overall significance or which obscure fabric of greater value.

SIGNIFICANCE OF ELEMENTS

Setting

<p>Setting Although no date is available for the concrete steps leading down to the boathouse, they appear to be of relatively recent construction. The setting has some significance.</p>
<p><i>Some Significance</i> Concrete steps (LF?).</p>
<p><i>Not relevant</i> Recent plantings (RF). Boardwalk to boathouse (RF).</p>

Building Exterior

<p>Roof The roofing was probably reused when the upper storey was added. Overall, the roof has some significance.</p>
<p><i>Considerable Significance</i> Gabled roof form (OF).</p>
<p><i>Some Significance</i> Corrugated steel roofing, flashings etc (OF?) Timber cover boards (LF?)</p>
<p>North Elevation The north elevation essentially retains its form from 1932 when the upper storey was added. It has some significance.</p>
<p><i>Considerable Significance</i> Exposed rafters and T&G soffit at eaves (OF).</p>
<p><i>Some Significance</i> Timber windows (modified) & facings (OF). Weatherboarding, corner boxes & scribes (OF, LF).</p>
<p>East Elevation The south elevation essentially retains its form from 1932 when the upper storey was added. It has some significance. The stairs are currently being reconstructed and the lower door may not be original. It has considerable significance.</p>
<p><i>Considerable Significance</i> Bargeboards and mouldings (OF). Shingles & window in gable end (OF). Bargeboards and mouldings (OF).</p>
<p><i>Some Significance</i> Weatherboarding, corner boxes & scribes (OF, LF). Doors at upper & lower level (LF?)</p>
<p><i>Not Relevant</i> Reconstructed stairs & landing (RF).</p>
<p>South Elevation The south elevation essentially retains its form from 1932 when the upper storey was added. It has some significance.</p>
<p><i>Considerable Significance</i> Exposed rafters and T&G soffit at eaves (OF).</p>
<p><i>Some Significance</i> Timber windows & facings (modified) (OF). Weatherboarding, corner boxes & scribes (OF, LF).</p>

West Elevation

The west elevation essentially retains its form from 1932 when the upper storey was added. The upper doors are not original and the deck has been rebuilt. It has considerable significance.

Considerable Significance

Flagpole (OF).
Shingles & window in gable end (OF).
Bargeboards and mouldings (OF).
Doors to ramp at lower level (OF).

Some Significance

Weatherboarding, corner boxes & scribes (OF, LF).
Opening above doors to ramp (LF).

Not Relevant

Deck & balustrade (reconstructed) (RF).
Doors to deck (RF).

Foundations and Ramp

The foundations and ramp date from the original construction period. They have considerable significance.

Considerable Significance

Original timber piles & cross braces (OF).
Concrete filled drum foundations (OF).
Railway line runners and stanchions to ramp (OF).
Telegraph pole cross arms reused as planking to ramp (OF).

Some Significance

Timber bearers (OF).
Concrete footings to ramp (OF).

Not relevant

Later timber piles and fixings (RF).

Building Interior**Internal Fabric**

Both levels of the building are generally as originally constructed, the lower level in 1913 and the upper in 1932. The interior is rated as having considerable significance.

Considerable Significance

Lockers in lower level (OF).
Patterned plaster ceilings with exposed battens (LF).
Tongue & groove wall lining in upper level (LF).
Cupboard fittings in upper level (LF).
Tongue and groove flooring (OF, LF).

Some Significance

Metal tie rods (LF?).

Intrusive

Plywood flooring in lower level (RF).

STATEMENT OF SIGNIFICANCE

The Statement of Significance describes the overall significance of the Herne Bay Cruising Club building and takes into account the significance of the elements of which the building is comprised.

**Historic
Significance****Events**

The boathouse is associated with the various activities undertaken by a yacht club. These include the summer racing programme, committee meetings, social events and out-of-season activities such as boat maintenance.

Associations

The building is also an important part of Auckland's maritime history. It is associated with the various commodores, officials and members of the Richmond Cruising Club who were involved in the establishment of the club and then in the design and construction of the building.

**Aesthetic
Significance****Context**

The building has occupied a prominent position on Sloane's Beach on the southern shores of the Waitemata harbour since it was constructed in 1913.

Architectural

The club was designed after the "bungalow type of house" and evidence of this can be seen in details such as the timber shingles in the gable end and the exposed rafters at the eaves. It is readily identifiable as a boathouse and has similarities to other comparable buildings on the Waitemata, notably the Takapuna Boating Club building at Bayswater.

Landmark

The building is a landmark in the area in which it is located, particularly when viewed from the Waitemata Harbour.

**Social
Significance****Cultural**

A temperate climate and easy access to the water has meant that yachting, as a sport, has been an important part of the lives of Auckland's citizens since the town was first established in 1840. Auckland's Anniversary Day Regatta is still one of the largest events of its type in the world.

The Richmond Yacht Club took part in these activities and was responsible for introducing a large number of Aucklanders to yachting. It also had a reputation as a club that encouraged younger people to participate in the sport.

The building also represents the "do-it-yourself" attitude that was prevalent in many clubs of its type. With funding always at a premium, club members often resorted to providing their own labour.

Community Values

The boatshed was erected by club members from the surrounding area and has continued to have close associations with the local community.

**Scientific
Significance****Technological**

The boathouse is able to demonstrate some of the construction techniques that were used at the time. Of interest are the timber foundation piles and the railway line runners that support the ramp. The use of telegraph pole cross arms for the slats to the ramp is a novel use for material that was no longer required for its original purpose.

Functional

The layout of the building with the lower level containing the boat lockers and upper level with its higher standard of finish contributes to an understanding as to how the building was used.

Rarity Values

The Herne Bay Cruising Club is one of relatively few historic boatsheds and similar structures that survive on the Waitemata Harbour.

Overall Rating

The Herne Bay Cruising Club building is considered to have **considerable** significance under historic, aesthetic social and scientific criteria.

**PART TWO
CONSERVATION POLICIES**

DAVE PEARSON ARCHITECTS LIMITED
27

4 A FRAMEWORK FOR CONSERVATION POLICIES

CONSTRAINTS ARISING FROM THE STATEMENT OF SIGNIFICANCE

From the Statement of Significance, a number of constraints arise that will have an influence on the conservation policies.

- No work should be carried out that removes or conceals evidence of the way the boathouse may have been formerly used or removes evidence of past events that may have occurred there.
- No work should be undertaken that reduces the building's architectural value or aesthetic integrity.
- No work should be carried out that further removes evidence of the earlier form or plan layout of the building.
- No work should be carried out that conceals or reduces technological evidence such as original construction techniques.
- No work should be carried out that removes or changes significant fabric other than where deterioration has occurred to such an extent that it is affecting other fabric.

AIMS OF THE BUILDING OWNER

The boathouse is owned by the Herne Bay Cruising Club. It currently provides facilities for its members in the way of storage for windsurfers, dinghies, kayaks and sailing gear. The upper level is used for meetings.

The club intends to continue to provide those facilities for its members and the local community.

HERITAGE PROTECTION

The Auckland Regional Council in its Proposed Regional Plan, Coastal, contains a number of provisions relating to historic structures. It recognises that places and areas which are an important part of Auckland's and New Zealand's maritime heritage should be protected and preserved.

The Proposed Plan contains schedules of particular buildings that it considers to be worthy of protection. At present, the boathouse is not included in the schedules.

LEGISLATION

THE RESOURCE MANAGEMENT ACT

The purpose of the Resource Management Act is "*to promote the sustainable management of natural and physical resources*". If the boathouse was protected by way of it being scheduled by the Territorial Authority or being registered by the New Zealand

Historic Places Trust, a resource consent would be required for all work not classified as a permitted activity. As noted, the boathouse is currently not protected and it is therefore unlikely that a resource consent would be required for any work.

THE BUILDING ACT 1991 AND THE NEW ZEALAND BUILDING CODE

The purpose of the Building Act is to ensure that buildings that will be used by the public are "safe and sanitary for users". The Act also requires that an existing public building be upgraded to a standard comparable, as far as possible, with a new building if major alterations are proposed or if its use changes, requiring alterations.

At present, the Herne Bay Cruising Club is essentially a private facility. If greater use of the building is to occur, it may then be deemed to be a public building and there may be a requirement to upgrade it to comply with the Building Act.

Any work to the boathouse and its fabric, other than maintenance, will require a Building Consent. If the building is deemed to be a public building, there may be a requirement to upgrade it before a Building Consent can be granted. Typically, areas where buildings need to be upgraded are noted below and include facilities for people with disabilities or the ability to escape from the building in the event of fire. The Act does, however, make provision for local authorities to take into account a place's cultural and heritage value.

The Act also requires that existing buildings be regularly checked for compliance with current codes with respect to safety and access.

- **Disabled Access & Facilities**

Within the Building Code is a requirement that buildings used by the public comply with the requirements of Section 25 of the Disabled Persons Community Welfare Act 1975. This includes a need to provide adequate sanitary facilities for people who may be expected to visit or work in that building.

With respect to access requirements, the Act allows for waivers or modifications with respect to the alteration of an existing building. The Building Industry Authority (BIA) is empowered to grant a waiver or modification if the authority determines that it is reasonable to grant such a waiver.

At present, the boathouse does not have an accessible toilet, and with the present low use of the building, it is unlikely that such a facility would be required. A toilet would require services to be connected and this would have cost implications. If the use of the building was to change or increase, the provision of services may need to be addressed.

In any event, it is also obviously impracticable to provide wheelchair access and this may limit the uses to which the building can be put.

- **Escape From Fire**

Because of the small size of the boathouse, it is unlikely that any special provisions will need to be made to ensure that occupiers can safely escape in the event of fire, given its present usage. If other uses are to be contemplated, the situation may need

to be reviewed. The east stairs, for example are likely to be non-complying. It is recommended that a fire extinguisher be kept at the building at all times.

• **Structural Upgrading**

An opinion was sought from Andrew Marriott, a structural engineer with experience in heritage buildings, as to the nature of work that may be required to structurally upgrade the boathouse. His report is appended.

CONDITION OF THE BUILDING

EXTERIOR

Roof

The roof is in fair condition with rusting evident on the underside of the corrugated steel roofing at the eaves. Part of the ridging has lifted. A section of the ridge has dropped and the rafters are sagging at the northeast corner. The cover board at the same corner has cracked.

Some of the tongue and groove soffit lining at the northeast corner is missing and the area has been roughly repaired in the past. There is possible decay in the soffit and bargeboard at the southeast corner. A soffit board is also cracked and loose near the southwest corner. Decay is present in the lower purlin on the north elevation and there is possible decay in one rafter on the south elevation.

Walls

North Elevation

Nail fixings to the weatherboards are generally rusting. The lower two boards have been replaced with non-matching boards. The corner box at the northwest corner has cracked and some decay is present at the base. Scribes are missing from the corner box at the northeast corner and from the facing board in the centre of the wall. Metal trays under the window sills are rusting. The ends of the tie rods at roof level are also rusting.

West Elevation

The shingles to the gable end are cracked and split and are generally in poor condition. Some rusting of nail fixings to the moulding above the barge board is evident and the varnish finish to the flagpole has begun to weather. The French doors to the deck are binding.



Deteriorated shingles in west gable end.

At the lower level, the boards to the doors to the ramp are cracked and nail fixings and hinges are rusting. Holes from the old supporting brackets to the deck remain in the face of the wall. Nails fixings to the weatherboards are rusting and some of the weatherboards have also split. Sections of scribes are missing at the southwest corner below the deck, at the side of the window opening and from the corner box in the northwest corner. The bars set into the small opening above the large doors are rusting.

East Elevation

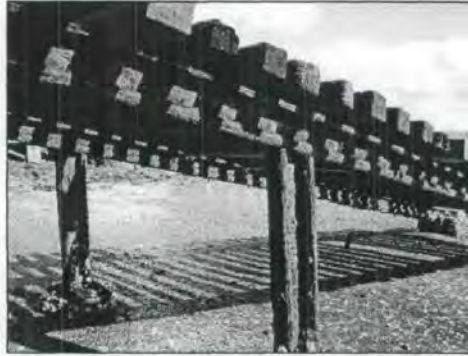
Some cracks are evident in the weatherboards and the nail fixings are generally rusting. The stringer to the stairs to the upper level has decayed and is in the process of being replaced. Scribes are missing from the corner box at the lower level. The lower door has some split boards. The door jamb has split and has been previously repaired.

South Elevation

Scribes are generally missing from the lower level and nail fixings to the weatherboards are rusting. The lower weatherboard towards the southeast corner has previously been replaced. Metal trays under the window sills are rusting and as are the ends of the tie rods at roof level.

Ramp

The railway lines supporting the ramp or skid have extensively corroded. The telegraph pole cross arm planks have cracked and weathered and some have previously been replaced. The bolt fixings have also rusted.



The ramp. Note heavily corroded runners and supports. Note also previously replaced planks.

INTERIOR

Subfloor

The steel plates that connected the bearers to the piles have extensively corroded or are missing. Bolt fixings connecting the angle braces to the piles have also rusted, in some cases completely through.

A full inspection was not able to be made of the piles due to the level of sand. The piles, as visually inspected, are in variable condition and a few have previously been replaced. Some piles have cracked or split and have been eroded by the action of the sea. There was some evidence of Toredoworm infestation. Some braces have split and, in some instances, decay is evident.

Joists and bearers have become "furry" as the result of the action of the sea. Some bearers, particularly along the north elevation have sagged.



Timber piles and braces. Note replacement piles. Brace has decayed.

Lower level

Some floor boards have worn and some have been previously replaced with plywood. Some locker doors have been replaced.

Upper level*Ceilings*

Ceiling panels at the upper level are generally sagging and some are in poor condition. There is evidence of previous water leaks. Some repairs have been carried out using plaster and elsewhere panels have been replaced with plain plaster board. Some battens are missing and others are loose.

Walls

The wall linings are generally in reasonable condition, however, there is some evidence of borer infestation.

Floor

Towards the doors at the western end of the building, some boards are loose and others have decay present at the ends. Decay is also evident in the ends of some boards. The nails in the floor boards have generally rusted and the varnish finish is wearing.

Services

Although a detailed electrical survey was not undertaken, it was observed that the wiring was the old fabric sheathed type.

5 CONSERVATION POLICIES

Following on from the Statement of Significance and taking into account statutory requirements, a series of conservation policies can be formulated to ensure that the significance of the Herne Bay Cruising Club building is retained and enhanced.

PROPOSED USES

Policy 1.1 The building should have a viable use as a means of aiding its survival. That use should be an appropriate use so as not to detract from significance.

The ICOMOS Charter states that *"the conservation of a place is usually facilitated by it serving a socially, culturally or economically viable purpose"*.

A heritage building should wherever possible, continue to be used for the purpose for which it was built as a way of maintaining its significance. The building in question was constructed as the boathouse for a yacht club and that use effectively ceased when the club relocated to Westhaven in 1940. It was then used by various other groups until it was purchased by the Herne Bay Cruising Club in 1965.

The lower floor is currently used as storage for kayaks and sailing gear, while the upper floor is used as a meeting space. The present low-key use is considered appropriate. More extensive use of the building is probably not feasible, at least in the short term, due to the lack of a water supply and foul water connection.

If these problems are resolved in the future, other uses could be contemplated. New uses should be those that do not require the removal of significant fabric or compromise the building's architectural integrity.

RETENTION OF SIGNIFICANCE

Policy 2.1 Significant fabric should be retained as a means of preserving overall significance. Any intervention should be undertaken with regard to the significance of the individual elements.

Fabric that is assessed as being significant contributes to the overall significance of the building and should be retained wherever possible. The building's significance is likely to be diminished if elements are subjected to inappropriate activities.

- *Considerable Significance. These elements should be retained unless extraordinary circumstances require their removal. Any intervention should be limited to processes of maintenance, repair or restoration.*
- *Some significance. Where possible, these elements should generally be retained in their present form. A greater degree of intervention may be permitted to accommodate, for example a new use.*

- *Not Relevant. These elements are generally not significant but allow the building to function. They may be retained, providing fabric of greater significance is not obscured.*

Policy 2.2 The contribution that fabric from different periods makes on the overall significance of the place should be considered.

Commentary

The ICOMOS Charter states *"the evidence of time and the contributions of all periods should be respected"*.

Following the completion of the lower level of the building in 1913, the upper storey was added in 1932. Since then, further minor changes have occurred.

Changes to a heritage building can be considered in two ways. The upper storey was obviously added to meet a particular need, being that for additional space. Although the upper storey is an addition, it should obviously remain as it makes a positive contribution to the overall significance of the place.

Similarly, the present French doors to the upper deck replaced an earlier single door and obviously make for a more pleasant environment. Although not exactly in the style of the building, they are indicative of a do-it-yourself philosophy and are relatively unobtrusive.

Policy 2.3 Remedial work should be carried out as required. Materials used should be consistent with the historic fabric and original construction techniques should be replicated.

Remedial work to the building should aim to preserve as much original fabric as possible. Material should only be replaced where it has ceased to function satisfactorily or where it is placing other fabric at risk. Material that has weathered but which is still in sound condition should be respected as evidence of the building's history.

Remedial work to the boathouse should match original work on the building. Original construction techniques and detailing should be replicated when undertaking such work and timber sizes and finish should match the original.

Policy 2.4 A regular maintenance regime should be implemented as a means of preserving fabric.

A planned regime of regular repair and maintenance will slow down the processes of decay and is an important weapon in any effort to preserve fabric in an historic building. In a structure such as the cruising club building with its location in a marine environment, regular maintenance assumes greater importance. It is recommended that a maintenance plan be prepared and implemented for the building once the remedial work has been completed.

INTERPRETATION**Policy 3-1 Appropriate interpretative material should be provided.**

It is suggested that appropriate interpretive material be placed in the boathouse describing its early history. Any changes that have taken place could also be described.

CONSERVATION STANDARDS**Policy 4.1 Appropriate standards should be maintained whenever work is carried out.**

As a way of maintaining the integrity of the place all work should conform to principles set out in the ICOMOS New Zealand Charter for the Conservation of Places of Cultural Heritage Value and in accordance with international standards for the conservation of places having cultural significance.

Policy 4.2 Conservation processes and other activities involving intervention should be recorded.

A record should be made by photographic or other means of the activities to which the boathouse is subjected and placed in an appropriate archive. This will ensure that a comprehensive account of the place is maintained for future reference. Recording is particularly important in areas where changes are occurring or where fabric is being removed or modified.

CONSULTATION AND REVIEW**Policy 5.1 Proposals for work on the boathouse should be reviewed by relevant authorities.****Commentary**

Any proposals for work on the boathouse should be discussed at an early stage with organisations such as the New Zealand Historic Places Trust to ensure that the work is generally in accordance with the principles as set down in the conservation plan and the requirements of ICOMOS.

Policy 5.2 This conservation plan should be reviewed from time to time and amended as necessary.**Commentary**

No conservation plan should ever be considered to ever be a final or completed document. Rather the plan should be viewed as a "working document". It should be reviewed from time to time, say every five years, and amended as required to incorporate new information.

6 IMPLEMENTATION OF CONSERVATION POLICIES

REMEDIAL WORK

A number of defects were observed and remedial work is required to return the building to a good condition. External remedial work should include the following:

Roof

- Rust was observed on the underside of roofing sheets and this is probably indicative of a wider problem. Replacement of the roof should be contemplated in the near future. Because of the difficulty of obtaining warranties for steel roofing in close proximity to the sea, an alternative material such as aluminium may need to be contemplated.
- At the time the roof is replaced, repairs should be carried out to soffit linings, rafters, purlins and bargeboards as required. The cause of the rafters sagging at the southeast corner should be investigated and the problem remedied.
- New cover boards should be fitted as required.

External Walls

- Non-matching weatherboards should be replaced. Rusting nails should be repunched and puttied. Split weatherboards should be repaired.
- Repairs should be carried out to trim such as corner boxes. Missing scribes should be replaced and loose scribes refixed.
- Deteriorated shingles, particularly in the west gable end, should be replaced with new shingles.
- Rusting metalwork such as sill trays and tie rod end plates should be treated or replaced.
- Repairs should be carried out to the joinery as required. This should include repairs to cracked boarding to doors and treatment of rusting hinges. Repairs are required to the door jamb on the east elevation. The doors to the upper deck should be eased.

Ramp

- The railway line runners have severely corroded and although collapse is unlikely, the engineer's report (see Appendix 3) recommends that ancillary members be provided alongside the original rails. Treatment of the rust should also be contemplated. Treatments of rusting metalwork range from simple repeated applications of fishoilene to sophisticated epoxy coatings. Further investigative work should be undertaken to determine the best method of treating the steelwork to the Herne Bay building. It is unlikely that any treatment will totally eliminate corrosion – at best it should slow the process down.

- Rusting bolts should be replaced with stainless steel fixings.
- Deteriorated and unsafe planks should be replaced. It is recommended that replacement planks be hardwood, dimensioned to match the original planks, rather than non-matching tanalised pine as has been used recently.

Subfloor

- A comprehensive investigation should be carried out of the piles and their footings, including the sections below the level of the sand. In the future, new piles, sized to match the original, should be provided as required.
- The condition of the braces below the sand level should also be investigated. New braces should be provided where the existing braces have decayed. Stainless steel fixings should be used throughout.
- New stainless steel plates should be provided to connect the bearers to the piles. Fixings, again should be stainless steel.
- Consideration could be given to treating the "furry" joists and bearers with an epoxy based boat-building consolidant such as "Everdure".
- Additional support may be required to the sagging bearers, particularly along the northern wall line.

Building Interior

Internal defects are as noted previously. Remedial work may include the following:

- Deteriorated and previously repaired ceiling panels should be replaced. New panels should match the original. Timber trim should be repaired and refixed as required or replaced where missing.
- The building interior should be treated for borer.
- At the upper level, decayed flooring timber should be repaired as required and a new clear finish applied to the entire floor.
- The electrical wiring should be replaced.

7

BIBLIOGRAPHY AND INFORMATION SOURCES

Published

- Ashford, Jeremy *The Bungalow in New Zealand.* Penguin Books, New Zealand. 1994.
- Kerr JS *The Conservation Plan: A Guide to the Preparation of Conservation Plans for Places of European Cultural Significance (3rd Edition)* National Trust of Australia.
- Kidd H & Elliott R *Lee Rail – A Centennial History of the Richmond Yacht Club 1903 – 2003.* Richmond Yacht Club 2003.
- Salmond, Jeremy *Old New Zealand Houses 1800 – 1840,* Reed Methuen. 1986.
- Shaw, Peter *New Zealand Architecture from Polynesian Beginnings to 1990.* Hodder & Stoughton New Zealand. 1991

Unpublished

- Pearson, Dave *Takapuna Boating Club – a Conservation Plan.* Prepared for the Takapuna Boating Club by Dave Pearson Architects Ltd, April 2002.

Other Sources

- The ICOMOS New Zealand Charter for the Conservation of Places of Cultural Heritage Value.
- New Zealand Historic Places Trust. *Guidelines for Preparing Conservation Plans.*
- Herne Bay Cruising Club. Discussion document in support of an application for Coastal Consent to continue occupation of the Coastal Marine Area.

APPENDICES

APPENDIX 1

Plans and Drawings

APPENDIX 2

Certificate of Incorporation

APPENDIX 3

Structural Engineer's report (Gary Chester Consultants)

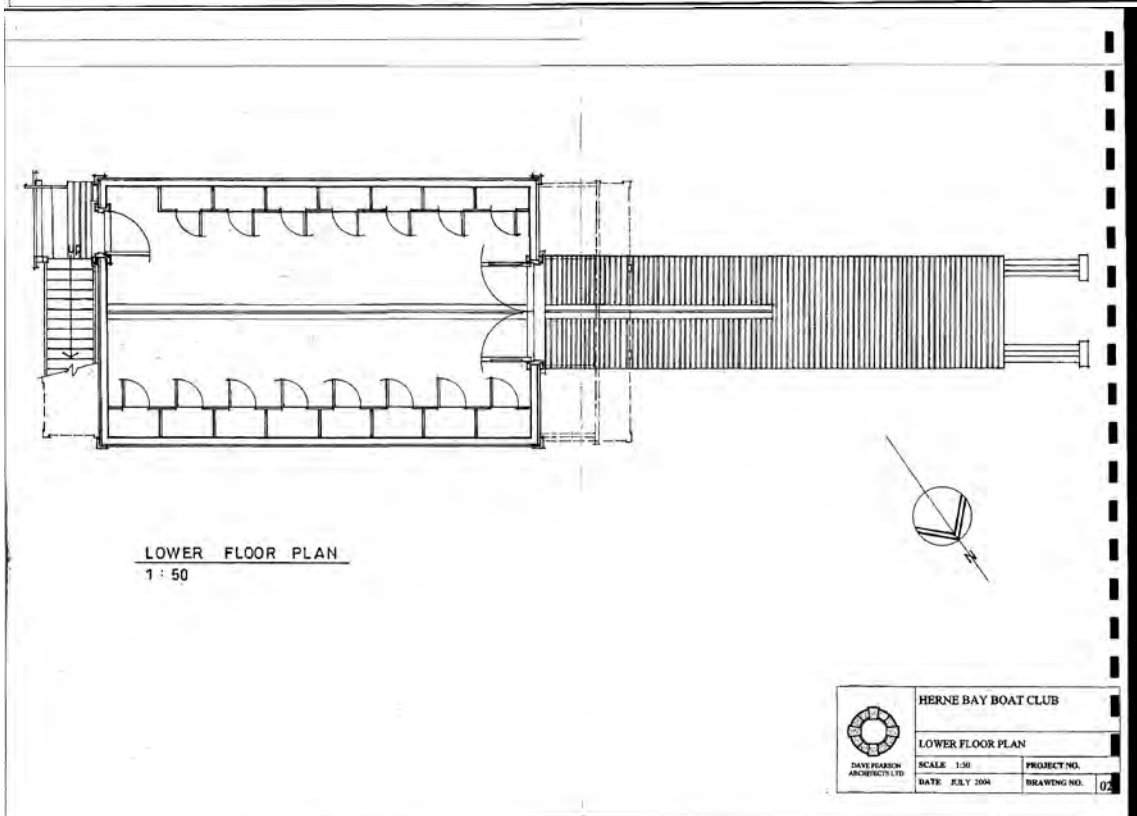
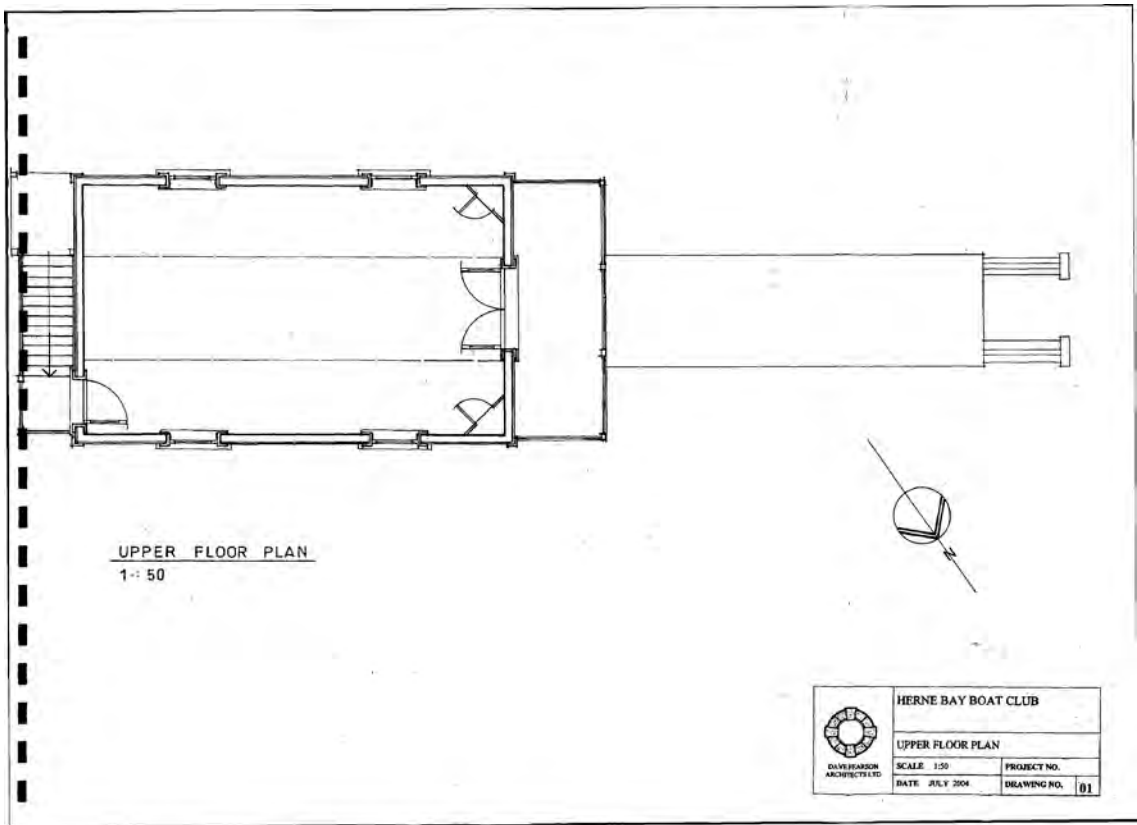
APPENDIX 4

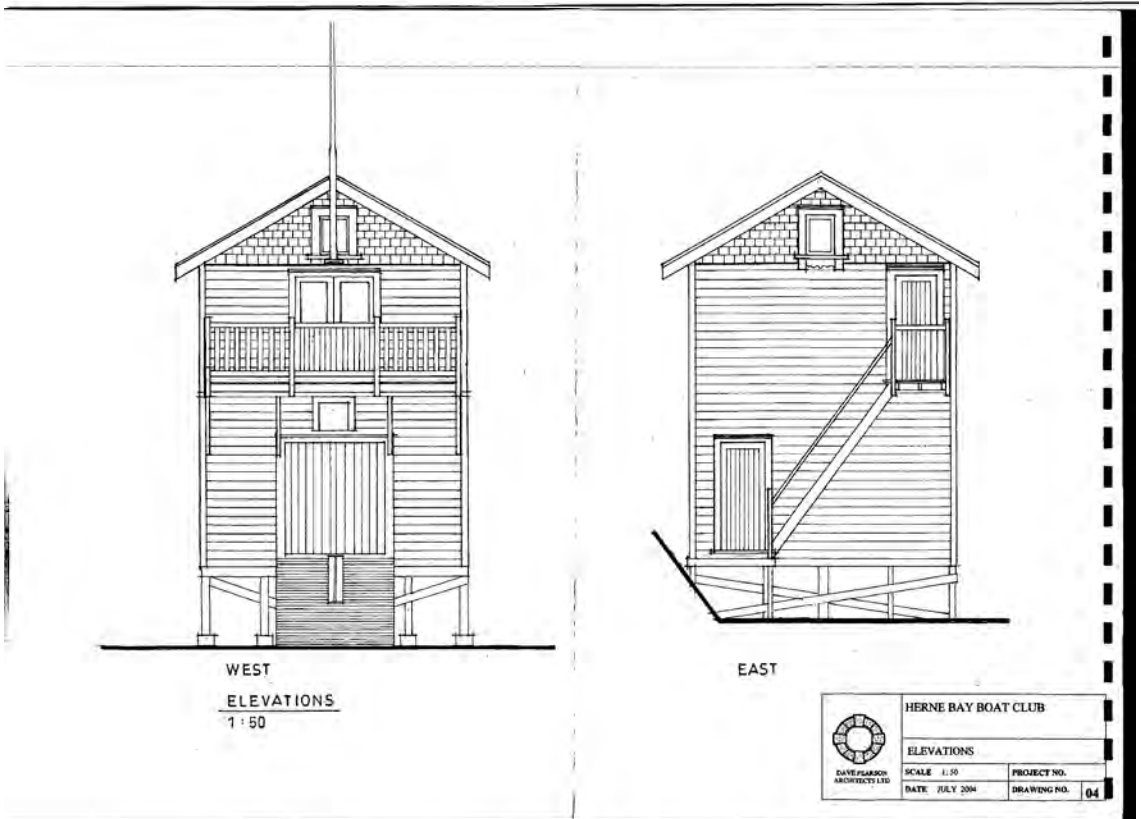
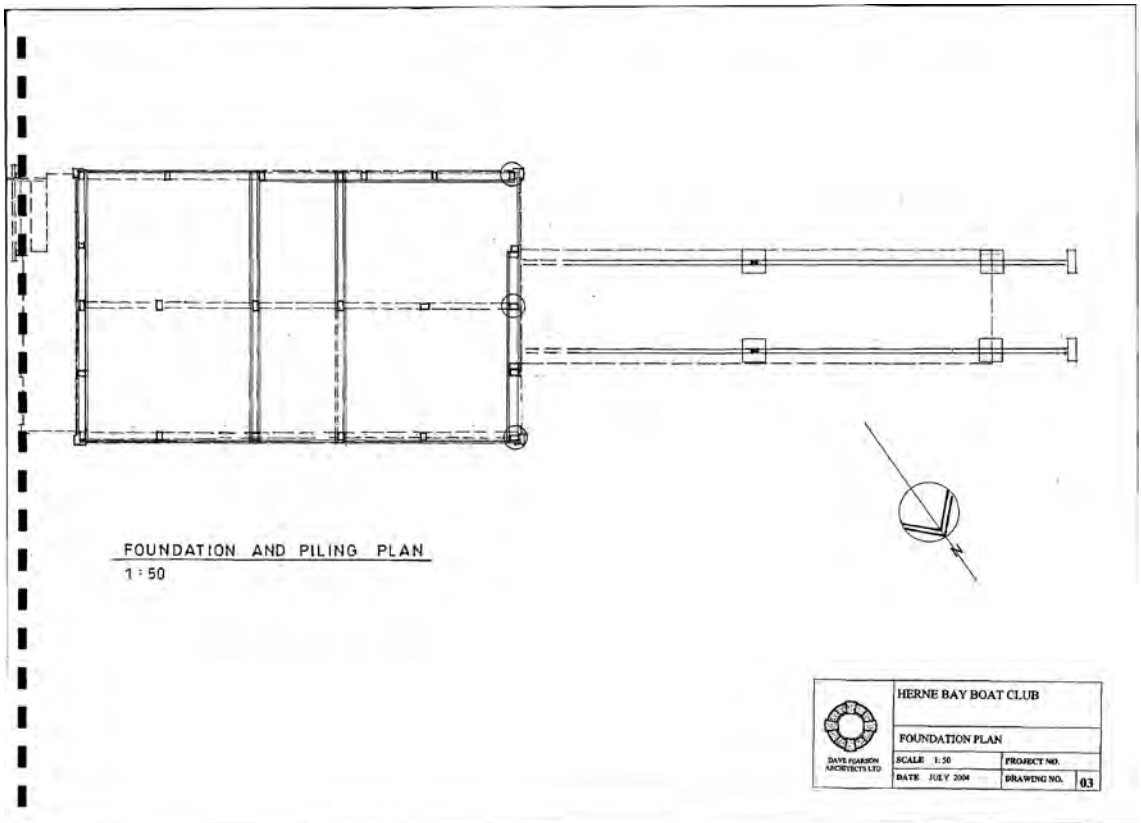
ICOMOS New Zealand Charter for Places of Cultural Heritage Value

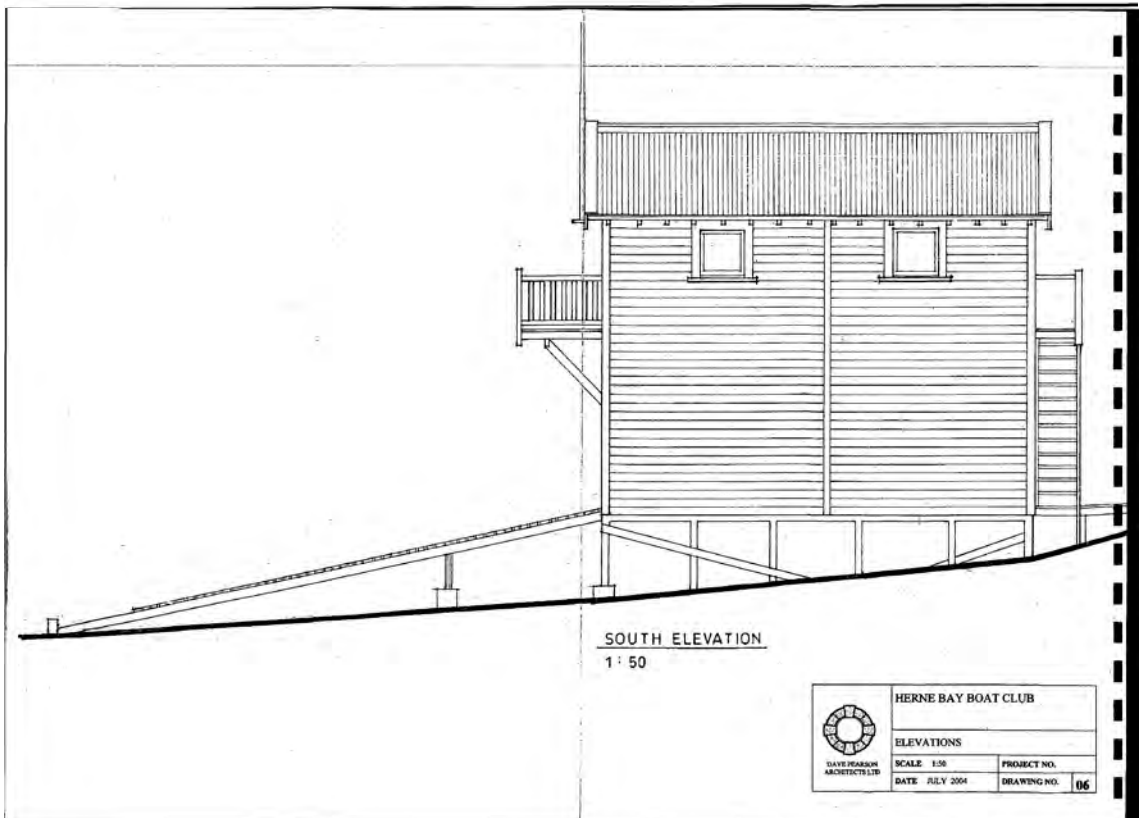
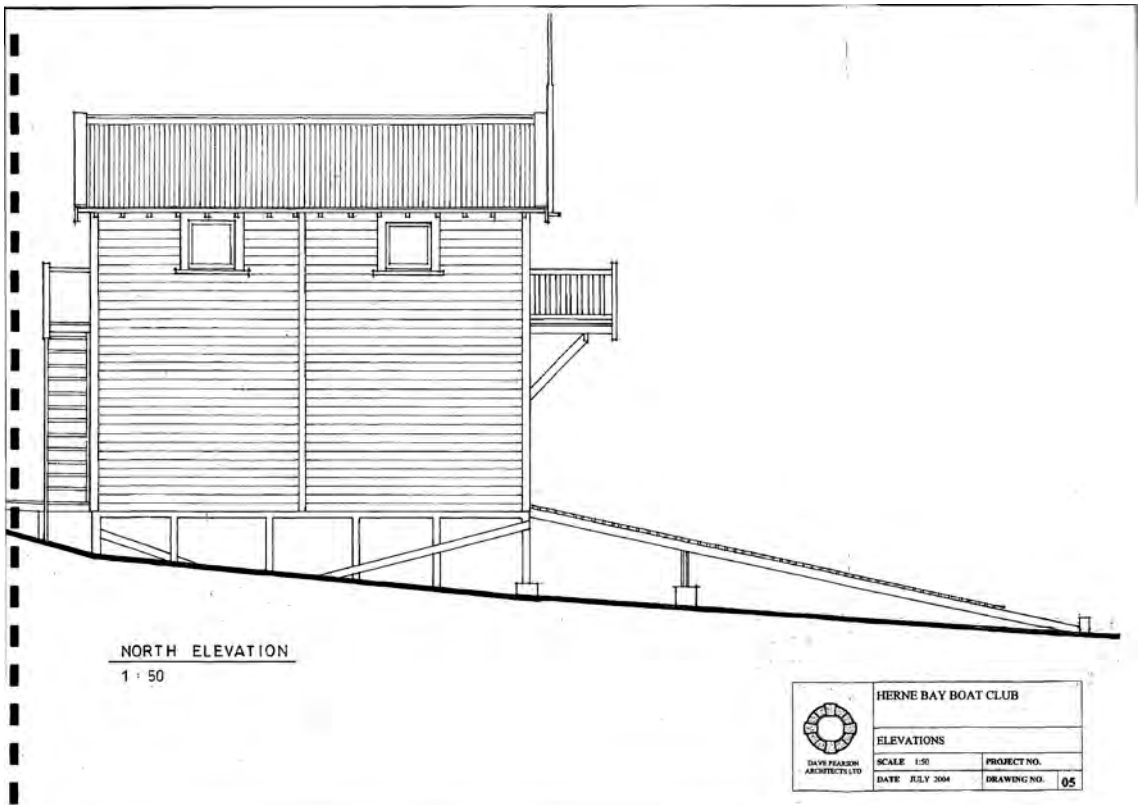
APPENDIX 1

DRAWINGS & PLANS

Floor Plan and Elevations as Drawn by Dave Pearson Architects Limited, August 2004.







APPENDIX 2

CERTIFICATE OF INCORPORATION



CERTIFICATE OF INCORPORATION

**HERNE BAY CRUISING CLUB
916135**

This is to certify that HERNE BAY CRUISING CLUB was incorporated under the Charitable Trusts Act 1957 on the 31st day of July 1998.



Neville Harris

Neville Harris
Registrar of Incorporated Societies
31st day of July 1998



1 OF 1

APPENDIX 3

STRUCTURAL REPORT

As prepared by Gary Chester Consultants Ltd

phone
09 418 1551

fax
09 480 1148

email
andrew@gccons.co.nz

20 September, 2004

our ref: 3473

Dave Pearson Architects Ltd
P.O. Box 32 318,
Devonport
NORTH SHORE CITY

fax: 4458447

Dear Dave,

Re: Boathouse, Herne Bay Cruising Club, Marine Parade, Herne Bay.

Further to your request we write to report on the structural condition of the ground floor substructure on the above building.

We visited the building on 30 August 2004 at low tide and carried out a visual inspection of the piles, bearers, braces and floor joists. The piles were embedded in the beach within concrete and could only be viewed above ground level.

Piles

The piles appear to be original with the exception of three supplementary members which are 125mm Senton piles installed to provide support to defective piles. The original piles have varying degrees of defects due to their location in the tidal zone and from toredo worm. At the present time based on our inspection the piles appear to be able to adequately support the loads from the structure above, no piles appeared to need replacement at the time of our inspection, their condition should however be monitored and defective piles replaced in the future. The connection between the piles and bearers is through badly rusted steel bolts and plates. These need to be supplemented by new stainless steel fixings to ensure that the building is adequately tied to the foundations.

Bearers

The bearers are meanly sized by today's standards and as a result have deflected significantly. The use of the boathouse should be monitored to ensure that the loads on the bearers are not increased to the point where the bearers fail. One of the internal bearer spans has the joint between two bearers occurring between piles, this should be rectified by supplementing the bearer with a new member alongside it spanning pile to pile.

Braces

The building is in a very high wind zone and relies on timber braces to transfer the lateral loads down from the suspended ground floor to the base of the piles. The braces rely on bolted fixings top and bottom which have in almost every case badly corroded or appear to be missing. Thus the bracing capacity has been significantly reduced. The bolts should be replaced with stainless steel bolts and washers as soon as practicable to avoid potential damage to the building. Defective timber members should also be replaced with H5 treated members of a similar size as soon as practicable.

Floor Joists

The joists appeared to be adequately sized for their present loading but if changes to the usage of the building are made an assessment of their capacity will need to be undertaken. Around the perimeter of the building the exposed ends of the joists are partially rotting and should be protected with a timber preservative and monitored to ensure they retain their strength.

Boat ramp

A suspended ramp is attached to the seaward side of the building to allow small boats and canoes to be accessed from storage. The main bearers are made of railway track which have corroded badly and should be supplemented with engineer designed beams. The bearers should be treated with a protective coating to arrest the corrosion. Some of the original timber planking has split and is in need of replacement. The remaining planking should be monitored and replaced as necessary. All bolts and washers should be replaced with stainless steel of a similar size.

At the time that the remedial work is carried out to the braces, bearers and fixings further investigation of the piles should be carried out by exposing all piles down to their concrete foundations. We recommend that we be retained to reinspect the piles at that stage and advise further.

We trust the above meets your requirements at the present time. If you require any further assistance please contact us.

Yours Faithfully
GARY CHESTER CONSULTANTS LTD

A.H.MARRIOTT
(BE MIPENZ)

phone
09 418 1551

such heritage exists. Particular knowledge of heritage values is entrusted to chosen guardians. The conservation of places of indigenous cultural heritage value therefore is conditional on decisions made in the indigenous community, and should proceed only in this context. Indigenous conservation precepts are fluid and take account of the continuity of life and the needs of the present as well as the responsibilities of guardianship and association with those who have gone before. In particular, protocols of access, authority and ritual are handled at a local level. General principles of ethics and social respect affirm that such protocols should be observed.

3 CONSERVATION PRACTICE

Appropriate conservation professionals should be involved in all aspects of conservation work. Indigenous methodologies should be applied as appropriate and may vary from place to place. Conservation results should be in keeping with their cultural content. All necessary consents and permits should be obtained.

Conservation projects should include the following:

- (i) definition of the cultural heritage value of the place, which requires prior researching of any documentary and oral history, a detailed examination of the place, and the recording of its physical condition;
- (ii) community consultation, continuing throughout a project as appropriate;
- (iii) preparation of a plan which meets the conservation principles of this charter;
- (iv) the implementation of any planned work; and
- (v) the documentation of any research, recording and conservation work as it proceeds.

GENERAL PRINCIPLES

4 CONSERVATION METHOD

Conservation should:

- (i) make use of all relevant conservation values, knowledge, disciplines, arts and crafts;
- (ii) show the greatest respect for, and involve the least possible loss of, material of cultural heritage value;
- (iii) involve the least degree of intervention consistent with long term care and the principles of this charter;
- (iv) take into account the needs, abilities and resources of the particular communities; and
- (v) be fully documented and recorded.

5 RESPECT FOR EXISTING EVIDENCE

The evidence of time and the contributions of all periods should be respected in conservation. The material of a particular period may be obscured or removed if assessment shows that this would not diminish the cultural heritage value of the place. In these circumstances such material should be documented before it is obscured or removed.

6 SETTING

The historical setting of a place should be conserved with the place itself. If the historical setting no longer exists, construction of a setting based on physical and documentary evidence should be

the aim. The extent of the appropriate setting may be affected by constraints other than heritage value.

7 RISK MITIGATION

All places of cultural heritage value should be assessed as to their potential risk from any natural process or event. Where a significant risk is determined, appropriate action to minimise the risk should be undertaken. Where appropriate, a risk mitigation plan should be prepared.

8 RELOCATION

The site of an historic structure is usually an integral part of its cultural heritage value. Relocation, however, can be a legitimate part of the conservation process where assessment shows that:

- (i) the site is not of associated value (an exceptional circumstance); or
- (ii) relocation is the only means of saving the structure; or
- (iii) relocation, provides continuity of cultural heritage value;

A new site should provide a setting compatible with cultural heritage value.

9 INVASIVE INVESTIGATION

Invasive investigation of a place can provide knowledge that is not likely to be gained from any other source. Archaeological or structural investigation can be justified where such evidence is about to be lost, or where knowledge may be significantly extended, or where it is necessary to establish the existence of material of cultural heritage value, or where it is necessary for conservation work. The examination should be carried out according to accepted scientific standards. Such investigation should leave the maximum amount of materials undisturbed for study by future generations.

10 CONTENTS

Where the contents of a place contribute to its cultural heritage value, they should be regarded as an integral part of the place and be conserved with it.

11 WORKS OF ART AND SPECIAL FABRIC

Carving painting, weaving, stained glass and other arts associated with a place should be considered integral with a place. Where it is necessary to carry out maintenance and repair of any such material, specialist conservation advice appropriate to the material should be sought.

12 RECORDS

Records of the research and conservation of places of cultural heritage value should be placed in an appropriate archive and made available to all affected people. Some knowledge of places of indigenous heritage value is not a matter of public record, but is entrusted to guardians within the indigenous community.

CONSERVATION PROCESSES

13 DEGREES OF INTERVENTION

Conservation may involve, in increasing extent of intervention: non-intervention, maintenance, stabilisation, repair, restoration, reconstruction or adaptation. Where appropriate, conservation processes may be applied to parts or components of a structure or site.

Re-creation, meaning the conjectural reconstruction of a place, and replication, meaning to make a copy of an existing place, are outside the scope of this charter.

14 NON-INTERVENTION

In some circumstances, assessment may show that any intervention is undesirable. In particular, undisturbed constancy of spiritual association may be more important than the physical aspects of some places of indigenous heritage value.

15 MAINTENANCE

A place of cultural heritage value should be maintained regularly and according to a plan, except in circumstances where it is appropriate for places to remain without intervention.

16 STABILISATION

Places of cultural heritage value should be protected from processes of decay, except where decay is appropriate to their value. Although deterioration cannot be totally prevented, it should be slowed by providing stabilisation or support.

17 REPAIR

Repair of material or of a site should be with original or similar materials. Repair of a technically higher standard than the original workmanship or materials may be justified where the life expectancy of the site or material is increased, the new material is compatible with the old and the cultural heritage value is not diminished. New material should be identifiable.

18 RESTORATION

Restoration should be based on respect for existing material and on the logical interpretation of all available evidence, so that the place is consistent with its earlier form and meaning. It should only be carried out if the cultural heritage value of the place is recovered or revealed by the process.

The restoration process typically involves reassembly and reinstatement and may involve the removal of accretions.

19 RECONSTRUCTION

Reconstruction is distinguished from restoration by the introduction of additional materials where loss has occurred. Reconstruction may be appropriate if it is essential to the function or understanding of a place, if sufficient physical and documentary evidence exists to minimise conjecture, and if surviving heritage values are preserved. Reconstruction should not normally constitute the majority of a place. Generalised representations of typical features or structures

should be avoided.

20 ADAPTATION

The conservation of a place of cultural heritage value is usually facilitated by it serving a socially, culturally or economically useful purpose. In some cases, alterations and additions may be acceptable where they are essential to continued use, or where they are culturally desirable, or where the conservation of the place cannot otherwise be achieved. Any change, however, should be the minimum necessary and should not detract from the cultural heritage value of the place. Any additions and alterations should be compatible with the original fabric but should be sufficiently distinct that they can be read as new work.

21 INTERPRETATION

Interpretation of a place may be appropriate if enhancement of public understanding is required. Relevant protocol should be complied with. Any interpretation should not compromise the values, appearance, structure or materials of a place, or intrude upon the experience of the place.

22 DEFINITIONS

For the purposes of this charter:

adaptation means modifying a place to suit it to a compatible use, involving the least possible loss of cultural heritage value

conservation means the processes of caring for a place so as to safeguard its cultural heritage value

cultural heritage value means possessing historical, archaeological, architectural, technological, aesthetic, scientific, spiritual, social, traditional or other special cultural significance, associated with human activity

maintenance means the protective care of a place

material means physical matter which is the product of human activity or has been modified by human activity

place means any land, including land covered by water, and the airspace forming the spatial context to such land, including any landscape, traditional site or sacred place, and anything fixed to the land including any archaeological site, garden, building or structure, and any body of water, whether fresh or seawater, that forms part of the historical and cultural heritage of New Zealand

preservation means maintaining a place with as little change as possible

reassemble (anastylosis) means putting existing but dismembered parts back together

reconstruction means to build again in the original form using old or new material

reinstatement means putting components of earlier material back in position

repair means making good decayed or damaged material

restoration means returning a place as nearly as possible to a known earlier state, by reassembly, reinstatement and/or the removal of extraneous additions

stabilisation means the arrest of the processes of decay

structure means any building, equipment, device or other facility made by people and which is fixed to the land

Appendix 2 Supplementary research



AUCKLAND REGION BOATSHED
STUDY
PREPARED FOR
AUCKLAND REGIONAL COUNCIL
BY
MATTHEWS & MATTHEWS ARCHITECTS LTD
Draft July 2007

TABLE OF CONTENTS

A STUDY OF BOATSHEDS WITHIN AUCKLAND REGION	1
BACKGROUND	1
METHODOLOGY AND RESOURCES	2
GENERAL OVERVIEW OF BOATSHEDS	3
BOATSHEDS IN NEW ZEALAND	5
BOATSHEDS IN THE AUCKLAND CONTEXT.....	8
GROUPS AND INDIVIDUAL SHEDS	8
RECOMMENDATIONS.....	11
HERNE BAY BOATSHEDS	12
NORTH SHORE	58
UN-CONSENTED NORTH SHORE STRUCTURES	98
TAMAKI ESTUARY	107
HUIA – MANUKAU HARBOUR.....	120
WAIHEKE ISLAND	127
KAWAU ISLAND.....	151
APPENDICES	162
Appendix A- Schedule of Structures researched	163

A STUDY OF BOATSHEDS WITHIN AUCKLAND REGION

This study has been commissioned by Auckland Regional Council to provide a regional context for the evaluation of the heritage significance of boatsheds in the Auckland Region. Part of this study was to identify significant individual sheds and groupings of sheds.

BACKGROUND

Boatsheds, together with other built structures, were identified by Taylor (1994) as a significant gap in the Auckland Regional Council's inventory of maritime heritage. This lack of inventory and assessment of the significance of boatsheds in the region creates difficulties when assessing applications to remove, modify, or retain boatsheds.

The outcomes of the study are

- A (brief) discussion on the significance of boatsheds within NZ generally and in the Auckland Region in particular
- A general overview of geographical locations of boatsheds in the region, and where those of heritage value are most likely to be located (e.g. Herne Bay);
- Record of sheds visited, including digital photograph/s of exterior/s on CDROM. Field records of sheds of possible or actual heritage value are to include sufficient information to create CHI records. This is to include GPS location & buffer, whether the structure is in the CMA, ACZ, LBD or a combination, and any cross reference to coastal databases.
- A list of known sheds or groups of sheds not visited. CHI record forms have been completed for consented sheds not visited at Arran Bay Waiheke (1) and Kawau Island (7) Forms have previously been completed for sheds on Rangitoto and Hobson Bay and are therefore not included within this study (although their contextual significance is considered in the wider context).
- A list of boatsheds of regional or district significance recommended for inclusion in Schedule 2 of the Regional Plan Coastal (where they lie within the CMA) or in district plan schedules, with a brief statement of significance for each.
- Recommendations for further work
- Bibliography of sources on boatsheds in the Auckland Region and on the significance of boatsheds in New Zealand generally, with an indication of any sources not consulted

METHODOLOGY AND RESOURCES

Records were provided by ARC of both consented and unlawful structures which have no consent. A third list of sheds was provided of 244 structures compiled from a Marine Department database.

There are a number of agencies holding files for boatshed structures. The main repositories are National Maritime Museum (Auckland Harbour Board Files), Archives New Zealand (Marine Department Files) and the ARC. The ARC inherited a number of files relating to foreshore structures. Many of these files are held at Archives New Zealand but are not part of the archives collection. Viewing these files was arranged via the archivist at ARC.

A review of the ARC database and mapping was carried out which identified consented structures and their approximate location. This was cross-checked with the lawful structures file provided by ARC. Some structures on the database list were not found. A list of 84 sheds with known file references and consent numbers was prepared ((refer to appendices). Research of the ARC files has been completed for all these sheds and a CHI form completed.

In parallel with this an inspection of shed was carried out. A record of all boatsheds that are partially in the CMA zone was carried out which included recording a brief description, GPS location and photographing each structure.

Information gathered as part of this inspection process was cross referenced to the sheds researched, and GPS information and photographs added to the CHI forms. In addition forms were filled out for sheds for which there appears to be no consent and therefore no file reference. These are noted as such.

GENERAL OVERVIEW OF BOATSHEDS

Boatsheds form part of a range of foreshore structures that are associated with the intensive use of the harbour and waterways for transportation and recreation, particularly prior to the extensive development of road-based transportation in the 1950s.

Historic photographs of foreshore locations around Auckland dating from the late 19th and early 20th centuries show that boatsheds were a common feature in many coastal locations. Typically designed and constructed as economical utilitarian structures, they provided simple shelters for the storage and servicing of boats. Generally they were timber framed clad with corrugated steel roofs, with walls finished in weatherboards, timber board and batten or corrugated steel.

Mark Tatton in his thesis, *The boatshed and Living Beyond the Edge* identifies three types of sheds¹. The first type is found on inland lake areas where there is no tidal change and the sheds are built over the water. The second type which is that predominately found in the Auckland Region is where the shed is constructed on timber or concrete piles over the water. The third type identified is where the shed is constructed on land above the high watermark and a concrete or timber ramp or slipway connects it to the water. Of course there are also many sheds which are both built on land and partly over the water.

Surviving boatsheds or groups of these structures have become an iconic feature of parts of the Auckland coastline. Although the shoreline in Auckland has changed significantly over time, with reclamations, road, rail and motorway development, boatsheds have been enduring as a building type, providing evidence of the ongoing use of the harbour in Auckland. Many of the sheds that now remain are primarily related to recreational use rather than commercial maritime activities.



View in 1896 of boatsheds in Judges Bay, Auckland. While these particular sheds no longer exist the typical building form remains a familiar sight in many other locations. Auckland City Libraries 1-W 188.



View of St Heliers Bay in 1911, showing boatsheds along the shoreline. These no longer remain, the foreshore having been modified by the construction of Tamaki Drive which opened in 1931.
Auckland City Libraries 1-W127.



Boatsheds on the east side of St Georges Bay in Auckland in 1911. Reclamation of much of the harbour close to central Auckland has meant that many of the early groups of boatsheds no longer remain.
Auckland City Libraries 1-W1473

BOATSHEDS IN NEW ZEALAND

Boatsheds have been part of the New Zealand coastline for almost as long as Europeans settled here from the mid 1800s onwards. The simple gabled shed built on or close to the sea edge was documented in a sketch of Kawau Island in 1853.

Sheds were constructed for commercial use associated with boat repair or services. Early photographs of the Auckland regions such as Herne Bay show these larger structures. Generally this type has disappeared from the Auckland area, with the decline in commercial boating or replaced with more modern larger structures. A memorable example is the shed located near Whangateau east of Warkworth.



Boatshed near Whangateau



View of boats and boatsheds in St Mary's Bay, Auckland in 1914. The construction of the motorway linking to the Harbour Bridge has changed the foreshore in this part of Auckland. Auckland City Libraries 1-W459.

The largest number of sheds and subsequently many that now remain are associated with private recreational use, built to house 'pleasure craft'. However sheds were also constructed for yacht clubs and the Herne Bay Cruising Club building is probably the only remaining one of these in Auckland from the early 20th century.

Some of the oldest surviving boatsheds in New Zealand are the Antigua Boatsheds in Christchurch, which were built in 1882, to cater for demand for boats for hire. These provided a range of facilities for boating including a landing stage, ladies waiting room, gentleman's changing room and storage for a large number of recreational boats.



The nearby Montreal Street Boatsheds were destroyed by fire in 1929, so the Antigua Boatsheds survive as the only commercial boatsheds on the Avon in Christchurch. These boatsheds are registered in Category II by the New Zealand Historic Places Trust, the only example of this building type to be registered by the Trust.

Antigua Boatsheds, Christchurch

Other significant groups of boatsheds include the iconic group at Titahi Bay and in Oriental Bay in Wellington. These sheds were built to house pleasure craft by private individuals.



View of boatsheds in Oriental Bay in Wellington, in 1891
Alexander Turnbull Library 42051_ac_1_1



Boatsheds in Oriental Bay, Wellington.
Alexander Turnbull Library 37621_ac_3_1.



Boatsheds in Titahi Bay in Wellington are a local landmark. Image source: Te Ara

¹ Tatton, Mark. S – The Boatshed and Living Beyond the Edge. Undergraduate Thesis School of Architecture University of Auckland 1987

BOATSHEDS IN THE AUCKLAND CONTEXT

Surviving boatsheds in Auckland primarily date from the 20th century. Research indicates that the earliest boatshed structures include the Herne Bay Cruising Club (first constructed around 1913), the eastern-most sheds in Herne Bay as well as the group of boatsheds at Rocky Bay on Waiheke Island.

The majority of boatsheds date from the interwar period with the Herne Bay Cruising Club being the single exception to this. There also appears to be a number of sheds that were constructed after the Second World War. There do not appear to be any surviving boatsheds from the late 19th century.



Yachts and boatsheds in Herne Bay, Auckland. Alexander Turnbull Library, 46260_ac_1_1

GROUPS AND INDIVIDUAL SHEDS

There are identifiable groups of sheds within a small geographic locations such as those at Rocky Bay Waiheke, Herne Bay and Rangitoto. These sheds are generally individual in that they were constructed by individual private owners over a period of time.

There are also individual sheds that are generally isolated and do not form part of a recognisable group. An example of this would be the single boatshed at Little Huia built by the Turners. There may have once been other sheds in these areas but they have now been removed or destroyed.

Unique within the Auckland Region is the group of sheds at Hobson Bay where all sheds are designed using similar materials and forms to give a unified appearance. These were constructed over a relatively short time frame and the design of these was controlled to ensure a consistent uniform appearance. Other comparable groups would be the Tetahi bay or Oriental Bay sheds in Wellington

Generally all remaining sheds are associated with early residential subdivisions dating from the late 19th and beginning of the 20th century. Land owners applied to be able to construct sheds that were immediately adjacent to their properties that had waterfront access. There are exceptions and these include the groups of sheds at Rocky Bay and Hobson Bay. In these locations sheds were grouped close together.

In Auckland the Harbour Board were responsible for the issuing of leases for sites for boat sheds and skids below Mean High Water Mark within the Board's Endowment area.

By the 1930s, there were an increasing number of requests for licences to construct boatsheds from individual boat owners wishing to house their craft, as well as businesses wishing to erect sheds for commercial purposes.ⁱⁱ Around the same time town planning became a statutory obligation. All boroughs with populations exceeding 1000 were to provide the Town Planning Board with a scheme governing future building.ⁱⁱⁱ The Harbour Board began to consider standardising the design and colour of boatsheds in some locations, such as those in Hobson Bay, and they also consulted periodically with Auckland City Council's Town Planning Board.

In August 1930 the Auckland Harbour Board were informed by the Marine Department that plans of all structures below the high water mark were required to be approved by the Governor General. This was a requirement of the Harbours Act, 1923. Plans of all proposed sheds were sent to Wellington for approval.^{iv}



The boatsheds in Hobson Bay were built after the formation of first rail and then road connections from the city to the eastern bays, which occurred in the mid to late 1920s and early 1930s. Requests for sites for boatsheds in Hobson Bay were made to the Auckland Harbour Board from as early as 1924, but the first of the boat sheds were not actually built until 1930. A standard design, with three different size variants was prepared by the Harbourmaster and an engineer in August 1930. The individual boatsheds were required to be based on this model.



Boatsheds at Huia, Auckland.



Boatsheds at Herne Bay, Auckland.



Boatsheds at Rocky Bay on Waiheke Island.

The most significant and consistent groups found as part of this study of boatsheds in the Auckland Region are the groups at Rocky Bay on Waiheke Island, and the Herne Bay Group.

Groups such as the Hobson Bay Boatsheds are recognised as being of heritage significance and are included in the Auckland Regional Plan heritage schedule.

The Auckland Regional Council have recognised that the Auckland region has a rich maritime heritage that contributes significantly to the character of the coastal environment and to the identity of the region. This maritime heritage includes places associated with Māori settlement and tradition; historic buildings, wharves and jetties; shipyards; boatsheds, lighthouses and beacons; seawalls, coast defence installations; and shipwrecks and hulks. The Regional Council's Cultural Heritage Inventory includes a range of these places and structures, including some examples of boatsheds.

RECOMMENDATIONS

Sheds considered for scheduling

The most significant groupings of sheds found as part of this study are the Herne Bay and Rocky Bay Sheds. The Herne Bay group are some of the earliest remaining sheds in the Auckland area, and are an iconic and high visible group when viewed from the harbour and bridge. One shed is associated with prominent business family the Courts, owner of John Courts.

The Rocky Bay, Waiheke sheds are significant in that they are relatively early, and are the single largest group of individual boatsheds constructed in the Auckland Region. Unlike the Hobson Bay sheds they are not designed to a uniform style. In this regard they are representative of the groupings of sheds that use to be located in small bays around the Auckland area such as Judges and St Georges Bay.

There are individual sheds that are significant in that they a representative of the boatshed type, are associated with prominent people or organisations or have high setting or landscape values. The most significant example would be the Herne bay Cruising Club. This was originally constructed around 1913 and added to in 1932. It is associated with the Club which was established over 100 years ago in 1906.

Another example is the Turners boatshed near Little Huia. This shed dates from the 1930s, has high landscape and setting values and was built by the Turner family associated with the Turners and Growers Ltd business.

FURTHER WORK

Further work is require to inspect sheds I Arran Bay on Waiheke Island, and sheds on Kawau Island. Further work is also required to assess what sheds may remain on other Gulf Island such as Great barrier and smaller islands such as Rakino. Preliminary research using the ARC data base indicates there are no current consents for structures on these islands but there is likely to be existing boatshed.

ⁱⁱ Whakatakataka Bay Boat sheds: General, 1930-1995, HOB/NGA/001, Auckland Regional Council, 20 June 1930.

ⁱⁱⁱ G. W. A. Bush, *Decently and in Order: The Centennial History of the Auckland City Council*, Auckland, 1971, p.290.

^{iv} Whakatakataka Bay Boat sheds: General, 1930-1995, HOB/NGA/001, Auckland Regional Council file held at Archives New Zealand, Auckland, Box 40538, 19 August 1930.N

HERNE BAY BOATSHEDS



Auckland Public Libraries Map 4180



Herne Bay Auckland Public Libraries Neg W216



Herne Bay Auckland Public Libraries Neg 1 W26



Home Bay, End of Wallace St A P L Neg 1 W836



Tibbs Bay, Cremorne St A P L Neg 1 W844

'Site Type'	BOATSHED
Name	
Location	Adj 55 Sarsfield Street Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	<p>Single level gabled boatshed. Corrugated steel roofing, bevelled back weather board walls. Timber framed windows and timber tgv front doors. New timber piles in concrete footing. Original timber ramp in place.</p> <p>The boatshed was constructed in horizontal weather boards with a gabled corrugated iron roof. To one side of the boat shed is a 'lean to', however, the style of the weatherboard and the roofing suggests the 'lean-to' was built at the same time as the structure. The boatshed roofline on one side has a steep pitch giving the frontal appearance of being lop sided. The boatshed measures 47ft (14.33m) x 22ft.6inches (86m) and was built on 8inch x 10ft concrete piles. The landing and ramp was constructed in timber on concrete piles with the length and width of the slipway 30m x 1.83m.^v The total measurements of the boatshed, landing and ramp are: the boatshed is 98m², the landing is 10m² and the ramp is 55m². The shed has two windows and a door that leads out onto a deck (14.1ft x 6ft.6inches) and to a set steps.^{vi}</p> <p>A boat could be launched at all tides due to the tide reaching the cliff face.^{vii}</p>
Notes	<p>In 1929 George Hodgson applied to the Auckland Harbour Board for a foreshore licence to construct a boatshed and skids. The Marine Department, Wellington had to approve the plans before the Auckland Harbour Board would grant a licence. The property has riparian rights.^{viii}</p> <p>In 1941 the property, boatshed, skids and automatic transfer of the foreshore licence at 55 Sarsfield Street was purchased by James Bell Donald an Auckland Merchant. The foreshore licence fee was set at £2 per annum.^{ix}</p> <p>Previous Owners: Hodgson sold the property and boatshed to Donald in 1941. Donald transferred the foreshore licence to C.S. Lumley in 1946 and then to Mrs. Ramsay at 57 Sarsfield Street in 1951. Donald sold the property plus the boatshed and ramp to N.S. Hopwood in 1952, and he sold it to Allan Barlow Cheavins & Maina Adeline Pascoe in 1966. They sold it to Dr. Frederick Stuart & Diana Margaret Felicity Ferguson in 1979 (there was not a current foreshore licence operating). In 2005 the property was owned by the Ferguson's and David Hugh Rishworth and the foreshore licence was transferred to the Sarsfield Trust & Trust Members.^x After the death of her mother, Mrs. Ferguson became part owner of 5 Masefield Street, Herne Bay. In 1972 it was Mrs. Ferguson's intention to build another boatshed at this location. In 1979 she had decided not to proceed with the construction of a boatshed; however, a plan (dated 1981) is lodged in the ARC file No: PTCSARS005. MHWM at the rear of the boatshed. Rocky cliff face.</p>
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCSARS005
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	S & D Ferguson
'Owners Address'	55 Sarsfield Street
'Date of Construction'	1929

'Photo Image'

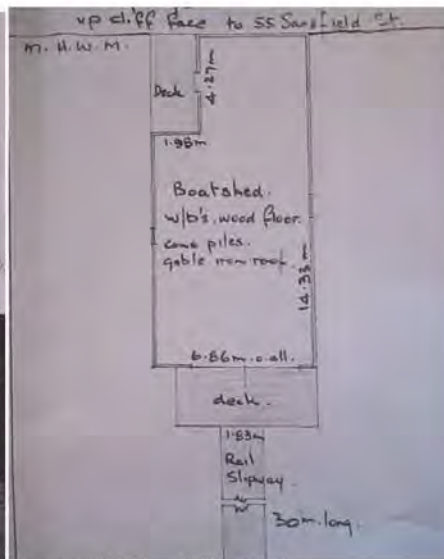


'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
'Consent No'

2665366
64832818
Historic Structure
GPS
9279 issued in 1993, 11491 expired in 1998 & 31141 issued in 2005




Photographs from ARC files



Contemporary plan of boatshed from ARC files

- ^v Foreshore Licence Details, QPSR560, 3 July 1991.
^{vi} AHB, Plan of Boatshed, 14 May 1979 and ARC, File H9026, 2005.
^{vii} ARC, Site Assessment, 3 March 1976.
^{viii} Letter, R.S. Ferguson to Auckland Regional Council, 17 April 1990.
^{ix} ARC File PTCSARS005.
^x ARC File PTCSARS005.

'Site Type' BOATSHED
Name
Location Herne bay
'Local Authority' Auckland City Council
Description Platform with timber framed and concrete base
Notes
Keywords BOAT SHED
BOATSHED CMA
'Biblio References' ARC FILE NO:
May 2006
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland
'Last Visited' May 2006
Owner
'Owners Address'
'Date of Construction'
'Photo Image'



'GPS Easting' 2665341
'GPS Northing' 6483194
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No

'Site Type' Name	BOATSHED
Location	Adj 69 Hamilton Road Herne Bay Auckland
'Local Authority' Description	Auckland City Council Single level boatshed – lean-to form. Weather board cladding on timber frame. Corrugated iron roof. Recent treated timber files and floor framing. Early or original steel piles utilising railway track are in place. A single tgv faced door is located on the north wall and a pair of tgv faced doors on seaward face. A new ramp has been constructed and there are a flight of timber stairs leading to the adjacent property. Dimensions 6.2m x 3.7m, and associated slipway - 9.2m x 1.8m.
Notes	In 1964 R.L. Bowden applied for an Auckland Harbour Board foreshore licence to construct a boatshed and skids. ³³ The Harbourmaster and the AHB Chief Engineer had no objections to the proposed boatshed to be built at 69 Hamilton Road, Herne Bay. ³⁴ On the 28 April 1964 Bowden's plans were approved by the Marine Department and the AHB approved the foreshore licence for a term of 14 years, with the standard terms and conditions, on the 1 May 1964. The rent was set at £7-10s-0p per annum. ³⁵ In February 1968 the property, foreshore licence, boatshed and slipway were sold to Reginald Beaumont Millar, and by 1969 he had the licence transferred into the name of Erin Cove Apartments. In 1979 the foreshore licence was transferred to a Body Corporate. ³⁶ In 1990 the Auckland City Council planted the bank on either side of the steps that lead to the boatshed. ³⁷ MHWL is up to the cliff face - a sand and shell foreshore. The design of the boatshed is simplistic and is reminiscent of a style that was replicated many times on the foreshore of the Waitemata Harbour. The boatshed measures 20ft 6inches (6.2m) x 12ft (3.7m) and was constructed in horizontal weatherboards with a flat sloping corrugated iron roof. The landing measured 6ft 3inches in width and the ramp was 6ft wide and 30ft in length. The wooden floor was supported by three rows of 8inch x 9inch steel piles with the boatshed elevated 8-9ft above the foreshore. The high water tide reaches the cliff face at the rear of the boatshed making the boatshed accessible three hours either side of high tide. The shed is accessed by 48 steps from the property to a concrete deck by the boatshed door and a further 12 steps to the beach. The steps were constructed in timber but were supported by steel strangs and girders. The total area of the boatshed and ramp was 246m ² . During a storm in 1986 the boat ramp was partially washed away. ³⁸ In 2002 the boat ramp was completely rebuilt; plans were drawn by URS New Zealand (Engineering and Environmental Management), on the 11 June 2002. ³⁹ The newly installed ramp was built with timber accessed from the Albany Timber Distributors Ltd. During an annual inspection (2002) the shed required painting and the front shed doors replacing; in 2003 the boatshed was refurbished. ⁴⁰
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCHAM004 May 2006
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2005
Owner	Body Corporate No. 78543 Erin Cove Apartments c/- Property Supervisors Ltd. Miss P Gray
'Owners Address'	P O Box 37499, Parnell

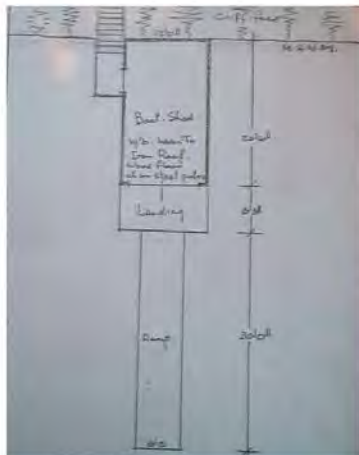
'Date of Construction' 1964
'Photo Image'



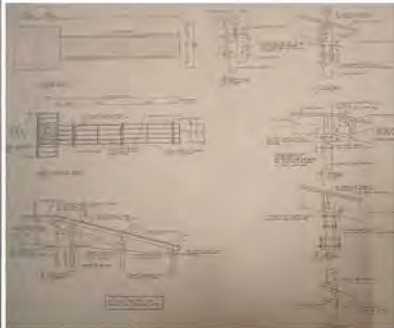
2006



'GPS Easting' 266533
'GPS Northing' 6483165
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 8615



Plan of shed and ramp



Plan and section of ramp

^{xii} Letter, Bowden to the Auckland Harbour Board, 6 March 1964.

^{xiii} Letter, AHB Harbourmaster and Chief Engineer to AHB, 15 April 1964

^{xiv} Letter, Marine Department, 28 April 1965 & Letter, AHB, 1 May 1964 & Marine Department, Plan No. 11814.

^{xv} ARC File PTCHAM4.

^{xvi} ARC File PTCHAM4.

^{xvii} Letter, Mrs. Ferguson to the AHB, 20 August 1986.

^{xviii} Ramp plans, URS New Zealand, 11 June 2002.

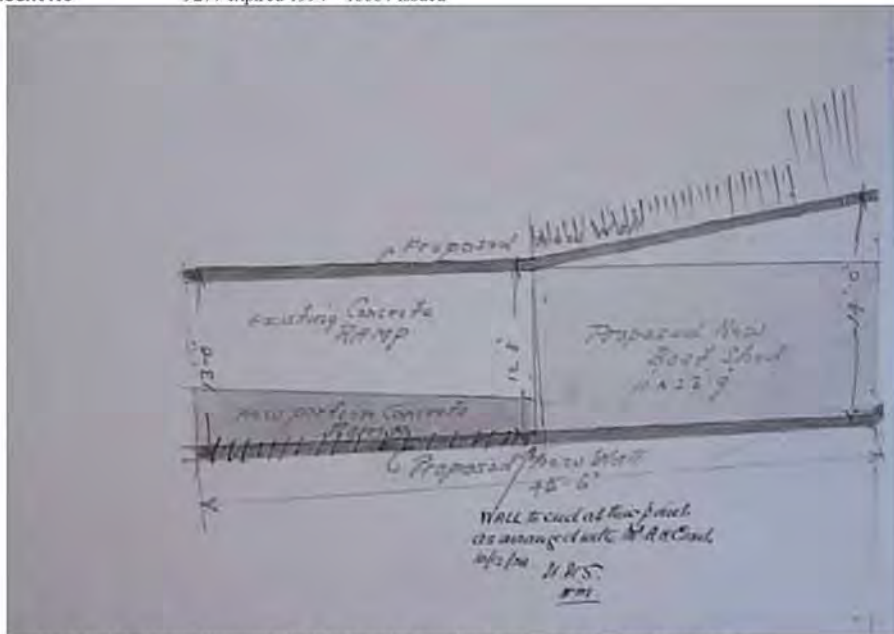
^{xix} ARC File H/8427.

'Site Type'	BOATSHED
Name	A.H. Court Boatshed
Location	Adj 71a Sarsfield Street (No. 45 on the Plan) Herne Bay, Auckland
'Local Authority'	Auckland City Council
Description	Single level gabled boatshed constructed 1929. Corrugated steel roofing with exposed rafters to eaves. Vertical corrugated steel wall cladding above floor level. Horizontal weatherboards to base. Timber framed sash windows. Vertical TGV faced double doors to ramp. Timber boat ramp. Circular concrete piles with timber bearers
Notes	<p>On the 7 July 1920 Mr. A.H. Court wrote to the Auckland Harbour Board concerned that the cliff at the foot of his property was breaking away. In 1933 he sought permission from the ARC to construct a six foot retaining wall. The AHB gave Court permission to build a further retaining wall but it was not to encroach on the Boards property. In the same letter Court was granted permission to build a boatshed and skids on the Waitemata foreshore. The foreshore licence fee was set at £2 per annum for the shed and skids, and £1 per annum for the concrete ramp.³⁵⁵ The conditions set by the AHB for the foreshore licence conditions were: 1. <i>That the boatshed be kept at a height of no less than 6 feet above the beach level in order to allow pedestrians traffic to pass underneath.</i> 2. <i>That the tenancy be terminable by one months notice on either side.</i> 3. <i>That the rent at the rate of £2 pa be paid yearly in advance.</i>³⁵⁶</p> <p>In 1934 Mr. Court was a neighbour of Mr. Webster who lived at 75 Sarsfield Street who also owned a large boatshed.</p> <p>Mr. A.H. Court was the owner of John Courts Ltd., Queen Street, Auckland, they were Drapers and Furnishers.</p> <p>Previous owners: A.H. Court 1929to 1963, G.W. Cole 1963 to 1966, Ivan Gould 1966 to 1972. C. Gilberd 1972 to 1977, Charles K.G. Macindoe 1977 to 1981, Peter Stewart Bromly in 1981 to 1987, P.E. & N.I. Fraser in 1987 -</p> <p>MHW at the stone wall. A shell beach surface.</p> <p>According to A.H. Courts boatsheds plans (1928) it clearly demonstrates that a boatshed and concrete ramp had previously been built on this site.³⁵⁷ A previous boatshed must have been demolished and the skids removed, leaving the concrete ramp in situ. In 1929 the boat ramp was 100ft long and about 10ft wide. The shed had a 10 ft stud, wooden floor and concrete piles.³⁵⁸ The size of the boatshed was smaller in size than the new boatshed built in 1933.³⁵⁹ During the intervening years Court enlarged the boatshed as the original boatshed measured 23ft x 11ft but it now measures 21ft 3inches x 68ft x 8inches equalling 1,459sq ft (66ft the boatshed 2ft 8inches the landing). It is unclear whether the shed was always constructed in corrugated iron. In 1981 the boatshed required maintenance; the owner of the shed was Bromley. The shed required minor repairs to the corrugated iron roof on the western side and the shed needed to be painted as it was an 'eyesore' from the harbour bridge. The shed was painted green.³⁶⁰</p> <p>In 2002, during routine ARC Compliance Monitoring it was noted that the boatshed required painting, the ramp appeared unsound and barnacles covered almost the entire structure, and some of the jetty boards were missing. The shed was upgraded that year.³⁶¹</p>
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCSARS0023&11357
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166, Symonds St Auckland
'Last Visited'	May 2006

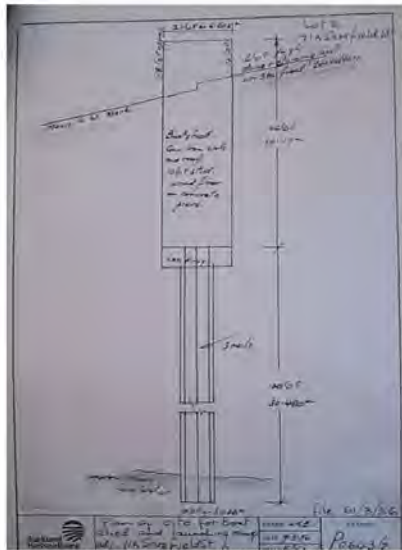
Owner Peter Edward and Norma Ivy Fraser
'Owners Address' P O Box 46195
'Date of Construction' 1929
'Photo Image'



'GPS Easting' 2665225
'GPS Northing' 6483125
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 9277 expired 1994 – 15557 issued



Drawing of boatshed dated December 1929. This shows the stone walls and remnants of this are existing.
ARC File, A.H. Courts Boatshed Plans, PTCSAR002



Plan showing boatshed and ramp dated 1976
ACR File PTCSARS0023



John Court Letterhead
ACR File PTCSARS0023

- ^{xix} Letter, Auckland Harbour Board (Harbourmasters Office) to AHB, 27 November 1933.
- ^{xx} Letter, AHB to A.H. Court, 20 October 1929.
- ^{xxi} ARC File, A.H. Courts Boatshed Plans, PTCSAR002.
- ^{xxii} AHB, Information Sheet, ARC File PTCSAR002.
- ^{xxiii} Letter, AHB (Harbourmasters Office) to AHB, 27 November 1933.
- ^{xxiv} ARC Compliance Monitoring, 1981.
- ^{xxv} ARC Compliance Monitoring, 2002.

'Site Type'	BOATSHED
Name	
Location	75 Sarsfield Street (connected with 77) Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	Single level gabled boatshed constructed between 1930 and 1947. Corrugated steel roofing with exposed rafters to eaves. Vertical corrugated steel wall cladding above window sill level, horizontal weatherboards below. Timber framed sash windows. Sashes to north and south faces covered with vertical boarding. Double tgv faced doors to ramp. Timber boat ramp on concrete piles. Timber piles to boatshed (recent) on concrete sleepers. The boatshed (1947) measured 17 metres in length and 6.4metres in width with a ten foot stud, and was constructed nine to ten feet above the beach. A store shed was added at one end it measured 3.6m x 2.3m. The Marine Department Plans indicated that part of the structure was originally constructed in wood four feet up each side with the remainder constructed in corrugated iron (including the roof). The boatshed and skids (28.96m in length) were erected on concrete piles.
Notes	1930 retaining wall, 1934 ^{xxxv} skids and a small weatherboard shed & 1947 boatshed - weatherboard and corrugated iron walls and roof. The original owner of the boatshed located at 75 Sarsfield Street, Herne Bay was Trentham Charles Webster. In 1930 he was issued with a license to construct a retaining wall at the edge of his property. ^{xxxvi} In 1934 Webster applied to the Auckland Harbour Board for a license to construct boat skids on the foreshore of the Waitemata Harbour. The skids were constructed on part of the neighbouring foreshore, at 47 Sarsfield Street, which belonged to Webster's wife. ^{xxxvii} The foreshore licence was issued in the name of Mrs. Rose Webster. ^{xxxviii} In October 1934 the Auckland Harbour Board granted permission for Webster to erect skids and a ramp. He had to adhere to the AHB's conditions of tenancy: <ol style="list-style-type: none"> 1. Tenancy to be terminable by one month's notice on either side. 2. Rent to be at the rate of £1 per annum. 3. The structure to be kept in repair to the satisfaction of the harbourmaster. The plans were sent to the Marine Department in Wellington for approval, which were dually given. ^{xxxix} In 1947 Webster sought consent to build his boatshed on part of the Waitemata Harbour foreshore. A previously built shed was demolished and a new larger boatshed was constructed, much in the same position (see enclosed plans). The Auckland Harbour Board and Marine Department approved the plans (drawn in August 1947) and Webster was given permission to proceed with the construction of his boatshed. ^{xxxix} The foreshore rental increased from £1 to £2 per annum due to the construction of a new boatshed In 1990 the property and boatshed was owned by Blair Doanld Webster. He was in the process of refurbishing the boatshed and piles to house his 38ft Logan-built (1892) keeler 'Kotiri'. The tenure for the foreshore licence was set at 14 years at \$1,250 per annum. In 2003 Webster was instructed by the AHB to do some maintenance on his boatshed such as: replacing the wooden piles, restoring the steel rails and parts of the corrugated iron walls and roof, before his licence would be renewed. MHW at retaining wall. Sand and shell bottom with flat papa rock. It is unclear what construction materials were used or the size of the first shed that had been built between 1934 and 1947.
Keywords	BOAT SHED

'Biblio References' BOATSHED CMA
'Date Recorded' ARC FILE NO:
 May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166, Symonds St, Auckland
'Last Visited' May 2006
Owner Stephen Robert Ullrich
'Owners Address' 58C Sackville Street, Grey Lynn
'Date of Construction' 1930 retaining wall, 1934^{xxxii} skids and a small weatherboard shed & 1947 boatshed
'Photo Image'



Photographs taken March 1997

'GPS Easting' ARC File
 2665171
'GPS Northing' 6483080
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 27794

^{xxxv} Marine Department 7302, 4/2371, 3 November 1934.

^{xxxvi} Marine Department 6785, 4 April 1930.

^{xxxvii} Auckland Harbour Board, letter, 17 October 1934, ARC Archives, PTCSAR020.

^{xxxix} In the High Court of New Zealand Registry, A.No.194/81, Executors of the Estate of T.C. Webster and the Auckland Harbour Board, ARC Archives PTCSAR020.

^{xxxviii} If a foreshore Licence is revoked or the respondent does not wish to continue with a licence the boatshed must be demolished, Auckland Harbour Board Tenancy Agreement.

^{xxxix} In the High Court of New Zealand Registry, A.No.194/81, Executors of the Estate of T.C. Webster and the Auckland Harbour Board, ARC Archives PTCSAR020.

^{xxxii} Marine Department 7302, 4/2371, 3 November 1934.

'Site Type'	BOATSHED
Name	
Location	Adj 79 Sarsfield Street Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	<p>Single level gabled boatshed. As constructed the Boatshed was built 4ft (1.39m) inside the perimeter of Gifford's property with a stone wall either side of the shed on the boundary line. The shed was constructed in timber with a corrugated iron roof; it measured 22ft (6.7m) x 12ft (3.6m) with a covered rear porch. Walls are clad with timber bevelled back weatherboards. A timber framed 6 light double hung sash is located on the south wall. Foundations are original or early timber files set on concrete foundations. The floor is timber framed.</p> <p>The shed was elevated 10ft off the beach with concrete and timber piles (6inch x 6 inch timber). The deck/landing measured 6 ft in depth and was the same width as the boatshed. The slipway was 48ft (14m) in length x 8ft (2.44m) in width. This was built in timber with the majority of the piles concrete. At the rear of the shed was a stone wall, which had a picket fence constructed on top. The facade has two landing doors with three small decorative ventilator grills located above.</p> <p>In 1987 the boatshed was re-roofed with colour steel roofing.^{xxxiii} Currently roofing is pressed metal tiles.</p> <p>A 1976 a report suggested that the boatshed may have been rebuilt at some stage.</p>
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166, Symonds St, Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2665153
'GPS Northing'	6483043
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

^{xxxiii} ARC Files PTCSAR008 & 9974 and Marine Department Plan PO900.

'Site Type'	BOATSHED
Name	
Location	Home Bay Beach at the end of Wallace Street
'Local Authority'	
Description	Single levelled timber framed boatshed. Roof clay Marseilles tiled roof, walls weatherboard with timber circular end wall vents. A pair of tgy faced doors to beach
Notes	This shed sits on a stone wall above the MHW mark and is outside the CMA area. THERE is no permit record and no file found. A record was made of its location
Keywords	BOAT SHED
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166, Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	circa 1920s
'Photo Image'	
'GPS Easting'	2664983
'GPS Northing'	6483027
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

Photo 21 July 1914. Is this the shed?. Home Bay, End of Wallace St A P L Neg 1 W836

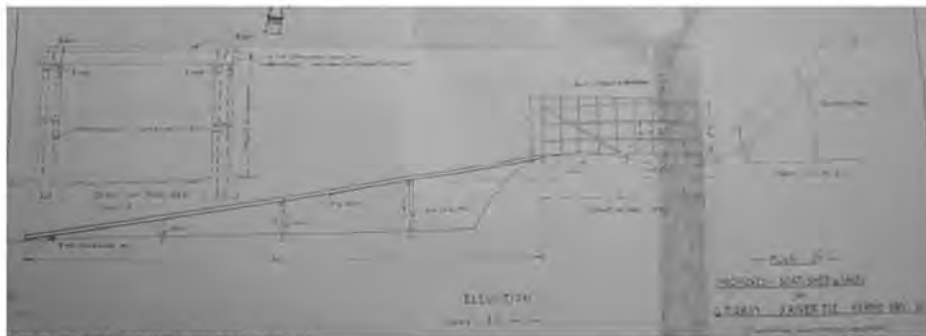
'Site Type'	BOATSHED
Name	
Location	5 River Terrace Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	Single levelled boatshed with deck on roof. Weatherboard cladding on timber frame. Timber doors to north south and west walls. The west sliding door and a flight of timber steps on the east wall has been added within the last 20years Gray's boatshed was constructed in timber with a timber and malthoid roof ; it measured 26ft x 12ft. The flat roof was surrounded with a graduated railing with the highest point facing out to sea. The height of the boatshed, including the railing, was 8ft at the rear and 9ft at the front. The launching ramp, 80ft in length, was built on 6inch concrete piers, which were supported by 70lb iron rails. The set of steps, which lead from the property down to the rear of the boatshed, existed prior to its construction in 1955. ^{xxxv} In 1997 another set of double doors were built on a side wall. In 2002 another inspection noted that the launching skids had not been reconstructed and the deck was still in place. The principal structure was 52m2. The boat ramp has been removed.
Notes	On the 19 January 1950 a foreshore licence was issued for 5 River Terrace, Herne Bay, Auckland. ^{xxxv} On the 3 August 1955 George Tasker Gray applied to the Auckland Harbour Board to build a boatshed and skids at 5 River Terrace, Herne Bay. ^{xxxvi} The boatshed was constructed on Gray's property and the skids on the foreshore. The AHB granted a licence, the rate was set at £2 per annum, and construction began in December 1955. ^{xxxvi} In 1979 the property, boatshed and skids was owned by Gray's daughter, Mrs. Josephine Stewart Ffowes Williams. ^{xxxvii} In 1981 the foreshore licence was \$160 per annum and \$449 per annum in 1997. In the New Zealand Herald the Ffowes Williams advertised the free use of their boatshed in exchange for the occasional use of a boat. ^{xxxviii} The ARC informed the Ffowes Williams they were in breach of their foreshore licence's terms of agreement and therefore could not proceed with this venture. ^{xi} In 1997 during a routine ARC Monitoring Compliance inspection, it was noted the ramp had been removed and an illegally constructed deck had been built plus an extra set of doors built on the side wall. The Ffowes Williams were instructed to rebuild the ramp. In 1999 the Ffowes Williams sold their property, including the boatshed, to Lookout Holdings Limited. In January 2003 a new boatshed was proposed but this did not eventuate and the existing boatshed was revamped. However, some alterations were made, which were not in accordance with the submitted plans. MHWL - flat area on cliff face. Rock to shell bottom.
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: H11460
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	Lookout Holdings Ltd
'Owners Address'	3 River Terrace, Herne bay
'Date of Construction'	1955

'Photo Image'



'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
Consent No

26652664753
6483033
Historic Structure
GPS
15697



Plan of boatshed and skids for GT Grey. ARC File

- xxxiv ARC File H11460, 1955 Plan.
- xxxv ARC File H11460 (Not recorded in the Marine Department file).
- xxxvi ARC File H11460.
- xxxvii ARC File H11460, 6 October 1955 and 20 December 1955.
- xxxviii Tomos Marlais Ffowes Williams was a Manufacturer and Wholesaler in Auckland.
- xxxix NZ Herald, 2 September 1981, Section 2, p. 14.
- xl Letter, ARC to Ffowes Williams, 4 September 1981.

'Site Type'	BOATSHED
Name	
Location	15 Cremorne St, Herne Bay
'Local Authority'	Auckland City Council
Description	A single levelled flat roof boatshed on concrete skids or frame. Exterior walls clad with VTGV cedar boards, natural finish. Timber framed ramp with steel rail tracks. The shed measures 4m x 10m and was built on the existing concrete platform. There are two sets of double doors, Pair of facing doors to north landing and one set positioned on the side wall. Steps lead to and from the boatshed's entrance, however, another set of steps lead up to the viewing platform, located on the roof top. The platform is encased with a 100-200mm high parapet
Notes	<p>In 1963 a boatshed and skids were constructed at 15 Cremorne Street, Herne Bay. In 1961 Christian Arthur Shields purchased the property; however, it wasn't until 1963 that he applied for a foreshore licence from the Auckland Harbour Board to erect a boatshed at the edge of his property (a sketch of the boatshed was enclosed). The site was approved by the AHB and the Marine Department in August and November respectively.⁴¹ However, nine neighbours objected to the location and the construction of the proposed boatshed, sighting the reasons that it was a popular local beach (commonly known as Tibbs Beach) for swimming.⁴² The AHB responded, stating, there would be no grounds for complaint as 60 - 75% of the boatshed would be built on Shields property. On the 26 May 1964 the foreshore licence was approved.⁴³ The boatshed is located a short distance from the Ponsonby Wharf.</p> <p>In 1963 the majority of the boatshed was constructed, at the edge of the cliff, on Shields land. The shed was built under a group of pohutukawa's; it was constructed in weatherboard with a gabled corrugated iron roof and a timber floor. The boatshed and launching ramp were placed on timber and concrete piles; the ramp was constructed with steel rails. The shed measured 28ft x 12ft with access to the structure directly from the property to a timber ramp and the sheds entrance.⁴⁴ The shed can be accessed at all levels of the tides.</p> <p>In 1972 Shields sold his property to Theodoros A. Johannink (a children's wear specialist); the foreshore licence was transferred into his name in 1973. At this time, the boatshed was not painted and required some maintenance. In 1972/73 the boatshed was painted and the structural work was completed. The Licence was issued for 14 years with the rental set at \$120 per annum. Later in 1985 the boatshed required some major alterations, which were designed by the architect Geoff Ward and the shed was totally renovated in 1986. In 2003 Johannink wrote to the ARC asking for the foreshore licence to be transferred from his name to the Cremorne Trust.</p> <p>In 1986 the boatshed and slipway were completely refurbished. The shed required an engineers report before work began.⁴⁵ The alterations included the following: the corrugated roof was replaced, the wooden floor was replaced with pre-cast concrete slabs, the boatshed was painted a dark green and fitted with grille-style doors and the existing concrete piles were repaired.⁴⁶ The original concrete flooring slabs were not strong enough to take the right loading; stronger concrete slabs were used. In April 1988 the work was completed and a new licence was issued for 14 years (2002) with the rental set at \$380 per annum. In 2002 during routine ARC Compliance Monitoring, it was noted that the shed had piles missing and had rotten boards on the ramp.</p> <p>In 2003 the boatshed was badly damaged by a pohutukawa tree, which had fallen down the cliff. Johannink planned to construct a new boatshed and ramp. The new shed was built using the same footprint as the former and the design was in keeping with neighbouring boatsheds in the area. All building materials were to be barged or carried from the property; no construction work was done on the foreshore.</p> <p>In 2003 a new boatshed was designed by Hulena architects of Newmarket and built by Mark Bayer of City Build Construction.⁴⁷ The boatshed (40%) and the entire ramp are below the MHWL. Papa rock shelves on cliff face.</p>

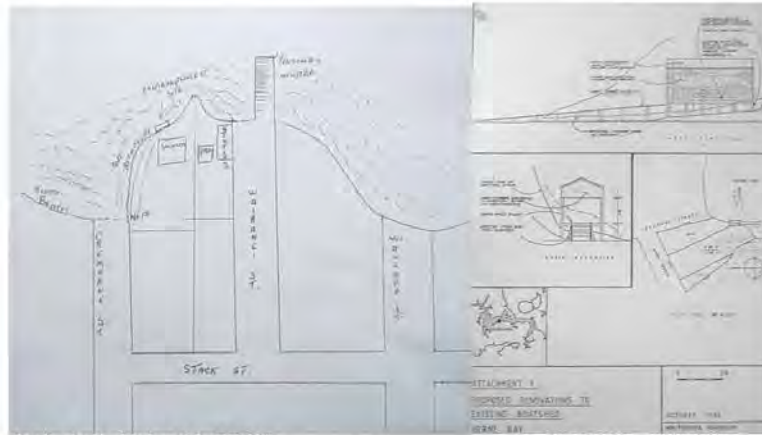
Keywords BOAT SHED
BOATSHED CMA
'Biblio References' ARC FILE NO: 16123/ PICRE10
'Date Recorded' May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166, Symonds St
Auckland
'Last Visited' May 2006
Owner The Cremorne Trust
'Owners Address' PO Box 91509, Auckland
'Date of Construction' 1963⁴⁸ & rebuilt in 2003
'Photo Image'



Photograph dated 1973 from ARC Files

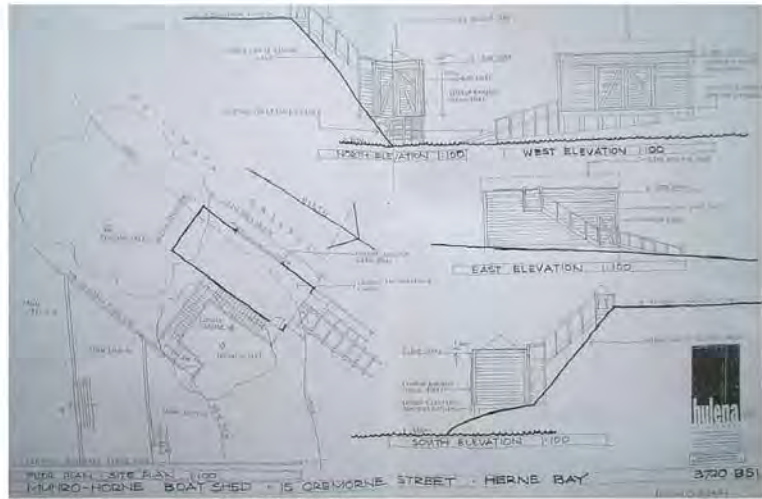


'GPS Easting' 26654755
'GPS Northing' 6483010
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 28914



Site plan showing location of shed ARC file

Plans on 1986 refurbishment ARC Files



Drawings showing reconstruction dated 2003 ARC Files

⁴¹ Auckland Harbour Board Application Form dated 6 August 1963 and Marine Department, 22 November 1963.

⁴² Letter, Herne Bay Residents, 16 November 1963.

⁴³ Letter, AHB to Herne Bay Residents, 10 December 1963.

⁴⁴ ARC File PTCRE010.

⁴⁵ ARC File PTCRE010 & letter, AHB, 23 September 1986.

⁴⁶ Application to the AHB for renovation, 7 October 1986.

⁴⁷ ARC File PTCRE010.

⁴⁸ Marine Department 11676, M4/5437, 18 November 1963.

'Site Type'	BOATSHED
Name	
Location	18 Cremorne St, Herne bay
'Local Authority'	Auckland City Council
Description	<p>The Boatshed was constructed in wood with a corrugated iron roof in 1975. The shed measured approximately 40ft x 20ft (74.32sqmetres) and the ramp 120ft x 6ft (66.88sqmetres). The structure is raised ten feet off the ground on wooden piles, and the total area of the boatshed and ramp was 141.20sqmetres. Up until 1985 there had been no alterations made to the boatshed.^{21ix}</p> <p>In 1997 a slip occurred at the property of 12 Cremorne Street and the consequences were, a pohutukawa tree fell through onto the boatshed damaging the roof and floor of Frankham's shed. The out come was, the tree required trimming for the boatshed to be repaired. At this time the ramp was starting to rust and required immediate attention.¹</p>
Notes	<p>In 1973 the Marine Department approved J.C. Frankham's plans (M.D. 15094) to construct a boatshed and rails at 18 Cremorne Street, Herne Bay.¹ The ARC granted a foreshore licence for a term of ten years. However, it appears that Frankham did not construct his shed and rails until 1975. The boatshed was constructed at the foot of the property against the cliff face but elevated off the beach to allow public access along the foreshore. The launching of the boat can be carried out during most tidal flows.^{lii}</p> <p>In 1997 a slip occurred at the property of 12 Cremorne Street and the consequences were, a pohutukawa tree fell through onto the boatshed damaging the roof and floor of Frankham's shed. The out come was, the tree required trimming for the boatshed to be repaired. At this time the ramp was starting to rust and required immediate attention.^{liii}</p> <p>MHWM at the foot of the cliff under the boatshed.</p>
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCCRE006
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	JC Frankham Trust
'Owners Address'	PO Box 12306, Penrose
'Date of Construction'	1975
'Photo Image'	

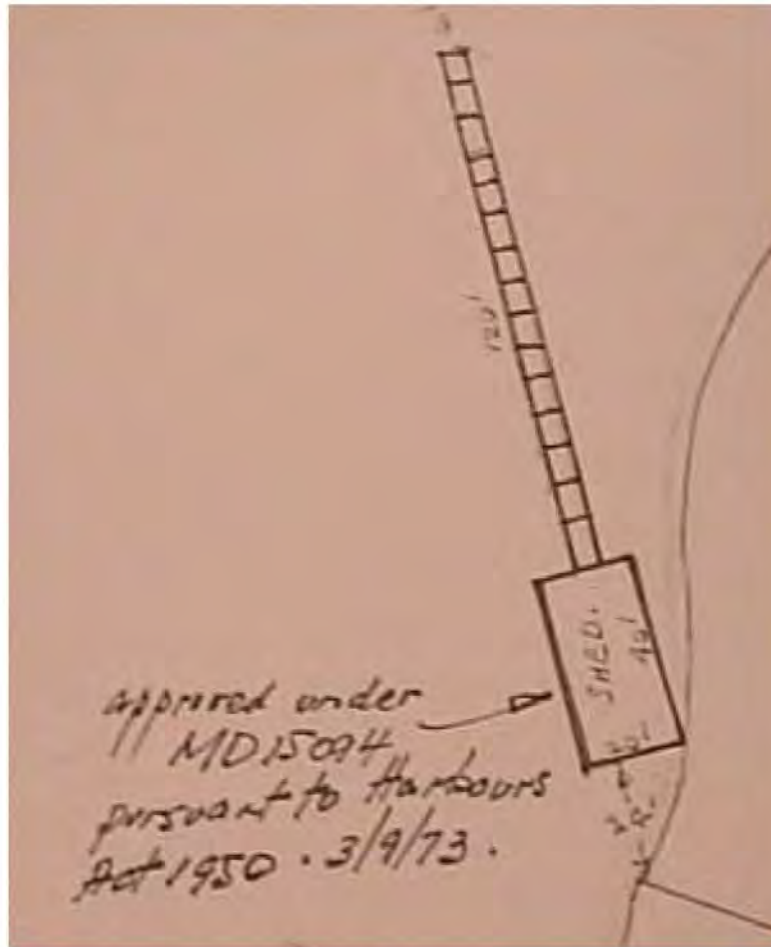




'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
Consent No

Historic Structure
GPS
9261





^{xlix} Auckland Harbour Board Application, 153, 22 August 1985.

¹ Resource Consent, Auckland City Environments, 21 October 1997 (plus attached correspondence).

ⁱⁱ Marine Department 15094, 43/191/1, 30 August 1973.

ⁱⁱⁱ Auckland Regional Council Foreshore Licence, 26 July 1989, PTCCRE006.

ⁱⁱⁱⁱ Resource Consent, Auckland City Environments, 21 October 1997 (plus attached correspondence).

'Site Type' Name	BOATSHED
Location	Adj 5 Marine Parade Herne Bay Auckland
'Local Authority' Description	<p>Auckland City Council</p> <p>The boatshed was built in 1950/51 with the sheds walls constructed in corrugated iron and timber, and the roof built in timber and butyl rubber. The front wall has a low curved profile. It is a small shed, measuring 14ft (4.267m) x 8ft (2.438m) with the slatted timber ramp exactly the same measurements as the shed.^{1b} The shed was buttressed against the cliff face with the rear of the shed being 4ft above the foreshore whilst the front was 5ft above the beach.^{1b} The boatshed was accessed via a hatch in the roof. The size of land the boatshed occupies is 10.4m².^{1d}</p> <p>In 1983 major repairs were carried out including the replacement of, the front support piles and the rear western piles, the timber above the doors on the western side, the timber overhangs on both sides, the launching ramp stringers and the rusted iron sheathing on both sides. The boatshed was repainted in a colour approved by the ARC. Currently dark green.^{1vi}</p> <p>In 1987, during a Foreshore Licence Inspection, the corrugated iron walls showed signs of rusting requiring the owner to have them fixed.^{1vii}</p> <p>In 2002 the new owner constructed a railing around the roof top for which he did not get consent from the ARC. This was eventually approved and retained for safety reasons.^{1s}</p>
Notes	<p>In 1950/51 the original owner of the property and the boatshed was Victor Kennedy Simpson. He was granted permission by the Marine Department to construct a boatshed and skids on the foreshore of the Waitemata Harbour.^{1b} In 1952 Simpson sold the property and the boatshed to Jack Huffman Plowman (Company Manager for NZ Towel Supply and Laundry). The boatshed was sited on the western side of the foreshore and was usable one hour either side of high tide.^{1b} However, there were two boatsheds located on the foreshore at 5 Marine Parade, Herne Bay; the other belonged to the neighbouring property.^{1bi}</p> <p>In 1979 the renewal of a foreshore licence was dependent on carrying out necessary repairs. The boatshed was repainted and repaired with the corroded iron removed.^{1viii} In 1983 further repairs were required for the continuation of a foreshore licence.</p> <p>In 1959 Plowman sold the property to Roland A. Beuth who in 1983 sold it to James Stuart Mitchell. In 1991 Mitchell sold the property to Thomas Malcolm and Raywn Anne McCall. In 2001 the McCall's sold the property to Brett Murdoch.</p> <p>MHWM at the rear of the structure. Sand and shell beach.</p>
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCMAR014 & H8229
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	BW Murdoch
'Owners Address'	PO Box 74355, Market Road
'Date of Construction'	c 1950

'Photo Image'



Photos from ARC File



'GPS Easting'	266443633
'GPS Northing'	64832760
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	25485

- ^{liv} ARC File PTCMAR014 and Marine Department Plan No PO901.
- ^{lv} Marine Department Plan No PO901.
- ^{lvi} Letter, Brett Murdoch to the ARC, 20 December 2002.
- ^{lvii} Letter, Senior Property Manger to Messrs Greig Bourke Kettlewood & Massey, 17 August 1983.
- ^{lviii} ARC File PTCMAR014.
- ^{lix} Letter, Brett Murdoch to the ARC, 20 December 2002.
- ^{lx} Marine Department 9078, 29 December 1950.
- ^{lxi} Foreshore Licence Inspection, 12 January 1987.
- ^{lxii} ARC File PTCMAR014, 12 December 1952.
- ^{lxiii} ARC File PTCMAR014, 1979.

'Site Type' BOATSHED
Name
Location Herne Bay
'Local Authority' Auckland City Council
Description Two levelled boatshed with accommodation at upper level. Concrete block base built on sea wall. Vertical board and batten walls and VTG doors to beach. Fibrolite super six roof. Approx 20-30years old. Structure above MHW. No records
Notes
Keywords BOAT SHED
'Biblio References' ARC FILE NO:
'Date Recorded' May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
 May 2006
'Last Visited'
Owner
'Owners Address'
'Date of Construction'
'Photo Image'



'GPS Easting' 2664300
'GPS Northing' 6482704
'ArcView Category'
'Grid Refs Source' GPS
Consent No NA

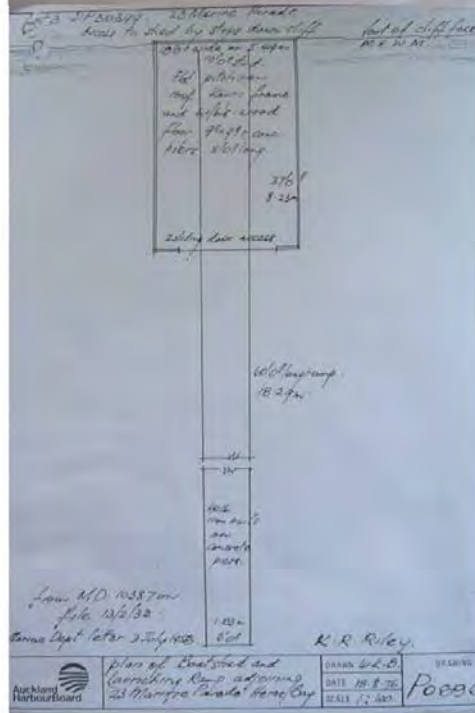
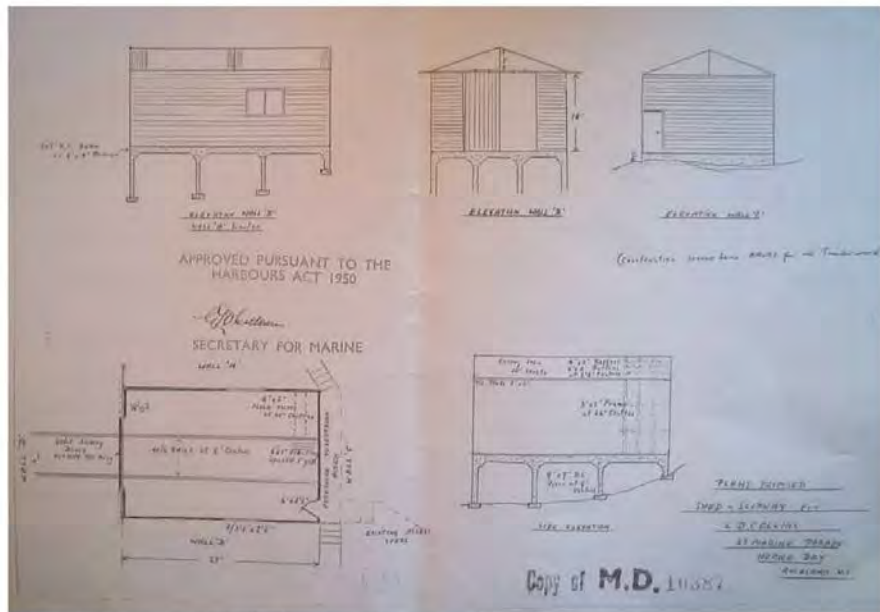
'Site Type'	BOATSHED
Name	
Location	Herne bay
'Local Authority'	Auckland City Council
Description	Two level boatshed built over CMA area. (this may be boatshed associated with no 23 Marine parade, but description and photos do not match)
	Timber framed walls clad with plywood and battens. Aluminium doors at upper level with cantilever deck.
	Concrete piles and timber framed floor
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland May 2006
'Last Visited'	
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2664275
'GPS Northing'	6482694
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Adj 23 Marine Parade Herne Bay Auckland Lot 3 DP30399 Pt Allot 1 Sec 8 Sub Auck Area 0-1-12,3 (237'6" x 30'1" x 238'9" x 92")
'Local Authority'	Auckland City Council
Description	This may be the shed on the previous form, but if so has been considerable altered and enlarged. No records have been found documenting these changes. Alternatively this shed may have been removed. The consent has expired. The design of Collins boatshed is replicated many times on the foreshore of Herne Bay. The exterior of the boatshed was constructed in recycled kauri weatherboards with a flat iron roof with minimum pitch. In 1958 the boatshed was built in horizontal weatherboards, however, by 1989 the weatherboards were vertical. ^{lxiv} The interior comprises, a wooden floor, two sliding doors that open on to the boat ramp, two windows and a back door giving access to the shed from the property. The boatshed can be accessed from the above property by steps that were constructed down the cliff face. The boatshed and ramp measure 27ft (8.23m) in length x 18ft (5.49m) in width, and 60ft (18.2sqm) in length and 6ft (1.83m) in width respectively. The boatshed stud is 10ft in height and is raised on 8ft piles from the beach. The floor was constructed with 9inch width timber boards. The ramp was constructed with 40lb iron rails on concrete piles. At the back of the boatshed a pedestrian access was built (92ft) with steps leading up to the back of the shed and back down to the beach. This allowed the public to walk around the boatshed at high tide. The ramp can be accessed three hours either side of high tide. The Auckland harbour stipulated that alterations or extensions to the boatshed could not be carried out without the Board's approval and at all times the structure must be well maintained. In 1980/81 the boatshed was upgraded with two new front landing doors, the front decking was replaced, the piles were recast in concrete and the exterior was repainted. In 1985 the boatshed was repainted and was assessed by the AHB to be structurally sound.
Notes	On the 10 March 1958 Leo Benedict Collins applied to the Auckland Harbour Board for a foreshore licence for his property at 23 Marine Parade, Herne Bay. On the 21 March 1958 the Auckland Board committee approved that a licence would be granted if the Marine Department approved the plans. The proposed boatshed and slipway plans were sent to the Marine Department in Wellington for approval. This was granted on the 3 July 1958 and the Licence fee was set at £7-10s-0p per annum. ^{lxv} A foreshore licence was granted by the Auckland Harbour Board to L.B. Collins on the 1 May 1958. In 1964 the foreshore licence was \$15.00 per annum. ^{lxvi} In 1979 the foreshore licence fee had risen to \$170.00 per annum. ^{lxvii} Previous owners of the boatshed were: 1958 – 1964 L.B. Collins, 1964 – 1976 Mr. E.J.A. & Mrs. B.I. Burke & 1976 – K. Riley. MHW at the foot of the cliff face. Level papa rock surface.
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCMAR016
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland

'Last Visited' May 2006
'Owner'
'Owners Address'
'Date of Construction'
'Photo Image'



'GPS Easting' 266533
'GPS Northing' 6483165
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
'Consent No' 9270 expired 1993



^{lxiv} Foreshore Licence Details, 2 July 1993. QPSR560.

^{lxv} Hogben, Wilkin, Clendon & Feeney to Messrs Russell McVeagh & Co., Letter dated 3 July 1964 & Marine Department Plan No. PO896.

^{lxvi} Auckland Regional Council File No: PTCMAR016.

^{lxvii} Auckland Harbour Board, 13/2/32, 9 July 1979.

'Site Type'	BOATSHED
Name	
Location	Adjacent - 31 Marine Parade Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	Two level boatshed, timber framed with corrugated steel cladding to walls. Front wall clad with rusticated weatherboards. Pair TGV faced doors. Timber slipway/ramp with steel rails on concrete piles. Aluminium framed windows and doors to upper level and timber framed handrail. The upper level has been enlarged since photographed in the 1970s/80s (see photo below)
Notes	<p>The boatshed plan (PO798) was approved by the Marine Department in 1962. The shed was elevated 6 to 8 ft above the beach and the walls and roof were constructed in corrugated iron. The frame, joists and floor were built in timber and the shed was supported by concrete piles. The boatshed measured 36ft x 24 ft with a timber front and rear deck each 6ft in depth with a 90ft slipway (constructed of iron rails), which was 6ft wide.¹⁶⁵⁸ In 1963 Donald built unauthorised alterations to the boatshed. The AHB instructed Donald to demolish the additions. Donald had a draughtsman prepare a new set of plans and specifications, which were submitted to the AHB. During these alterations the west side of the building was extended by 5ft in depth and the length was the same as the existing boatshed.¹⁶⁵⁹ Features that had to be remedied were: the loft floor had to be strengthened; the loft verandah required a handrail, and a lean-to had to be supported with bearers and concrete piles.¹⁶⁶⁰ The specifications were: a storage and gear area (loft) was built above the existing boatshed and was 228sqft (12ft x 19ft) with an eight foot stud. It has four windows, two at 5ft x 3ft and two at 6ft 6inches x 2ft 6 inches. The shed was painted in three coats of green. Wooden steps from the house site lead down to the rear of the boatshed.</p> <p>In 1962 Alexander Percy Donald applied to the Auckland Harbour Board for a foreshore licence for his property at 31 Marine Parade, Herne Bay.¹⁶⁶¹ Owners of adjoining properties gave Donald permission to erect a boatshed on the foreshore and on the 28 March 1962 the AHB approved the licence.¹⁶⁶² On the 3 April 1962 the Marine Department approved Donald's plans. The Auckland Harbour and the Marine Department expected the licensee to adhere to the conditions of the foreshore licence. However, in August 1963 Donald ignored this and proceeded with unauthorised additions, workmanship and materials all of which were substandard.¹⁶⁶³ It wasn't until 1964 that discussions with both parties were held to try and resolve the unauthorised work. Donald was granted permission to keep the additions as long as he carried out the necessary structural work. He had to pay a bond of £250 until the Chief Engineer had inspected the work: all work had to be completed by March 1965.¹⁶⁶⁴ The annual foreshore licence was increased from £7-10s-op to £10 per annum.¹⁶⁶⁵ In 1979 the foreshore Licence fee had increased to \$20.00 per annum.</p> <p>Owners of the Boatshed: A.P. Donald 1962 to I.W. Thompson 1966 to Clarence Malcolm & Audrey Diane Simpson 1973 to Graham Maxwell Gould 1974 to Dr. G. Mammsen 1980 to I. Van Segedin 1983.</p> <p>MHWM is at the base of the cliff. Papa rock surface.</p>
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCMAR017
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	1962

'Photo Image'

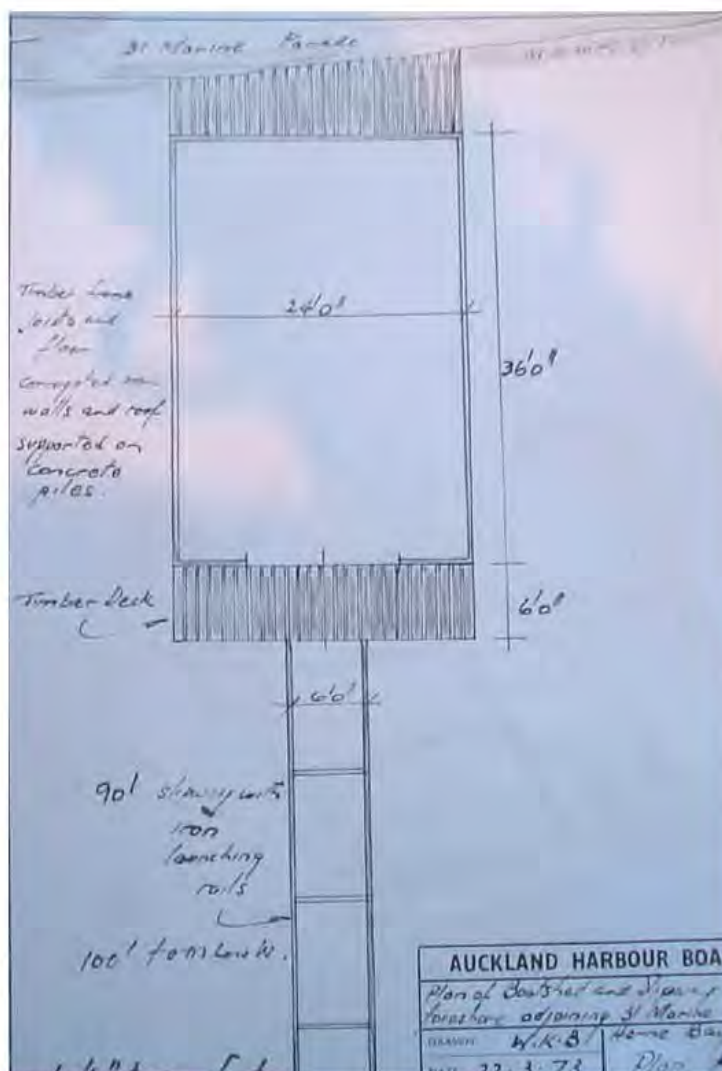


Photo from ARC file no date



'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
Consent No

2664236
6482623
Historic Structure
GPS
9271



¹⁰⁰ⁱⁱⁱ Auckland Harbour Board, & Marine Department Plan, PO798.

^{100k} Specifications For Extension, December 1963, ARC File No: PTCMAR017.

^{100l} Letter, AHB to A.P. Donald, 12 June 1964.

^{100m} Letter, A.P. Donald to Auckland Harbour Board, 23 March 1962.

¹⁰⁰ⁿ Owners of neighbouring property were S.H. Leyland, 33 Marine Parade, Herne Bay and W.

Robson, 'Rosewallis', 29 Marine Parade, Herne Bay.

^{100o} Letter, AHB to A.P. Donald, 23 August 1963.

^{100p} Letter, Auckland Harbour Board to Auckland Harbour Board General Manager, 30 September 1964.

^{xxxv} Letter, Auckland Harbour Board to AHB General Manager, 27 March 1962, and Letter, AHB to A.P. Donald, 16 April 1962 (Plans approved by the Marine Department under the Harbours Act 1950).

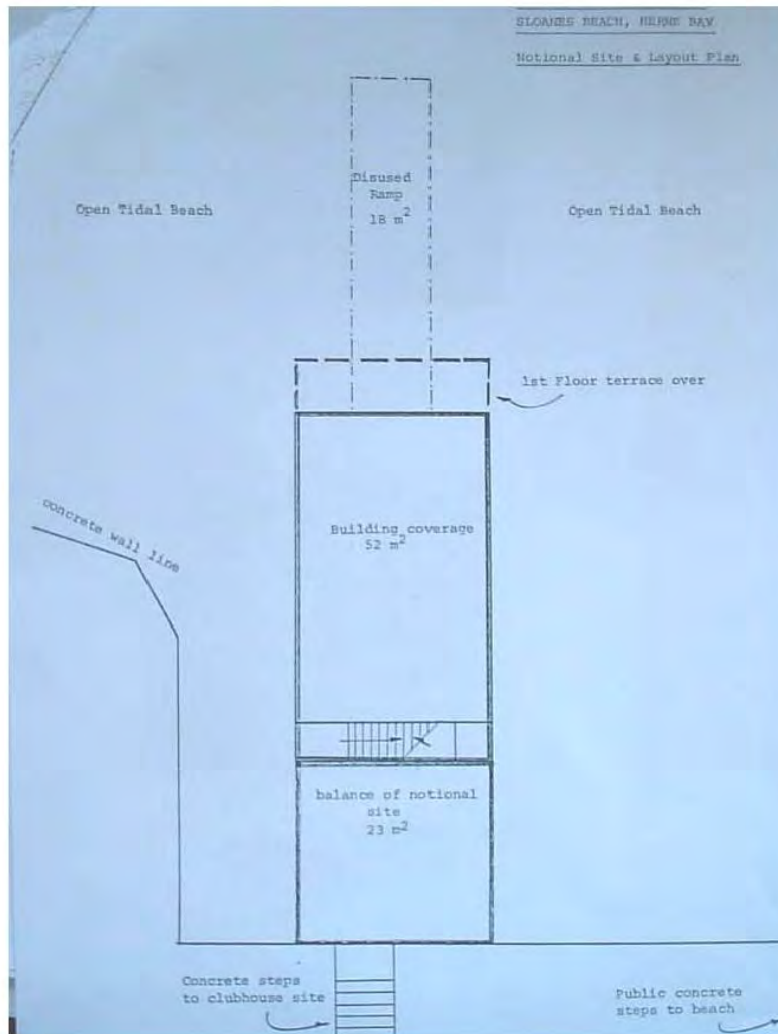
'Site Type'	BOATSHED
Name	
Location	Herne bay
'Local Authority'	Auckland City Council
Description	Structure above MHW. Two level boatshed with accommodation over. Plastered concrete based with weatherboard gabled first floor and aluminium window and door joinery. TGV faced boatshed doors.
Notes	No records on ARC files
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	C 1970s/80s
'Photo Image'	
'GPS Easting'	2664233
'GPS Northing'	6482591
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	Herne Bay Cruising Club
Location	Sloanes Beach (off Marine Parade) Herne Bay, Auckland
'Local Authority'	Auckland City Council
Description	<p>The boatshed was constructed in horizontal weatherboard sheathing with a pitched corrugated iron roof. The boatshed has timber piles and beams with the ramp constructed in timber and steel runners. It has windows on either side with two rear entrances, one on the first floor and the other on second floor, and a set of double Landing doors. Located above the upper storey door, is a ventilator grill surrounded by wooden shingles, this feature is repeated at the rear. This boatshed has many features, which are reminiscent of bungalow construction. At the rear there are steps that led down from the upper floor to an area where there is space to store boats against a concrete wall. The total area of the ground floor and first floor are of equal size of 41m² (26.2ft x 16ft – the size of the boatshed) with the ramp occupying 18m² (20ft x 7ft 6inches).^{bxxvi}</p> <p>The boatshed was built on Sloanes Beach and occupies an area below MHW and is approximately 50 metres from the low water mark. The shed faces north west.^{bxxvii}</p>
Notes	<p>This is one of the oldest boatshed in Herne Bay and the Auckland region. In 1913 the Marine Department approved the plans for a boatshed and the Auckland Harbour Board granted them a foreshore licence for the Richmond Yacht Club.^{bxxviii} The club now called the Herne Bay Cruising Club was a small single storied boatshed with a seaward ramp. In 1932 the club raised £222-12s-0p to construct a second storey complete with a deck looking out to sea.^{bxxix} The second floor was used as a club house and the bottom storey used as storage for equipment and some dingys.</p> <p>The club was first established at Coxes Bay, a suburb of Richmond, in the early 1900s, the boatshed was small and painted red. By 1906 the club's membership had increased to 100 members and, eventually this was the catalyst for the club to consider building new premises. It took approximately five years for the Marine Department to grant the club a licence. The Richmond Club moved to Sloanes Beach in Herne Bay; the new yacht club was named Richmond retaining its historical links. This is one of Auckland's oldest surviving yacht clubs built at the beginning of the second decade of the twentieth century. The club was founded by local patrons such as: Sir Dove Myer Robinson and Mr. Ritchie MacDonald MP. The club rooms were a focus for the local community and the venue has been used for other events such as; committee meetings, Ladies meetings, card evenings and social events.</p> <p>The boatshed is accessed from a dedicated but unformed road known as Short Street.</p> <p>In 1932 the upper storey of the boatshed was built during the depression, which provided work for unemployed members; they were paid 12 shillings per day. In 1940 the Richmond Yacht Club moved its headquarters to the newly Westhaven development. The Herne Bay boatshed was still used by local club members to store their dinghies and equipment. In the 1960s the Auckland West Division Sea Rangers applied to the AHB and the Auckland City Council to make the Richmond Yacht Club their headquarters. This was granted by the AHB but declined by the ACC. The ownership of the Richmond Yacht Club Incorporated was assigned to the Herne Bay Cruising Club Incorporated in the latter part of 1965.</p> <p><i>The boatshed is protected by the Hauraki Gulf Marine Park Act 2000 (HGMPA), which provides that all historic resources within the park are protected and must be reserved in perpetuity as of national importance.</i>^{bxxx}</p>
Keywords	<p>Map Reference R11 643826 BOAT SHED BOATSHED CMA</p>
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166, Symonds St

'Last Visited' Auckland
Owner May 2006
Owner Herne Bay Cruising Club
'Owners Address' PO Box 1211, Auckland
'Date of Construction' 1900s, 1913, 1932, 2003
'Photo Image'



'GPS Easting' 266533
'GPS Northing' 6483165
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 9265





Sloanes Beach prior to construction of the Cruising Club Shed. APL Neg No. 551



Cruising Club Shed on the right. APL Neg No. 219

^{lxxvi} Letter, ARC, 12 August 1996 and photographs 1981, PTCHER012.

^{lxxvii} ARC File PTCHWE012

^{lxxviii} Marine Department 4125, M12/4034, 8 August 1913.

^{lxxix} ARC File PTCHER012.

^{lxxx} Auckland Regional Council File PTCHWE012

'Site Type' Name Location	BOATSHED Watchman Island Yacht Club 37 Marine Parade (located on Sloanes Beach) Herne Bay Auckland Pt Bed Waitemata Harbour CT240/455 (T228) - Lot 2 DP29583 pt Allot 1 of Sec 8
'Local Authority' Description	Auckland City Council Weatherboard with a fabric roof over wooden sarking. Steel boatshed doors. Concrete piles to shed and ramp. Pamp has steel rails Two boatsheds have occupied this property; one in 1951 and one in 1996. In 1951 James Clayton Frankham commissioned a builder to construct a single storey boatshed and skids at 37 Marine Parade, Herne Bay. The shed measured 20ft x 40 ft and was constructed in weather board sheathing with a fabric roof over timber sarking. The launching ramp was 20ft x 20ft (400sqft - facing west) deck with the slipway 132 ft in length. ¹⁵⁵⁵ⁱ In 1993 the boatshed was damaged by high winds and collapsed. Between 1994 and 1996 a new structure was built on the original floor footprint and foundations. ¹⁵⁵⁵ⁱⁱ The new boatshed designed by Philip Walter Williams (Harrison Grierson Consultants) replicated the original (1951) design. The walls, concrete piles and roof were strengthened in steel (to allow for a helicopter to land) to take the weight of the new steel-framed boatshed. ¹⁵⁵⁵ⁱⁱⁱ The new boatshed was designed in the style and size of the previous shed. The design features of the new structure are; windows on either side of the shed, the entranceway located at the rear and sliding timber landing doors. There are two sets of steps; one that leads from the property and the other that leads from the shed to the beach. It was noted in 2002 that the boatshed had new additions including: the addition of a roof and landing rails and the words written above the landing doors: <i>'Watchman Island Yacht Club' - members only</i> . ^{1555iv}
Notes	In 1942 James Clayton Frankham purchased the property at 37 Marine Parade, Herne Bay (he later owned another property and boatshed at 18 Cremorne Street, Herne Bay in 1975). In 1947 the Marine Department approved Frankham's plans (M.D. 8686 ^{1555v}) for a boatshed and slipway. However, it appears this boatshed was not constructed and Frankham sent another set of plans to the M.D. in 1950 (M.D.9100 ^{1555vi}), which were approved and was granted a foreshore licence by the ARC in the same year. ^{1555vii} In 1979 the ownership of the property changed from J.C. Frankham to A. J. Frankham. ^{1555viii} The boatshed can be accessed for a short period at high tide due to the depth of the water. The site is exposed to the prevailing westerly wind and at times this restricts the launching of a boat. ^{1555ix} Two boatsheds have occupied this property; one in 1951 and one in 1996. In 1951 James Clayton Frankham commissioned a builder to construct a single storey boatshed and skids at 37 Marine Parade, Herne Bay. The shed measured 20ft x 40 ft and was constructed in weather board sheathing with a fabric roof over timber sarking. The launching ramp was 20ft x 20ft (400sqft - facing west) deck with the slipway 132 ft in length. ^{1555x} In 1993 the boatshed was damaged by high winds and collapsed. Between 1994 and 1996 a new structure was built on the original floor footprint and foundations. ^{1555xi} The new boatshed designed by Philip Walter Williams (Harrison Grierson Consultants) replicated the original (1951) design. The walls, concrete piles and roof were strengthened in steel (to allow for a helicopter to land) to take the weight of the new steel-framed boatshed. ^{1555xii} In 1981 Frankham sold the property to Alexander Louis James Adams. Adams sold to the David William Morris Family Trust in 1986. In 1996 Morris leased the boatshed and foreshore licence to Charles St Clair Brown. ^{1555xiii} MHW at the cliff face. Shell and Papa.

Keywords BOAT SHED
BOATSHED CMA
'Biblio References' ARC FILE NO: H11417
'Date Recorded' May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland
'Last Visited' May 2006
Owner Charles R St Clair Brown
'Owners Address' P O Box 6660, Wellesley St
'Date of Construction' 1947 & 1951 & 1996

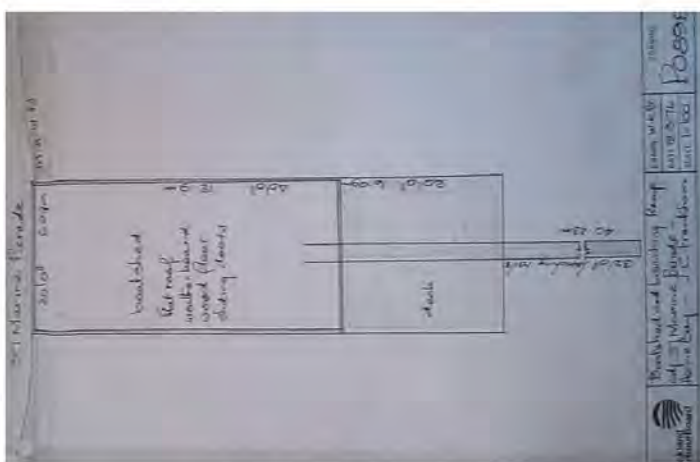
'Photo Image'



Photo from ARC files prior to reconstruction.



'GPS Easting' 26654190
'GPS Northing' 6482572
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 15636



- lxviii ARC File H11417 & Marine Department Plan PO898.
- lxviiii Letter, ARC to D.W. Morris, 25 July 1994.
- lxviiii ARC Coastal Resources Officer to Group Manager, Environmental Management, 2 December 1996.
- lxviiii ARC Coastal Resources Officer to Group Manager, Environmental Management, 2 December 1996.
- lxviiii Marine Department 8686, 4/3714, 5 December 1947.
- lxviiii Marine Department, 9100, 4/314, 28 March 1950.
- lxviiii Auckland Regional Council File H11417
- lxviiii ARC File H11417 (1979).
- lxviiii C.F. Bennett Ltd, Real Estate Agents, Valuers and Auctioneers, 26 July 1979.
- xc ARC File H11417 & Marine Department Plan PO898.
- xc Letter, ARC to D.W. Morris, 25 July 1994.
- xcii ARC Coastal Resources Officer to Group Manager, Environmental Management, 2 December 1996.
- xciii ARC File H11417

'Site Type'	BOATSHED
Name	
Location	Adj 41 Marine Parade Herne Bay Auckland Pt Bed of Waitemata Harbour CT 24C/455 (T228)
'Local Authority'	Auckland City Council
Description	Single level gabled roof boatshed, timber framed walls with fibrilite and batten cladding. A pair of TGV faced boatshed doors. Corrugated steel roof, concrete piles. Timber ramp on concrete frame.
Notes	<p>On the 22 June 1951 J.R. Smyth applied to the Auckland Harbour Board to construct a dinghy and small yacht wharf at 41 Marine Parade, Herne Bay. It was approved on the basis that it was for personal use not commercial.^{xxxv} Smyth did not build a boatshed at this time and^{xxxv} the jetty was approved by the AHB on the 14 November 1951.^{xxxvi} His Excellency the Governor General approved the Marine Department plan M.D. 9250 of J.R. Smyth to build a jetty on the Auckland harbour foreshore on the 19 November 1951.^{xxxvii} The following conditions were stipulated: 1. <i>Rent of site to be at the rate of £1 pa payable in advance</i>, 2. <i>The tenancy, which will commence from the date of construction is authorised by the M.D. as under, to be terminable by one month's notice on either side</i>, 3. <i>Structure to be kept in good order and repair to the satisfaction of the Harbourmaster.</i>^{xxxviii} Smyth's ramp was approximately 50 yards west of Frankham's^{xxxix} and was usable two hours either side of the high tide. In 1969 Ronald Ernest (plumber) & Norma Alice Griffiths purchased the property and sought permission from the Auckland Harbour Board and Marine Department to build a boatshed in 1976.</p> <p>Smyth & Redwood were manufacturing engineers and it was their company that designed the jetty ramp. Smyth's dingy and small yacht ramp measured 36 ft in length x 9ft in width, constructed in hardwood with ten reinforced concrete piles each measuring 6inch x 6inch and 4 ft (1.8m) in height. The entire ramp is planked in 6inch x 2inch timber boards.^{xl}</p> <p>In August 1971 Mrs. Griffiths lodged a plan of their proposed boatshed. The boatshed was rectangular in design with a gabled roof and was built on part of the existing boat ramp. The plans and specifications were drawn by C. Paske, a design engineer, and the boatshed was built by a friend of the Griffiths who was in the building trade. The boatshed measured 30 ft (9.44m) x 12 ft (3.658m) (360sqft) with a stud height of 10ft. The boat ramp was 24 ft in length (6ft = 1.829m as the landing and 18ft = 5.486m as the ramp) and 9ft wide. The materials used in the construction were; the walls were built in fibrilite and the roof in corrugated iron, the two sets of windows were built with cedar and the bi-fold doors were constructed in tongue and groove. All timber used in the construction of the boatshed was tanalised</p> <p>Previous Owners: Smyth sold the property to Cole in 1965 and he sold it to Griffiths in 1969</p>
Keywords	MHW at the rear of the structure Fairly level papa rock BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCMAR009
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	Mr & Mrs R E Griffiths
'Owners Address'	41 Marine Parade
'Date of Construction'	1951 Jetty ^{xl} & 1976 Boatshed

'Photo Image'



Photo from ARC files no date



'GPS Easting'	26654179
'GPS Northing'	64832548
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent No'	15514

- ^{xciv} Letter, M.D. to AHB, M.4/4034, 19 November 1951.
- ^{xcv} Letter, J.R. Smyth to AHB, 30 June 1951.
- ^{xcvi} Marine Department Plan M.D. 9250.
- ^{xcvii} Marine Department, Reference 4/4034, 19 November 1951.
- ^{xcviii} Marine Department, Reference 4/4034, 19 November 1951.
- ^{xcix} Letter, J.R. Smyth to AHB, 25 May 1951.
- ^c Marine Department Plan M.D. 9250.
- ^{ci} Marine Department 9250, 4/4034, 10 August 1951.

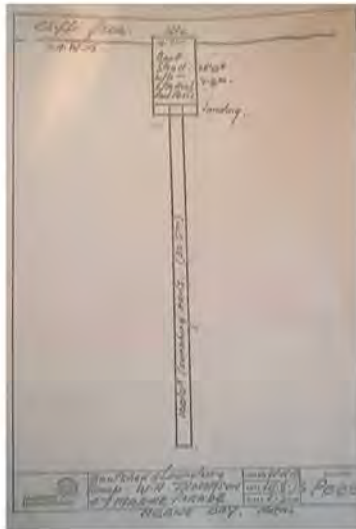
'Site Type'	BOATSHED
Name	
Location	Adj 47 Marine Parade Herne Bay Auckland
'Local Authority'	Auckland City Council
Description	Vertical weatherboard sheathing with a corrugated iron roof The shed is constructed in vertical timber boards with a 'lean to' corrugated iron roof and measured 25ft (7.6m) x 15ft 6inches (4.7m). At the front of the boatshed doors there is a wooden landing where the ramp extends 100ft - 30.5m (5ft in width) to the mean low water tide mark. The shed was erected on steel posts (tram rails or similar) and was raised about 4 - 6 ft above the beach. A steep set of wooden steps leads from the property to the door of the shed. In 1977 an inspection of the boatshed revealed it required painting and some general maintenance. In 1987 part of the boat ramp rails and two piles had corroded, which required attention. At this time the boat ramp was shortened by 20 ft. ⁶⁴ Timber piles in new concrete footing
Notes	In 1963 William Arthur Thompson applied to the Auckland Harbour Board to construct a boatshed and slipway at 47 Marine Parade, Herne Bay. The harbourmaster had no objections after an inspection of the site (2 August 1963) and recommended that Thompson's application be approved subject to approval of the AHB. The conditions set down were: <i>a. Marine Department approval, b. Licence to be for a term of 14 years at a fee of £7-10s-op pa, c. Boatshed and slip to be constructed and maintained to the satisfaction of the Chief Engineer.</i> ⁶⁵ On the 29 August 1963 the AHB approved a foreshore licence and the Marine Department approved the plans for the boatshed and slipway on the 22 November 1963. ⁶⁶ The boatshed was for personal use to house the family 17ft runabout. The owners of the adjoining properties, Dr. H. Nelson and Mr. M. Manson, agreed to Thompson constructing a boatshed on his foreshore. ⁶⁷ At the rear of the boatshed were a number of mature pohutukawa trees. In 1996 the foreshore licence for 47 Marine Parade was transferred into the name of Ronald Ernst and Norma Alice Griffiths who already held another foreshore licence for 41 Marine Parade. Owners: Walter Arthur Thompson 1963 and now The Thompson Family Trust.
Keywords	Cliff face at the rear of the boatshed is the MHWMark Nature of foreshore - papa reefs and shell - fairly level BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO: PTCMAR019 & H9012
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	W A Thompson Family Trust (William Arthur Thompson & Barry Campbell)
'Owners Address'	P O Box 68 078, Auckland
'Date of Construction'	1963

'Photo Image'



Photo from ARC file - no date

'GPS Easting'	26654438
'GPS Northing'	6482760
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	28722



- ⁱⁱ Foreshore Licence Inspection and Report, 12 January 1987.
- ⁱⁱⁱ Correspondence, Harbourmaster to Auckland Harbour Board, 2 August 1963.
- ^{iv} Letter, M.D. to AHB, 22 November 1963
- ^v Correspondence, Foreshore Licence, 2 August 1963.

NORTH SHORE



Auckland Public Libraries Map 4180

'Site Type'	BOATSHED jetty 1980? Boatshed/Dinghy Locker 1996
Name	Original owner: ?
Location	Ngataringa, Bay, Stanley Point
'Local Authority'	North Shore City
Description	<p>Historical</p> <p>The jetty is at least 26 years old and would have required a permit. However, this information is not recorded in the Auckland Regional Council file. A search is required through the Marine Department consent files to establish the year the jetty was erected.</p> <p>In 1995 John Wilfred Manning (architect) applied to the Auckland Regional Council to construct a boatshed/dinghy locker on the Waitemata Harbour foreshore at Stanley Point, Devonport. Manning had written approvals from his immediate neighbours, Ngati Paoa Whanau Trust Board, Te Hao o Ngati Whatua and Department of Conservation. Manning has paid particular attention to the site when deciding the design and style of the boatshed/dinghy locker.</p> <p>In 1996 a neighbour complained to the ARC that Manning had not sited his boatshed/dinghy locker in the corrected location. It was surveyed again and was found to be correctly located.</p> <p>Architectural</p> <p>A wooden jetty and ramp were built about 1980.</p> <p>In July 1995 Manning & Associates registered architects located at Canterbury Arcade 47 High Street Auckland designed a dinghy locker for himself at Ngataringa Bay Stanley Point Devonport. The ARC required the plans to be altered and these were amended in November 1995. The boatshed was designed in the style of a dinghy, and measures 4.5m in length x 1.8m in width and 2 metres in height. It was constructed using 300 cedar shiplap sawn weatherboards with a flat roof. The boatshed was built with a wooden floor and was erected on concrete piles. The seaward wall was curved to match the contour of the cliff face. The decorative elements are; the top and bottom timber boards of the shed were stained a different colour giving a nautical feel to the shed, and a portal hole was placed on one of the facades. A set of double wooden landing doors were added to open onto the existing jetty with the seaward side wall having a sliding door. Access to the boatshed from the property was down a set of existing wooden steps with another set of existing wooden steps giving access to the beach.</p>
Notes	<p>ARC File No DEVSTA15 & H10496</p> <p>Consent 9163 expired 2004, 14030 expired 2005, and 30059 expired 2004</p> <p>ARC Approved Plan No H16496</p> <p>Legal Description Lot 1 DP40929 CT 1096/282</p> <p>Map Ref NZMS 260 R11 687845</p> <p>260 R11 2668678E 6484743W</p> <p>File Ref H9510496</p> <p>Application No 30059</p> <p>MHWM under the boatshed</p>
Keywords	<p>BOAT SHED</p> <p>BOATSHED</p> <p>DINGHY LOCKER</p>
'Biblio References'	<p>Auckland Regional Council</p> <p>Marine Department Consent Files</p>
'Date Recorded'	July 2006
'Reported By'	Antony Matthews
'Recorders Address'	<p>Matthews and Matthews Architects</p> <p>PO Box 108166</p> <p>Symonds St</p> <p>Auckland</p>
'Last Visited'	1997
Owner	John Wilfred Manning
'Owners Address'	15b Second Ave, Devonport, Auckland
'Date of Construction'	C1980
'Photo Image'	



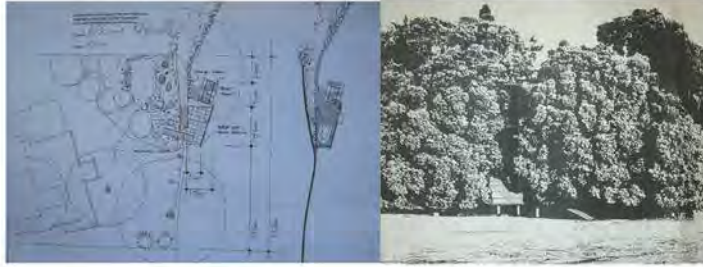
Plans drawn July 1995 revised November 1995



'GPS Easting'	2668686
'GPS Northing'	6484729
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	30059

'Site Type'	BOATSHED Boatshed/Dinghy Locker, Ramp and Steps 2000.
Name	Original Owner: Stephen Mills and Nancy Symestvedt
Location	Ngataranga Bay
'Local Authority'	North Shore City
Description	<p>Historical In October 2000 Stephen Mills and Nancy Symestvedt applied to the Auckland Regional Council to construct a boatshed/dinghy locker, ramp and steps at 9 First Avenue Ngataranga Bay. The Department of Conservation gave permission to Mills and Symestvedt to build their boatshed with the application granted on the 23 November 2000.</p> <p>Location The design and the location of the boatshed were part of the original brief; the shed was to blend into the natural landscape. The property is next door to a council park and consideration to the location of the shed, ramp and stairs was instrumental in its placement. The stairs were constructed on the southern end of the property away from the existing public walkway through the park. On the cliff face there are protected pohutukawa trees and the placement of the boatshed meant an arborist was consulted as a few tree limbs required trimming during the construction of the shed.</p> <p>Over the years a number of structures have been built along this stretch of coastline. There are an array of boatsheds, boatramps, landings, jetties, stairways and seawalls - old and new, which give a particular character to the area. The boatshed was designed to have a minimum impact on the environment and the architectural landscape.</p> <p>Architectural The boatshed was designed by Mac & Co architects and designers Devonport Auckland. The boatshed was small in design and was built on six piles to elevate the shed above the MHWL. The shed was built in hand sawn cedar cladding over laid with horizontally placed battens and stained a sandstone colour to harmonise with the cliff face. The remainder of the timber work including the shutters, doors and surrounding cladding was left to weather. The shed roof is flat and this provided a lookout platform; there are a set of wooden steps to the platform from the property and a ladder placed on the side of the shed giving access to the beach. The shed measured 5.6m in length x 2m at its narrowest and 3.5m at its widest in width. The deck was designed in the shape of a dingy to minimise the building footprint placed on the coast line. The ramp is 3 metres in length and was erected in a timber slatted board arrangement.</p>
Notes	<p>Auckland Regional Council File No 15158 Consent No 24502 issued 2000 24501 expired 2002 File Ref COA 933 DOC Customer No 9391 Permit No 24502 Legal Description Lot 3 DP37618 Pt Allot 34 Sec 2 Parish of Takapuna CT IB/192 Map Ref R11 688847 Map Ref NZMS260 R11 687847</p>
Keywords	BOAT SHED BOATSHED DINGHY LOCKER
'Biblio References'	Auckland Regional Council Files Marine Department Consent Files
'Date Recorded'	July 2006
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	2000
Owner	Stephen Mills and Nancy Symestvedt
'Owners Address'	9 First Avenue, Ngataranga Bay, Devonport, Auckland
'Date of Construction'	2000

'Photo Image'



'GPS Easting'	2668835
'GPS Northing'	6484777
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent No'	24502 issued 2000

'Site Type'
Name
Location
'Local Authority'
Description

BOATSHED/DINGY SHELTER Boatshed, slipway and deck 1963

Original Owner: LINZ

Bayswater

North Shore City

Historical

In 1963 the Marine Department gave the owner of 5a Beresford Street verbal permission to construct a small jetty, however, a dinghy shelter was also constructed. When Eric John Cooper purchased the property in 1964 the boatshed/dingy shelter, jetty (deck) and slipway were in situ at 5a Beresford Street, Takapuna. In 1970 the jetty was rebuilt and enlarged to its present size. The shed, jetty and slipway were legalised under Section 178(1) (b) of the Harbours Act 1988 with the plan approved by Department of Conservation DOC(CM)AK00150 in 1989. Plans of the boatshed, jetty and ramp were drawn by the Engineering Firm Alan G. Franklin of Mount Eden (plan 497) for E. Cooper in 1989. A foreshore licence was granted on the 1 January 1991 for a term of 14 years with the licence fee set at \$160 per annum.



1976

Location

The boatshed is painted green to blend into its surroundings and was constructed at the base of the 15 metre cliff face located at the western end of the Bayswater peninsula. The shed is exposed to the westerly wind, which creates a significant amount of wave action underneath the jetty. The cliff face is covered in trees with the coastline fringed with pohutukawa trees. The North Shore coastline is dotted with other similar boatsheds with one located adjacent to the Coopers.



Architecture

The boatshed was constructed in fibrolite with a corrugated iron pitched roof. A viewing platform was constructed on top of the roof giving the shed an unbalanced appearance. This in effect distracts from the perceived historical view of what a typical New Zealand boatshed should emulate. The shed does not interfere with the tidal flow as it was elevated on iron piles encased in concrete and placed into the rock seabed. The jetty (deck) and slipway were constructed in wood but at the base of the slipway a concrete pad was placed. A set of concrete steps had been constructed prior to Cooper purchasing the property in 1964, however, now a set of wooden steps lead from the owner's property to the sheds roof/viewing platform with access

to the boatshed and beach via a ladder. In 1976 the boatshed viewing platform encompassed the entire roof, however, by 1998 it covered about one third. The structures occupy an area of 30.53m² with; the shed measuring 3.0m in length x 2.55m in width, the jetty/deck measuring 0.8m down the side of the boatshed and 2.7m wide in front of the structure, and the slipway measuring 5.2m in length x 2.2m in width. The boatshed has no landing doors and was used to store a dingy for recreational purposes.

North of the boatshed another small structure was constructed against the cliff face, which was used for clearing fish.

In 2005 the Cooper's replaced five piles and some of the structures timbers.



2005

Notes	Auckland Regional Council File 18170 (TAK/BAY/022) Consent 9296 and 30602 Approved Plan DOC (CM) AK00150 Customer No: 1031 & 4933 User I.D. No: 524 PT Bed Waitemata Harbour File Reference C/030602/1 Associate Reference 8713 – CZW 50029-DOC File 1990 Map Reference NZMS 260 R11 2668147E, 6485854N Coastal Protection Area 2 Last Inspected March 2005 Foreshore Licence Expiry 2033 (date commenced 1 April 1990) MHWS at cliff face
Keywords	Depth of water at High Water Mark 1.9 metres BOAT SHED BOATSHED DINGY SHELTER
'Biblio References'	Auckland Regional Council File Marine Department File
'Date Recorded'	July 2006
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	2005
Owner	Current Owner: Eric John Cooper
'Owners Address'	5a Beresford Street, Bayswater, Auckland
'Date of Construction'	Boatshed, Slipway and deck constructed 1963 and legalised in 1988.
'Photo Image'	



Plans 1989



1976

'GPS Easting'	2668165
'GPS Northing'	6485872
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

<p>'Site Type' Name Location 'Local Authority' Description</p>	<p>BOATSHED/DINGY SHELTER Boatshed, slipway and deck 1964?</p> <p>Original Owner: ? Bayswater North Shore City</p> <p>Historical Eric John Cooper purchased the property in 1964 and the boatshed/dingy shelter, jetty (deck) and slipway were in situ at 5a Beresford Street, Takapuna. The shed, jetty and slipway were legalised under Section 178(1) (b) of the Harbours Act 1988 with the plan approved by Department of Conservation DOC(CM)AK00150 in 1988. Plans of the boatshed, jetty and ramp were drawn by the Engineering Firm Alan G. Franklin of Mount Eden (plan 497) for E. Cooper in 1989. A foreshore licence was granted on the 1 January 1991.</p> <p>Location The boatshed was constructed right up against the cliff face and painted green to blend into the surroundings. The cliff face is covered in trees with the coastline fringed with pohutukawa's. The North Shore coastline is dotted with other similar boatsheds with one located adjacent to the Coopers.</p> <p>Architecture The boatshed was constructed in fibrolite with a corrugated iron pitched roof. A viewing platform was constructed on top of the roof giving the shed an unbalanced appearance. This in effect distracts from the perceived historical view of what a typical New Zealand boatshed should emulate. The shed does not interfere with the tidal flow as it was elevated on iron piles encased in concrete and placed into the rock seabed. The jetty (deck) and slipway were constructed in wood but at the base of the slipway a concrete pad was placed. A set of concrete steps had been constructed prior to Cooper purchasing the property in 1964, however, now a set of wooden steps lead from the owner's property to the sheds roof/viewing platform with access to the boatshed and beach via a ladder. The structures occupy an area of 30.53m² with; the shed measuring 3.0m in length x 2.55m in width, the jetty/deck measuring 0.8m down the side of the boatshed and 2.7m wide in front of the structure, and the slipway measuring 5.2m in length x 2.2m in width. The boatshed has no landing doors and was used to store a dingy for recreational purposes.</p> <p>In 2005 the Cooper's replaced five piles and some of the structures timbers. The walls are still clad with corrugated fibrolite (super six roofing)</p>
<p>Notes</p>	<div style="text-align: center;">  <p>2005</p> </div> <p>Auckland Regional Council File 18170 (TAK/BAY/022) Consent 9296 and 30602 Approved Plan DOC (CM) AK00150 Customer No: 1031 File Reference C/030602/1 Associate Reference 8713 - CZW 50029-DOC File 1990 Map Reference NZMS 260 R11 2668147E, 6485854N Coastal Protection Area 2 Last Inspected March 2005 Foreshore Licence Expiry 2033 (date commenced 1 April 1990)</p>

Keywords MHWs at cliff face
BOAT SHED
BOATSHED
DINGY SHELTER

'Biblio References' Auckland Regional Council File
Marine Department File

'Date Recorded' July 2006

'Reported By' Antony Matthews

'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited' 2005

Owner Current Owner: Eric John Cooper

'Owners Address' 5a Beresford Street, Bayswater, Auckland

'Date of Construction' Boatshed, Slipway and deck constructed 1963 and legalised in 1988.

'Photo Image'



Plans 1989



Photographs 2006

'GPS Easting' 2668138

'GPS Northing' 6485855

'ArcView Category' Historic Structure

'Grid Refs Source' GPS

'Site Type'	BOATSHED 1972
Name	Original Owners: Trevor J. & J. L. Wood
Location	Adj 31a Norwood Street, Bayswater, Auckland
'Local Authority'	North Shore City
Description	<p>Historical</p> <p>In the early 1970s Trevor J. Wood (LINZ for date) constructed a boatshed and slipway adjacent to 31a Norwood Road, Bayswater on the Waitemata foreshore. This structure was illegal and Wood had never sought approval from the Auckland Harbour Board nor the Marine Department. In 1976 the boat ramp was realigned to an angle that would make it easier to launch a boat. The ramp extensions were approved by the AHB; it appears the issue of a foreshore licence was not raised. The McKenzie's have owned the property since 1978. However, in October 1989 the issue of a foreshore licence was brought to the board's notice as the McKenzie's were in the process of re-constructing the boatshed, jetty and ramp. The Auckland Regional Council ordered an engineering assessment with drawn plans in September 1990. The ARC instructed the McKenzie's to meet the structural requirements before a consent notice was issued. However, the boatshed, jetty and ramp had 'existing use rights' and was not a navigational hazard. In March 1991 they decided to go ahead with a new boatshed and had it almost completed before lodging an application for maritime planning permission. The McKenzie's were instructed to stop work immediately on the shed until the foreshore licence deed was signed on the 25 November 1991.</p> <p>The boatshed can be accessed four hours either side of full tide.</p> <p>Architectural</p> <p>It appears that the boatshed/dingy shelter constructed by Wood was built prior to 1972. It has the appearance of a temporary structure roughly built in vertical timber boards with a flat roof, and a landing ramp constructed with horizontal wooden boards. The boatshed measures 10ft (3.048m) in length and 9ft (2.74m) in width, and is elevated off the beach by tanalised timber piles. The Landing is 4ft 6 inches (1.4m) in length and is the same width as the shed; the ramp is 10ft (3.048m) in length and 6ft (1.8m) in width. The total area is 52.55m². The shed was on a lean and the stairs that were connected to shed were providing some bracing. In 1990 the ARC wrote to the senior foreshores officer stating that the boatshed required the following structural maintenance to be carried out; replacing boat ramp pile and bearers and joists and all bearer to pile connections and the landing decking to be joined together to form a continuous bearer. In March 1991 the new structure was built in a form of corrugated iron sheathing with a flat corrugated iron roof. The ramp appears to have been re-furbished rather than re-constructed, as it looks like the original in age, style and materials. The boat shed is accessed by a set of steep wooden steps down the cliff to the boat landing. In 1998 the boatshed was visited and it was noted that several boards at the bottom of the ramp were missing and required replacing.</p>
Notes	<p>Auckland Regional Council File TAKBAY026 Consent No 9469 DOC(CM)AK00248 File reference 46/11/6A Customer No 5641 Map Series R11 DP33501 MHWM cliff face under structure Papa rock formation</p>
Keywords	<p>BOAT SHED BOATSHED JETTY BOAT RAMP</p>
'Biblio References'	SE
'Date Recorded'	1998. This shed not found 2006. Access only by boat.
'Reported By'	Antony Matthews
'Recorders Address'	<p>Matthews and Matthews Architects PO Box 108166 Symonds St</p>

'Last Visited' Auckland
Owner Last inspected 28 April 1998
'Owners Address' Current Owners: David and Nancy McKenzie
'Date of Construction' 31a Norwood Road, Bayswater, Auckland
'Photo Image' Before 1972 (a dingy shelter)



Photo taken 1978

'GPS Easting' 2668585
'GPS Northing' 6485622
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' Name	BOATSHED Dinghy Locker c1960s Launching platform and rails c1965
Location	Original Owner: Alan Gordon Aspden Stokes Point - Little Shoal Bay (off 1 Princes Street)
'Local Authority' Description	North Shore City Historical Since the early 1960s a dinghy locker had been located at 1 Princes Street, Northcote Point. In April 1965 Alan Gordon Aspden applied to the Northcote Borough Council to erect a launching platform and rails to be located at the foot of his property and next to an existing boatshed on the Waitemata Harbour foreshore. The Auckland Harbour Board Chief Engineer and the Harbourmaster had no objections and the application was sent to the Marine Department for their approval. The plans were approved by the MD and the deed was signed on the 28 September 1965. During a storm in 1975 the boat broke from its moorings damaging the slipway. Aspden advised the AHB of the incident and that due to the storm the ramp was considerably shorter in length. He was advised by the AHB that there would not be a decrease in fee as the structure was beyond the low water mark. Between 1979 and 1981 Aspden sold his boat and the property was sold to Brian Holloway in 1981. In August 1983 Holloway applied for a new foreshore licence, which was duly granted; the licence fee was set at \$265 per annum. By 1987 Holloway informed the AHB that due to personal problems he could no longer afford the annual fee and asked for a decrease in the licence fee in exchange for removing part of the slip. In 1988 the ARC wrote to Holloway stating he had to dismantle the unauthorised deck additions, remove the debris and upgrade parts of the structure. Holloway refused to pay his licence fee and by January 1989 the ARC terminated his licence and Holloway was instructed to dismantle his structure. In April 1989 all the licence arrears were paid and his licence was renewed. In 1997 cyclone Orena washed away most of his ramp facility and so it became unsafe and unstable. The ARC instructed Holloway to do the repairs or dismantle the entire structure. Architectural According to AHB plan PO935 the launching platform measured 98ft 6inches (30m) in length x 14ft 9 inches (4.5m) at its widest and 6ft (2m) at its narrowest having a total area of 110m ² . The launching ramp was built in steel with a timber deck, which was positioned on concrete piles. A winch cradle was built at the shore edge of the ramp near the dinghy locker. Wooden steps (14.5m ²) lead from the property down the cliff face to the ramp and another set that lead to the foreshore. It appears there has always been a boatshed/dinghy shed at this property since the 1960s. Perhaps the shed was built on Aspden's property and therefore required a building permit rather than a foreshore licence. It was constructed in timber with a pitched corrugated iron roof. The shed has been simplistically decorated with a small portal and finials near the apex. In 2006 the shed was still in place as was the slipway. The slipway is in poor condition with the steel rails bent and broken and the concrete piles deteriorated.
Notes	Auckland Regional Council File No NORSHO 001 Consent 9245 expired Marine Department Plan No MD Plan 12056 Legal Description PT Bed of Waitemata harbour Pt CT 24D/851 (T541) Moorings Permit A104 Site NA7 Northcote Maximum water depth at high water 1.5-2m Usable at least four hours either side of high tide Extent of tidal influence - up to two hours either side of high water at spring tides Foreshore mud/sand over and papa and papa rock shelves
Keywords	BOAT SHED BOATSHED DINGHY LOCKER LAUNCHING RAMP

'Biblio References' Auckland Regional Council Files NORSHO1
Marine Department files
July 2006

'Date Recorded'

'Reported By' Antony Matthews

'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland

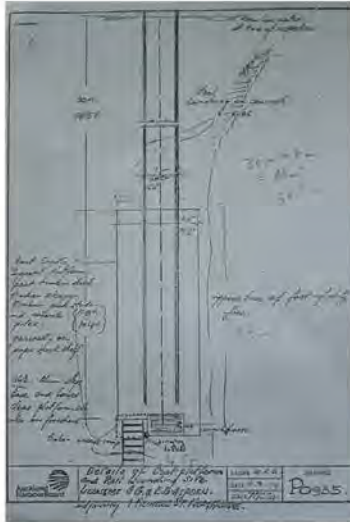
'Last Visited' 1997

Owner Brian Keith Holloway

'Owners Address' 1 Princes Street, Northcote Point, Auckland

'Date of Construction' 1965

'Photo Image'



Photos from ARC files



'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
'Consent No'

Photos 2006
2666502
6484646
Historic Structure
GPS
9245 expired 1997

'Site Type' Name Location 'Local Authority' Description	<p>BOATSHED c1926 Original Owner: LINZ for owner Northcote Point North Shore City</p> <p>Historical The boatshed at 92 Queen Street, Northcote Point was located at the southern end of Hall's Beach in Little Shoal Bay opposite Beach Road. The boatshed was in existence when Athol R. Myers purchased the property in 1948. Another shed, which was positioned at the rear of the boatshed on the owner's property, was used for the storage of boat gear and was issued with a building permit in 1926. This suggests the boatshed was built at the same time or a little later. Previous property owners plus others along Hall's Beach had built yachts in the past, and some of the ramps and the sheds from this period still exist in Little Shoal Bay. Either side of the boatshed are two sets of steps that gave access to the beach from the above properties. The cliff face is planted with stands of mature pohutukawa and native trees including puriri and nikau.</p> <p>In 1989 Myers had a swing mooring site in Little Shoal Bay (No. LJ 12), which was used to launch his 36 foot launch 'Zircon' and the boatshed and ramp located at his property used to house the dinghies.</p> <p>In March 1990 Myers undertook some unauthorised works of the structures and was informed by the Auckland Regional Council he required consent. The ARC eventually dropped their objection to the alterations due to the age of the owner. However, he was instructed to paint his boatshed in a colour that would blend into the background. The property was sold to Graham John & Margaret Eva Collings in 1991.</p> <p>In 1992 the new owners planned to alter the boatshed and applied for a foreshore licence from the ARC. One of the conditions for consent was that the boatshed had to be painted in an appropriate colour and the colour required approval from the council. The Collings stated that many other boatsheds in the immediate area were painted a light cream with a green roof and it would be fitting for theirs to be painted like the rest giving continuity on the foreshore.</p>
	<p>Location: The boatshed's location is highly visible on the Waitemata Harbour. It can be seen from the Auckland Harbour Bridge, Herne Bay, Birkenhead Point, and from other craft that used the inner harbour. The Waitemata Harbour foreshore is dotted with many historic and relatively modern structures; however, this does not visually impair the landscape. The sheds give character with many surrounded by mature plantings of trees.</p> <p>Architectural The boatshed was constructed at the foot of a very steep cliff face, partly on the property and partly on the foreshore. The shed was originally built as a dinghy locker and was used for the storage of these small craft and equipment. Originally the shed was rectangular in shape and constructed in timber with a flat corrugated roof. There was no back wall as the shed was built right up against the cliff face. The boatshed was constructed in horizontal weatherboard sheathing with a corrugated iron roof. The total area of the structures was 24.55m² with the boatshed measuring 6.25m in length x 2.2m in width a total of 13.75sqm and the ramp measuring 6m in length x 1.8m in width encompassing an area of 10.8m². In 1991 the new owners planned to build a pitched roof over part of the boatshed leaving the remaining part of the structure with the existing flat roof.</p> <p>In June 1991 Graham Collings, the owner of the property, drew plans to alter the style of the boatshed. The outlay of the alterations was to cost approximately \$4,500. In 2005 during a routine inspection by the ARC it was noted that the boatshed was painted cream with green doors and the roof had not been altered to a pitched roofline. The exterior had also been clad in fibrolite.</p>
Notes	<p>Auckland Regional Council File No 18/W/339 & 18240 Consent No 11692 expired & 30767 issued Marine Department Plan No MD 4233? File No BRD/26/1/7 NZMS260</p>

Map Reference NZMS 260 R11663855
 Legal Description Lot 1 DP66251 CT 40c/413
 Bed Waitemata Harbour Survey Plan 67176
 MHW on cliff face under the structure and 1.25m above ground.
 Spring Tide Velocity (Max = F6 knots/hr
 Low tide 130m of foreshore for pedestrian traffic
 Useable water 2 to 3 hours either side of high tide
 Hard mud and shell surface
 Last Inspected 26 April 2005

Keywords

BOAT SHED
 BOATSHED
 DINGHY LOCKER

'Biblio References'

ARC File
 Marine Department Consents File

'Date Recorded'

July 2006

'Reported By'

Antony Matthews

'Recorders Address'

Mathews and Mathews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited'

2005

Owner

G & M Collings

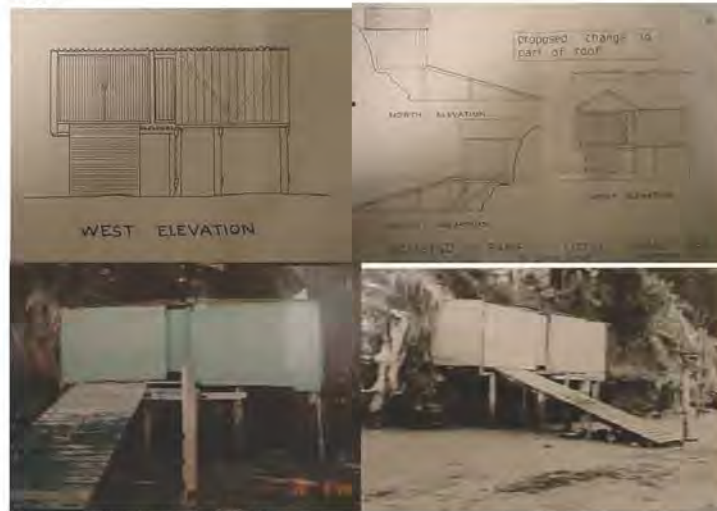
'Owners Address'

92 Queen Street, Northcote Point, Auckland

'Date of Construction'

C1926

'Photo Image'



Photos and drawing from ARC files



Photos 2006

'GPS Easting'	2666184
'GPS Northing'	6485347
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent No'	30767

'Site Type' Name Location 'Local Authority' Description	<p>BOATSHED Boatshed and Ramp c1920s/30s Original Owner Hutchinson /Sharp? Little Shoal Bay North Shore City</p> <p>Historical It appears a boatshed was constructed at 57a Maritime Terrace Birkenhead during the 1920s/30s. In the early 1900s the Hutchinson family owned a very large tract of land in Maritime Terrace, and at some stage a timber boatshed and ramp was constructed. A 1939 photograph from the Alexander Turnbull Library clearly demonstrates that a boatshed had been erected at this location. In 1942, the current owner's parents-in-law, purchased part of the original estate including the boatshed. In 1978 Graham Stretz, purchased part of his parents-in-law property, which included the ownership of the boatshed.</p>
	<p>Location The shed is discreetly tucked into the cliff face and does not impinge on the marine landscape. It was situated to blend into the background and become part of its natural surroundings. Public access to the foreshore is not hindered by the presence of the boatshed except at high tide. The shed is located around the coastline from the Birkenhead Wharf.</p>
	<p>Architectural The shed was oblong in design with exposed rafter ends, and is a typical example of an iconic kiwi boatshed. This style is replicated many times with many examples dotted around the Waitemata Harbour. The shed was constructed parallel to the cliff face, it was partly built into the cliff on NSCC esplanade reserve and partly built over the foreshore. It was constructed in a rough flat pinus weatherboard that was originally oiled and had a pitched matloid roof that was laid over sarking. The shed measures 14 ft 6 inches (4.3m) in length x 8ft (2.3 m) in width.</p>
	<p>The deck and ramp are of a horizontal slatted timber board construction. The deck and ramp measure 7ft and 9ft in length respectively (total length 6.4m in length x 1.6m in width) with the length tapering to a width of 4ft 6 inches at the base. Access to the boatshed from the property is via a set of steep wooden steps down the cliff wall.</p>
	<p>In 1989 the architectural firm of 'Buisson & Stretz', Queen Street drew new plans of the existing boatshed as the boatshed and ramp were to undergo maintenance. Originally the boatshed and ramp had eight wooden piles set into concrete but during the maintenance the five shed piles were replaced with four Senton piles of tanalith timber H5 (C2). The decking and ramp piles were replaced with tanalith treated timber to marine/ground retention quality of H4 (C3), and increased to six piles instead of three. The timber on the ramp was replaced with tanalised H4 (C3) slats of wood. In 1990 during a routine engineering assessment by the ARC design engineer, it was noted that nails were used instead of M16 galvanised bolts. Other irregularities were also noted and before the boatshed and ramp could be considered to be in a satisfactory state these structural problems had to be corrected. In 2005 the boatshed was inspected and the ramp was found to be in a poor condition.</p>
	<p>In 2006 the shed was in poor condition. External doors are plywood. Wall are creosoted weather board on timber framing. Corrugated steel roof. New 125 square treated piles with timber framed floor.</p>
Notes	<p>Auckland Regional Council File No BIRSHO003 & 18416 Consent No 13626 expired 2005 30960 issued 2005 Marine Department Plan No DOC(CM)AK 00190 (1990/91) User ID 1474 Legal Description Lot 1 DP84747 CT41A/801 Area 2374m2 Pt Bed Waitemata Harbour User Activity 1474 Map Reference R11 266 5600 6485700 ZNMS 260 R11 656857 Associate Ref CHA25003 - DOC approved foreshore licence Associate Ref CMAK00190 - DOC approved plan</p>

Keywords Last inspected 2005
 MHW at the cliff face under the boatshed
 Depth of water at HW is 1metre
 BOAT SHED
 BOATSHED
'Biblio References' Auckland Regional Council Files
 Marine Department Files
'Date Recorded' July 2006
'Reported By' Antony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
 2005
'Last Visited'
Owner Graham Francis and Pleasance Strez
'Owners Address' 57a Maritime Terrace, Birkenhead, Auckland
'Date of Construction' c1920s/30s
'Photo Image'



Photos and plans from ARC file



2006 Photos

'GPS Easting' 2665560
'GPS Northing' 6485581
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
'Consent No' 30960

'Site Type' Name Location 'Local Authority' Description	BOATSHED 1965 (not a permanent structure) 1967 Boatshed, Slipway and Retaining Wall. Original Owner: Donald Lester Cruel (Dental Surgeon) Birkenhead North Shore City
	<p>Historical</p> <p>The structures are located on the western side of Little Shoal Bay. In 1965 a temporary shed and a permanent concrete block retaining wall were built on reclaimed land by Donald Lester Cruel; he planned to build a permanent structure within the next couple of years. The neighbours who resided at 59 and 65 Maritime Terrace gave Cruel permission to proceed with the construction of his boatshed. In February 1967 Mr. Gordon N. Ellistair, a Surveyor of Glenfield, surveyed the site for Cruel. In June 1967 Cruel applied to the Auckland Harbour Board to construct a boatshed and slipway and have the retaining wall made official. The AHB approved the location and the Marine Department approved the plans on 30 August 1967. The foreshore licence fee was set at £4 per annum with the usual terms and conditions; the deed was signed on October 1967. By 1981 the licence fee was \$65 per annum.</p> <p>In 1987 the property was sold to Robert Peter and Lucie Jean Ward; however, the licence was put in the name of Averil Homes Limited. The transfer of the licence was approved, for a term of 14 years at \$400 per annum. A year later the property was sold to Michael Charles and Vicki Thelma Wiggins and the foreshore licence was transferred to the Wiggins Family Trust. The foreshore licence deed was signed on the 1 August 1988 for a term of 13 years.</p> <p>In 1992 the property was sold to Roger John Tomes (philosopher) and the licence was transferred into his name.</p> <p>Architectural</p> <p>The boatshed is plain in design, and was constructed of timber with a pitched corrugated style asbestos roof. Approximately one sixth of the boatshed/dingy locker was built on the owner's property with the rest built above the foreshore on iron railing piles set into concrete. The structure was built at the same height as the existing retaining wall, which was 16.67m in length. The boatshed occupies an area of 24 m² with; the northern side measuring 14ft in length, the southern side measures 12ft 2inches in length and the western and eastern side's measures 11ft 8inches in length. The slip and ramp occupies an area of 57m² with the shed and slip connected by a wooden ramp (1990). The shed does not have landing doors and is used primarily for storing dinghies. The boatshed is usable one hour either side of high tide with the depth of the water being 1.5 metres. The boatshed is accessed from the property by a set of steep steps and can be accessed as well across the mud flats. In 1987 during an ARC routine inspection it was noted that some of the piles required replacing.</p> <p>In 1990 a jetty was constructed replacing part of the slipway. The work was unauthorised and was contrary to the Marine Department plan 12671. For additions and alterations to proceed permission from the appropriate authorities had to be sort through the application process.</p> <p>In 1995 during a field trip it was noted the boatshed required some further maintenance to some of the iron railing piles and railing.</p> <p>In 2006 the shed was clad with plywood sheet over timber framed with a gabled corrugated iron roof and exposed eaves. TGV faced doors have been fitted. Floor framing is timber joist on concrete plies. Part of the shed sits on a retaining wall constructed of recycled bricks and bluestone sitting partly on steel railings. The wall extends beyond the shed retaining a small grassed area. A small slipway with steel rails is located to the north east of the shed.</p>
Notes	ARC File No BIRSHO001 Consent No 9148 expired 2001, 20721 expired 2001 & 20757 Associated Consents: 11914, 12611, H938944, H9510510, H9510598, H9611113 Marine Department Plan No MD 12671 (30 August 1967) Marine Engineers Plan F368 Legal Description Lots 1 DP 77483 CT 33D Folio 508 PT Bed Waitemata Harbour RB DUR PT CT 12A/786 (T90) Map Reference R11 2665500 6485800 MHWL at the foot of the cliff

Keywords BOAT SHED
BOATSHED
DINGHY LOCKER

'Biblio References' Auckland Regional Council Files BIRSH01
Marine Department Consent Files

'Date Recorded' July 2006

'Reported By' Antony Matthews

'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland

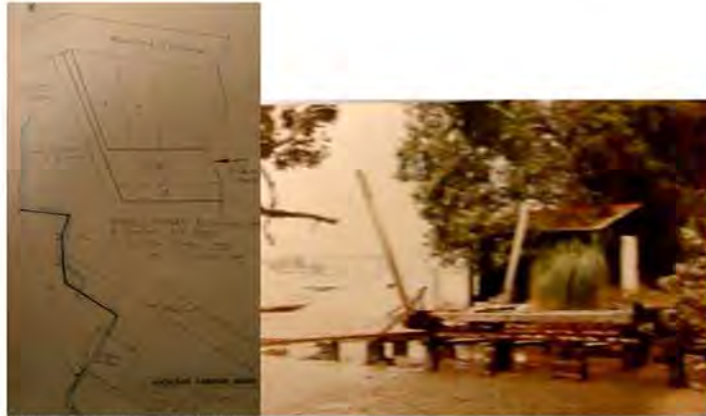
'Last Visited' 1995

Owner Current Owner: Raymond John Tomes

'Owners Address' 59 Maritime Terrace (formerly 45a Marama Terrace), Birkenhead, Auckland

'Date of Construction' 1967

'Photo Image'



Plan and photo from ARC Files



'GPS Easting' 2685587

'GPS Northing' 8485661

'ArcView Category' Historic Structure

'Grid Refs Source' GPS

'Consent No' 20727 expired 2001, 20757

<p>'Site Type'</p> <p>Name</p> <p>Location</p> <p>'Local Authority'</p> <p>Description</p>	<p>BOATSHED - Boatshed built on a barge c1963 (a foreshore consent was not required – a Birkenhead Borough Council building permit was required) - Jetty & Slipway 1973 (consent)</p> <p>Original Owner: David Albert Jackson</p> <p>Hellyers Creek</p> <p>North Shore City</p> <p>Historical</p> <p>In May 1973 the Marine Department in Auckland received an application from David Albert Jackson to construct a jetty and slipway on the southern side of Hellyers Creek near Beachhaven. The usual terms and conditions would apply: 1. must not affect the rights of public access along the foreshore, 2. nor navigation, 3. and not be visible obtrusive or, 4. detract from the general appearance of the bay. In October 1973 Jackson submitted plans in duplicate and a Lands & Survey map to the Auckland Harbour Board and the Birkenhead Borough Council for comment. The Marine Department Auckland, the Auckland Harbour Board, the Ministry of Works and the Birkenhead Borough Council gave their approval and a permit was issued.</p> <p>The MOW reported the length of the Jackson's jetty was longer than the measurements on the approved plans and was instructed to furnish an amended set of plans. Jackson decided to extend the jetty further not adhering to the measurements of the amended plans. He was instructed again to send another set with the correct measurements. On the 21 May 1974 a permit was issued for a period of 14 years with the rental set at \$3 per annum. In October 1986 Jackson sold the property, boatshed, jetty and slipway to George Edward and S.R. Lane. They applied to the AHB to transfer the foreshore licence into their names and it was their intention to upgrade the boatshed by moving it off the barge locating it near the existing jetty. By 1987 the foreshore licence rental had increased to \$240pa. The Lanes sold the property to Muir and Rogers in June 1991.</p>
---	---



Before the boatshed was relocated



Jetty plan 1973

Architectural

The jetty, slipway and boatshed are located in Hellyers Creek, and these structures occupy an area of 55.25m². The boatshed was erected on top of an old coastal barge and was constructed in timber, prior to the construction of the jetty and slipway in 1973. The jetty and slipway were built in horizontal weatherboards and were positioned approximately 6 metres from the boatshed. In August 1989 Lane's application to extend the jetty and relocate the boatshed was granted and the Engineering Consultancy Firm of Gary Chester Consultants Ltd drew the plans. However, it was the new owners of Muir and Rogers who relocated the boatshed to the shoreward end of the jetty in 1998 (Plan No 3439/5). The barge had to be demolished and all the timber removed from the foreshore. This relocation visually improved the shoreline in Hellyers Creek; over the years many unauthorised barges have been beached along the banks of Hellyers Creek. The barges were part of a fleet that were used to make deliveries on the North Shore and, around the Waitemata Harbour.

In the early 1960s the boatshed was constructed but it wasn't authorised until 1989. The original shed was clad in corrugated iron with a mono pitch roof. It was refurbished (1989) after it was

relocated and painted pohutukawa green to blend in with its natural surroundings.

The original length of the jetty and slipway was 50feet. The jetty was extended sideways by 4.6m in length and 3.6m in width to provide support for the relocation of the boatshed in 1998. All timber used in the construction had to tanalised and to a standard of Marine H6. The supporting structure comprised of 100 x 25mm decking (radiata pine) on 100 x 50 joists at 400mm centres supported by 150 x 50 bearers at 1000mm centres. The bearers are supported by 2/300 x 50 beams on 175mm diameter piles (Australian hardwood and kauri) with the piles concreted into predrilled holes in the papa rock.



The boatshed relocation Plan 1989

Notes

Auckland Regional Council File NO BIRHEL010
 Consent No 9141
 Marine Department Plan No MD 15172 – Jetty and Slipway (1973) & DOC(CM)AK 00159 – Boatshed 1991 (the Lanes on the 22 August 1989 applied to relocate the boatshed and extend the jetty).
 Customer No 1709
 User ID 369
 Ref 54/3/1027 & 54/2/620 MOT
 Legal Description Lot 55 DP 20048 Pt Allot 44
 PT Bed Waitemata Harbour CT 12A/786 (T90) NSCC
 Licence plate 15172
 Map Ref R11 2660400 6488900
 Site - Map Ref NZMS 260 R11 604889
 Plan Folio 8
 Nautical Advisor folio 17/4/74 7 13/11/73
 Harbour Board Folio 28/3/74
 Power Line File 54/0/24
 Account No 52431
 MHWM 2ft under the jetty
 Papa Rock
 Water depth available at high tide 8ft (2-3m) and at low tide – dries out
 Tidal useable four hours per tidal cycle

Keywords

BOAT SHED
 BOATSHED
 JETTY

'Biblio References'

Auckland Regional Council Files
 Marine Department Files

'Date Recorded'

July 2006

'Reported By'

Antony Matthews

'Recorders Address'

Mathews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited'

2005

Owner

Mark S. Rogers & Jean S. Muir

'Owners Address'

14 Beachhaven Road, Beachhaven, Auckland - C/- UK Sails Makers, Hong Kong

'Date of Construction'

1973

'Photo Image'



1987

'GPS Easting'	2660337
'GPS Northing'	6488916
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type'	BOATSHED 1970 Jetty (in 1970 a boatshed was constructed on a moveable barge, and therefore it did not require a licence. A boatshed licence was issued in 1985).
Name	Original Owner: Gavin Ernst Crooks
Location	Hellyers Creek
'Local Authority'	North Shore City
Description	<p>Historical</p> <p>In 1970 the jetty and boatshed were located off Rambler Crescent, Beachhaven in Auckland. In 1970 G. A. Crooks required permission from both the Auckland Harbour Board and Marine Department as well as from the Birkenhead Borough Council to construct his jetty on the Waitemata foreshore. He did not require a licence from the authorities for the boatshed because it was constructed upon a barge, which could be easily removed from its site. In the same year the Auckland Harbour Board approved an application from Gavin Ernst Crooks to construct a jetty on the Waitemata foreshore. The Marine Department had no objection as the structure did not pose a navigational hazard; neither did the BCC. In February 1970 the foreshore area was surveyed by B.E. Budsall. On the 10 December 1970 Crooks was granted a foreshore licence with the deed being signed on the 15 December 1970. The construction of the jetty was completed by May 1971. The licence fee was set at \$4.00 per annum; however, by 1978 the fee had risen to \$16.50 pa.</p> <p>In 1974 Crooks sold the property, jetty and boatshed to Ian Alexander Forsyth. The foreshore licence was transferred into his name on the 4 December 1974. During a routine inspection of the boatshed it was discovered the owner was using it for commercial purposes. In September 1985 Forsyth applied to the Auckland Harbour Board for a licence to occupy part of the foreshore for the establishment of a boatshed/workshop. During a routine inspection (1985) the boatshed was now considered to be a permanent structure, and therefore required a foreshore licence. A registered engineer, D.G. Croyden of St Heliers, was employed by Forsyth to produce a certificate of the boatsheds overall condition. Forsyth submitted two sets of drawings to the AHB with plan B 3240 accepted. On the 23 June 1986 the plans were approved, and the deed was signed on the 1 September 1986. In 1989 the property was sold to Ian Cook. The licence was transferred into his name and the licence was issued for 14 years at \$240 per annum. In 1995 during a routine inspection it was noted that the area around Cook's boatshed was littered with boating paraphernalia, demolition materials and a new platform had been constructed. He was instructed to remove the rubbish and had to apply for a permit for the platform. In 1989 Cook was asked to remove another barge and boat from the site.</p> <p>Architectural</p> <p>The jetty and the boatshed were built at 54 Rambler Crescent, Beachhaven in 1970. The total area of the boatshed and jetty occupies an area of 145.38m². The construction of the jetty required a permit but the shed did not as it was constructed on a movable barge. The jetty measured 15.1 metres in length x 4.8 metres in width. It was constructed in hardwood, some of which was rimu, with the piles set in a shingle and concrete mix. The MHWM was 2ft 6inches below the deck of the jetty.</p> <p>In 1970 the original boatshed was built and was revamped in 1986 when it was structurally secured to the barge with totara piles. The boatshed measured 13.5 metres in length x 5.4 metres in width. It was constructed in timber weatherboards with a corrugated iron roof constructed on a beached kauri barge (5.4m x 13.5m). In 1986 the shed was revamped and clad in Hardiplank weatherboards and a new corrugated iron roof replaced the existing roof. The shed consisted of three sets of windows on the western side, a side door, a roller door placed on the back wall with the façade designed with a set of double landing doors and a window. The shed had drain bungs set in the floor due to the spring tide.</p> <p>A set of hardwood wooden steps leads from the property to the front deck of the shed with another set leading to the waters edge.</p> <p>In 2006 the following was noted.</p> <p>Single level boatshed built on barge, with landing adjacent. Gabled roof with corrugated steel. Wall fibre cement sheet on timber frame. Aluminium windows and French doors. Timber retaining wall at base and part barge wall.</p>

Notes Auckland Regional Council File BIRHEL007
 Consent No 9138 expired 2000
 Marine Department Plan No MD14002 1970
 Marine Department MD(N)1308 1986
 CT17D 1661 (1974)
 Legal Description BLKX1 Waitemata S.D. Pt Allot 144 Takapuna Parish DP61670
 PT Bed Waitemata Harbour PT CT12A/786 (T90)
 Map Reference R11 2660500 6489000
 NZMS 260 R11 605890

Keywords BOAT SHED
 BOATSHED
 JETTY

'Biblio References' Auckland Regional Council Files
 Marine Department Files

'Date Recorded' June 2006

'Reported By' Antony Matthews

'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited' 16 February 1999

Owner Ian Cook

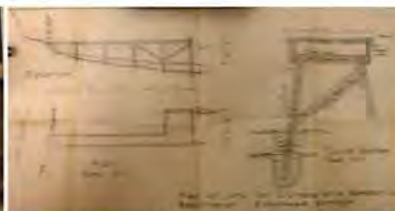
'Owners Address' 54 Rambler Crescent, Beachhaven, Auckland

'Date of Construction' 1970

'Photo Image'



1970 Boatshed



1970 plan of the jetty



1986 revamped boatshed





	Photographs 2006
'GPS Easting'	2660483
'GPS Northing'	6489057
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'GPS Northing'	
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent No'	

'Site Type'	BOATSHED Jetty earlier - Dinghy Locker 1967
Name	Original Owner: Lucy and Richard Bakewell
Location	Beach Haven
'Local Authority'	North Shore City
Description	<p>Historical</p> <p>In August 1967 Mrs. Bakewell applied to the Borough of Birkenhead to construct a dinghy locker next to an existing jetty and slipway adjacent to 15 Beachhaven Road. Approval by the council was granted as long as the structure was built according to the plans submitted and a building permit was obtained. The structure was expected to be completed by 31 October 1967, however, the plans were not sufficiently detailed and Bakewell had to submit another set. In September 1968 the Marine Department approved the second set of plans. In October 1968 the BBC approved the structure and issued a building permit (no A024408). In January 1969 the Marine Engineer was still concerned that the plans lacked detail but eventually a foreshore licence was issued and the deed signed on the 26 February 1969 for a term of 14 years. The licence was renewed again in 1983 for a period of a further 14 years.</p> <p>Architectural</p> <p>The jetty was constructed prior to 1967; the Auckland Regional Council file starts with an application by Mrs. Bakewell to construct a dinghy locker on part of the foreshore of the Waitemata Harbour. The jetty measured 25ft (7.8m) in length x 5ft (1.4m) in width and the slipway measured 4m in length x 1m in width, which were built in rows of horizontal slatted timber. The dinghy locker/boatshed measured 10ft (3m) in length x 8ft (2.5m) in width and was constructed in 4 x 2 inch batten and board treated pine with a pitched corrugated iron roof. The flooring was laid with 3 x 2 inch timber and spaced 1 inch apart. The shed was bolted to the jetty with extra piles set in concrete. On one side of the boatshed there are two east facing windows with the façade having a set of wooden double landing doors. The boatshed is surrounded by trees and was positioned close to the bank. A set of steep wooden steps lead from the property down to the shed.</p> <p>In 1989 during a routine inspection it was noted the shed had been extensively refurbished including repiling of the jetty.</p> <p>During the 2006 inspection the following was recorded: Single level boatshed offset gabled roof with corrugated steel. Walls timber framed with board and batten lining. Timber framed louvered window to west face. Timber piles in concrete footing wit some remnant railway track steel piles. Timber framed slipway and ramp with steel track slipway next to shed.</p>
Notes	<p>ARC File No BIRBCH002 & 14066 Consent No 9118 expired 1998 & 25531 issued Marine Department Plan No MD 2599 & M.O.T MD 13318 Permit No 1240 Legal Description Lot 8 DP682 Parish of Takapuna Allot 144 Waitemata Harbour NSCC CT 12A/786 (T90) Licence No 2871 Map Reference NZMS 260 R11 605888 Map Reference R11 2660500 6488800 File No 12/2/323 File No 54/2/323 MD & 13/2/323 File No M.O.W. 12/11/1 User ID No 346 MHWS at Jetty (6 feet of water) Depth of water at high tide - 1 metre Mud and sandstone</p>
Keywords	BOAT SHED BOATSHED
'Biblio References'	Auckland Regional Council Files Marine Department File
'Date Recorded'	June 2006
'Reported By'	Anthony Matthews

'Recorders Address' Mathews and Mathews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited' File finishes 1997

Owner Current Owner: Barbara Ellen White (nee Bakewell) & Brett Bakewell-White

'Owners Address' 15 Beach Haven Road, Beach Haven, Auckland

'Date of Construction' 1967

'Photo Image'



ARC Plans and drawings



Photographs 2006

'GPS Easting'	2660385
'GPS Northing'	6488648
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent no'	9118 expired 1998

'Site Type'	BOATSHED - c1981
Name	Original Owner: Richard Winston Cunningham since 1973
Location	132 Aeroview Drive Esplanade, Beach Haven
'Local Authority'	North Shore City
Description	<p>Historical</p> <p>In 1973 Richard Winston Cunningham purchased the property at 132 Aeroview Drive, Beachhaven, Auckland, it appears he built an illegal boatshed on the foreshore of Beachhaven at some stage prior to 1981. On the 10 August 1981 the illegal boatshed was brought to the attention of the Registered Secretary for Transport; the Cunningham's boatshed was not licensed. Cunningham was informed that he must obtain permission from the ARC and the Marine Department before any structure could be built on the foreshore. The shed was not up to standard and required considerable maintenance before being eligible for a permit. The maintenance was completed by the 4 October 1982, fulfilling the requirements of the ARC. In 1983 Cunningham applied to the Marine Department for a foreshore licence in Beachhaven, Auckland. The plans were approved by the Marine Department, M.D. (N) 907, on the 19 January 1983. To the left and right of Cunningham's boatshed is Malcolm's and Young's (M14657) and all are located not far from the Beachhaven Wharf. On the M.D. Plan there are two other boatsheds which are highlighted and are located between Young's and the Wharf. Previous owners of the property but not the boatshed are, Technicrete Limited, Otahuhu and Margaret Sibun.</p> <p>Architectural</p> <p>Cunningham's boatshed, ramp and deck covered an area of 93m². The boatshed measured 6.1m in length and 4.9m in width = 32.3m², the ramp measured 7.4m in length and 2.4m wide = 17.8m², of which the ramp and the deck covered an area of 42.9m². At the rear of the boatshed there was a small room, which measured 2.4m in length x 2.3m in width = 5.5m². The boatshed is of batten and board construction with a pitched corrugated iron roof with the floor built in tongue and groove. The shed is not symmetrical, on the left hand side is an addition that has a lean-to appearance. There are two sets of doors; the sliding landing door (batten and board and 2.4m wide) and a side door that can be accessed from the deck; there are no windows. The landing ramp and deck were built in timber in a vertical arrangement with the deck having a series of wooden poles for tying boats to the deck. A wooden stairway leads from the property to the deck and boatshed. The shed was built in tanalised timber with the wooden piles set in concrete up to the floor boards. Other materials used in the construction of the shed were; steel rafters, building paper, tanalised joists and posts and sislation used for under the floor boards. In 1996 Cunningham added a deck to the side of his boatshed, and this was noticed during a routine inspection by DOC. Cunningham had to apply to have the extensions approved because the structure could interfere with public access. After consultation over this issue Cunningham was granted approval and consent was granted with a new licence issued for 35 years on the 19 November 1996.</p> <p>On the 27 April 2004 during ARC routine Compliance Monitoring it was noted that the boatshed had planks missing and the handrail for the steps was rotting.</p>
Notes	<p>Auckland Regional Council File: H11320 (H9611320/54287)</p> <p>Consent No: 11347 (1983), 15515 (1996)</p> <p>Marine Department Plan No MD(N) 907</p> <p>Licence No 2864</p> <p>File References: 54/2/857 & 54/3/21</p> <p>Map References: R11 604884 & NZMS260 R11 604884</p> <p>River No: 077480</p> <p>Coastal Consent: App 12743, Property 15136, Coastal Site 1647</p> <p>Legal Description: Lot 16 DP56178 Pt Allot 143 Takapuna Parish</p> <p>Last Inspected 1 December 2004.</p> <p>MHWS at the cliff.</p>
Keywords	BOAT SHED BOATSHED
'Biblio References'	Auckland Regional Council No: H11320
'Date Recorded'	July 2006
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166

'Last Visited' Symonds St
Auckland
2004
Owner Richard Winston Cunningham
'Owners Address' 132 Aeroview Drive, Beach Haven, Auckland
'Date of Construction' C1981
'Photo Image'



'GPS Easting' 2660403
'GPS Northing' 6488405
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

<p>'Site Type' Name Location 'Local Authority' Description</p>	<p>BOATSHED Jetty, Pontoon and Small Boating Storage Shed 1964 Boatshed 1969 Original Owner: Lloyd Albert Mumme 82 Paragon Place, Beach Haven, Hellyers Creek North Shore City</p> <p>Historical In 1964 Lloyd Albert Mumme wrote to the Birkenhead Borough Council for permission to construct a jetty, pontoon and a small boating storage shed on the Waitemata Harbour foreshore. The Auckland Harbour Board raised no objections as long as the jetty did not extend any further than the 70ft from the Medium High Water Mark. The Commissioner of Works (MOW) Wellington stated that he had no objections as long as the jetty did not infer with navigation in Hellyers Creek. On the 19 July 1965 the plans were approved by the Marine Department (MD 12062), and the foreshore licence deed was signed on the 21 July 1965. The jetty, boatshed and pontoon were located on the south east side of Hellyers Creek. The jetty was built sloping away from the cliff face towards the channel. There is a one chain reserve between the property boundary and the cliff face; there are several other structures located in the same vicinity. In 1980 during an annual inspection it was noted that the jetty was in a sound condition. In 1987 the licence fee increased from \$102 per annum to \$430pa; Mumme refused to pay due to the unreasonable increase. In 1997 Mumme was granted a continuation of his licence for a further term of 35 years, which would expire in 2032.</p>
<p>Notes</p>	<div data-bbox="555 748 1276 967" data-label="Image"> </div> <p>Architectural The jetty, pontoon and a small boating structure were constructed in 1964; the other boatshed was built in 1969. The structures occupy a total area of 63.5m². The jetty was erected in; treated pine and galvanised nails and bolts, a wooden handrail that runs the length of the jetty with wooden piles set in concrete and measures 70ft in length (21.3 m) x 4ft in width (1.2m) in width (26m²). The pontoon was constructed in Ferro Concrete and measures 18ft in length (5.5m) x 8ft in width (2.5m) occupying a total area of 13.75m². The two boatsheds were constructed by Mumme in corrugated iron with a mono pitch and a pitched corrugated iron roofs. The sheds measure 2.5m in length x 2.5m in width (6.25m²) and 7m in length and 2.5m in width (17.5m²) respectively. The boatsheds are suspended on outriggers, which are attached to the pontoon. During a routine inspection in 1997 it was noted that some of the rear corrugated iron panels replacing, in the near future.</p> <p>Auckland Regional Council File NO BIRHEL002 & H11562 Consent 9133 expired 1993, 15844 expired 1997 Marine Department Plan MD 12062 Coastal Permit 9711562 Permit no 358 File Ref 13/1/1 File Ref 12/11/1 File Ref 54/2/231 Plans Folio 1 File 54/3/74 Legal Description Pt Bed Waitemata Harbour PT CT 12 A/786 (T90) NZMS 260 R11 614894 MHWS at the foot of the jetty Maximum water depth at high tide three metres and at low water 0.4m Permanent water available to the pontoon Sand/mud on papa rock</p>

Keywords BOAT SHED
BOATSHED
BOATSHED STORAGE
JETTY

'Biblio References' Auckland Regional Council Files
Marine Department Files
New Zealand Herald, 21 August 2002

'Date Recorded' July 2006

'Reported By' Antony Matthews

'Recorders Address' Mathews and Matthews Architects
PO Box 108166
Symonds St
Auckland

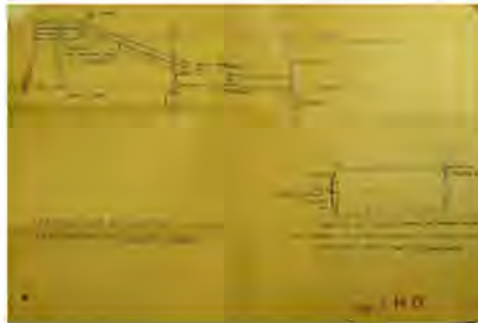
'Last Visited' 2005

Owner Lloyd Albert Mumme

'Owners Address' 82 Paragon Avenue, Birkdale, Auckland (formerly Karoka Road, Birkdale)

'Date of Construction' Jetty 1964

'Photo Image'



'GPS Easting' 2661255



'GPS Northing' 6489399

'ArcView Category' Historic Structure

'Grid Refs Source' GPS

'ArcView Category' Historic Structure

'Grid Refs Source' GPS

<p>'Site Type'</p> <p>Name</p> <p>Location</p> <p>'Local Authority'</p> <p>Description</p>	<p>BOATSHED Boatshed, Slipway and Small Jetty 1970 and Jetty extension 1976</p> <p>Original Owner: Ian and Hazel Malcolm (painter)</p> <p>128 Aeroview Drive, Hellyers Creek</p> <p>North Shore City</p> <p>Historical</p> <p>In 1970 Ian and Hazel Malcolm applied to the Birkenhead Borough Council to construct a boatshed on their property at 128 Aeroview Drive, Birkenhead; the BBC granted permission and a permit was issued (No. BO13332) on the 2 April 1970. In 1976 the BBC wrote to the Marine Department objecting to the Malcolm's proposal to build an extension to their existing jetty.</p> <p>In 1976 the Malcolm's submitted their plans to the Ministry of Transport Marine Department for approval. The Regional Secretary of the M.O.T. wrote to the Malcolm's stating that their plans were not adequately detailed, and for them to obtain permission they were required to furnish the M.O.T. with another set. During this period a Waitemata Heritage Study was in progress and they thought the application was unjustified. However, approval was granted by the MD because the jetty posed no navigational problems or would it restrict public access to the beach in 1977.</p> <p>There was considerable controversy over the location of the boatshed. The boatshed (1970) was built on Malcolm's land (three feet from the beachfront) not on the foreshore; this required a council building permit rather than permission from the Marine Department. However, over time erosion has left the shed on part of the foreshore. The matter was clarified when the Malcolm's furnished the Marine Department with a BBC building permit.</p> <p>In 1984 Malcolm sold the property and boatshed to G. Cunningham. He refused to fill out the foreshore licence transfer forms as the boatshed was located on his land. In 1988 because the licence had not been transferred the authorities were going to remove the shed. Cunningham explained to the appropriate authorities that his land extended beyond the MHWM. In 1991 a foreshore licence was granted and the rental was set at \$70.31c per annum. The licence expired in 2003.</p> <p>Architectural</p> <p>In 1970 a boatshed and a small jetty & slipway were constructed; the jetty was extended in 1976. The boatshed measures 20ft in length x 10ft in width and occupies an area of 24.4m². The jetty extension measured 6.4 metres (21ft) in length x 1metre (3.5ft) in width. The landing ramp & jetty occupy a total area of 34m². The shed was sheathed in galvanised iron, which had the appearance of weatherboards, with a mono pitched corrugated iron roof. The shed sits on a concrete base raised on wooden piles. A side door gives access to the shed, which has a set of steps that lead down to the foreshore. The tongue and groove landing doors were constructed in heart rimu. The landing, decks and jetty are all constructed in horizontal weatherboards.</p> <div style="display: flex; justify-content: space-around; margin-top: 20px;">   </div>
<p>Notes</p>	<p>Auckland Regional Council File BIRBCH010</p> <p>Consent No 9126 expired 2003</p> <p>Marine Department Plan No MD(N)301</p> <p>File No 54/2/731</p> <p>Legal Description (Block No 813 Sec 18) Lot 8 DP56178 CT80/273</p> <p>Licence No 2765</p> <p>MOT No 5903</p>

Map Reference R11 2660400 6488400
 Map NZMS 260 R11604884
 MHWM at ordinary Spring – well under the boatshed (water depth at high tide 0.8 metres)
 Papa rock foreshore

Keywords
 BOAT SHED
 BOATSHED

'Biblio References'
 Auckland Regional Council Files
 Marine Department Consent File

'Date Recorded'
 July 2006

'Reported By'
 Antony Matthews

'Recorders Address'
 Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited'
 1989

Owner
 Current owner: G. Cunningham

'Owners Address'
 128 Aeroview Drive, Beach Haven, Auckland

'Date of Construction'
 1970 & 1976

'Photo Image'



'GPS Easting'
 2660415

'GPS Northing'
 6488360

'ArcView Category'
 Historic Structure

'Grid Refs Source'
 GPS

'Site Type' Name Location 'Local Authority' Description	<p>BOATSHED Original Owners: C.H. Wild - Wild & Salthouse – Salthouse Brothers Limited 84 Rame Rd, Lucas Creek North Shore City</p> <p>Historical On the 21 August 1951 C.H. Wild of 84 Rame Road, Greenhithe applied to the Marine Department to construct a boatshed and slipway on the foreshore of the Waitemata Harbour. The Marine Department approved the plans (M.D. 9253) and Wild was granted a licence for a term of 14 years on the 24 October 1951. A new licence was issued to R.E. Wild and John Frederick Salthouse (Salthouse and Wild) to construct a boatbuilding shed on additional foreshore land the partners had acquired to extend their boat building business, in September 1957. In December 1957 the M.D. transferred the licence from Wild and Salthouse to John Frederick Salthouse. On the 30 January 1967 Ferro-Cement Limited were granted a foreshore licence to construct a boatbuilding shed and slipway as shown on the M.D. plan 11894. Salthouse applied to the Waitemata County Council to reclaim part of the foreshore, which was eventually approved after a lengthy consultative period. Messer's Salthouse Brothers Limited & Ferro-Cement Limited proposed to build a new boat building workshop in 1972, the Waitemata County Council approved the plans and issued the firm with a building permit (no 16899).</p>
	
	<p>Architecture In 1951 C.H. Wild was granted approval from the Marine Department to build a boatshed at Lucas Creek, Greenhithe. It is unclear what materials were used in the construction of the building. In 1957 the partnership of Wild and Salthouse constructed a boat building shed on their Greenhithe property. Salthouse applied to the Marine Department for permission to construct a wharf and pontoon in 1969. In 1970 the buildings that were owned by Salthouse Brothers Limited included; two boatsheds, a boat hauling out area, a slipway, a wharf, and a pontoon/jetty. In 1972 Salthouse Brothers Limited applied to the WCC to build another boatbuilding shed, however, they planned to construct this on reclaimed land. Because the WCC had not given approval to the reclamation it was not disposed to issuing a permit; Salthouse had to apply to the WCC for a special permit and after some deliberation the WCC gave the boat building firm permission, and a permit was issued for the construction of a new building. However, this did not include the re-erection of an existing building. This had to be either removed or a separate permit would have to be issued for its re-erection. The 1972 boat building workshop and slipway were constructed in Ferro concrete, costing \$32,000. The building was completed by the 31 March 1972. In 1973 Salthouse Brothers Limited were issued with another permit from the WCC to alter one of the workshops by adding an amenities area with toilets on the ground floor, a lunchroom and a first aid area and changing rooms on the first floor. The extension was built on the eastern corner of the northern wall with the floor area 20m x 20m. At this time a further extension of 20m was erected out from the southern end of one of the boatbuilding sheds.</p>
Notes	<p>Auckland Regional Council File No UPWGRE/001 – 9389 Consent No's 9389, 20594, 14954 Consent No 20594 (security gate and utility services) Marine Department Plan No MD 9257, 14706 & MD 10266 Building Permit 16899 3 March 1972 Legal Description Land adjacent Lot 9 DP9092 (Boat building workshop & offices) File Ref 13/2/80 MD File 54/2/80 M File Ref 54/9/59</p>

Keywords Waitemata Harbour NSCC
BOAT SHED
BOATSHED

'Biblio References' Auckland Regional Council File
Marine Department Consent Records
Marine Department Plans – Micro Fiche
Extract NZ Gazette 17 December 1970 No82, p.2492.

'Date Recorded' July 2006

'Reported By' Antony Matthews

'Recorders Address' Mathews and Mathews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited' Licence Expiry date 2017

Owner Salthouse Builders

'Owners Address' 84 Rame Road, Greenhithe, Auckland

'Date of Construction' 1951 & 1957, 1972

'Photo Image'



'GPS Easting' 0

'GPS Northing' 0

'ArcView Category' Historic Structure

'Grid Refs Source' GPS

UN-CONSENTED NORTH SHORE STRUCTURES

'Site Type' BOATSHED
Name
Location Stanley Point east side
'Local Authority' North Shore City Council
Description Boatshed platform and piles. Shed has been removed. Ramp/slipway still insitu
Notes
Keywords BOAT SHED
 BOATSHED CMA
'Biblio References' ARC FILE NO:
'Date Recorded' May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
 May 2006
'Last Visited'
Owner
'Owners Address'
'Date of Construction'
'Photo Image'



'GPS Easting' 2668651
'GPS Northing' 6484567
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No


'Site Type'	BOATSHED
Name	
Location	Stanley Point east side
'Local Authority'	North Shore City Council
Description	Two level boatshed on timber retaining wall. Boatshed above MHW. Shed timber framed clad with plywood. Corrugated iron roof. Steel handrails
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2668778
'GPS Northing'	6484763
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type' BOATSHED
Name
Location Bayswater - may be 3a Beresford Street
'Local Authority' North Shore City Council
Description Single level boatshed. Plywood lined walls with corrugated iron roof. Gabled form
No research or file

Notes
Keywords BOAT SHED
BOATSHED CMA
'Biblio References' ARC FILE NO:
'Date Recorded' May 2006
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland
'Last Visited' May 2006
Owner GT & PF Fyfe?
'Owners Address' 3 A Beresford Street?
'Date of Construction'



'Photo Image'
'GPS Easting' 2668143
'GPS Northing' 6485831
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No 21946?


'Site Type'	BOATSHED
Name	
Location	Bayswater -
'Local Authority'	North Shore City Council
Description	Single level boatshed built on retaining wall. Board and batten walls and corrugated steel roof. Timber framed sash window. Bluestone base walls Structure above MHWL. No research or file found
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO;
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland May 2006
'Last Visited'	
Owner	
'Owners Address'	
'Date of Construction'	
	
'Photo Image'	
'GPS Easting'	2668284
'GPS Northing'	6485985
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Bayswater
'Local Authority'	North Shore City Council
Description	Single level boatshed. Plywood lined walls with corrugated iron roof. Gabled form
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2668241
'GPS Northing'	6485924
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

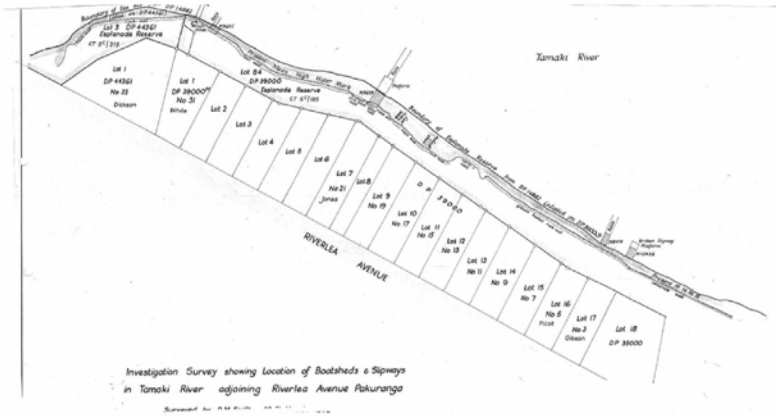
'Site Type'	BOATSHED
Name	
Location	Bayswater
'Local Authority'	North Shore City Council
Description	Three storied boatshed. Weatherboard lined walls with corrugated iron roof. Gabled form Old sea wall/swimming pool formed within the bay. No research or file found
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2668128
'GPS Northing'	6485471
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Northcote Point - adjacent to Queens St
'Local Authority'	North Shore City Council
Description	Single level gabled boatshed with adjacent deck. Weatherboard lined walls. Timber framed windows and doors. Bay window on north wall Not being used for boat storage. Timber piles on concrete footing. Partly in CMA area.
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO:
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2666079
'GPS Northing'	6485465
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Between Beach Haven Rd and Aeroview Drive
'Local Authority'	North Shore City Council
Description	Single level gabled shed with adjacent deck. Corrugated steel lined walls and sliding shed doors Timber framed windows and doors. Lean-to on south side. Timber framed slipways with steel tracks Has sign with "ARC 348" on front wall
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO;
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland May 2006
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2660539
'GPS Northing'	6488583
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Between Beach Haven Rd and Aeroview Drive adjacent to boatshed at no 15 Beach Haven Rd
'Local Authority'	North Shore City Council
Description	Single level dingy locker located on timber framed landing with slipway/ramp. Walls and roof corrugated steel
	Has sign with 'ARC 348" on front wall
Notes	
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	ARC FILE NO;
'Date Recorded'	May 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland May 2006
'Last Visited'	May 2006
Owner	
'Owners Address'	
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2660428
'GPS Northing'	6488647
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

TAMAKI ESTUARY



'Site Type' BOATSHED
Name
Location 24 A Riverview Road
 Panmure
'Local Authority' Auckland City Council
Description Boatshed, jetty and grid structure
Notes Additional notes by Susan Yoffe June 2006:
 1967 Dinghy shed Consent to Mr Douglas William Guthrie, 27 Omana Ave, Auckland 3
 1970 Application for jetty to Mt Wellington Borough Council
Keywords BOAT SHED
 BOATSHED CMA
'Biblio References' SE File 11800 TamPan 004
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner K.M. Guthrie
'Owners Address' 24A Riverview Road, Panmure or 559 Riddell Road Glenelvie
'Date of Construction' 1967, 1970
'Photo Image' File name
'GPS Easting' 2676306
'GPS Northing' 6475754
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
 S

'Site Type'	BOATSHED
Name	
Location	49 Millen Ave Pakuranga Tamaki Estuary
'Local Authority'	Manukau City
Description	Jetty, Dinghy Locker, Ramp and Pontoon
Notes	Additional notes by Susan Yoffe, June 2006: TamPak 005 1978 September Application for jetty by George McGoram Upon inspection it was found that the structure was existing and had been in service for some time and repairs were needed. November Manukau City refused permission for jetty and dinghy lockers but would consent to a boat ramp. Ordered to remove the jetty and lockers. 1979 Submission to the Council. Plans were approved for jetty and dinghy locker and licence issued. 1984 October Letter from Manukau City insisting on the removal of the structure by January 1985 as it is on or near a public reserve 1986 Property sold to R J Boyd who took the matter with two other owners to the Council 1988 Licence issued 1989 Fibrolite & corrugated iron locker and concrete ramp 1996 Mark Clavert and Richard McAdams 1997 J.F. Howard 2001 Trevor Colin Rooderkerk File 16861 2002 Anthony & Ingrid Fawkner
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	SE TamPak 005 File 16861
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Anthony & Ingrid Fawkner
'Owners Address'	49 Millen Ave Pakuranga
'Date of Construction'	Pre 1978

'Photo Image'



Photo from ARC file

'GPS Easting'	2675992
'GPS Northing'	6474649
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type' BOATSHED
Name
Location 33 Riverlea Ave adjacent to
 Pakuranga Tamaki Estuary
'Local Authority' Manukau City
Description Boatshed 38.5m x 24.5m x 2.1m
Notes Additional notes by Susan Yoffe June 2006:
 1954 Application for boatshed Mr G. W. Cole
 1966 Trans to P B White
 1998 Licence to P B White renewed
 1999 Trans. to Robert & Jennifer Brown, 9 Riverlea Ave, Pakuranga.
Keywords BOAT SHED
 BOATSHED CMA
'Biblio References' SE H9711599 TamPak 012
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner Brown Family Trust (R & J Brown & D White)
'Owners Address' 33 Riverlea Ave Pakuranga
'Date of Construction' 1954

'Photo Image'



Photo from ARC file

'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
S

2676855
6475454
Historic Structure
GPS

'Site Type'	BOATSHED
Name	
Location	125 Waipuna Road off Finn Place Tamaki Estuary Mt Wellington
'Local Authority'	Auckland City
Description	Boatshed 1.83x 1.83m = 3.35m ² Jetty 12.8 x 1.23m = 15.61m ² Landing (including shed) 2.7m x 4.0m = 10.70m ² 2 mooring piles one birth
Notes	Additional notes by Susan Yoffe June 2006: 1977 Application for extension from Merv Young 1990 Transfer to Dick Norton Taylor 1994 Transfer to Keith Rupert Calvert
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	SE Tampan 25 no prior file
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Keith R. Calvert
'Owners Address'	63 Wallace Road, Papatoetoe
'Date of Construction'	unknown
'Photo Image'	File name
'GPS Easting'	2675816
'GPS Northing'	6475218
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type' BOATSHED

Name

Location 21 Riverlea Ave
Pakuranga Tamaki River
Manukau City

'Local Authority'

Description Boatshed – timber frame, weatherboard and iron roof on timber piles
Landing & slipway of timber plank and steel rails on hardwood piles and sleepers

Notes Additional notes by Susan Yoffe, June 2006:
1953 Application for boatshed and slipway Mr R H Steadman
his address c/o Shipbuilders Ltd. Porre St City
1964 Transfer to Mr K E Jones
1979 Transfer to Mrs I L Whitehead
1987 Transfer to M A and L R Pearce and then to
Mr J MacArthur Jaymac Builders Manukau City
1988 Transfer to Mr & Mrs R E Larcombe
1991 Transfer to Mr & Mrs Wu
1996 Transfer to Wai Hung Chan
1998 Transfer to Angel Sutton and Ross Hill

Keywords BOAT SHED
BOATSHED CMA

'Biblio References' SE File 12394

'Date Recorded'

'Reported By' Anthony Mathews

'Recorders Address' Mathews and Mathews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited'

Owner Angela Sutton & Ross Hill

'Owners Address' 21 Riverlea Ave, Pakuranga

'Date of Construction' 1953

'Photo Image'





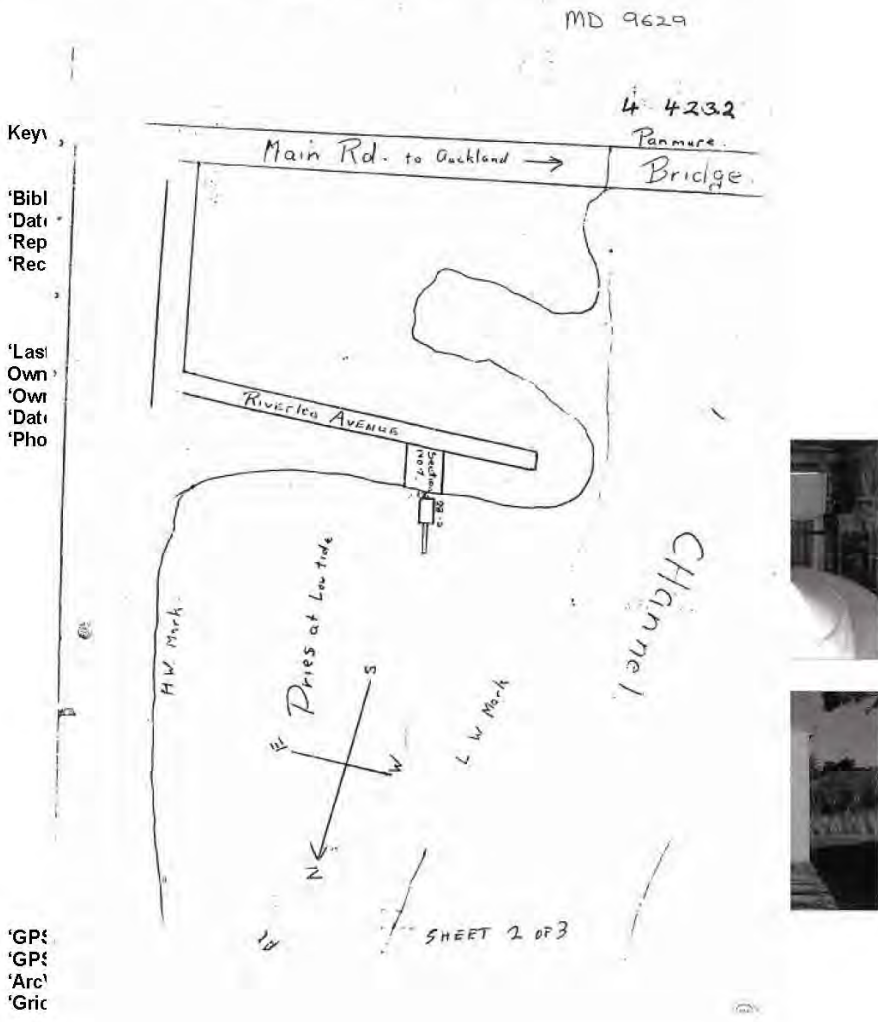
Photos from ARC file




Drawing from ARC file

'GPS Easting'	2676713
'GPS Northing'	6475504
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type' BOATSHED
Name
Location 5 Riverlea Ave, Pakuranga Tamaki Estuary
'Local Authority' Manukau City
Description Fibrolite & timber shed on concrete piles, timber ramp shed 3.47x 6.59.
Notes Additional notes by NAME (date): Susan Yoffe June 2006-06-29
 Tom Dalrymple



Key
 'Bibl'
 'Dat'
 'Rep'
 'Rec'
 'Las'
 'Own'
 'Ow'
 'Dat'
 'Pho'
 'GP'
 'GP'
 'Arc'
 'Gric'

'Site Type'	BOATSHED
Name	Leader Boatbuilders Ltd.
Location	5 Pakuranga Road or 33 Old Bridge St, Panmure
'Local Authority'	Manukau City
Description	Building, 3 slipways, duckwalk, jetties and pontoon jetty.
Notes	Additional notes by NAME (date): Susan Yoffe June 2006 1953 Application for erection of boatshed and slipway for boatbuilding by Owen G Woolley 1954 Licence approved in February 1954 before the present Pakuranga bridge was built and before Pakuranga was zoned for residential development. 1955 Application for small jetty for fuelling. 1956 application to extend the building as business was going very well 1965 More expansion for another 975 sq. ft. on the already 2,850 sq.ft. 1974 O.G. Wolley & Ventura Yachts Ltd. 1976 Building leased to Kerry Lilley 1992 Transfer to David Patterson & Justine Patterson
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE TamPak16, vol. 1&2 File 8090
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	David & Justine Patterson
'Owners Address'	P O Box 14318 Pakuranga
'Date of Construction'	1953
'Photo Image'	
'GPS Easting'	Plan ARC File 2675993
'GPS Northing'	6475196
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS Photos ARC file

'Site Type'	BOATSHED
Name	
Location	8 Riverview Road, Panmure Tamaki Estuary
'Local Authority'	Auckland City
Description	Boatshed, jetty, slipway
Notes	Additional notes by NAME (date): Susan Yoffe June 2006 TamPan 009 1962 Application for jetty and slipway by F. Brake and R L McGaffin 1968 Transfer to F Brake and W C Ridley 1982 Transfer to Keith Kay 1984 Mr & Mrs Hill of 6 Riverview Rd. added to licence 1989 Application to extend 1990 Transfer to Mr Kay and G.E. Pairy File 8509 1992 Transfer to Haran Holdings 2006 Transfer to Refinances Services Ltd. & G M Fitzgerald Investments Ltd.
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE TamPan 009 File 8509
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	G M Fitzgerald Investments Ltd Refinance Services
'Owners Address'	P O Box 14158, Panmure
'Date of Construction'	
'Photo Image'	
'GPS Easting'	2676107
'GPS Northing'	6475666.5
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

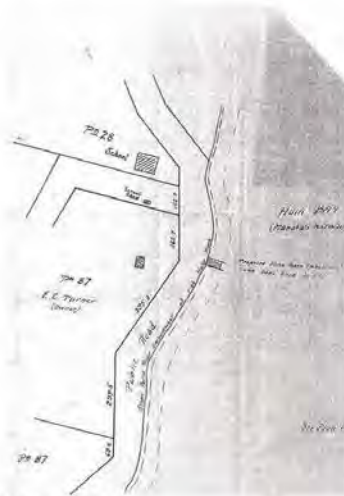
HUIA – MANUKAU HARBOUR



Auckland Public Libraries Map 851

'Site Type' BOATSHED
Name
Location Opposite 1336 Huia Road, Huia
'Local Authority' Waitakere City
Description Boatshed fibrolite, concrete ramp 4.5m
Notes Additional notes by NAME (date): Susan Yoffe June 2006
1934 Application E C Turner
1988 Licence renewed by Stanley Eric Turner
1998 Addition of daughter Elizabeth M Logan and her husband Howard A Logan to the licence
2003 Licence renewed
Keywords BOAT SHED
BOATSHED
'Biblio References' SE File 13046, ManHui 014
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland
'Last Visited'
Owner Mr & Mrs H A Logan
'Owners Address' 2142 Awhitu Road, Pollok RD 4, Waiuku
'Date of Construction'
'Photo Image'

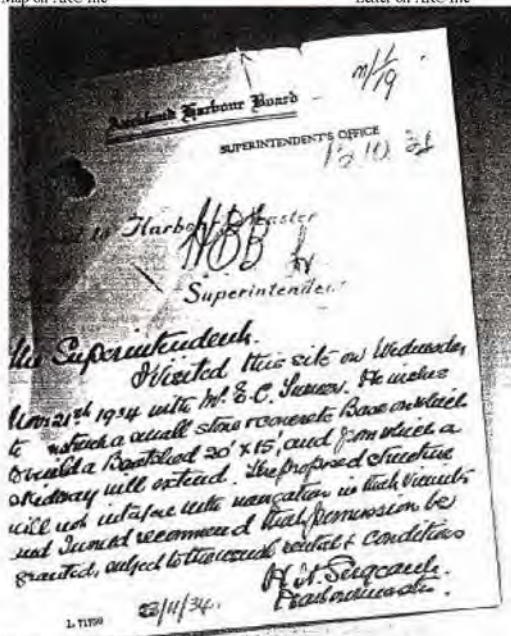




Map on ARC file



Letter on ARC file



Memo on ARC file
Not given

'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'

Historic Structure
GPS



Little Huia approximately 1890, Turners house on left and Barr farmhouse on right. The Boatshed is located along the coast to the right of the Barr cottage. Huia Museum collection.

'Site Type' BOATSHED
Name
Location 17 Dorothy Road, Laingholm Manukau Harbour
'Local Authority' Waitakere City
Description
Notes Additional notes by NAME (date): Susan Yoffe
 1951 Application by D A Taylor
 1980 Transfer to Peter Bruce Jacobson
 1986 Transfer to Mr & Mrs Wansbrough
 1997 Ramp washed away
 2000 Licence renewed
Keywords BOAT SHED
 BOATSHED
'Biblio References' SE ManLai 007
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner Mr & Mrs Wansbrough
'Owners Address' 17 Dorothy Road, Laingholm
'Date of Construction' 1951
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type'	BOATSHED
Name	
Location	End of Huia Road, Huia Bay Manukau Harbour
'Local Authority'	Waitakere City
Description	Boatshed in 4 sections. Concrete block building with concrete tiled gabled roof. End walls to gable timber framed and clad with weatherboards. Building sits on a concrete slab base and retaining wall to seaward side. Timber framed windows to seaward face. Window and door opening to east face have been roughly infilled with block work
Notes	Additional notes by NAME (date): Susan Yoffe 2006 1960 Application for licenses for G Selwyn, Philip and Arundel 1970 File commences with Mr G S Turner assigning the licence to A R Turner 1997 Site visit – 4 boatsheds within one building
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE ManHui 010.011,009
'Date Recorded'	June 2006
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	June 2006
Owner	Mr J P Turner, 26 Summit Drive, Mt Albert. Mr A R Turner, 20 Alexis Ave Mt Albert Mr F K L Harding, 41 Allendale Road, Mt Albert Sir Harvey Turner CBE, 40 Summit Drive, Mt Albert
'Owners Address'	As above
'Date of Construction'	1960?
'Photo Image'	
'GPS Easting'	2649054
'GPS Northing'	6463802
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
'Consent No'	9206, 9207, 9208- all expired 1999

'Site Type'	BOATSHED
Name	
Location	3 Pohutakawa Road, Beachlands
'Local Authority'	Manukau City
Description	Boatshed and ramp
Notes	Additional notes by NAME (date): Susan Yoffe June 2006 File 11139 1984 Letter from David Melvor States that it was built by Mr Porter in 1938 Mr Mr Melvor had bought the property in 1968 he wants to refurbish the boatshed and ramp 1995 Barry Keon bought the property and begins refurbishments. 1996 Locals complain that Barry Keon is cutting steps into the cliff to access the boatshed. His right of ownership is questioned (Manukau Courier 13.8.96) 1997 Licence granted to Barry Keon and occupation rights. 2005 Property sold to Charles and Sandra Cooper
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File 11139
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Charles and Sandra Cooper
'Owners Address'	3 Pohutakawa Road, Beachlands
'Date of Construction'	1938 and reconstructed 1996
'Photo Image'	File name
'GPS Easting'	2689300
'GPS Northing'	6478400
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

WAIHEKE ISLAND



Auckland Public Libraries Map 851

ROCKY BAY – WAIHEKE ISLAND



'Site Type'	BOATSHED
Name	Harrison Boatshed
Location	14 Pohutakawa Drive, Rocky Bay, Waiheke Island
'Local Authority'	Auckland City Council
Description	Gabled single level timber framed boatshed on timber piles. Corrugated steel roofing. Rusticated timber weatherboard cladding. Pair TGV doors to east. Timber framed single light window to north wall. 1 TGV door to north wall. Timber framed ramp.
Notes	<p>Historic significance: 70 years old, in a group of old boatsheds</p> <p>Additional notes by Susan Yoffe June 2006: Built by H.M. Burgess 1937 Prior File 54/9/8 1937 11 March request for site and permit to erect private boatshed on beach at Omiha Bay from Harry Noel Burgess. Northerly wall will be approximately 10ft south of Miss Buddle's shed and seaward end approximately the same line as Miss Henderson's shed. Back of shed would be approx. 3 to 6 ft from bank below road, Pohutakawa Ave. Roof of shed would be considerably below the level of the road approximately half way down the cliff. Permit granted with the following specifications. Weatherboards of pinus radiata, roof galvanised iron, flooring pinus or matai. Painted white or light colour of green stain, the roof red. Land below low water mark 1959 30 June Transfer to G.I. Brown 2004 October Consent granted for repairs. Extend the boat ramp and add a water tank</p> <p>BOAT SHED BOATSHED CMA</p>
'Biblio References'	SE File 17555, Prior Files 54/9/8, 54/2/36 MD 7568 4/3030.
'Date Recorded'	3 April 2007
'Reported By'	Antony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	3 April 2007
Owner	Natalie Berle & Lara Harrison
'Owners Address'	6B/18 Federal Street Auckland
'Date of Construction'	1937
'Photo Image'	
'GPS Easting'	2693986
'GPS Northing'	6484710
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	29785 29376
§	
'Site Type'	BOATSHED
Name	Kowhai Trust Boatshed

Location	16 Pohutakawa Ave Omaha Bay Waiheke Island.
'Local Authority' Description	Auckland City Council Mono-pitched single level boatshed timber framed on timber piles. Trapezoidal profiled roofing. Vertical and horizontal corrugated steel wall cladding. Pair of TGV timber doors to east elevation. Timber steps at rear above MHW mark. Timber ramp Historic significance. 70 years old, built by a woman whose family had some social standing. Successive owners women Additional notes by Susan Yoffe, June 2006: File 54/2/35 1936 Application by Miss Gladys Buddle 26 Victoria Road Remuera. 1957 transfer to Miss A. Henderson 1959 transfer to Mary F.T. Lott File 12259 1984 Jacobus A Urselmann 2006 February Compliance monitoring. Noted that the corrugated iron added to side significantly alters the character of the shed.
Keywords	BOAT SHED BOATSHED CMA
'Biblio References'	SE File 12259. Prior ref. 54/9/7. 54/2/35 MD 7520
'Date Recorded'	3 April 2007
'Reported By'	Antony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	3 April 2007
Owner	Kowhai Trust Attn Mr Statton
'Owners Address'	P O Box 58353 Greenmount Auckland 1730
'Date of Construction'	1936
'Photo Image'	
'GPS Easting'	2693997
'GPS Northing'	6484722
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	20695

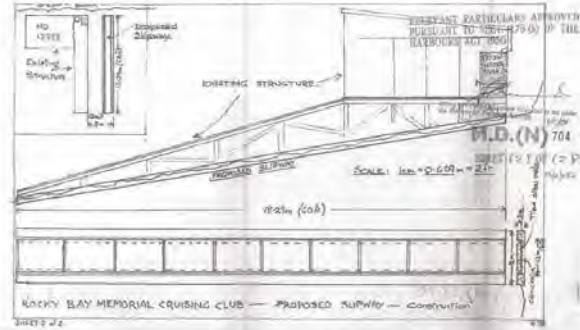
'Site Type' BOATSHED
Name Rocky Bay Memorial Cruising Club
Location Pohutakawa Ave
 Omiha
 Waiheke Island
'Local Authority' Auckland City Council
Description Mono-pitched timber framed storage shed. Corrugated steel roofing and vertical corrugated steel walls with corrugated acrylic to upper wall areas. Pait vertical slatted doors north elevation. Separate WC to north plywood lined. Concrete boat haul out ramp and timber framed jetty.

Notes Historic significance: The building itself dates only from 1970 but possibly there was another previous structure. The Cruising club has been active for 60 years and is a community group. Rocky Bay is an old settlement on Waiheke. The other community group is the Rocky Bay Welfare Society which looks after the hall.
 Additional notes by Susan Yoffe, June 2006:
 File 54/2/492
 1970 July Application by Rocky Bay Memorial Cruising Club to build a slipway and boatshed. December approved by Waiheke County Council. Plan MD 13988
 The Rocky Bay (Memorial) Cruising Club has been active for 60 years, teaching sailing, holding an annual regatta.
 1988 Plan of proposed slipway – attached

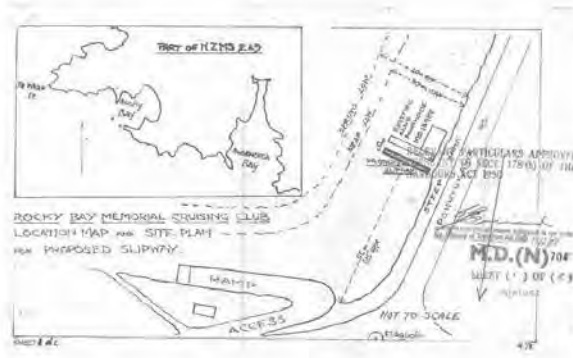
Keywords File 14919
 2004 October application with plans to modify the boatshed.
 Photos attached.

'Biblio References' BOAT SHED
'Date Recorded' BOATSHED ACZ
 SE File 14919, Prior file 54/2/492 Plan MC 13988
 3 April 2007
'Reported By' Antony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited' 3 April 2007
Owner Rocky Bay Memorial Cruising Club
'Owners Address' C/o Graeme D'Arcy
 P O Box 283
 Ostend
 Waiheke Island
'Date of Construction' 1970
'Photo Image'





Drawing from ARC file



Drawing from ARC file


<p>'GPS Easting'</p> <p>'GPS Northing'</p> <p>'ArcView Category'</p> <p>'Grid Refs Source'</p> <p>Consent No</p>	<p>From files 2693800 Recorded 3 April 2007 2694006</p> <p>From files 6484700 Recorded 3 April 2007 6484747</p> <p>Historic Structure</p> <p>GPS</p> <p>24090</p>
---	---

'Site Type' BOATSHED
Name
Location Rocky Bay, Waiheke Island
'Local Authority' Auckland City Council
Description Mono-pitched single level timber framed boatshed on timber piles. Corrugated aluminium roof and walls (vertical). TGV front door and rear door. Timber ramp


Notes Historic significance: part of a group of historic boatsheds. No files or further historic information available
 BOAT SHED
 BOATSHED CMA


'Biblio References'
'Date Recorded' 3 April 2007
'Reported By' Antony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
 3 April 2007

'Last Visited'
Owner
'Owners Address'
'Date of Construction' Not known
'Photo Image'




'GPS Easting' 2693993
'GPS Northing' 6484730
'ArcView Category'
'Grid Refs Source' GPS
Consent No

'Site Type'	BOATSHED
Name	
Location	Rocky Bay, Waiheke Island
'Local Authority'	Auckland City Council
Description	Gabled single level timber framed boatshed on timber piles. Corrugated steel roofing with exposed timber rafters. Vertical board and batten walls and pair of doors to east elevation. No ramp
Notes	Historic significance: part of a group of historic boatsheds. No files or further historic information available Record number on building M8483 BOAT SHED BOATSHED CMA
'Biblio References'	
'Date Recorded'	3 April 2007
'Reported By'	Antony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	3 April 2007
Owner	
'Owners Address'	
'Date of Construction'	Not known
'Photo Image'	
'GPS Easting'	2693984
'GPS Northing'	6484702
'ArcView Category'	
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Rocky Bay, Waiheke Island
'Local Authority'	Auckland City Council
Description	Mono-pitched single level timber framed boatshed on timber piles. Corrugated steel roofing. Vertical butt-boarded clad walls to east addition and board and batten to original western portion. Pair of butt-boarded doors to east elevation. 4 panelled door to west (rear) wall. Part timber ramp
Notes	Historic significance: part of a group of historic boatsheds. No files or further historic information available BOAT SHED BOATSHED CMA
'Biblio References'	
'Date Recorded'	3 April 2007
'Reported By'	Antony Matthews
'Recorders Address'	Mathews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	3 April 2007
Owner	
'Owners Address'	
'Date of Construction'	Not known
'Photo Image'	
'GPS Easting'	2693995
'GPS Northing'	6484696
'ArcView Category'	
'Grid Refs Source'	GPS
Consent No	

'Site Type' BOATSHED
Name
Location Rocky Bay, Waiheke Island
'Local Authority' Auckland City Council
Description Mono-pitched single level timber framed boatshed on timber piles. Corrugated steel roofing. Bevelled back weatherboards. Addition to front(east) portion. Pair of plywood lined doors to east elevation. VTGV door to rear(west) wall Timber ramp. Timber steps to rear.
Notes Historic significance: part of a group of historic boatsheds. No files or further historic information available
 Record number on building M7520
 BOAT SHED
 BOATSHED CMA
'Biblio References'
'Date Recorded' 3 April 2007
'Reported By' Antony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited' 3 April 2007
Owner
'Owners Address'
'Date of Construction' Not known
'Photo Image'


'GPS Easting' 2693998
'GPS Northing' 6484692
'ArcView Category'
'Grid Refs Source' GPS
Consent No

'Site Type'	BOATSHED
Name	
Location	Rocky Bay, Waiheke Island
'Local Authority'	Auckland City Council
Description	Gabled single level timber framed boatshed on timber piles. Mono-pitched addition to front. Corrugated steel roofing. Vertical corrugated cladding to original area. Board and batten to addition. Board and batten front doors. Timber ramp. Timber steps to rear.
Notes	Historic significance: part of a group of historic boatsheds. No files or further historic information available Record number on building M7568 BOAT SHED BOATSHED CMA
'Biblio References'	
'Date Recorded'	3 April 2007
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	3 April 2007
Owner	
'Owners Address'	
'Date of Construction'	Not known
'Photo Image'	
'GPS Easting'	2693989
'GPS Northing'	6484686
'ArcView Category'	
'Grid Refs Source'	GPS
Consent No	

Type' BOATSHED
Name
Location Omiha Bay Waiheke Island
'Local Authority' Auckland City Council
Description Based on drawing and site plan as part of research this shed is believed to be the shed as shown below.
 Single level with gabled roof paralleled to shore line with gabled roof over doors. Flag pole fitted to gable. Weatherboard timber cladding. Timber piles. Timber ramp Corrugated steel roof. Timber walkway and ramp.

Notes
 Historic significance: Oldest structure 1925 – I suspect that Charles Chambers would have a profile.
 Additional notes by Susan Yoffe, June 2006:
 File 54/2/3
 1924 December Application from Charles Chambers, Waldamere, Birkenhead to lease part of the foreshore, to build boatshed and slip, least for 14 years £2 per annum. Shown on plan 5877 attached.
 1947 October sold to William Marks
 1948 November to Mrs Olive Jean Inglis Newdick, Huia St. Devonport.
 1951 July Letter asking owners to make access from steep slope at rear easier for bathers – refused.
 1962 Transfer to Mrs Maud Bogue
 1963 to N and M J Carr-Smith
 1970 Repairs to roofing and spouting
 1995 To Patricia m Joe & Robin K. Mangos
 1998 to Vernon Neil Heydon
 2003 return to Trisha Joe consent No 10052

Keywords BOAT SHED
 BOATSHED

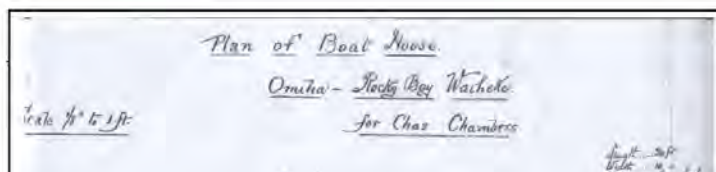
'Biblio References' SE File 54/2/3
 3 April 2007

'Date Recorded'

'Reported By' Antony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St, Auckland

'Last Visited' 3 April 2007
Owner Trisha Joe
'Owners Address' 18 Park Ave, Takapuna
'Date of Construction' 1925
'Photo Image'

'GPS Easting' 2694212
'GPS Northing' 6484786
'ArcView Category' Historic Structure
'Grid Refs Source' GPS
Consent No






EAST END OMIHA BAY

No records have been found for the following boat sheds



'Site Type'	BOATSHED
Name	
Location	Omiha Bay Waiheke Island
'Local Authority'	Auckland City Council
Description	Single level timber framed gabled boatshed, clad with timber bevelled back weatherboards. Approx 4.5x3.5m Exposed timber rafters to eaves and vertical tgv pair of front doors. Low concrete wall to base sits just above MHWM. Timber boat ramp to the front. Square concrete piles
Notes	No historical information found. The style of the building, with narrow weatherboards and exposed rafters would indicate the shed was constructed in the 1920s or 1930s.
Keywords	BOAT SHED BOATSHED
'Biblio References'	No References found
'Date Recorded'	
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	April 2007
Owner	unknown
'Owners Address'	unknown
'Date of Construction'	unknown
'Photo Image'	
'GPS Easting'	2694455
'GPS Northing'	6484648
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

'Site Type'	BOATSHED
Name	
Location	Omiha Bay Waiheke Island
'Local Authority'	Auckland City Council
Description	Single level timber framed gabled boatshed, clad with timber bevelled back cedar weatherboards. Exposed timber rafters to eaves and vertical tgy pair of front doors. Timber boat ramp to the front. Newer 150dia SED timber piles in concrete footing.
Notes	No historical information found. The style of the building, which is very similar to the previous shed to the west, with narrow weatherboards and exposed rafters would indicate the shed was constructed in the 1920s or 1930s.
Keywords	BOAT SHED BOATSHED
'Biblio References'	No References found
'Date Recorded'	
'Reported By'	AntonyMatthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	April 2007
Owner	unknown
'Owners Address'	unknown
'Date of Construction'	unknown
'Photo Image'	
'GPS Easting'	2694474
'GPS Northing'	6484753
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

ARRAN BAY – EASTERN WAIHEKE ISLAND

Sheds at Arran Bay were not visited as part of this study. Material and information gathered as part of the research phase have been included and a field sheet completed for one shed. This is attached. Photographs show that there are or have been other shed in this area. No consent records or files have been found.

'Site Type'	BOATSHED
Name	
Location	6 Arran Bay Waiheke Island
'Local Authority'	Auckland City Council
Description	Boatshed not found. No access off public road.
Notes	Additional notes by Susan Yoffe, June 2006: File 10371 1995 Application for ramp adjoining existing shed from Clifford Cook 2000 Transfer C. Cook to Faulkner Fishing Lodge 2003 Transfer from Faulkner Fishing Lodge to Clifford James Cook 2005 Note in the application for boatramp that the boatshed is located above MHW New owner Robert J. Gray
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File H10371 no prior
'Date Recorded'	
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Robert Gray
'Owners Address'	133 Pinnacle Hill Road Bombay Auckland 1850
'Date of Construction'	unknown

'Photo Image'



Looking N
Arram Quay
10371



Looking S
& Typical
Ramp



Rock
Close up



Photos from ARC files

'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
Consent No

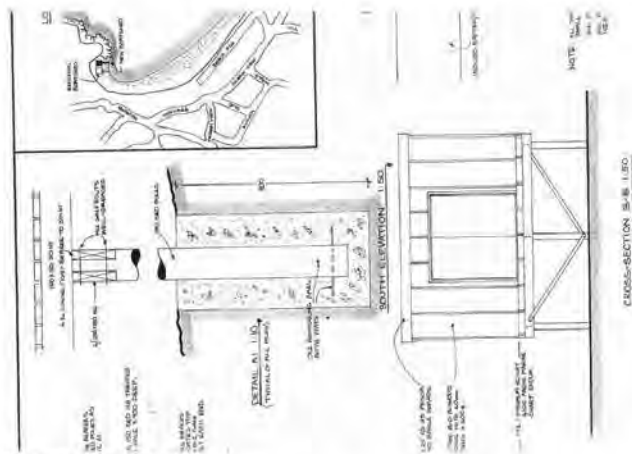
Historic Structure
GPS

ONEROA BEACH

'Site Type'	BOATSHED
Name	
Location	North end Oneroa Beach
'Local Authority'	Auckland City Council
Description	Size 4.8 x 5.8m. Single level lean-to boatshed. Vertical corrugated steel to roof and walls. Note that the corrugated steel has been fixed over the original vertical tgv cladding. Part of this is still exposed on the rear wall. Three pairs plywood doors on front elevation. Timber deck, steps and ramp. Floor 150x50 boards laid on 150x50 joists. Timber bearers on concrete piles.
Notes	Additional notes by Susan Yoffe June 2006: File 54/9/12 1945 MD Permit 8353 to G. Jackson 1958 Transfer to John Alexander McDonald 1986 Transfer from A Spitzbart to John Laughland 1999 June letter to John Laughland established that this was the original structure and that the adjacent boatshed owned by Mr Montgomery has been wrongly associated with his. 2000 Transfer from J & ME Laughland to Craig Greenwood, No. 2 Trust and the Travellers Trust
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File 12113 Prior Files 54/9/12,
'Date Recorded'	
'Reported By'	Antony Mathews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Craig Greenwood
'Owners Address'	No 2 Trust and the Travellers Trust C/o Duncan Cotterill P O Box 5326 Auckland
'Date of Construction'	1945 (?)
'Photo Image'	
'GPS Easting'	2693900 Recorded 3 April 2007 2689922
'GPS Northing'	6484700 Recorded 3 April 2007 6489634
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	24192

'Site Type' BOATSHED
Name
Location North end Oneroa Beach
 Waiheke Island
 Auckland City Council
'Local Authority'
Description Size 4.2 x 6.3m
 Single level lean-to boatshed. This is shed is new (c 2003) replacing an earlier structure. Walls are plywood and batten lined. Roof corrugated steel. Concrete piles and timber ramp
Notes Additional notes by Susan Yoffe June 2006
 File 54.9.13
 1970 owned by S Geard
 1998 Plans to rebuild shed after a storm
 1999 Owner James Montgomery
 2002 Transfer to Frank Colette
 2003 Permit to rebuild boatshed and ramp
Keywords BOAT SHED
 BOATSHED CMA
'Biblio References' SE
'Date Recorded'
'Reported By' Antony Mathews
'Recorders Address' Mathews and Mathews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner Frank Colette
'Owners Address' 1/5 Barrington Road
 Grey Lynn
 Auckland
'Date of Construction' unknown
'Photo Image'





'GPS Easting'
'GPS Northing'
'ArcView Category'
'Grid Refs Source'
Consent No

Drawing from ARC file
2689888 Recorded 3 April 2007 2689926
6489678 Recorded 3 April 2007 6489632
Historic Structure
GPS
20503

'Site Type'	BOATSHED
Name	
Location	Matiatia, Waiheke Island
'Local Authority'	Auckland City Council
Description	Single level timber framed gabled boatshed, clad with corrugated steel to walls and roof areas. Split level with workshop at rear. Shed sits just above MHWL.
Notes	No historical information found. The style of the building, with narrow weatherboards and exposed rafters would indicate the shed was constructed in the 1920s or 1930s.
Keywords	BOAT SHED BOATSHED
'Biblio References'	No References found
'Date Recorded'	
'Reported By'	Antony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	April 2007
Owner	unknown
'Owners Address'	unknown
'Date of Construction'	unknown
'Photo Image'	
'GPS Easting'	Current photos 2007 26988037
'GPS Northing'	6489625
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS
Consent No	

KAWAU ISLAND



Auckland Public Libraries Map 853

Sheds on Kawau Island were not visited as part of this study. Material and information gathered as part of the research phase have been included and a field sheet completed. These are attached.

'Site Type'	BOATSHED
Name	
Location	Swansea Bay, Bon Accord Harbour, Kawait
'Local Authority'	Rodney County Council
Description	
Notes	Additional notes by NAME (date): Susan Yoffe, July 2006 File 54/3/83 1965 with letter to Dorothy Lange, 'Out Moana', Urenui Taranaki stating that the structure is in bad repair and appears to be illegal 1966 Application to rebuild wharf, jetty and boatshed. Slipway to be removed Permit issued J Newsom and D & J Newton 1973 Jurisdiction passed to Rodney Council - file ends no further file ref.
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File 54/3/83 MD 12291
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	J Newsom and D & J Newton
'Owners Address'	95 Urenui, Taranaki
'Date of Construction'	unknown
'Photo Image'	File name
'GPS Easting'	
'GPS Northing'	
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type' BOATSHED
Name
Location Section 297 Southern Shore, Bon Accord Harbour. DP 7675
'Local Authority' Rodney County Council
Description
Notes Additional notes by NAME (date): Susan Yaffe July 2006
 File 11123 This is probably the same as 54/3/83
 1996. Application to retain and rebuild boatshed and jetty. Janice Stevenson & James Scott
 Expired 30 June 2006
Keywords BOAT SHED
 BOATSHED
'Biblio References' SE File 11123
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner Janice Stevenson & James Scott
'Owners Address' 8 Tudor Place, Mount Maunganui
'Date of Construction' unknown
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type'	BOATSHED
Name	
Location	Lot 258 Bon Accord Harbour DP 7675
'Local Authority'	Rodney County Council
Description	
Notes	Additional notes by NAME (date): Susan Yoffe July 2006 File H 8741 1993 Application for jetty by Andrew Hawksworth and Kristin Brown. Boatshed already existing on land. Permit granted to May 2023 1998 Transferred to S K Nelson 1999 Trans to Phillip Southerden
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File H 8741
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Mathews and Mathews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Phillip Southerden
'Owners Address'	unknown
'Date of Construction'	
'Photo Image'	File name
'GPS Easting'	
'GPS Northing'	
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type'	BOATSHED
Name	
Location	Adjacent to Lot 216 Harris Bay (Tui Bay), Kawau DP 7675
'Local Authority'	Rodney County Council
Description	
Notes	Additional notes by NAME (date): Susan Yoffe July 2006- File 9013 1948 Jetty built 1962 Application for wharf by Selwyn Ashton Mason 1988 Transfer to Mrs Mason 1992 to Mr and Mrs J A Bradley 1993 to Zakara Investments Ltd 1994 Application for timber boatshed and deck that are combined and connected to the residence protruding over the main high water spring area. Timber jetty walkway extends 40 metres with pontoon of timber and steel. Permit extended to June 2024
Keywords	BOAT SHED BOATSHED
'Biblio References'	SE File 9013
'Date Recorded'	
'Reported By'	Anthony Matthews
'Recorders Address'	Matthews and Matthews Architects PO Box 108166 Symonds St Auckland
'Last Visited'	
Owner	Zakara Investments Ltd
'Owners Address'	Private Bag 92, Auckland
'Date of Construction'	
'Photo Image'	File name
'GPS Easting'	
'GPS Northing'	
'ArcView Category'	Historic Structure
'Grid Refs Source'	GPS

'Site Type' BOATSHED
Name
Location Foreshore Little Vivian Bay
'Local Authority' Rodney County Council
Description Boatshed 5.4 x 3.6 and ramp
Notes Additional notes by NAME (date): Susan Yoffe, July 2006
H9510405
1981 Permit for boatshed and slipway issued to Colin Gibbs
1992 Permit renewed to May 2007

Keywords BOAT SHED
BOATSHED

'Biblio References' SE H 9510405
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited'
Owner Colin O Gibbs,
'Owners Address' P.O. Box 40 130, Upper Hutt
'Date of Construction' 1981
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' BOATSHED
Name
Location Lot 169 North Cove Kawai DP 6849
'Local Authority' Rodney County Council
Description Shed 3.6 x 2.5m Ramp W 3.8 x 4m
Notes Additional notes by NAME (date): Susan Yoffe, July 2006
File 9611170
1996 Application by M & M Rowe for jetty, shed and ramp, boatshed & ramp existing
Keywords BOAT SHED
BOATSHED
'Biblio References' SE File 9611170
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
PO Box 108166
Symonds St
Auckland
'Last Visited'
Owner M & M Rowe
'Owners Address' 14 Hayes Place, Pakuranga
'Date of Construction'
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' BOATSHED
Name
Location Lot 129 and Pt 130 North Cove, Kawau DP 4961
'Local Authority' Rodney County Council
Description Boatshed timber 10 x 3.6m, jetty 69.2 x 1.5 m, retaining wall post & timber 79m
Notes Additional notes by NAME (date): Susan Yoffe, July 2006
 File H 10303
 1995 Renewal of licence for existing jetty, shed and seawall at Moana Cove
 by Dave Schmuck for L & M Pardey.
 Mr & Mrs Pardey lived in the USA – locals complained that their alterations were illegal.
 Permit issued expired 2015
Keywords BOAT SHED
 BOATSHED
'Biblio References' SE H 10303
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Matthews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland
'Last Visited'
Owner L. & M Pardey – agent Dave Schmuck
'Owners Address' Post Office, Opua
'Date of Construction'
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' BOATSHED
Name
Location
'Local Authority'
Description
Notes Additional notes by NAME (date)
Keywords BOAT SHED
BOATSHED

'Biblio References' SE
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Mathews and Matthews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited'
Owner
'Owners Address'
'Date of Construction'
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' BOATSHED
Name
Location
'Local Authority'
Description
Notes Additional notes by NAME (date)
Keywords BOAT SHED
 BOATSHED

'Biblio References' SE
'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Mathews and Matthews Architects
 PO Box 108166
 Symonds St
 Auckland

'Last Visited'
Owner
'Owners Address'
'Date of Construction'
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

'Site Type' BOATSHED
Name
Location
'Local Authority'
Description
Notes Additional notes by NAME (date)
Keywords BOAT SHED
BOATSHED

'Biblio References' SE

'Date Recorded'
'Reported By' Anthony Matthews
'Recorders Address' Mathews and Matthews Architects
PO Box 108166
Symonds St
Auckland

'Last Visited'
Owner
'Owners Address'
'Date of Construction'
'Photo Image' File name
'GPS Easting'
'GPS Northing'
'ArcView Category' Historic Structure
'Grid Refs Source' GPS

APPENDICES

Appendix A- Schedule of Structures researched

Tamaki Estuary North/Panmure Basin	Consents	Description	File References	Listing
Adj 87 Kings Rd, Panmure	27695/27937/23359/21473 proposed	Small boathouse at end of jetty – weatherboard and pitched roof	12616	Lawful
Adj 87 Kings Rd, Panmure	15692 issued 1996	Occupation of CMA with jetty, steps and pontoon <u>but</u> mentions existence of small boathouse at seaward end of jetty. This appears to be the structure mentioned in above consent.	H9511385	
Adj 1 Bhoge St, Panmure	27864 issued 2003 9362 issued 1990	19sqm boatshed	16721/TAMPAN12	Lawful
Tamaki Estuary – 13 Bridge St Panmure	28024	Boatshed	8386	Lawful
Tamaki Estuary – Adj 6-8 Riverview Rd, Panmure	29176	Boatshed	8509	Lawful
Adj 24a Riverview Rd, Panmure	8688 expired 1996 9364 expired 1995 16239 issued 1997	Boatshed 3 x 5m	H9711800/TAMPAN4/CZW40014	Lawful
5 Pakuranga Rd, Pakuranga	6321 expired 2004 9346 expired 2004 29334 issued 2004	Boatshed – part of larger boatbuilding operation covering 2400sqm (Leader Boat Builders)	H916090/TAMPAK16/6090	Lawful
125 Waipuna Rd, off Finn Place, Mt Wellington	9447 issued 1991 (expired 2005)	Aluminium boatshed 1.63m x 1.63m (3.35sqm)	TAMPAN25	On lawful list but expired
Adj 31 Riverlea Ave, Pakuranga	9342 expired 1996	Fibrolite and timber boatshed 24.5sqm	TAMPAK12	
Off 3 Riverlea Ave, Pakuranga	9344 expired 2003	Boatshed 5.5m x 4.25m (23.49sqm)	TAMPAK14	
Adj 5 Riverlea Ave, Pakuranga	9345 expired 1994 13621 expired 1998 27924 issued 2003	Fibrolite and timber boatshed on concrete piles with iron roof measuring 3.5m x 6.6m (20sqm)	18743/H9410153/TAMPAK15	Lawful
Riverlea Ave, Pakuranga	9341 expired 1997 21070 issued 2001	Timber frame and weatherboard boatshed on timber piles with iron roof	12394/TAMPAK11	Lawful
Tamaki Estuary – adj 31	15894	Boatshed measuring 38.5sqm with	H9711598	Lawful

Page 163

Riverlea Ave, Tamaki Tamaki Estuary 48 Millen Ave, Pakuranga Tamaki River	28170	24.5sqm in CMA Dinghy locker	16861	Lawful
Glendowie				
Off Riddell Rd, Glendowie	9313 expired 2005	Boatshed 17m x 7m (120sqm)	TAMGLE8	
Adj Glendowie Ave, Glendowie	9314 expired 2003	Wooden boatshed 4m x 2.6m (10.4sqm)	TAMGLE8	On lawful list but expired
Beachlands				
Shelly Bay, Beachlands	20850 issued 1997 15603 issued 1997 (subsequently surrendered)	Boatshed measuring 28sqm Consent given to demolish and reconstruct part of existing boatshed (1997).	H961139 (H9611139?)	Lawful
Huia				
Adj Huia Wharf Rd, Little Huia	9205 expired 1999	Boatshed measuring 12sqm	MANHUI9	On lawful list but expired
Adj Huia Wharf Rd, Little Huia	9207 expired 1999	Boatshed measuring 40sqm	MANHUI10	On lawful list but expired
Off Huia Wharf Rd, Little Huia	9208 expired 1999	Boatshed	MANHUI11	On lawful list but expired
Opp 1336 Huia Rd, Huia	9210 expired 2002 22275 issued 1999 22606 issued 1999	Boatshed measuring 20sqm	13048/MANHUI14	Lawful
Little Huia	9471 expired 2005	Boatshed 3m x 5.5m (16.5 sqm)	MANHUI15	
Muddy Creek				
17 Dorothy Street Lainholm	9217 expired 2001	Timber and fibrolite boatshed with malthoid roof 2.7m x 4.57m adjacent boatramp on concrete piles	MANLAI7	On lawful list but expired
Shelly Beach				
Adj to end of Shelly Beach Road, Kakaia Harbour	15142	Timber framed, fibrolite clad boatshed with corrugated fibrolite roof measuring 12.19m x 7.31m.	H9611045	On lawful list but expired
Herne Bay				
5 Marine Parade	25485	Boatshed measuring 4.2m x 2.4m (10sqm)	8229/PTCMAR14/86022	Lawful
Adj 23 Marine Pde, Herne	9270 expired 1993	Boatshed	PTCMAR16	Unlawful

Page 164

BOATSHEDS STUDY - AUCKLAND

Bay					
27 Marine Pde, Herne Bay	29763 issued 2005 29772 issued 2005	Boatshed measuring 42.5sqm	17770		
Adj 31 Marine Parade, Herne Bay	9271 expired 1997	Boatshed measuring 12m x 7m (84sqm)	PTCMAR17		
37 Marine Parade, Herne Bay	15636	Boatshed measuring 14.5m x 6m (87sqm)	H9811417/PTCMAR18/2687		
41 Marine Parade, Herne Bay	15614	Boatshed 9.1m x 3.7m	H9811326/PTCMAR9/92677	Lawful	
47 Marine Parade Herne Bay	28722	Boatshed measuring 7.5m x 4.8m – listed on CHI (number 14460) and said to be over 100 years old – see CHI printout.	9012/PTCMAR18/92727/11474?	Lawful	
Adj 55 Sarsfield St Herne Bay	9279 expired 1993 11491 expired 1998 31141 issued 2005	Boatshed measuring 98sqm thought to be approx 50 years old.	H9380268-H9380269/PTCSAR5	Lawful	
Sarsfield Street, Herne Bay	27794	Boatshed	16690/PTCSAR20		
Adj 67 Sarsfield St, Herne Bay	9281 expired 1994	Boatshed 16m x 3.9m (52.8sqm)	PTCSAR15		
Adj 71a Sarsfield St, Herne Bay	9277 expired 1994 15657 (?) issued	Boatshed 20.1m x 6.4m	H9811357/PTCSAR2	Lawful	
79 Sarsfield St, Herne Bay	9280 expired 1993 13247 expired 2005 30260 issued 2005	Weatherboard boatshed with tiled roof on concrete and hardwood piles, measuring 6.7m x 3.6m (24.12sqm).	9974/H949974/PTCSAR6	Lawful	
75 Sarsfield St, Herne Bay	27797	Boatshed	16692	Lawful	
15 Cremorne St, Herne Bay	28914	Boatshed	16123/PTCRE10	Lawful	
Adj 69 Hamilton Rd, Herne Bay	8815	Boatshed 6.2m x 3.7m	H928427/PTCHAM4	Lawful	
Waitemata Harbour – 3 River Toe Herne bay	15697	Boatshed and slipway measuring 52sqm	H9811480	Lawful	
16 Cremorne St, Herne Bay	9261	Boatshed measuring 12.2m x 6m (72sqm)	PTCORE8	On lawful list but expired	
Herne Bay – Herne Bay Cruising Club-Sloanes Beach, Marine Parade	9265	Two storey boatshed	PTCHER12	Lawful	

Page 165

BOATSHEDS STUDY - AUCKLAND

Parnell					
Waitemata Harbour – 4 Crescent Rd Parnell	11723	Boatshed measuring 6.9m x 2.9m	H939146	On lawful list but expired	
Te Atatu					
Adj Renata Cres, Te Atatu	9379 expired 2004	Boatshed measuring 2.6m x 4.6m (13.47sqm)	TEAHEN6		
Adj 36 Renata Cres, Te Atatu	9380	Boatshed	TEAHEN7	On lawful list but expired	
Off 22 Murrays Rd, Te Atatu	9382 expired 1988	Timber framed corrugated iron boatshed measuring 4.2m x 2.1m (8.8sqm).	TEAHEN 10	On lawful list but expired	
Northcote Point					
Off 1 Princes St, Northcote	9245 expired 1997	Small boatshed – noted as being in the process of being removed by consent holder.	NORSHO1	On lawful list but expired	
92 Queen Street, Northcote	11692 expired 2005 30767 issued 2005	Timber boatshed on wooden piles measuring 6.25m x 2.2m (13.75sqm).	18240,18/W/338	Lawful	
Birkenhead					
57a (1/57 ?) Maritime Toe, Birkenhead	13826 expired 2005 30660 issued 2005	Timber boatshed 4.3m x 2.3m – only part of boatshed is located in CMA.	18416/BIRSHO3	Lawful	
Adj 59 Maritime Toe, Birkenhead	9148 expired 2001 20721 expired 2001	Boatshed measuring 24sqm	BIRSHO1		
59 Maritime Toe, Birkenhead	20757	Boatshed measuring 24sqm	BIRSHO1	On lawful list but expired	
Stanley Point					
Adj 9 First Ave, Ngataranga Bay	24501 expired 2002 24502 issued 2000	Construction of boatshed on poles	15158	Lawful	
Adj 15b Second Ave, Devonport	9163 expired 2004 14030 expired 2005 30059 issued 2004	Dinghy locker.	17900/H9510496/DEVSTA15	Lawful	
Ngataranga Road					
45a Ngataranga Rd, Devonport	9458 expired 2005	Boatshed measuring 1.85m x 3.5m (6.3sqm)	DEVNGA5	On lawful list but expired	

Page 166

Bayswater				
Adj 5a Beresford Street	30602 issued 2005	Wooden boatshed measuring 2.35m x 3m	18170	
Adj 31 Norwood Rd Beach Haven	9469	Boatshed measuring 35sqm	TAKBAY26	Lawful
132 Aeroview Dr, Beachhaven	11347 15515 issued 1996	Timber boatshed with iron roof measuring 32.3sqm	H9611320/542857	Lawful
Adj 128 Aeroview Dr, Beachhaven	9126 expired 2003	Boatshed measuring 24.4sqm	BIRBCH10	On lawful list but expired
Adj 15 Beachhaven Rd, Beachhaven	9118 expired 1996	Boatshed measuring 7.5sqm	BIRBCH2	
14 Beachhaven Rd	9141 expired 2005	Boatshed measuring 3.6m x 4.6m (16.8sqm)	BIRHEL10	On lawful list but expired
Adj 82 Paragon Ave, Birkenhead	9133 expired 1993 15844 issued 1997	Two boatsheds measuring 2.5m x 2.5m and 7m x 2.5m	H9711562/BIRHEL2	Lawful
Off 54 Rambler Cres, Birkenhead	9138 expired 2000	Boatshed 3.5m x 5.4m	BIRHEL7	On lawful list but expired
Lucas Creek				
Off Flame Road, Greenhithe	9389 expired 1996 20594 issued 1988 14654 issued 1997	Boatbuilding shed 370sqm – Selfhouse Boatbuilders	9389/H969399/UPWGRE1	Lawful
Herald Island				
Adj 53 Ferry Parade, Herald Island	23202	Boatshed	UPWHER3/93569	Lawful
Point Wells				
Riverside Drive, Point Wells	9852 expired 1996	Dinghy locker measuring 4.57m x 1.83m (8.36sqm)	54/1/502	
Waiheke Island				
14 Pohutukawa Ave, Omaha Bay, Waiheke Island	29785 issued 2004 29376 issued 2004	Existing boatshed	17555	
16 Pohutukawa Ave, Omaha Bay, Waiheke Island	20696 issued 1998	Boatshed measuring 5.9m x 3m	12259	Lawful
Pohutukawa Ave, Omaha Bay, Waiheke Island	11128 expired 1996 24090 issued 2000	Boatsheds for Rocky Bay Cruising Club and wider Waiheke Island	14919; 54/2/482	Lawful

Page 167

Omaha Bay, Waiheke Island	10052 issued 1952?	Community Boatshed measuring 3m x 6m	54/3/3	Lawful
Arran Bay, Waiheke Island (Lot 10 DP1 7292)	50904	Boatshed	10371	
North end of Oreroa Beach, Waiheke Island	20503 issued 1997	Boatshed (rebuild) measuring 4.2m x 6.3m (26.42sqm)	12113	Lawful
Oreroa Bay, Waiheke Island	21192 expired 1996	Boatshed	54/9/12	On lawful list but expired
Kawau Island				
Svansea Bay, Kawau Island	11336 expired 1996	Boatshed measuring 35.8sqm	54/3/83	
Adj lot 297 DP 7675 on southern shoreline of Svansea Bay, Kawau Island	15279 issued 1996	Boatshed measuring 7.6m x 3.9m	H9611123	
Lot 258 DP 7675 Bon Accord Harbour, Kawau Island	9681 issued 1993	Shed measuring 2.2m x 2.2m	H836741	
Adj lot 216 DP 767, Hains Bay, Kawau Island	11473 issued 1994	Boatshed measuring 2m x 4m (8sqm)	H839013	
Little Vivian Bay, Kawau Island	9811 expired 1996 13885 issued 2001	Boatshed measuring 3.6m x 5.4m (19.44sqm)	H9510405; 54/1/559	Lawful
Stockyard Bay, Kawau Island	24871 issued 2001	Building – partly in CMA – not clear whether this is a boatshed	15372	
Pt 130, DP 4961, North Cove, Kawau Island	6661 expired 1996 3855 issued 1995	Boatshed	H9510303/C2W30111	
North Cove, Kawau Island	20691 issued 1995	Boatshed	H9510303	
Adj lot 169 DP 6849, North Cove, Kawau Island	15345 issued 1985	Boatshed	H9611170	

Page 168

Appendix 3 Photographs



Figure 13: View of boat sheds of the Richmond Cruising Club, Sloanes Bay. Henry Winkleman, 15 July 1914. Sir George Grey Special Collections, Auckland Libraries, 1-W21



Figure 14: Seacrest sailing by Sloanes Beach., Unknown, photographer, 1928-29, PH-NEG-C22767. Seek permissions to use Auckland Museum http://www.aucklandmuseum.com/collections-research/collections/record/am_library-photography-49280?k=sloanes&ordinal=1



Figure 15: Showing Richmond Cruising Club at Sloanes Beach off Marine Parade, Herne Bay with the yacht Waione (built by Sutherland Bros to a Charles Bailey jnr design in 1907) in the centre, 1937. Sir George Grey Special Collections, Auckland Libraries, 7-A15860



Figure 16: Richmond Cruising Club., Unknown, photographer, 1932-33, PH-NEG-C30481 Seek permissions to use Auckland Museum http://www.aucklandmuseum.com/collections-research/collections/record/am_library-photography-51331?k=richmond%20cruising&ordinal=0



Figure 17: Matariki (yacht), beside the Old Richmond Yacht Club at Sloanes Bay, Herne Bay., Unknown, photographer, PH-NEG-C22773. Seek permissions to use Auckland Museum http://www.aucklandmuseum.com/collections-research/collections/record/am_library-photography-49285?k=richmond%20yacht&ordinal=4

RICHMOND YACHT CLUB 1904
FIRST CLUBHOUSE

PRESENTED BY A.E. HOLLAND HON. SEC. 1904



DOORWAY · Left to Right · H · PORTER · W · WELCH ·
SITTING - Left to Right - J · LINDSAY · A · CROSS · P · FAHEY · J · LONGBOTTOM · R · SPIER
FRONT · ON · DINGHY · M · DEJONGH · A · DEJONGH ·
ON · BEACH · A · GOLLAR ·

Figure 18: James Donald's boatshed at Rawene Avenue, the beginnings of what is now the Richmond Yacht Club. Auckland Council Archives. ACC439/2m 55 MARINE PARADE (FORMERLY RICHMOND YACHT CLUB BOATSHED SLOANES BEACH AT END OF BELLA VISTA ROAD), 1965-1966



Figure 19: Ground floor interior showing the dingy storage, 2009. Mages from ARC property file



Figure 20: Ground floor interior showing the dingy storage, 2009. Mages from ARC property file



Figure 21: First floor interior, 2009. Images from ARC property file



Figure 22: First floor interior, 2009. Images from ARC property file



Figure 23: Stairs leading down to Sloanes Beach. 15 November 2016, Anna Boyer.



Figure 24: View of the boatshed and Sloanes Beach from Herne Bay Reserve, Short Street. 15 November 2016, Anna Boyer.



Figure 25: View of the boatshed and Sloanes Beach from Herne Bay Reserve, Short Street. 15 November 2016, Anna Boyer.



Figure 26: View of the boatshed from Sloanes Beach. 15 November 2016, Anna Boyer.



Figure 27: View of the north west elevation of the boatshed. 15 November 2016, Anna Boyer.



Figure 28: View of the north west elevation of the boatshed. 15 November 2016, Anna Boyer.



Figure 29: Panorama of the north west elevation of the boatshed, showing other building along the shoreline. 15 November 2016, Anna Boyer.



Figure 30: Detail showing piles and struts. 15 November 2016, Anna Boyer.



Figure 31: View of the wooden boardwalk connecting the building to the embankment. 15 November 2016, Anna Boyer.



Figure 32: Front door and stairs. 15 November 2016, Anna Boyer.



Figure 33: View of the boatshed from the embankment. 15 November 2016, Anna Boyer.



Figure 34: View of the boatshed from Sloanes Beach. 15 November 2016, Anna Boyer.



Figure 35: Shows the join connecting the single storey to the upper storey added in 1932. 15 November, Anna Boyer.

Appendix 4 Drawings

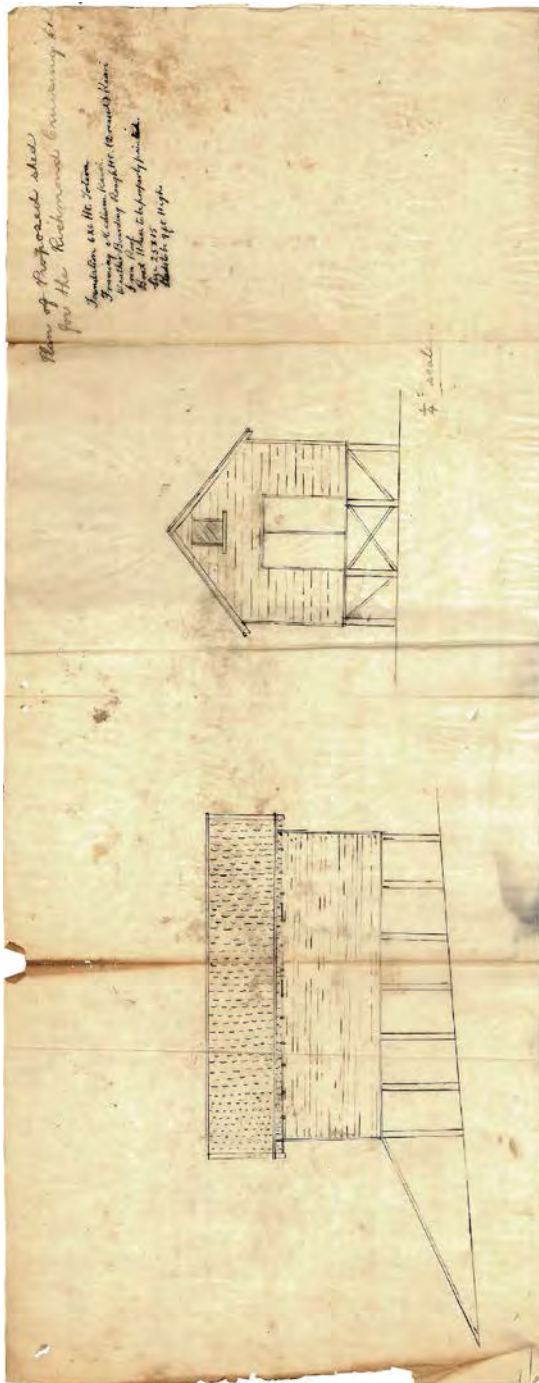


Figure 36: Plan of proposed shed for the Richmond Cruising Club. Auckland Council Archives. ACC 219/12-348 BOAT SHEDS FOOT OF FERGUSON STREET - SLOAN'S [SLOANES] BEACH, 1912-1938. Foundation 6 x 6 ft. totara. Framing medium kauri. Waether boarding rough ft (dressed) kauri. Iron roof. Boat H to be properly painted. Size 2.5 x 15. Studs to be 9 ft high

¹ Pearson, Dave. (August 2004). Herne Bay Cruising Club Building, Sloanes Beach, Auckland. Prepared for the Herne Bay Cruising Club.

² Matthews & Matthews Architects Ltd. (2007). Auckland region boatshed study. Prepared for Auckland Regional Council.

³ Auckland Council (2013). Heritage Assessment. Aotea Sea Hall (former Manukau Yacht & Motor Boat Club). 1 Orpheus Drive, Onehunga. P. 5

⁴ Matthews & Matthews Architects Ltd. (2007). Auckland region boatshed study. Prepared for Auckland Regional Council. p.11

⁵ Ibid. p. 11

⁶ Ibid. pp. 12-57

⁷ <http://hbcc.net.nz/history/>

⁸ Harold Kidd & Robin Elliot. (2003). Lee Rail : a centennial history of the Richmond Yacht Club 1903-2003.

⁹ Matthews & Matthews Architects Ltd. (2007). Auckland region boatshed study. Prepared for Auckland Regional Council.

¹⁰ Ibid. p. 11.