
Local Board Plan 2023

Kaipātiki Local Board

WRITTEN FEEDBACK

Date published: Friday, 01 September 2023



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not very well

Tell us why

outdated concepts and ideas: pouring concrete and stones into reserves is not a way to preserve nature but a fast way to destroy it. looking into renewable energy sources for public buildings? they should have been installed decades ago and you are just going to look at it in 2023? You sound like the current government (which you are) look into everything but get nothing done.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

a plan must have timelines and specific achievements dates with costs/figures

3. Have we identified the most important opportunities or challenges in our draft plan?

No

Tell us why

outdated ideas

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

Are they no New Zealanders? Are we not one country? Do they have different passports?
What about the Chinese? Indians?

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

concept is not fully understood and used as excuse when failings are about proper and well
designed infrastructure that is properly maintained

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Just use the funds you have better, no more resources to groups that can't manage the
current available

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows

	Pest (plant and animal) reduction	
	Retrofitting council facilities to be more environmentally sustainable	
	Tree planting	
	Waste minimisation	
	Water quality improvement	
✓	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Realise we are in 2023 and we need a plan that is visionary or at least in lines with the times. Understand that what you are currently doing to the environment is not a way to preserve it but you are destroying it. Concrete is not a solution, transporting stones with an helicopter is not environmentally friendly, local reserves have become glorified concrete foot paths, that is not nature. Unfortunately you have the whole concepts wrong



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

It's has a good broad view of all the critical areas of need.

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

I feel it is already very comprehensive

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

Climate change and the need to be equipped for future weather event. Focus on Maori and strengthen their voice and outcomes. Focus on already well used community area.

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

As I am not Māori it is hard to fully answer this. To me it looks good, however, they can only truly judge this.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

There are some clear opportunities to help.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

Do you mean an additional rate fee?

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting

	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Housing development

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input checked="" type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input checked="" type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Prioritisation of partnering with Maori

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Not very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

This sentence doesn't make sense to me; "The median age of Māori in Kaipātiki is ten years younger than the median age in the Kaipātiki local board area."



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Too generic & full of feel good words without any meaningful specifics about allocation of budgets & actual targeted expenditure.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

It needs to identify targeted expenditure (ie changes to existing expenditure)

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

Who are the advocates for Maori? Are these appropriate? Need to consider that a lot / most Maori are from Iwis outside of identified North Shore ones. How are they represented.

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

Very generic. How are you actually going to spend money in meaningful ways.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Why is Kaipatiki any different from any other area that it needs its own additional targeted rate

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting

	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Too much waffle, not enough substance



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

There's a wide range of considerations of most of the aspects that affect the local community

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

The aims in there are too vague, I can't tell what's actually going to be achieved

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

Again, it's vague. I want to know actual specific projects and goals, not just "Ensure something is supported"

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

I'm not Māori, so it's not for me to comment for them

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

Ineffective and vague measures. More drastic action needs to be taken, and quickly

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Can't afford another rate

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation

	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Just far more specific goals - I'd love to know exactly what projects are going to be worked on to meet those aims.

Also, get rid of the grass berms and replace them with cycle paths! It works double to reduce emissions from lawn mowing and encourage safe cycling



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Very well

Tell us why

Comprehensive and aspirational goals, but delivering on these promises, rather than just identifying them, will be where the rubber meets the road.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

An Asian/Maori/Pacifica/ and Pakeha market being held in the car park of Northcote Central every second Sunday. Northcote has a unique ethnic mix, with Asian citizens being well-represented and a market featuring our unique ethnic blend would be a real point of difference to other markets currently in Tamaki Makarau.

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Although Transport should be front and center, as the population is continuing to grow exponentially in our area, especially with all the new residential builds taking place in Northcote Central at present.

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Council rates are already through the roof and an added charge would be unrealistic and unaffordable.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction

	Retrofitting council facilities to be more environmentally sustainable	
✓	Tree planting	
✓	Waste minimisation	
✓	Water quality improvement	
	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Stop commissioning endless reports about a pedestrian and cycle crossing for the Waitemata and get it done already. Half a century of reports and feasibility studies are enough I think. Too much hui and not enough doee.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not well at all

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Because I can't afford it!

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Need to address urban density issues - safety, crowded housing and lack of super market options - especially in beach haven.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Other cultures. It aims to be inclusive, but instead isolates other people of other races.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

Crime is huge - where is the support? This feels rose tinted.

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

Feel like easy cheap options that can be reversed - nothing long lasting. "Investigate" and "support" are used to describe the initiatives that should be executed on. Feels like low commitment.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation

✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Very well

Tell us why

Big picture / strategic. Good to see that reality checks mean that plan does not over promise and picks up on social and environmental dynamics that will influence future development

2. Is there anything else you think should be included in the draft plan?

Tell us why

This Ward largely comprises established housing and facilities. The focus is on improvement and adaption - what will Council / Local Ward be able to do to assist its residents through climatic and geophysical damage and change?

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Good coverage from tangitawhenua and political expectations, adaption, enhancement to key infrastructure and social wellbeing

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

See above

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Reinstatement of walking tracks is only one aspect of remediation - what about stablisation and reinstatement of banks and erosion probe areas eg Little Shoal Bay where tennis courts have been closed since Anniversary weekend deluge

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

Council cannot view its ratepayers as the holders of deep pockets able to fund endless rate increases. The justification for the creation of Auckland Council was decreased rates and efficiencies. The Council decision making process on June 23 showed tha

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management

	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Good to be able to read and comment on your public discussion document.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Kauri kids are an important community resource. Where is the breakdown of any funding that is needed for them? Teacher's salary paid by MOE and buildings are council owned

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

Climate change is most important priority

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?

As we do not have a golf course, we should not be subsidising other areas golf courses. Please stop paying for people to play golf, while cutting early childhood education. ECE has an excellent return on investment while golf does not



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Major themes addressed

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

Almost feels you've spent more time on that than the rest of the plan

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

It sounds good, but I feel ill equipped to judge as I am not Māori myself and did not attend the hui where these things were discussed.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

We have a great natural environment in Kaipātiki and I would like to see it even further enhanced.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

I see that encouraging more active transport is a goal of the plan and I applaud this as it has both environmental and health benefits. Crossing the Waitemata is a vital part of this. You may be aware of an engineering report that Bike Auckland recently released showing that repurposing one lane of the harbour bridge for active transport would be safe and effective, without significant impacts on congestion:

<https://www.bikeauckland.org.nz/engineering-report-confirms-an-auckland-harbour-bridge-active-lane-is-safe-and-viable/>

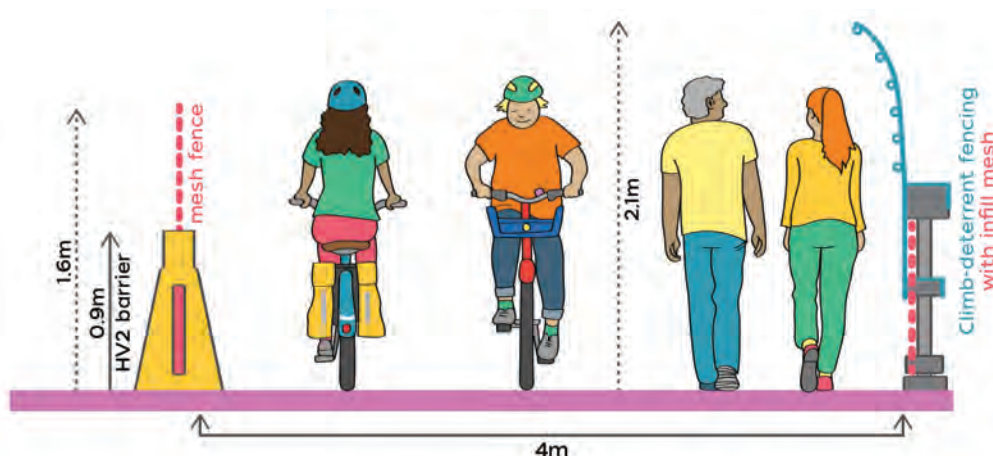
It would be great for our local plan to include explicit support for this as the most cost effective way to improve active transport links from Kaipātiki to our neighbours to the south.

Liberating the Lane, an assessment

Overview

Bike Auckland commissioned SmartSense Ltd to investigate the feasibility of liberating a lane on the Auckland Harbour Bridge for active mobility - walking, cycling and other personal mobility options. The resulting report concludes that:

- There is **existing capacity on the Harbour Bridge** to support a lane permanently being used for active modes without significantly impacting on motor traffic
- A **safe design** would include:
 - A shared path for active modes located on the most easterly clip-on lane. The east side provides for better protection from wind (predominantly Westerlies), easy access on and off the bridge, and stunning views of the harbour and city
 - The use of free-standing HV2 Steel/Concrete composite barriers with mesh fence between the shared path and the motor traffic. This is a relatively lightweight barrier which Waka Kotahi has already approved the use of in Aotearoa NZ
 - An anti-climb barrier along the eastern edge of the bridge for suicide prevention, as has been effective on comparable bridges overseas
 - Potential for a 4metre shared path width which is within the Austroads guidance for gradient (5%) and width with the expected usage
 - Potential for dampers to be placed between the bridge and the clip-on (to prevent movement caused when a significant number of pedestrians march across at the same time)
 - Potential for a 60km/h speed limit for the motor-traffic in the lane adjacent to the shared path for additional safety if considered required



We consider that a cross-section similar to the image above would be a pragmatic, cost effective, and realistic design for an Auckland Harbour Bridge Shared Path.

- Based on data from Waka Kotahi, weather conditions on the bridge appear suitable for walking and cycling for 98% of the year. The shared path would be affected by adverse weather conditions approximately 3 - 7 days per year, similar to other road users.



Artist's impression of a shared path over the Auckland Harbour Bridge

SmartSense Ltd. estimates that access for walking and cycling across the Auckland Harbour Bridge could be delivered within 8 months for under 30 million dollars.

Bike Auckland notes that this is both affordable and can be rapidly delivered, and is a vital equitable solution for transport choice and emissions reduction in Tāmaki Makaurau, Auckland.

Not included in Waka Kotahi's assessment are the following benefits;

- ★ Empowers people to choose climate-friendly travel resulting in emissions reductions
- ★ More transport choice for people who don't drive
- ★ Saves people money
- ★ Creates a more resilient transport system
- ★ Delivers greater health and wellbeing from active travel
- ★ Greater independence, especially for children
- ★ Culturally significant crossing
- ★ Fun to cross and with great views for locals and visitors alike!
- ★ Potential to relieve congestion across the transport network as people switch to walking, cycling, and public transport



Note: Bike Auckland commissioned SmartSense Ltd. to create the following two reports, but for simplicity we are referring to them as one report. They are included in this PDF as follows:

1. Assessment of Waka Kotahi Safety Concerns Related to Active Mode Provision (on the Auckland Harbour Bridge)
2. Report on Traffic Flows on Auckland Harbour Bridge 2013-2023

Auckland Harbour Bridge

Assessment of Waka Kotahi Safety Concerns related to Active Mode provision

June 2023



Prepared for



SmartSense
people driven intelligence

Contents

Executive Summary	1
Active Mode Option to be Assessed	3
Suicide Prevention	6
Outer edge of Clip-On	6
Shared Path to Road Traffic Barrier	8
Loading of Active Lane	9
Additional Barriers	9
Dynamic behaviour of bridge under pedestrian loading	9
Deflection of barrier during impact.	12
Bus Stability	16
Shared Path Width / Edge barriers	16
Gradient of Bridge	18
Speed of Cyclists	21
Weather Effects	24
Provision of wind related data from Waka Kotahi.....	24
Analysis of wind related data.....	24
Wind Profile	28
Waka Kotahi Safe Systems and Wind Assessments.....	30
Maintenance / Emergency Access and Assistance	32
Summary - Potential mitigations to safety items raised by Waka Kotahi	33
Barriers	33
Gradient and Width	34
Dynamic behaviour	34
Shared Path Cycling Speeds	34
Staffing, Maintenance and Emergency Access	35
Severe Weather	35

Auckland Harbour Bridge - Assessment of Waka Kotahi Safety Concerns related to Active Mode provision.

Executive Summary

The Auckland Harbour Bridge and approach viaducts (all referred to as the **AHB**) currently carries State Highway 1 across the Waitemata Harbour between Sulphur Beach (North Shore) and Westhaven (Downtown Auckland), an overall distance of 1.6km. To provide for marine clearance below, the AHB rises to 44m above High Water level at a maximum gradient of 5% (1:20) which extends approximately 500m on the northern side and 700m on the southern side.

Waka Kotahi has undertaken numerous assessments of many different active mode (**Active Mode**) options. Waka Kotahi appears currently reluctant to see any Active Mode trial on the AHB and it has stated that any one-off Active Mode events should not be seen to:

*'..set an expectation that further consideration will be given to providing live lane access' or
'..set unrealistic expectations around the likelihood of a dedicated lane becoming available in the short term.'*¹

Against this backdrop, Bike Auckland commissioned SmartSense Ltd to undertake two assessments.

The first report relates to the actual traffic flow volumes and the capacity of the AHB to carry known flows from 2013-2023. This assessment indicates that since 2016 peak traffic volumes have been reducing and that the conversion of a single traffic lane to Active Modes can be accommodated with very limited effects on recorded traffic levels.

Ongoing traffic flow trends both pre-COVID (prior to March 2020) and since then continue to support this conclusion. These findings were shared with Waka Kotahi which indicated that traffic flows were not the key reason why Waka Kotahi was not pursuing Active Mode provision on the AHB. Its stated concerns related to safety.²

The second commission from Bike Auckland is this report which undertakes an assessment of the safety concerns that Waka Kotahi has raised relating to motorised traffic sharing the bridge with Active Modes.³

It appears that Waka Kotahi has made particularly conservative assumptions (to the point of being unreasonable) about the likelihood and consequences of many of the safety risks associated with using a lane of the AHB for Active Modes. The inclusion of people walking and cycling on the bridge does present different risks to driving trucks, buses and cars. However, Waka Kotahi appear to have taken a pre-determined approach to raise risks that are not considered significant on other projects

¹ Waka Kotahi Investment and Delivery Paper – Auckland Harbour Bridge Walking and Cycling Event 23/11/21 released under OIA 9248

² Meeting between SmartSense's R Young and Waka Kotahi representatives D Hume, A Hooper, M Beamish, 27/5/22 2022.

³ Active Modes taken as being people walking, running, in wheelchairs, on cycles, scooters and other micro-mobility devices etc).

with longer and steeper shared paths, potentially overstated the risk of wind and have ignored the human factors and human interventions that could significantly reduce those risks to very acceptable levels of likelihood and consequence. SmartSense assesses the residual risk to Active Modes on the bridge to be far lower than Waka Kotahi's apparently overly pessimistic and simplistic conclusions.

The dynamic behaviour of the bridge under mass pedestrian events was recently flagged by Waka Kotahi as a significant safety issue preventing a Shared Path trial. Whilst there is evidence (including video) that the clip-on does sway when large numbers of people walk on it, a relatively simple mitigation solution was designed thirteen years ago -but never progressed. The single lane Shared Path long term trial proposed is unlikely to be utilised by the volume of people attending a mass participation event, so is far less likely to generate the sway, and the only area where there may be a crushing risk (between original bridge and clip-on bridge) is remote from the Shared Path. There are several active mitigation options to manage pedestrian numbers and therefore this issue is not considered to be a fundamental reason to delay any Shared Path trial.

SmartSense concludes that there are practical, cost effective, and realistic mitigations to address Waka Kotahi's safety concerns. Rather than Waka Kotahi continue to resist all reasonable attempts to enable Active Modes on the AHB, we recommend that it proactively and collaboratively engages with Bike Auckland and others to rapidly deliver a twelve month monitored trial of a single lane Shared Path on the Auckland Harbour Bridge for Active Modes.

Active Mode Option to be Assessed

Based on our first commission from Bike Auckland⁴, the only Active Mode option considered viable is the conversion of a single southbound traffic lane on the eastern clip-on to be a shared path (**Shared Path**).

This report focuses on assessing the stated safety concerns for that single lane option. This is generally referred to in Waka Kotahi documents as 'Option 3', the long term (permanent) reallocation of the most easterly clip-on lane from traffic to Active Modes. The other options that Waka Kotahi has identified are dismissed as unrealistic or overly disruptive.

Option 3 appears to be the most practical and attractive for the following reasons:

1. Single (4m) lane conversion from road traffic to Active Modes has a small and manageable impact on traffic flows. (Figure 1). The 4m path width has been retained for this analysis as this corresponds directly with Waka Kotahi Option 3. Whilst it may be possible to widen the path (to 4.5m) this would move the traffic lane further across the bridge deck and we are not able to assess if this is structurally feasible to achieve.
2. The 4m path could also serve as an emergency vehicle access lane (Figure 2)⁵
3. Waka Kotahi provided drawings show (Figure 3) the bridge gradient to be 5% and an original design report (1951 Figure 4) also references a 1 in 20 gradient on both sides.

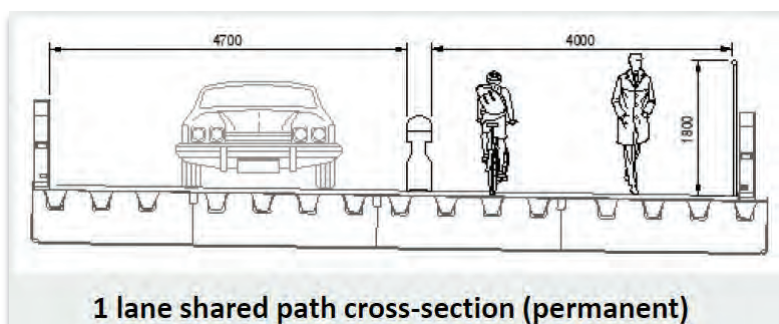


Figure 1 Waka Kotahi's 'Option 3' cross-section

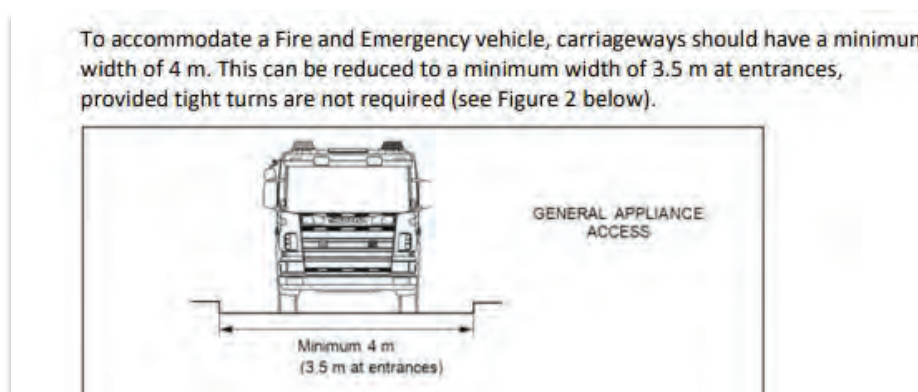


Figure 2 Emergency Access width for NZ Fire Service

⁴ SmartSense Report on traffic volumes and capacity 2022.

⁵ 4m is also the minimum recommended access width for a fire truck (F5-02-GD-FFO Emergency Vehicle-Access)

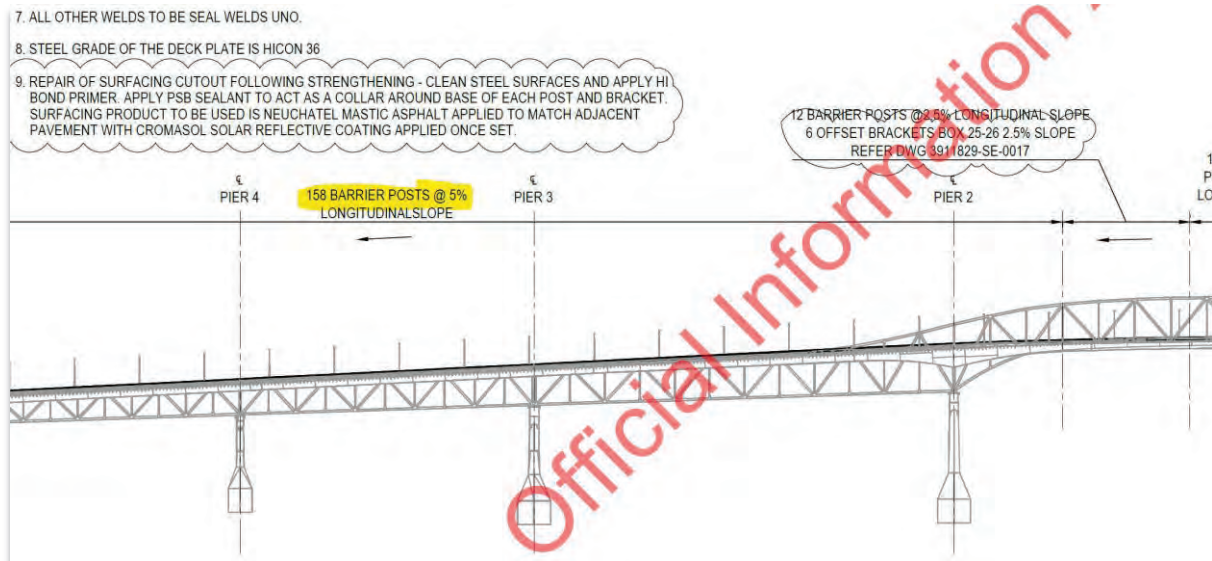


Figure 3 Waka Kotahi elevation of bridge showing 5% gradient (Northern end the same)

ALIGNMENT AND GRADIENTS

17. We have altered slightly the alignment recommended by the Royal Commission, so as to give better access to the south approach and to facilitate the possible future construction of a railway bridge, as explained in our letter to the Commissioner of Works dated 14.2.50, and approved by him in his letter to us of 14.3.50. The alignment proposed by the Royal Commission and the one recommended by us are shown on Drawing 1.

18. The road gradient of 1 in 20 on both sides of the navigation span and a vertical curve 800 ft. long over the main span are in conformity with modern standards for high speed motor traffic, and were approved by the Commissioner of Works in his letter of 3.2.50. A vertical transition curve of 4,000 ft. radius is proposed between chainages 1365 and 1565 on the South approach.

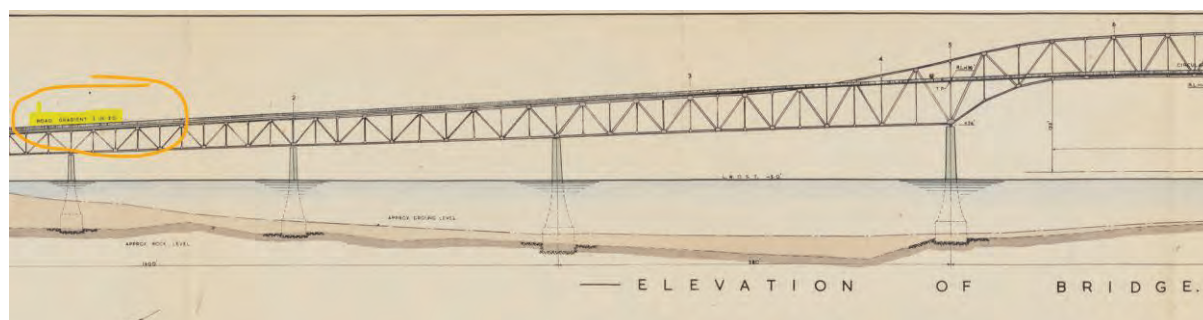


Figure 4 Extract from Auckland Harbour Bridge, Report by Freeman, Fox and Partners. March 1951 (ref b1865712 Auckland Libraries)

4. If required, it allows for five lanes of northbound traffic on the AHB.
5. The access / egress locations are relatively straightforward.
 - a. Sulphur Beach (North Shore) has existing subway and road access under SH1 (Figure 5).
 - b. Westhaven (Downtown Auckland) (Figure 6)

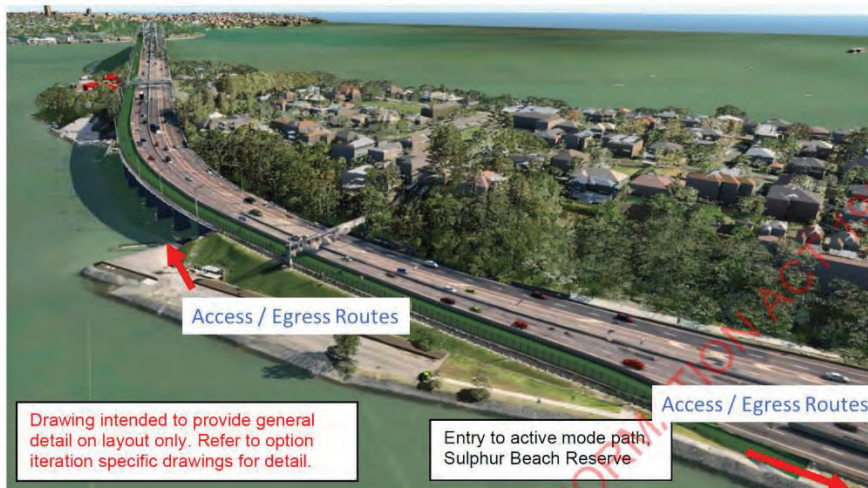


Figure 5 Sulphur Beach access to Active Mode path, under bridge (left) and through subway (right)

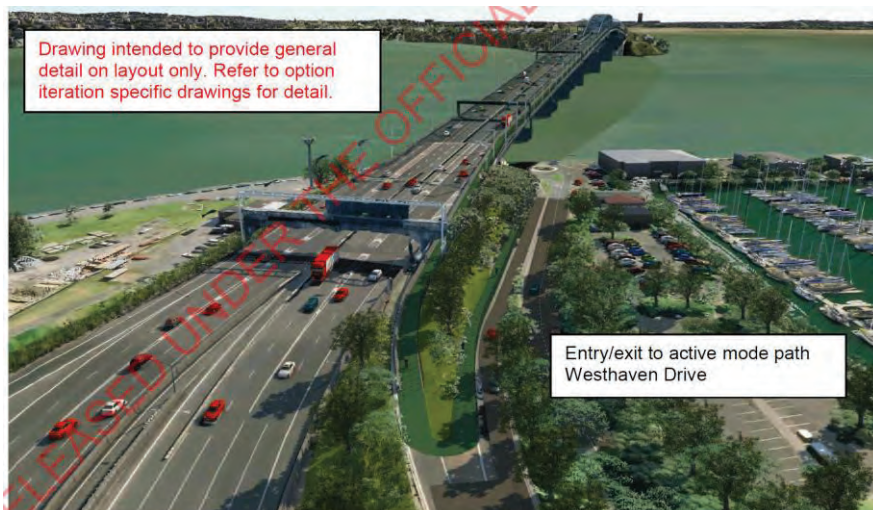


Figure 6 Westhaven Drive Access/ Egress. Local road realigned / repurposed Shelly Beach Rd off ramp may need to be closed.

6. Eastern location of Active Mode path provides a higher level of weather protection from prevailing westerly weather.
7. Eastern location of Active Mode path provides a clearer view of the wider Waitemata Harbour and Downtown Auckland than the west so is preferable from an aesthetic perspective.

On the basis of the above advantages Option 3 is the one that Smartsense considers is most practical and this report focuses solely on the safety concerns that Waka Kotahi have raised that relate to Option 3.

Suicide Prevention

Outer edge of Clip-On

Waka Kotahi has identified that the AHB is currently used by people for suicide attempts and any increase in access to the bridge is likely to increase these attempts.

Waka Kotahi has already designed⁶ what it considers to be a highly effective suicide prevention barrier solution for the AHB; but to date it has chosen not to install it, and so that risk remains unmitigated. Their existing scheme draws heavily on large and high (3.2m, Figure 7) close spaced mesh to prevent people who exit a vehicle from climbing over the outer edges of the clip-ons.

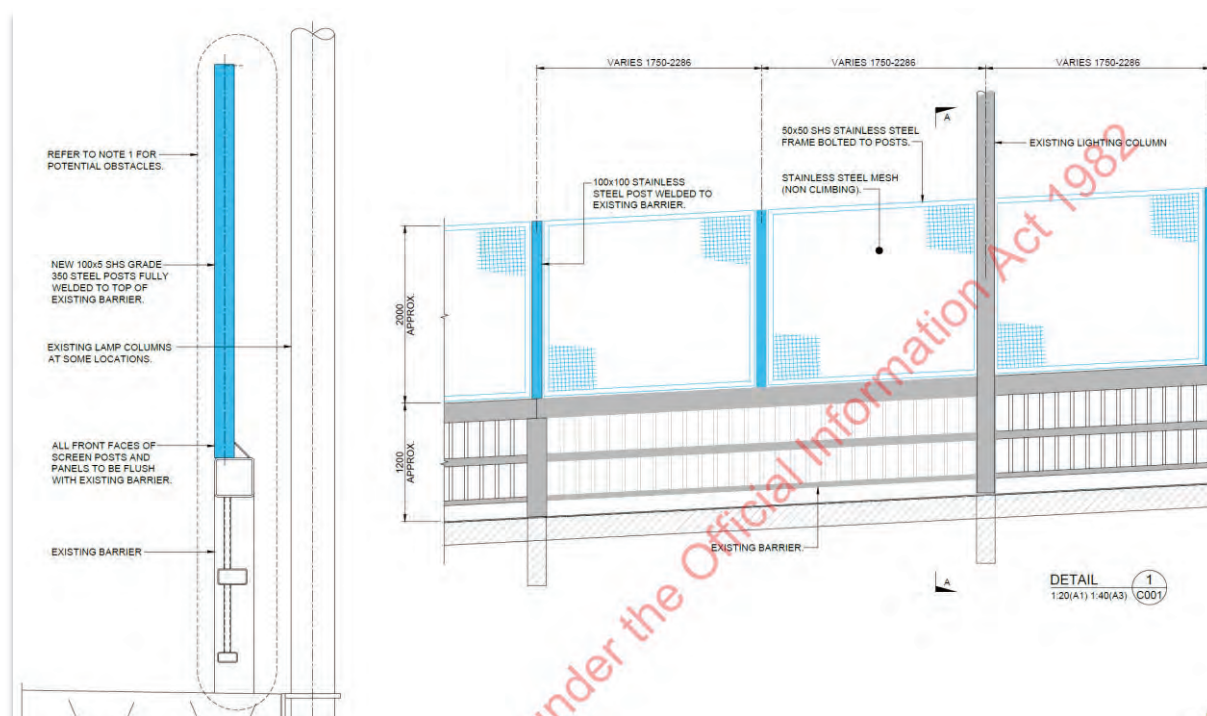


Figure 7 Schematic view of proposed Security Screen for Auckland Harbour Bridge

Subsequent structural assessments of the proposed scheme⁷ range from this design being 'insignificant in some locations' to 'feasible, with possible local strengthening.' in others. A budget of under \$13M was (in 2019) identified as being sufficient for this barrier on one side of the bridge.

Whilst this may be an appropriate solution for a high-speed highway it is considered that the height and appearance would effectively block any meaningful view of the city and would therefore be far from satisfactory for a successful and enjoyable Active Mode solution. The anti-climb designs that Waka Kotahi have provided for an Active Mode solution are significantly lower (1.6-1.8m) and do appear more in keeping with the scale and potential risk.

It is recognised that there are numerous other highway and other structures around Auckland that can, and have, been utilised for suicide attempts; so whilst there is obvious merit in providing a level

⁶ Suicide Prevention Barrier Feasibility Study Report 18_19 SH1N BSN 4232 Auckland Harbour Bridge 383545-18_19-BR-SH1-4232-RP-FS-001-REV_F1, May 2019 released under OIA 9593.

⁷ AHB Protection Screen Structural Feasibility Study NZTA Memo 30/4/20 released under OIA 9593.

of anti-climb on the AHB Active Mode lane there is no absolute requirement to prevent determined and deliberate climb/ jump activities.

The case studies that Waka Kotahi drew on for assessing anti-climb systems were limited. There are numerous other less intrusive examples of anti-climb on high-profile bridges. Of note is the 412m long, 101m high Clifton Suspension Bridge in Bristol, UK (Figure 8). This 200+ year old structure has had a long history of suicides and has deployed a range of measures to minimise the likelihood and success of suicide attempts. In 1998, a 1.9m high anti-climb barrier was added (Figure 9).



Figure 8 Clifton Suspension Bridge - Bristol, UK, 101m above River Avon.



Figure 9 1998 anti-climb barriers on Clifton Suspension Bridge (1.9m height)

Research from the UK⁸ indicated that this barrier halved the number of suicide deaths at the bridge from eight to four with little evidence to suggest that people used alternative suicide locations.

⁸ Effect of barriers on the Clifton suspension bridge, England, on local patterns of suicide: implications for prevention, The British Journal of Psychiatry, Cambridge University Press, January 2018.

The Clifton Suspension Bridge (Figure 9) also includes some ‘soft’ approaches that the Clifton Suspension Bridge Trust has taken to deter and prevent potential suicide attempts. In addition to the fence in the image there are:

- Free helpline phones located at either end;
- Signs promoting counselling services;
- A minimum of two staff trained to deal with people contemplating suicide;
- Live CCTV monitoring with night time staff alarms; and
- A quiet room for people in distress to use whilst waiting for emergency services.

These soft measures are combined with the visually unobtrusive anti-climb barrier to a height of 1.9m⁹ above the walkway. The use of horizontal wire stands reduces the visual impact from the shore, minimises degradation of view from the bridge and adds minimal wind loading, whilst providing an appropriate level of deterrence to anyone seeking to climb over.

Overall, we consider that the provision of an anti-climb barrier on the eastern edge of the AHB clip-on can be provided in a relatively unobtrusive manner so that it does not detract from the visual appearance of the AHB or views from the Shared Path. Waka Kotahi have already assessed the higher (3.2m) and more substantial barrier as being feasible (at \$13M per side) therefore a lower less obtrusive 2.1m high barrier would appear to also be viable, provide a reasonable suicide deterrent and be a cost-effective solution. The provision of any higher barrier will be an improvement on the existing edge protection.

Shared Path to Road Traffic Barrier

There is an inherent risk anywhere on Waka Kotahi’s road network of people climbing over a Shared Path/Traffic barrier (Figure 1) to enter a traffic lane. There should be some degree of anti-climb deterrent barrier between the Shared Path and Traffic lanes; but this should be consistent with the remainder of the motorway network.

For instance, the barrier preventing access onto the southbound approach to the AHB, at Sulphur Beach is a low ±1m fence and outside the ASM Annex / Police Station there are large, fully open grassed areas where anyone can walk directly onto the motorway (Figure 10).



Figure 10 Low fence and direct access onto SH1 at Sulphur Beach

⁹ Letter from Clifton Suspension Bridge Trust to Bristol Coroner’ Court 11/2/2019.

Loading of Active Lane

The assessment of the effect of a Shared Path on the structural capacity of the AHB is beyond the scope of this report as it would require highly specialist input and analysis. However, several Waka Kotahi documents have been made available that do provide a reasonable insight into the potential issues.

Additional Barriers

From the material provided by Waka Kotahi there is an underlying requirement that the barrier between the Active Mode lane (Lane 1 of the south bound clip-on) and the southbound clip-on traffic lane (Lane 2) would need to be freestanding. It is understood that this requirement is to prevent any barrier anchorages applying localised horizontal loads to the clip-on decks.

The assessment undertaken by Waka Kotahi¹⁰ assumed a barrier deadweight of 715kg/m length (SRTS Barrier) with Option 3 flagged as being 'Overloaded*' (the * is not explained) and a proposed restriction on buses and heavy vehicles required to prevent the live load capacity being exceeded.

This Waka Kotahi assessment appears to be reliant on using the same movable concrete barrier used on the central span on the clip-ons. Other material also provided in the OIA indicates there is a lighter and equally suitable free-standing alternate. The HV2 (MASH TL-4) barrier has a linear mass of 360kg/m close to half the mass used in Waka Kotahi's assessment. This barrier was approved for use on New Zealand roads by Waka Kotahi in May 2019¹¹.

There is no evidence that Waka Kotahi have performed a detailed loading assessment and analysis based on this lighter HV2 barrier. Additionally, Waka Kotahi do not provide any information on load factors applied to the extra barriers. These would be known fixed masses so could be considered as dead loads and have a significantly lower load factor applied than a more uncertain live load.

The removal of traffic live load from the outer lane on the clip-on would significantly reduce the eccentricity of the load and should have a beneficial impact on overall bridge loading and fatigue performance.

Waka Kotahi (OIA 9736) have separately advised that there is not expected to be any fatigue related issues related to increases in vehicle mass limits since 2009. It appears unlikely therefore that removing vehicles from the outer lane of the clip-on (longest cantilever) would be detrimental to the safety and longevity of the AHB. This is contrary to several statements made by Waka Kotahi about the structural capacity of the AHB being compromised and the bridge's life shortened should an outer clip-on be used for Active Modes.

Dynamic behaviour of bridge under pedestrian loading

During a media event to announce the advancement of options for a second harbour crossing (April 2023) Waka Kotahi's CEO publicly expressed concerns to Bike Auckland staff about the dynamic behaviour of the AHB under active mode loads that would limit its ability to safely cater for any active mode usage.¹²

This was obviously a matter of serious concern as until that time no meaningful information had been seen in any published Waka Kotahi material that related to the bridges' dynamic behaviour

¹⁰ O4.AHB WC Options Structural Review Memo Draft Redacted .

¹¹ Product Acceptance – HV2 Steel & Concrete Hybrid Temporary Road Safety Barrier System, Letter from NZTA (J Hughes) 3/5/2019 to CSP Pacific Ltd.

¹² Discussions between Nicole Rosie (WK CEO) and Bike Auckland staff in the presence of media.

affecting its ability to be used by active modes. As a result of this statement an Official Information Request¹³ was lodged on behalf of Bike Auckland and a response provided in late May 2023. Some clarifications on the material issued has been sought and this section will be updated if that material changes our understanding of the issue.

The OIA response indicated that lateral movements of up to 50mm had occurred during previous unplanned mass-pedestrian (protest) events on the clip-on spans. These movements were generated by large numbers of people subconsciously walking in step and inducing a degree of resonance in the bridge. This is a relatively well-documented effect on certain bridges (Synchronous Lateral Excitation) and in 2010 Waka Kotahi's advisors¹⁴ calculated the likely movement and proposed some relatively straightforward measures to mitigate the effect.

Most significantly, the reports released under the OIA identify that the movement was only likely to occur at high pedestrian loads across both lanes of the clip-on and that the observed lateral movement (Figure 11, Figure 12) is between lane 2-3 or 6-7, the boundary of the clip-on to the original centre-spans (the bridges are not structurally connected). This was observed to occur on an (undated) video film when both clip-on lanes were used for an un-authorised pedestrian event and only on some of the viaduct spans.

Other information released in the OIA confirmed that whilst the dynamic movement posed a comfort risk to pedestrians on the bridge (lateral accelerations exceeding code) and a significant crushing risk should anything be placed into the opening and closing gap. However it was noted that the movement of the bridge did cause any structural damage nor was likely to affect the long-term life of the structure.

As Option 3 (Active lane in 'Lane 1'¹⁵) does not permit pedestrians into the zone between the clip-on and the original structure (between Lanes 2-3), so there is no risk to Active Lane users coming close to the opening/closing gap. Also, this lateral movement was only associated with large numbers of people walking; there is no evidence that wheeled traffic generates such movements.

¹³ OIA 12397 responded to by Waka Kotahi 29/5/23.

¹⁴ OIA 12397 Attachment 1

¹⁵ Counting southbound from the left



Figure 11 OIA 12397 Attachment 5 - location of joint observed to move under pedestrian loading



Figure 12 a $\pm 50\text{mm}$ gap opening (left) and closing (right) on longitudinal joint between clip-on and main span. OIA 12397

In its current (un-damped) condition the OIA attachments refer to limiting pedestrian loadings on the bridge to 250 people per span on three of the approach spans (Figure 13) but not the main

centre span. The longest of these spans is 255m (Span 1) so this equates to 1 person every linear metre of the bridge.

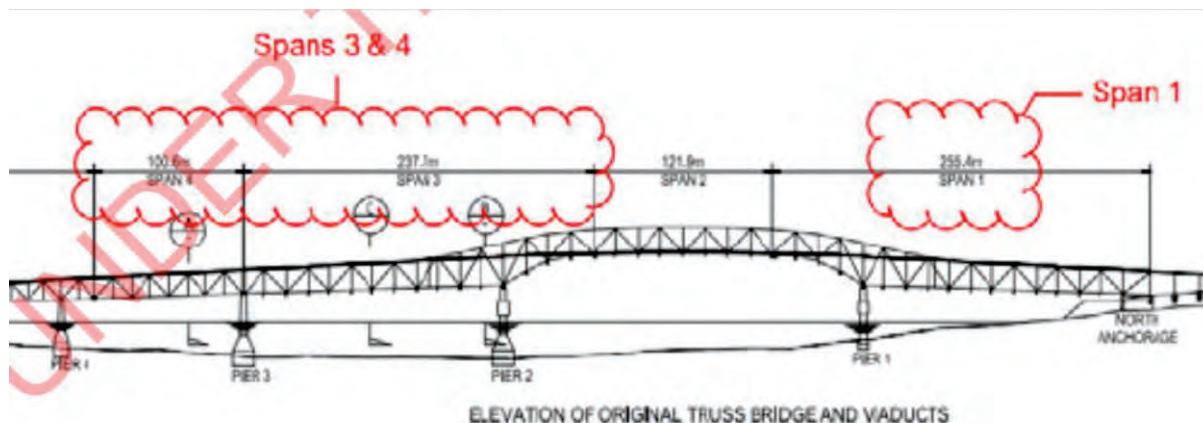


Figure 13 Spans 1, 3 & 4 identified for limiting pedestrian numbers

With a bridge length of 1,600m this would imply that if the pedestrian numbers were controlled that at least 1,600 people (evenly distributed) at any one time could walk across with no dynamic movements being triggered. Based on published estimates of daily usage this value of 1,600 exceeds the initial daily projection of pedestrian numbers. Therefore, the possibility that lateral movements could be generated on a routine day (commuting weekday) with a single walking and cycling lane are low.

To provide mitigation, real time people tracking and counting technology, combined with active (human) bridge wardens, a PA system on the active path (similar to that used in the Homer Tunnel on SH94) and remote control barriers to physically close the Shared Path path are all relatively simple measures that can monitor and if needed control pedestrian numbers on the bridge.

The dynamic swaying effect is far more likely to occur during managed events when both lanes of a clip-on are open for mass pedestrian crossings of the structure. This does call into question the plan to run one-off events rather than an extended trial.

It should be noted that the 2010 report in the OIA proposes permanent fixes to the issue that do not affect traffic and effectively removes the issue. Given that Waka Kotahi have been aware that the AHB does attract (unauthorised) mass pedestrian events there may be sound reasons to install the proposed dampers irrespective of any walking and cycling lane.

Deflection of barrier during impact.

Waka Kotahi refer to a single traffic lane running between barriers as a 'bull run'. We consider that this is inappropriate terminology as it portrays the notion of routinely 'out of control' vehicles bouncing between barriers. Instead, we have used the term 'constrained single lane running' where, under normal circumstances, a vehicle is freely driven on the road between barriers, but the barriers are able to provide side protection if required.

The potential ingress of the free-standing HV2 barrier into the Shared Lane during an impact is cited as a significant risk. Barriers are typically tested at impact angles of 5°, 10° and 15° and masses including 2.7T and 10T and speeds of up to 90km/h (Table 1, Table 2).

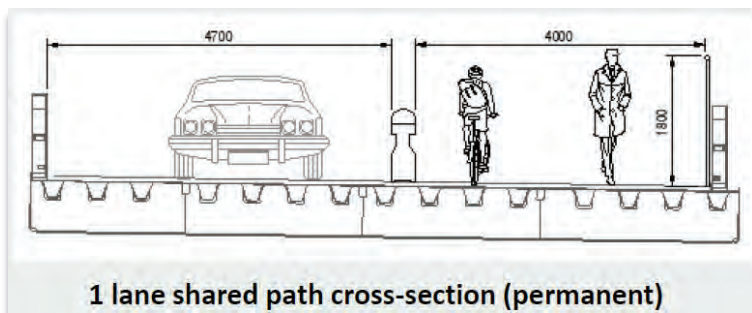


Figure 14 Schematic 'Option 3' cross section, 4.7m lane width maximum.

The impact angle of a long heavy vehicle running in constrained single lane to impact is governed by its speed (the higher speed the smaller the impact angle), the width of the lane and vehicle and the vehicle's dimensions. Figure 15 provides an approximate assessment of the maximum likely impact angle of the Auckland Transport bus running in a 4.7m wide constrained single lane. The impact angle is significantly less than the 5° impact angle in Table 2 which has a 0.71m deflection (0.24m for 2.4T vehicle, Table 1).

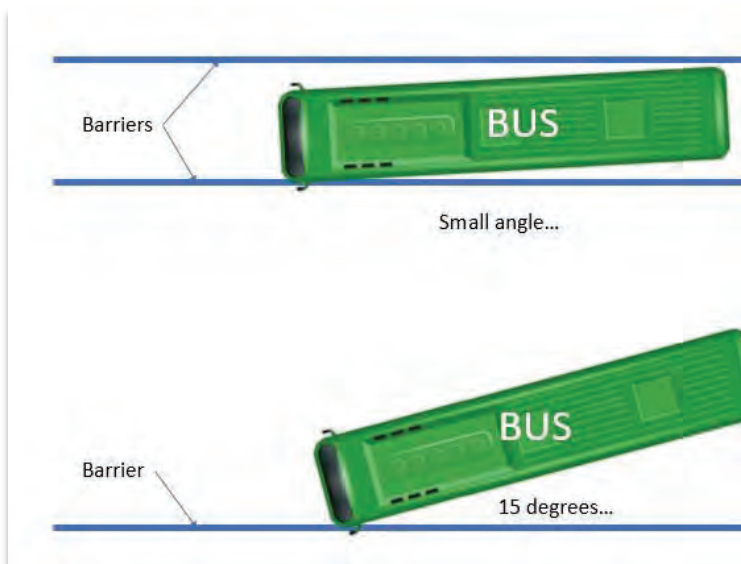


Figure 15 Restricted impact angle of vehicle in constrained single lane

Table 1 TL-3 deflections for 2.27T vehicle

TL-3 2,270kg VEHICLE					
Speed	25°	20°	15°	10°	5°
100km/h	1.47m	1.18m	0.88m	0.59m	0.30m
90km/h	1.33m	1.07m	0.80m	0.54m	0.27m
80km/h	1.18m	0.95m	0.71m	0.48m	0.24m
70km/h	1.03m	0.83m	0.62m	0.42m	0.21m
60km/h	0.89m	0.71m	0.53m	0.36m	0.18m
50km/h	0.74m	0.59m	0.44m	0.30m	0.15m
40km/h	0.59m	0.48m	0.36m	0.24m	0.12m

Table 2 TL-4 deflections for 10T vehicle

TL-4 10,000kg VEHICLE			
Speed	15°	10°	5°
90km/h	2.37m	1.58m	0.79m
80km/h	2.11m	1.41m	0.71m
70km/h	1.85m	1.23m	0.62m
60km/h	1.58m	1.06m	0.53m
50km/h	1.32m	0.88m	0.44m
40km/h	1.06m	0.71m	0.35m

Any deflection would be affected by vehicle mass with a bus weighing up to 18 tonnes (T) and this barrier does not appear to have been subjected to a comparable full-scale testing.

The New Zealand supplier of HV2 barriers (CSP) were contacted (Figure 16) to provide their assessment of a low angle impact of an 18T rigid vehicle at 80km/h¹⁶.

Based on a 2.5° impact angle of this vehicle at 80km/h they advised that the energy in the impact would be 8.5kJ. This energy is less than 4% of the energy of a 10T vehicle impacting at a 15° angle (Table 2). Based on the energy of the impact, their assessment was that a deflection of 0.5m would be expected. They suggest adopting a more conservative nominal maximum displacement of 0.75m (equivalent to a 10T vehicle at 80km/h impacting at an angle of 5° - Table 2).

¹⁶ Email from CSP to SmartSense 9/8/22 which included the response from Saferoads in Australia.

Your Enquiry

I'm undertaking an assessment of methods to protect a shared path next to a single lane of (80km/h) traffic that will be running between two barriers 4.7m apart. This protection barrier cannot be anchored and I'm looking at your HV2 product. The length will be around 1800m at a gradient not exceeding 5%. As all vehicles would be running between barriers any impact angle would be small (<<5 degrees). Are you able to provide an assessment of the deflection of an 18T vehicle with an impact speed of 80km/h and impact angle of 2.5 degrees? Many thanks.

Figure 16 Enquiry for technical assistance from HV2 provider

Their response appears to be supported by the trend data (Figure 17) from TL-3 (2.7T) and TL-4 (10T) where an 18T deflection line is postulated. Whilst this graph should be treated with caution and would require a more detailed analysis, it suggests that an 80km/h impact of an 18T bus at a more realistic 2° impact angle would result in a deflection of around 0.5m.

Whilst any deflection would encroach into the Shared Path (and therefore not be ideal), a deflection of 0.5-0.75m would be broadly comparable with a 0.5m minimum clearance between cyclists and the barrier (Figure 5.7 in Austroads AGRD06A). There is still a small probability that a cyclist or pedestrian would be within the 0.75m deflection zone of any impact. The presence of any anti-climb barrier above the 0.9m HV2 barrier may encroach further into the Shared Path and would have the ability to harm people.

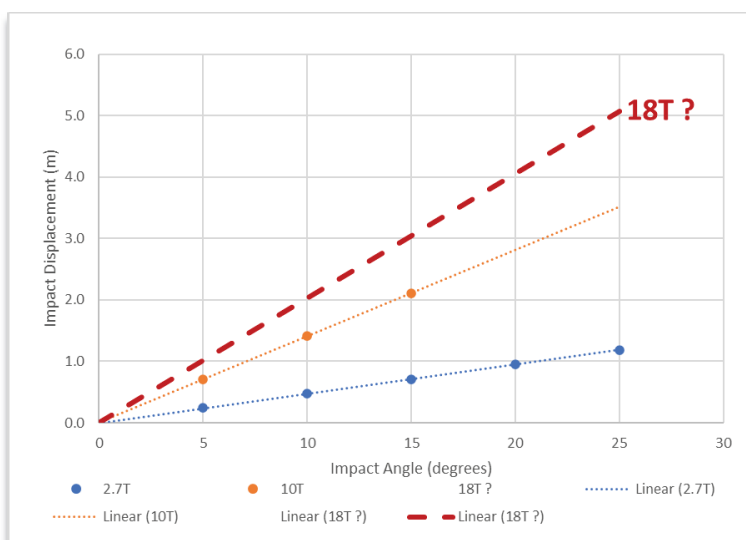


Figure 17 80km/h TL-3, TL-4 tests and potential extrapolated 18T test (based on factoring vehicle masses)

As a further mitigation against barrier deflection, there is no reason why a lower speed could not be posted on the constrained single lane on the southbound clip-on. A 60km/h limit appears to reduce barrier deflection by a further 25% for lower mass vehicles.

Whilst it is accepted there is a residual risk, this risk compared favourably to the risk on of many kilometres of other roads in Auckland with >60km/h traffic where pedestrians/cyclists are left totally unprotected.

By comparison the AHB HV2 barrier would appear to offer significantly higher protection than those other environments.

Bus Stability

Waka Kotahi¹⁷reference the risk of Northern Express busses suffering from ‘yaw-steer’ and cite this as posing an impact risk with the barrier or bridge superstructure (truss). Auckland Transport have been approached to provide a response to Waka Kotahi’s stated concerns, but to date have not responded.

Shared Path Width / Edge barriers

The proposed AHB Shared Path would be 4m wide and cater for all active modes. It is a reasonable assumption that in a similar manner to road traffic that the flows will be tidal with more southbound movements in the morning peak, northbound in the afternoon peak. Figure 22 recommends a 3.5m wide path with a minimum clearance between the cyclists and barrier of 0.5m (Figure 5.7 of Austroads AGRD06A).

The proposed 4m path would appear (Figure 18) to be suitable to up to 600 cyclists and 200 pedestrians per hour. Whilst a detailed analysis of expected flows has not been performed this would appear to be within the expectations of usage for the AHB Shared Path. It is noted that these capacities significantly exceed the 850 cyclists / 100 peds a day in the Waka Kotahi Memo dated 18/2/22.

The Waka Kotahi Safe Systems Assessment (**SSA**)¹⁸ noted that a 0.9m high barrier between the Shared Path and traffic lane would not prevent a cyclist who collided with the barrier or another person from over-topping the barrier, and this is accepted. Waka Kotahi then go on to say the due to ‘extreme danger’ to cyclists that a 1.6m high barrier should be placed between the Shared Path and the traffic lane. This 1.6m height is 0.2m higher than Austroads references and indicates that Waka Kotahi appear to be taking an overly conservative approach to their assessment.

¹⁷ Auckland Harbour Bridge shared path safety assessment, Memo dated Feb 2022, released under OIA 10175.

¹⁸ Auckland Harbour Bridge shared path safety assessment, 18/2/22 NZTA Memo from Safety and System Performance, System Design

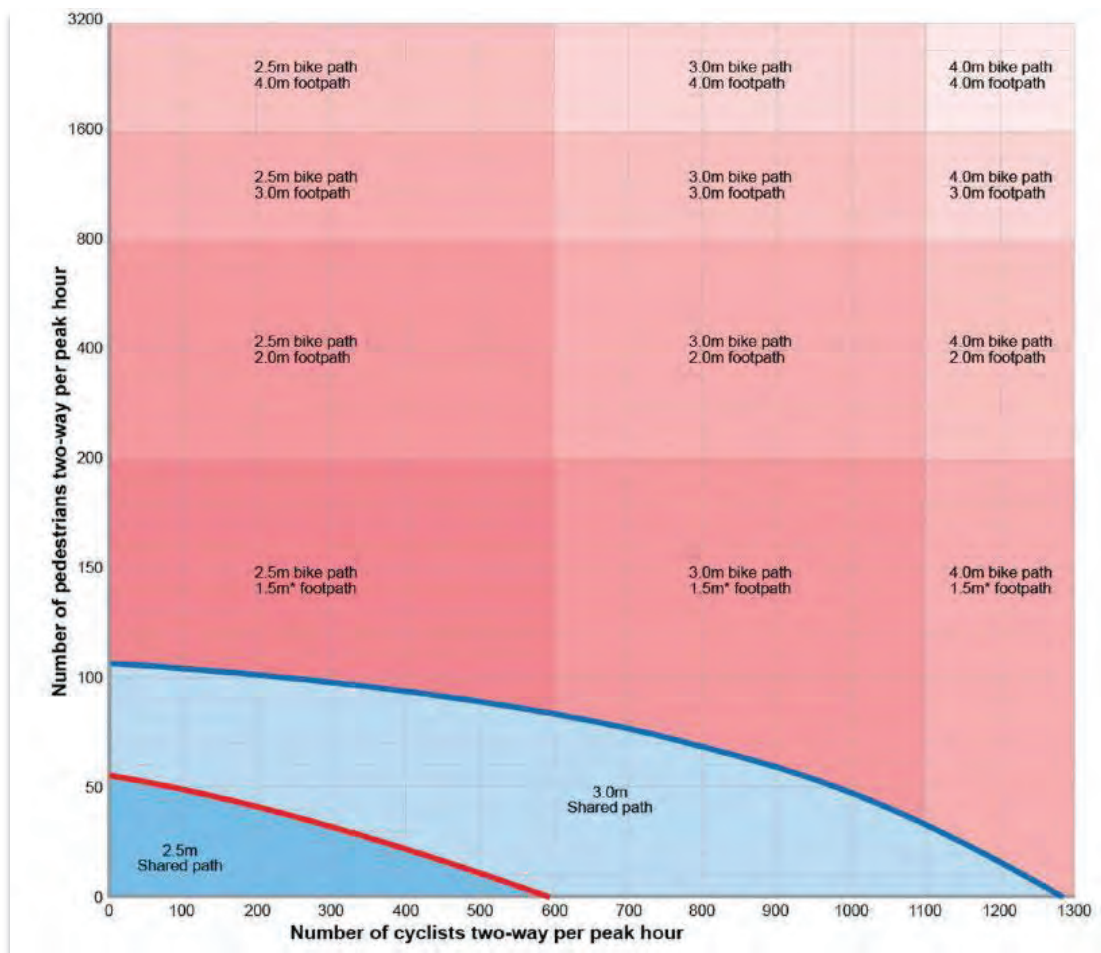


Figure 18 Austroads Figure 5.5 AGRD06A Path width for 75/25 directional split.

Gradient of Bridge

Waka Kotahi rank cyclist speed as a prime safety risk to the single Active Mode lane. It is recognised that there will be up to 700m of downhill 5% (not the 6% quoted by Waka Kotahi) and that there is the opportunity for cyclists to gather speed on these inclines. The 5% maximum gradient is not considered in the Austroads guide to be exceptionally steep and there are several other Waka Kotahi sanctioned / funded shared paths that are or comparable lengths and steeper. (Table 3)

Table 3 Schedule of shared paths with gradients

Location	Average Grade	Steepest Grade (over a min of 150m)	Length (m)	Height gain (m)	Comments	Direction	Reference
Auckland Harbour Bridge (northern approach)	4.5%	5%	990	45m			From Sulphur Beach path. Driven with barometric GPS
Auckland Harbour Bridge	4.3%	5%	930	40m			Assuming exit to Westhaven still 5m above ground level. Driven with barometric GPS
Grafton Gully	4.7%	8.3%	600	65 in 1400, 44 in 1000, 28 in 600	Shared path	2-way	Ridden with GPS
Wainuiomata, Wellington	9.1%		1,970	211m	Shared path	2-way	https://www.strava.com/segments/1131930
Te Ara Ki Uta Ki Tai	5.2%	10%	810	43m	Shared path	2-way	
Franklin Rd, Auckland	5.8%	8%	920	48m	Cycle path	1-way	https://www.strava.com/segments/6829833
Apirana Ave. Climb	4.4%	12%	1,020	42m	Shared path	2-way	https://www.strava.com/segments/1115273
Mount Eden Summit Climb	3.4%	11%	1,310	88m	Shared path	2-way	https://www.strava.com/segments/3389538
Ngaio Gorge Rd, Wellington	4.8%	>20%	1,800	139m	road		https://www.strava.com/segments/7282974
Te Ahu a Turanga	8.4%	10%	1,780	149m	3.5m shared path	W>E	OIA 10639
Te Ahu a Turanga	8.0%	9%	2,780	222m	3.5m shared path	E>W	OIA 10639

In the Grafton Gully example (Figure 19), a 600m section (from 0.4km to 1.0km) rises some 28m a slope of 4.7% with the steepest section rising 15m over 180m a slope of 8.3%.

The overall climb/ descent is comparable, and the maximum slope is significantly steeper than the AHB. We are not aware of any concerns raised about cyclist speed nor any proposals to close this shared path for safety reasons, nor are we aware of any major concerns raised by Waka Kotahi about cycling speeds or glancing/ head on collisions elsewhere on the cycling network.

Waka Kotahi cites just one example of a known collision (non-fatal) across the Auckland cycle path network. On this basis, we do not consider that Waka Kotahi is consistently applying the same assessment criteria to the AHB Shared Path as it has on other Waka Kotahi funded paths.

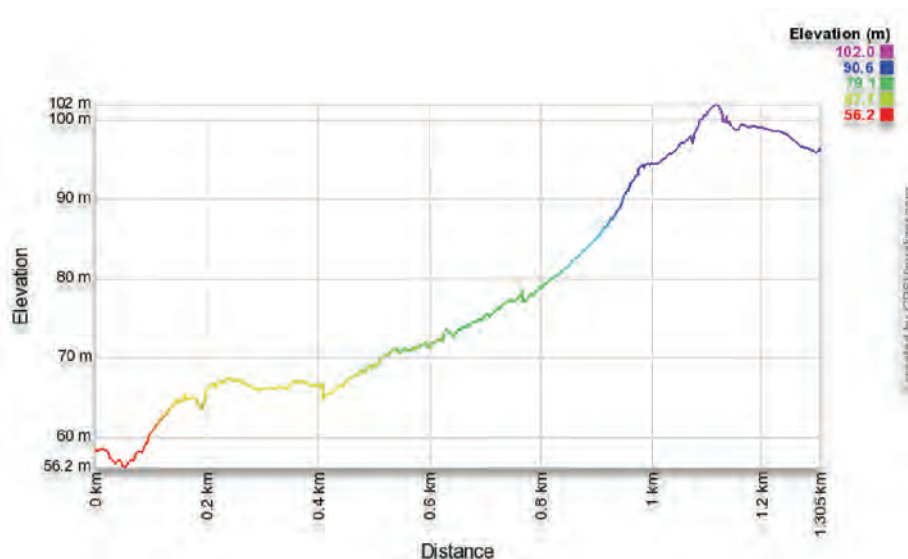


Figure 19 Grafton Gully Shared Path - Long Section, recorded with GPS and altimeter.

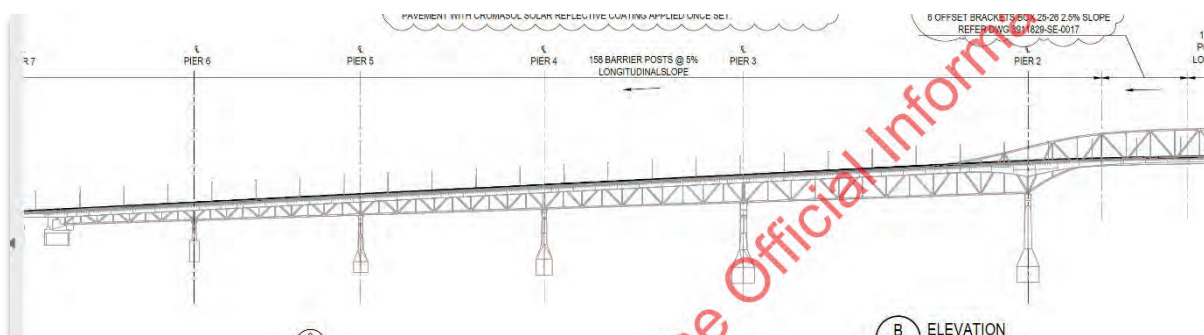


Figure 20 Southern Approach Spans on AHB - max gradient 5% released under OIA 9736

Based on Table 3 it appears that Waka Kotahi is applying a more onerous level of assessment on the AHB Active Mode path than they apply elsewhere. As the AHB Active Mode lane is the only example where a State Highway traffic lane is proposed to be repurposed for Active Modes then the mode neutrality of Waka Kotahi's approach must be questioned.

If the same approach in the Waka Kotahi SSA document was applied to other paths in Table 3 then it is unlikely that any of them would have been funded or constructed, yet they all were. To apply a

different threshold assessment to repurposing a lane of the Auckland Harbour Bridge appears to be inconsistent and appears to show bias or a significant logic failure¹⁹.

The most contemporaneous Shared Path that Waka Kotahi is currently constructing is Te Ahu a Turanga- Manawatū Tararua Highway²⁰. This is a 3.5m wide chip sealed shared path that follows the new highway that is under construction. The area also contains a large wind farm, so is by definition -prone to wind, Waka Kotahi have advised that **(bold added)**:

*“During the consenting process for Te Ahu a Turanga, Waka Kotahi NZ Transport Agency was instructed to provide a shared use path. As the shared path is a requirement **a risk assessment in relation to wind has not been undertaken.**”*

Table 4 summarises the path’s Chainage (distance from start) and Elevation and provides incremental and overall lengths and gradients.

Table 4 Gradient data for new Shared Path Te Ahu a Turanga - Manawatū Tararua Highway

Chainage (m)	Elevation (m)	Distance (m)	Height Change (m)	Gradient (%)
21880	93.783			
22180	121.228	300	27.445	9.1%
22260	123.264	80	2.036	2.5%
22940	183.273	680	60.009	8.8%
23240	203.63	300	20.357	6.8%
23540	232.344	300	28.714	9.6%
23660	242.574	120	10.23	8.5%
Total		1780	148.791	8.36%
28300	315.843			
28680	295.79	380	-20.053	-5.3%
29280	244.517	600	-51.273	-8.5%
29880	191.908	600	-52.609	-8.8%
30480	142.303	600	-49.605	-8.3%
30800	114.128	320	-28.175	-8.8%
31080	94.169	280	-19.959	-7.1%
Total		2780	-221.674	-7.97%

The shared path contains two long inclines with gradients in excess of 9% and *a sustained average gradient of 8.4% over 1.78km and 8.0% over 2.78km*. These two inclines are both significantly longer and steeper than either side of the Auckland Harbour Bridge which has a steepest incline of 5% over distance of under 1km.

There is a clear inconsistency in Waka Kotahi’s assessment whereby they have assessed a high likelihood of cyclists causing deaths and serious injuries on the Auckland Harbour Bridge which is

¹⁹ An OIA on the new Te Ahu a Turanga – Manawatū Tararua Highway linking the Manawatu to Hawkes Bay has been lodged to enquire about shared path protection, gradient and wind risk.

²⁰ Alignment and cross section information released under OIA 10639.

both significantly shorter and flatter, albeit busier, than the Te Ahu a Turanga Shared Path. The magnitude of this inconsistency indicates either that the Auckland Harbour Bridge assessment is overly pessimistic or that the Te Ahu a Turanga Shared Path has not been designed to meet Waka Kotahi's own high standards. We consider that Waka Kotahi's Auckland Harbour Bridge assessment is overly pessimistic.

Speed of Cyclists

Waka Kotahi's Safe Systems Assessment concludes that the length and gradient of the AHB Shared Path will be:

- Highly likely to result in head-on cyclist collisions significantly in-excess of 60km/h with a high potential (ranked 3 out of 4) of death or serious injury.
- Highly likely to result in cyclist vs pedestrian collisions of up to 60km/h with the highest potential (ranked 4 out of 4) of death or serious injury.
- Likely to result in sideswipe collisions between cyclists and e-scooters/skateboarders.

These statements are however based on an incorrectly assumed 6% gradient (Figure 21) and flows of 450-850 cyclists and 100 pedestrians a day. These flows appear to be exceptionally low and may account for Waka Kotahi assuming that the Shared Path is effectively empty for extended periods. The Austroads Guidance²¹ seeks to limit gradients to 5% (which is the steepest gradient on the AHB) and provides an indicative commuting (tidal flow) cross section (Figure 21) that could be comfortably accommodated within the available 4m AHB Shared Path corridor.

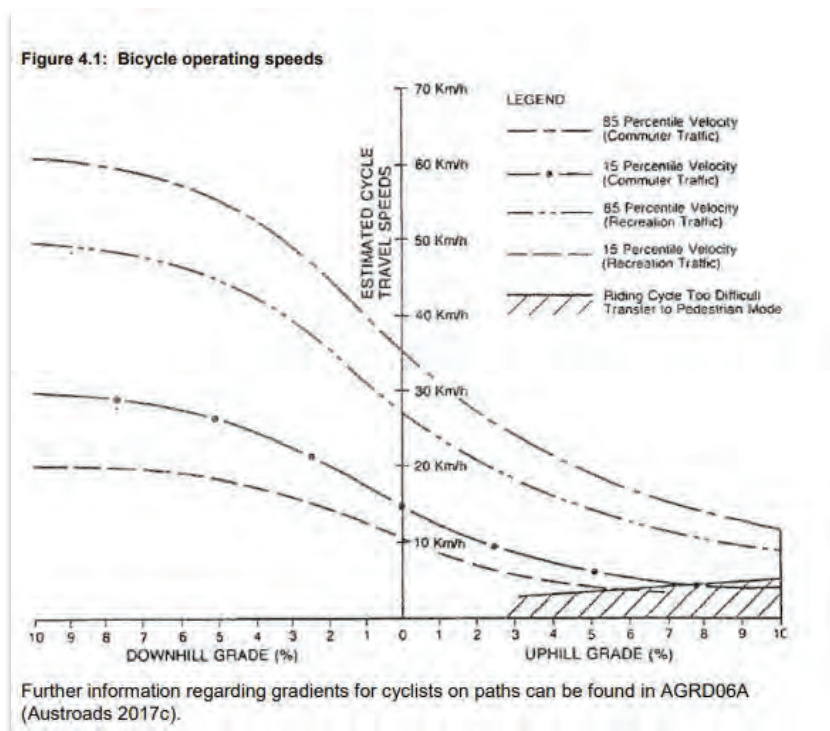


Figure 21 Austroads bicycling operating speeds

²¹ Austroads AGRD06A-17 2021.

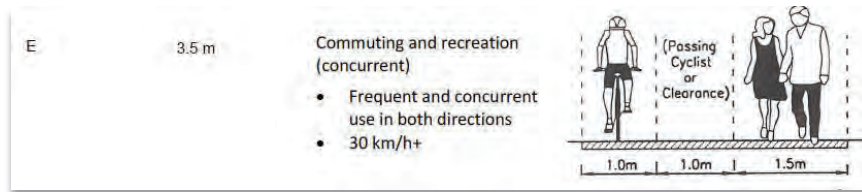


Figure 22 Austroads Guide AGRD06A-17 Shared path Figure A2 (commuting)

There appears to be a perception within Waka Kotahi that people on bicycles will ride at the maximum possible speed across the AHB, irrespective of the presence of other users. We consider that this approach is overly simplistic and may have resulted from them grossly under-estimating the path's expected usage.

To garner the views of cyclists on their actual cycling behaviour and likely speeds on a 700m shared path with a 5% downhill gradient, we posed some general questions to the NZ Cycle Action Network Facebook Page. Whilst not a scientific survey, it did result in over one hundred and sixty contributions. The consistent messages that the responses provided were:

1. **Speeds in excess of 50kph are readily achievable on downhill gradients,**
2. **These speeds were almost exclusively recorded on roads or occasionally empty shared paths,**
3. **No-one reported reaching high speeds on shared paths with mixed users,**
4. **People ride to the conditions (weather, other users),**
5. **People ride within their abilities,**
6. **People on cycles are generally conscious of the risks to themselves and others,**
7. **Some support for speed limit with a degree of enforceability,**
8. **Numerous steeper more challenging shared paths were cited,**
9. **People on cycles tend to self-regulate themselves and their peers,**
10. **Downhill speeds of 30kph on busy shared paths were generally referenced.**

The SSA²² approach taken by Waka Kotahi appears to be based on a range of overly pessimistic assumptions that take little account of the shared path expected user volumes and users reacting responsibly to the environment. In particular, we consider that the use of the (incorrect) 6% gradient and Waka Kotahi's belief that 15% of cyclists will routinely travel faster than 60km/h simply does not reflect reality.

Waka Kotahi could equally apply the same SSA approach they have used to assess the risk to Active Mode users on the AHB to Active Mode users on other infrastructure. Whilst there are a few recorded collisions between cyclists and/or pedestrians, there are far more frequent deaths of cyclists hit by motor vehicles. If the same SSA approach used to assess the risks to Active Mode users on the AHB were applied in all other locations, then Waka Kotahi would only permit walkers and cyclists in fully segregated and protected corridors, separate from motor vehicles. This is neither achievable nor realistic.

By way of comparison, the speeds of over 5,700 bicycles were recorded in a recent survey in the Waikato²³ on a flat segregated cycle path (Figure 23). This showed median speeds (50th percentile) of 18km/h, and 85th percentile of 28km/h, with no recorded speeds in excess of 40km/hr greater.

²² Auckland Harbour Bridge shared path safety assessment, 18/2/22 NZTA Memo from Safety and System Performance, System Design released under OIA 10175.

²³ Cambridge separated cycleway 27 Hamilton Rd. Undertaken by SmartSense for Waipa DC. 2022.

Table 5 indicates that whilst the Austroads value appear consistent with the Cambridge survey for the slowest 15% of cyclists (mix of commuter and recreation) that the Austroads threshold speeds (27/36km/h) for the fastest 15% of riders are significantly faster (23km/h) than those recorded in Cambridge. This may suggest that the Austroads graphs (Figure 21) may be too high for the New Zealand environment.

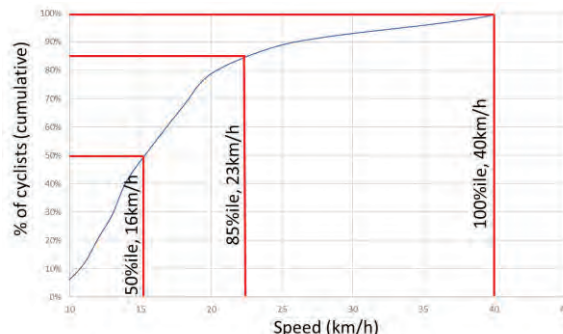


Figure 23 Distribution of cycle speeds - Hamilton Rd. Cambridge (Recorded in Lane 8 & 9)

Table 5 Comparison of Austroads and Actual cyclist speed recorded in Cambridge

% Sample (Cumulative)	AustRoads (Recreation/Commute)	Recorded in Cambridge
15%	10/14	12
50%	-	16
85%	27/36	23
100%	-	40

Weather Effects

There have been several well-publicised incidents where severe weather has closed or restricted the AHB. There are no options being proposed for providing Active Mode path users with wind or rain protection and due to the bridge loading constraints, we understand that this is not possible.

Waka Kotahi currently pro-actively advise motorists on the weather conditions ahead and the potential to restrict or close the AHB on occasions. This advance warning is important as traffic could be tens of kilometres away from the bridge and unaware of the local conditions around the AHB. This need for significant advanced warning is less important for Active Mode users, as unlike people in motor vehicles, pedestrians, cyclists and those using active modes are likely to be closer to the AHB and make self-informed decisions on whether to travel to the bridge.

It is accepted that there will be occasions (potentially at short notice) when due to high winds / exceptional rain that the Active Mode lane is closed by Waka Kotahi for safety reasons. We suggest that any decision on closure be based on the ability to safely walk (or push bicycles) across the AHB. Cyclists are generally able to make their sensible decisions whether to ride or push their bikes, especially if they have made a commitment reach the bridge.

Provision of wind related data from Waka Kotahi

Waka Kotahi have provided wind data²⁴ relevant to Auckland Harbour Bridge. The OIA request was for the last ten years of average and gust wind records (speed and direction) at fifteen-minute intervals. They were also requested to provide any risk assessment information over the last five years relating to people walking and cycling across the Auckland Harbour Bridge.

The only data that Waka Kotahi held was from the Met Service and was recorded at 67m above sea level, which is understood to be on the top of the bridge arch. This elevation is between 20-60m higher than the road deck.

The data provided only covers the wind records over 903 ten-minute periods across eleven years – this is around 0.15% of the requested period. The data provided relates only to occasions where wind gust speeds exceeded 75km/h and provides the gust speed and direction and the average speed/ direction over the preceding ten minutes.

Waka Kotahi has also advised that:

“Waka Kotahi NZ Transport Agency has not undertaken an assessment of wind risk to people walking and cycling across the Auckland Harbour Bridge.”

Analysis of wind related data

It is accepted that a wind gust of 75km/h (strong gusts) would make cycling a challenge; in the absence of any other Waka Kotahi data, this analysis will use that value as the speed where any cycling would cease. Walking (or pushing a cycle) is likely to be possible in higher windspeeds.

A summary of the high windspeed occasions is shown in Table 1. If the incomplete 2022 and night-time data (20:00-06:00) are excluded there are 643 occasions of strong gusts over eleven years.

²⁴ OIA 10638 August 2022 available on FYI.org.nz

Table 6 Number of 10-minute periods where wind speeds at 67m above sea-level exceeded 75km/h (strong gusts)

Year	06:00-20:00	20:00-06:00	Total
2011	97	25	122
2012	59	17	76
2013	47	12	59
2014	103	59	162
2015	28	14	42
2016	55	46	101
2017	59	12	71
2018	42	15	57
2019	84	30	114
2020	38	5	43
2021	31	12	43
2022	12	1	13
Grand Total	655	248	903

The data provided includes both direction and speed and these can be resolved into head/tail wind and crosswind components. Cyclists are well aware that a head/tail wind affect speed but rarely stability, stability is most affected by the crosswind component. This is shown in Figure 24

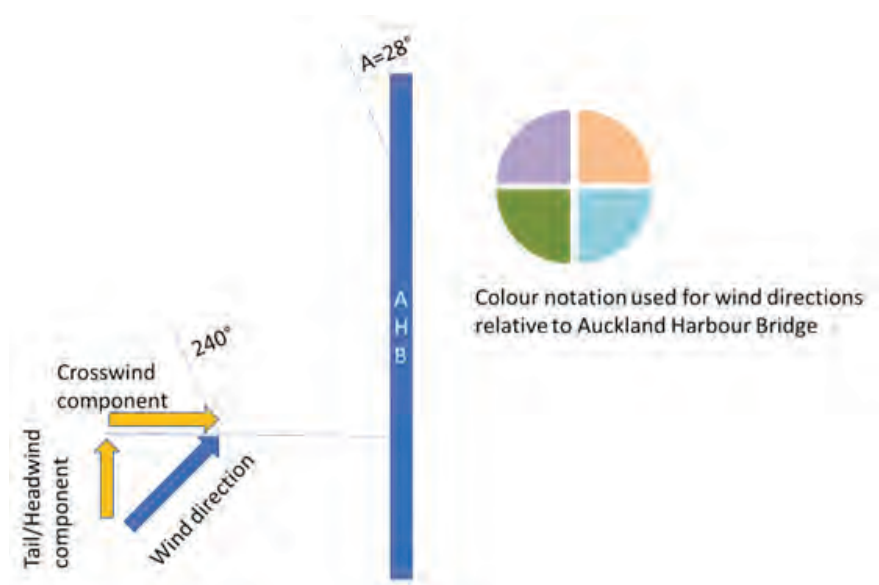


Figure 24 Schematic of wind analysis, Auckland Harbour Bridge alignment is 28° from North, Wind Direction resolved into Crosswind and Tail/Headwind elements.

As Waka Kotahi have already stated, the predominant wind direction is from the western side and the provided data (Figure 25) indicates that 83% of gusts were from the western side of the bridge, with only 17% of gusts were from the eastern side of the bridge.

Of this 83%, 40% are from directions that are more than 30° to the bridge and therefore more likely to affect cyclists. At smaller angles the crosswind component will be significantly reduced.

Any shared path is unlikely to have specific wind protection the fact that it would be on the eastern clip-on and the overwhelming direction of the gusts are from the west is significant. The bridge

superstructure is likely to provide a degree of passive wind sheltering to a path on the eastern shared path. This will be affected by the bridge structure aerodynamics and without testing and monitoring at road level the actual effect of strong gusts is difficult to predict.

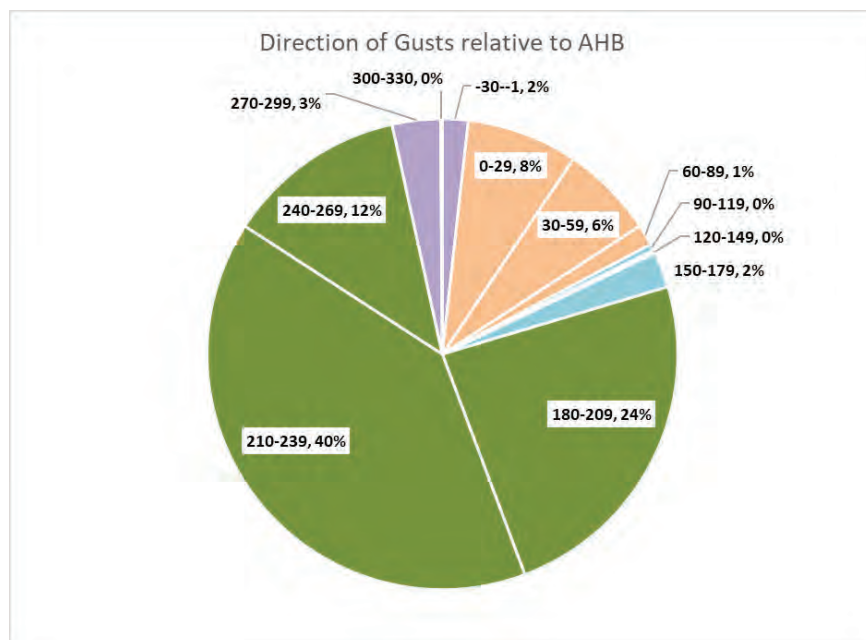


Figure 25 Frequency distribution of wind gusts 2011-2021

Cyclists naturally adapt to a steady crosswind or headwind and an annual headwind cycling event in the Netherlands²⁵ has recorded the wind conditions over a seven-year period. Whilst this is not being cited as a scientific survey it does indicate that windspeed alone is not the only factor to consider the ability to safely cycle and the direction relative to the cyclist is important.

Table 7 Wind Speeds during Dutch Headwind Cycling Championship

Wind Force (Beaufort number)	Description	Wind speed	Event Year
5	Fresh breeze	29–38 km/h	2013
6	Strong breeze	39–49 km/h	
7	High wind, moderate gale, near gale	50–61 km/h	2015, 2018, 2022
8	Gale, fresh gale	62–74 km/h	2014, 2020
9	Strong/severe gale	75–88 km/h	2016

²⁵ https://en.wikipedia.org/wiki/Dutch_Headwind_Cycling_Championships

A strong wind gust is more likely to have a destabilising effect on a cyclist than when its direction is significantly different to the preceding wind. Table 8 shows that on 51% of occasions the strong gusts reported are within 5° of the preceding wind and in total on 88% of occasions the gust is within 15° of the preceding wind direction. The fact that 88% of strong gusts are within 15° if the preceding wind direction makes it likely that cyclists would already be compensating for a significant crosswind. Therefore the times when a gust in excess of 75km/h occurs from an unexpected direction is only around 12% of recorded gust.

Table 8 Difference between average direction (10mins preceding) to Strong Gust

Difference of Average to Strong Gust direction (°)	Percentage of wind gusts
0-4	51%
5-9	25%
10-14	12%
15-19	5%
20-24	3%
25-29	1%
30-34	1%
35-39	1%

Figure 26 shows the distribution of crosswind components for the provided wind data. As would be expected the most frequent and strongest crosswinds are with the prevailing winds from the green (SW) quadrant. The yellow box highlights the percentage of records where the crosswind component exceeds 50km/h – a speed that is likely to make cycling challenging. Of the 600+ strong gust recorded 65% generate a crosswind component in excess of 50km/h representing around 40 occasions a year. The lack of wind data for over 99.8% of the time does prevent a more detailed analysis to be undertaken of the time when wind speeds above the bridge did not exceed 75km/h.

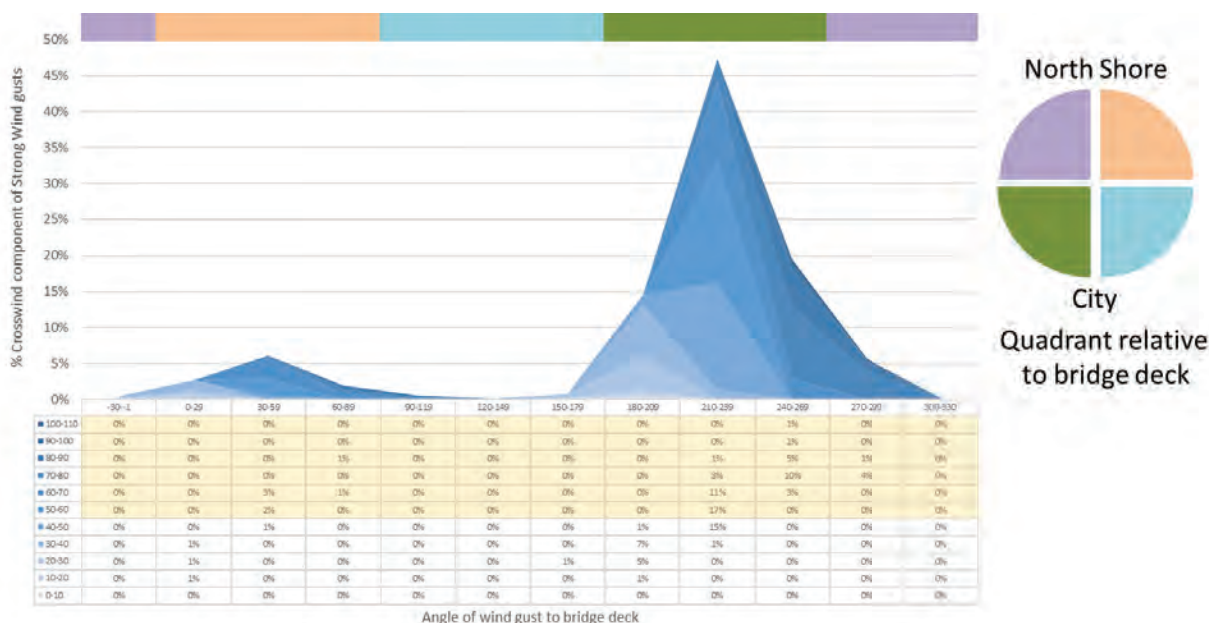


Figure 26 Crosswind component strength and direction distribution

Wind Profile

Waka Kotahi have advised that they have no wind speed data from bridge deck level, this lack of data makes any assessment of the actual wind profile of the bridge difficult to determine. The lack of data also makes it difficult for Waka Kotahi to make, or support, the statements that they have made around the risk of wind.

There are some theoretical relationships between winds speed and altitude with Figure 27 showing the gradient of wind across an open body of water. This can be considered likely to be an upper-bound solution and implies that at the bridge deck level (50m) the actual wind speed could be similar to that at 67m, reducing to around 60km/h as the road returns to ground level.

Applying a similar analysis, but assuming the bridge superstructure acts like a large town with high buildings, reduces the speed at 50m elevation to 70km/m and under 50km/h as the road returns to ground level.

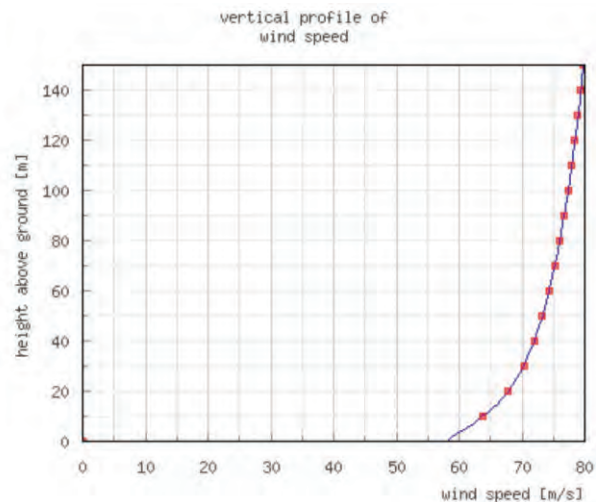
A reduction of wind speed at the bridge deck elevation is likely, but in the absence of any data being collected by Waka Kotahi we have not reduced the speeds to compensate for lower elevations. We consider this analysis to be a conservative approach.

Please specify parameters

height above ground	<input type="text" value="67"/> m	
wind speed	<input type="text" value="75"/> m/s	
roughness length z_0 (see table below)	<input type="text" value="0.0002"/> m	<input type="button" value="Refresh"/>

Result

height above ground	wind speed
150 m	79.75 m/s
140 m	79.34 m/s
130 m	78.91 m/s
120 m	78.44 m/s
110 m	77.92 m/s
100 m	77.36 m/s
90 m	76.74 m/s
80 m	76.05 m/s
70 m	75.26 m/s
60 m	74.35 m/s
50 m	73.27 m/s
40 m	71.96 m/s
30 m	70.26 m/s
20 m	67.87 m/s
10 m	63.79 m/s



Roughness Classes and Lengths

Figure 27 Wind Gradient profile across open water (75km/h at 67m)

It is accepted that gusts can occur at any time of year, but 65% of gusts occurred in the months of July-October.

Table 9 Distribution of wind gusts 2011-2021

Month	% of Gusts
Jan	3%
Feb	1%
Mar	5%
Apr	5%
May	7%
Jun	7%
Jul	18%
Aug	13%
Sep	16%
Oct	18%
Nov	6%
Dec	3%

Assessment of wind risk

By simply combining the analysis of frequency of gusts of greater than 75km/h we have make an assessment of the likelihood of unexpected high gust occurrences. The first assessment are gusts from the east.

- 58 gusts/year during daytime,
- Percentage of gusts from the least protected eastern side of the bridge 17%,
- Percentage of gusts where the wind shift is more than 15° is 12%

A simple estimate of the frequency of a gust of >75km/h, that is more than 15° different than the preceding wind from the side of the bridge least exposed to wind (easterly) is to multiply these values together, these equate to around once a year.

This figure does not reflect wind gusts from the west, these would undoubtedly effect a shared path on the east,

- 58/year during daytime,
- Percentage of gusts from the least protected western side of the bridge with at more than 30° from the bridge 40%,
- Percentage of gusts where the wind shift is more than 15° is 12%

A simple estimate of the frequency of a gust of >75km/h, that is more than 15° different than the preceding wind from the side of the bridge most protected from wind (westerly) is to multiply these values together, these equate to around three times a year.

The wind speeds provided are likely to be higher than those experienced on the bridge deck.

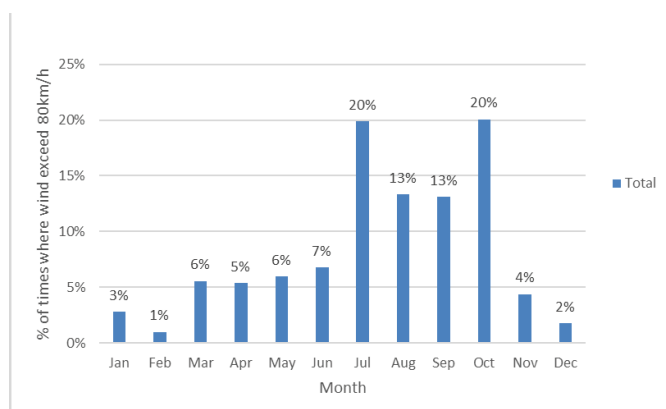
On the basis of the Waka Kotahi provided data there would appear to be a likelihood that unexpected winds gusts in excess of 75km/h may be experienced under five times in a year. On this basis it is not considered that unexpected or strong wind gusts would be a significant issue.

The wind data that Waka Kotahi have provided is incomplete and what has been provided is from an altitude significantly highest point of the bridge deck. On the data provided there could also be forty occasions a year when strong gusts could generate crosswinds in excess of 50km/h which may preclude cycling.

Waka Kotahi Safe Systems and Wind Assessments.

The SSA states that winds on the AHB reach 80km/h all year round, cycling in winds above 64km/h is 'impossible' and that a wind gust that occurred in September 2020 (resulting in a vehicle impacting the AHB and causing damage) occurred without any warning. However, the report also says that no assessment of weather risk has been made.

Table 10 Distribution of wind gusts > 80km/h



There appears to be a proven risk to the structural integrity on the bridge from trucks being blown over on the bridge. (per the structural damage of September 2020). If these winds are somewhat predictable then Waka Kotahi should be considering the banning of trucks from the AHB on days

where high winds are likely and made to use the existing alternative harbour crossing, namely SH16 and SH18 to Albany. If the winds are totally unpredictable, then allowing any truck to cross the AHB at any time appears to be unwise and potentially unsafe.

Other bridges where crosswinds are known issue have been equipped with windsocks to provide regular users with a simple but effective guide of wind strength and direction. The AHB does have prominent flags flying on the top of the structure, but these are 25m above the highest part of the bridge deck and designed to showcase the flags rather than specifically provide a visual cue of wind strength and direction. Figure 28 shows the windsock on the approach span of the Tacoma Narrows Bridge in Washington State, USA. It is recommended that similar windsocks be provided on the AHB approaches to assist Shared Path users (and drivers) assess the wind conditions on the AHB.



Figure 28 Windsock deployment to provide regular users with direction and strength information (Tacoma Narrows Bridge, Washington State, USA)

In addition to the provision of windsocks on each approach to the AHB it is considered sensible to provide full height automatic gates at either end of the Shared Path to facilitate occasional closures. At 1,600m long, most people could leave the bridge in under ten minutes on foot. On occasions, where conditions deteriorate rapidly there may need to provide some assistance by staff. The behaviour of users and Waka Kotahi to weather conditions on the AHB would be a key part of a twelve-month trial.

Maintenance / Emergency Access and Assistance

The AHB Active Mode path will be unique in New Zealand and as such Waka Kotahi could provide trained staff (Wardens) to assist users and ensure the safety of users on the Shared Path.

Fortunately, there is already a specialist staff base at the northern access point to the path which is co-located with the NZ Police.

The path will be 4m wide, so suitable for small maintenance vehicles (e.g. Polaris type vehicles) or purpose built double ended utilities (Figure 29), e-bikes, walking or e-scooter equipped Wardens and maintenance teams. Wardens can also be trained in first aid, suicide risk assessment and counselling. The AHB can have local by-laws enacted to place provisions on active mode users to limit their speed and empower Wardens with specific authority. We recognise that the provision of trained staff (Wardens) to interact with users of the Shared Path is not a task that Waka Kotahi provide elsewhere in Aotearoa, but given the unique characteristics of the AHB Shared Path we suggest they are warranted here.

Any vehicle incidents on the constrained single lane running lanes on the AHB will need to be managed effectively. Waka Kotahi and their contractors have existing experience with constrained single lane running on the AHB, so no significant new issues are anticipated.

The provision of a single southbound lane on the clip-on between two barriers makes a major impact (high angle / high speed) between a vehicle and the AHB barrier / superstructure less likely. The conversion of the outer clip-on traffic lane to be a shared path and the addition of an extra barrier would also provide additional protection to the outer barrier of the clip-on. We are aware that there is already barrier strengthening work underway on this barrier so further protection should be beneficial.

On the occasions where emergency services need to be attended incidents there are numerous transport options for first responders (Figure 30) with storage at the NZ Police station near Sulphur Bay. For more significant incidents, there is the capacity to utilise the adjacent traffic lane, or the 4m wide evacuated Shared Path.

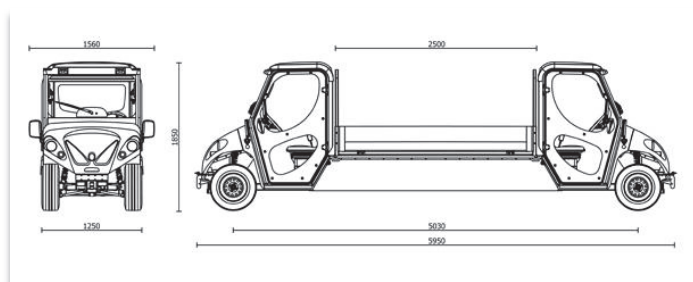


Figure 29 Specialised double ended maintenance vehicle



Figure 30 Restricted access maintenance and emergency vehicles

Summary - Potential mitigations to safety items raised by Waka Kotahi

Waka Kotahi appears to have predetermined that converting the outer southbound clip-on on the AHB to be a Shared Path is not something that it wishes to provide. It has cited safety concerns as the prime reason. We consider that, rather than seek practical reductions to some of the risks, Waka Kotahi's SSA approach considers several unrealistic worst-case scenarios and applies a level of conservatism that does not appear to be used on other projects. As such there is a significant likelihood that Waka Kotahi is applying unconscious institutional bias against enabling active mode travel on the Auckland Harbour Bridge.

Effective controls exist to reduce inherent risks to an acceptable residual level (most notably with the imposition of a 30 km/h speed limit in the Active Mode lane through by-laws and wardens). Waka Kotahi has overlooked such controls.

Converting one traffic lane on the Auckland Harbour Bridge to Active Modes introduces both risks and opportunities, and by focusing only on the inherent risks (before controls) of Active Modes, Waka Kotahi ignores the significant opportunities for social inclusion, improved health, financial betterment, commuter time savings and more for the many people that would use the Active Mode lane. A more balanced approach than is clearly more appropriate and useful.

There are safety related matters that remain to be addressed and below we have listed some pragmatic and reasonable mitigations.

Barriers

- There is no clear evidence that Waka Kotahi has made a detailed assessment of the lighter free-standing HV2 interlocking barriers that are available and approved²⁶ by them. This assessment must be undertaken. The assessment would need to specifically consider their use on a constrained single lane running environment with greatly reduced impact angles.
- Outer-edge anti-climb barriers have been shown to be structurally viable and whilst not eliminating the risk, a 2.1m high wire strand barrier has been empirically shown to reduce the likelihood of suicides in situ by 50%.
- A 1.6m high mesh barrier between traffic and the Shared Path appears to 0.2m higher than referenced on Austroads and would provide a reasonable anti-climb and screening barrier.

²⁶ NZ Transport Agency, Austroads Safety Barrier Assessment Panel (ASBAP) and specified in M23 – Appendix C, the HV2 BARRIER.

- Auckland Transport has been asked to respond to Waka Kotahi's stated concerns about the ability of their double decker busses to cause damage to the bridge or barriers.

Gradient and Width

- The AHB Shared Path is no steeper or longer than numerous other shared paths that Waka Kotahi have provided, some delivered as recently as 2022. Another shared path in construction by Waka Kotahi is nearly twice as steep and twice as long.
- The 4m width Shared Path has the capacity for at least 800 people/hour in a commuter environment, including 600 on bicycles.

Dynamic behaviour

- Waka Kotahi have identified a potential safety and comfort issue when two clip on lanes are used for mass pedestrian events.
- The safety issue related to a 50mm gap that rapidly opened and closed between the clip-on and original bridge that could severely crush anything inserted into it.
- The comfort issue was the lateral (sideways) sway generated by people subconsciously walking in step.
- In 2010 a relatively simple mitigation measure was identified (dampers) that could be installed with no impact on the bridge use.
- The one lane Shared Path proposed is remote from the gap and not an issue.
- The swaying is generated by pedestrians walking, not cyclists/scooters and using Waka Kotahi's undamped bridge data the structure could accommodate the total daily predicted pedestrian numbers at any one time. (1,600)

Shared Path Cycling Speeds

It is accepted that there is the potential for cyclist impacts on other Active Mode users. The SSA appears to make no allowance for users moderating their riding based on environmental and human factors - these factors include other users, rider experience and the weather. Waka Kotahi's assumptions on excessive speed were not supported when regular cyclists were consulted.

The risk of excessive speed and impacts cannot be fully ruled out so several options to reduce the risk are considered:

- Use of speed monitoring and enforcement. This may provide both official and peer enforcement of a proposed 30kmh speed limit that can be added as a Bylaw on the AHB. We consider that this approach is practical and likely to provide a safe and controlled environment for all users of the AHB Shared Path.
- Staged launch with cycles first and pedestrians and micro-mobility added later. This approach would provide for a staged launch of shared path so allow cyclists to familiarise themselves with the new path. This is also unlikely to meet the predicted demand of micro-mobility and pedestrians and could lull people on cycles into not expecting other modes – it is not a preferred option.
- Cycles only, this can be catered for without any legislative change (cycles can be permitted on motorways). The downside of this approach would be to encourage fast cycling in the knowledge that they were only cyclists present – albeit in both directions. This is also unlikely to meet the predicted demand of micro-mobility and pedestrians – it is therefore not a preferred option.
- Cycles only – tidal flow. It is reasonable to assume that there will be a predominant southbound flow in the morning and northbound in the evening. A tidal flow would remove

the risk of head-on and side-swipe collisions would encourage higher speeds. This is also unlikely to meet the predicted demand of micro-mobility and pedestrians and people cycling 'against the flow' – it is not a preferred option.

Staffing, Maintenance and Emergency Access

There are numerous operational access requirements that need to be addressed to provide access for emergency services:

- Pre-positioning customised maintenance and emergency vehicles at the NZ Police station by the northern access point to the shared path.
- Modify existing Waka Kotahi processes for managing vehicle incidents in constrained single lane running environments.
- The provision of trained staff (Wardens) to assist users, attend incidents and identify and intervene with any potential suicide attempt would be a prudent investment. These staff (minimum of 2 on duty when the Shared Path is open) would also have powers to control speed and other anti-social actions and summon assistance as required.

Severe Weather

It is accepted that on occasions the Shared Path will need to be closed due to severe weather.

- Some data on frequency and predictability of severe weather events has been provided by Waka Kotahi.
- The data provided indicates that on around 40 occasions a year cross winds would exceed 50km/h.
- The data also indicates that unexpected string gusts (>75km/h) that are likely to affect cyclists may be experienced up to five times a year.
- A need to enable users to make informed decisions about the likelihood of the path closing.
- Facility to prevent access to and then manage the exit of people crossing the bridge.
- The addition of windsocks to provide visual cues of weather conditions to regular users.

The data provided by Waka Kotahi indicates that, as expected there will be some occasions when cycling is not possible, but the level of unpredictability and frequency appears to be far lower than Waka Kotahi state. Waka Kotahi have stated that no risk assessment has been undertaken on the effect of wind on pedestrians and cyclists on the bridge.

Overall, we consider that a cross-section similar to that shown in Figure 31 would be a pragmatic, cost effective and realistic initial design for the Auckland Harbour Bridge Shared Path.



Figure 31 Proposed cross-section of Auckland Harbour Bridge Shared Path to address Waka Kotahi safety concerns (1.8m high people used for reference)

Richard Young

Chartered Engineer
Member of the Institution of Civil Engineers
Master of Business Administration
Bachelor of Science

Managing Director
SmartSense Ltd
June 2023

Assessment of traffic flows on Auckland Harbour Bridge 2013-2023

June 2023



Prepared for



SmartSense
people driven intelligence

Executive Summary

Bike Auckland (Client) commissioned SmartSense Ltd to undertake an analysis of the available traffic count data for the Auckland Harbour Bridge (AHB) to assess the what the impact of re-purposing one lane from vehicular traffic to active modes (walking / cycling / personal mobility) (Active Lane) would be on historic traffic volumes.

The AHB generally operates with a 5+3 Lane arrangement at peak times with the extra capacity southbound in the morning and northbound in the evening. The reassignment of a traffic lane to active modes would therefore reduce the capacity from 5 lanes to 4 lanes.

An Active Lane is understood to be a 4m wide shared path protected from traffic for people who choose to walk or run and safely ride bicycles or scooters up to 30km/h. The analysis used the available data to determine how a 4+3+1Active Lane bridge configuration would have coped with the actual traffic flows recorded from 2013 to date. As such, this was an analysis of historical data to assess the hours, days, months, and year that the AHB could (or could not) have carried an Active Lane.

The 3, 4, and 5 Lane hourly traffic capacities used by Waka Kotahi to assess traffic flow have been demonstrated to be conservative and flows of up to an additional 200 vehicles per hour for 4 Lane arrangements have been achieved. 2022 (Jan – July) data provided by Waka Kotahi demonstrates that 4 Lanes have been adequate for carrying all peak traffic flows during that period. 2023 summary

The average quarterly peak period traffic flows (Figure 1) from 2013 to 2020 (pre-lockdowns) show steadily reducing traffic volumes towards a flow that could be accommodated in 4 lanes, rather than the 5 provided. The (3 month rolling) average southbound morning flows could have been accommodated in 4 Lanes (rather than 5) from 2018 onwards and northbound the same trend achieved close to a 4 lane capacity in late 2019.

Post lockdown there has been a slow recovery in 2021 to levels well within the capacity of a bridge that has a lane dedicated to people choosing to walk or ride cycles and scooters. 2023 data

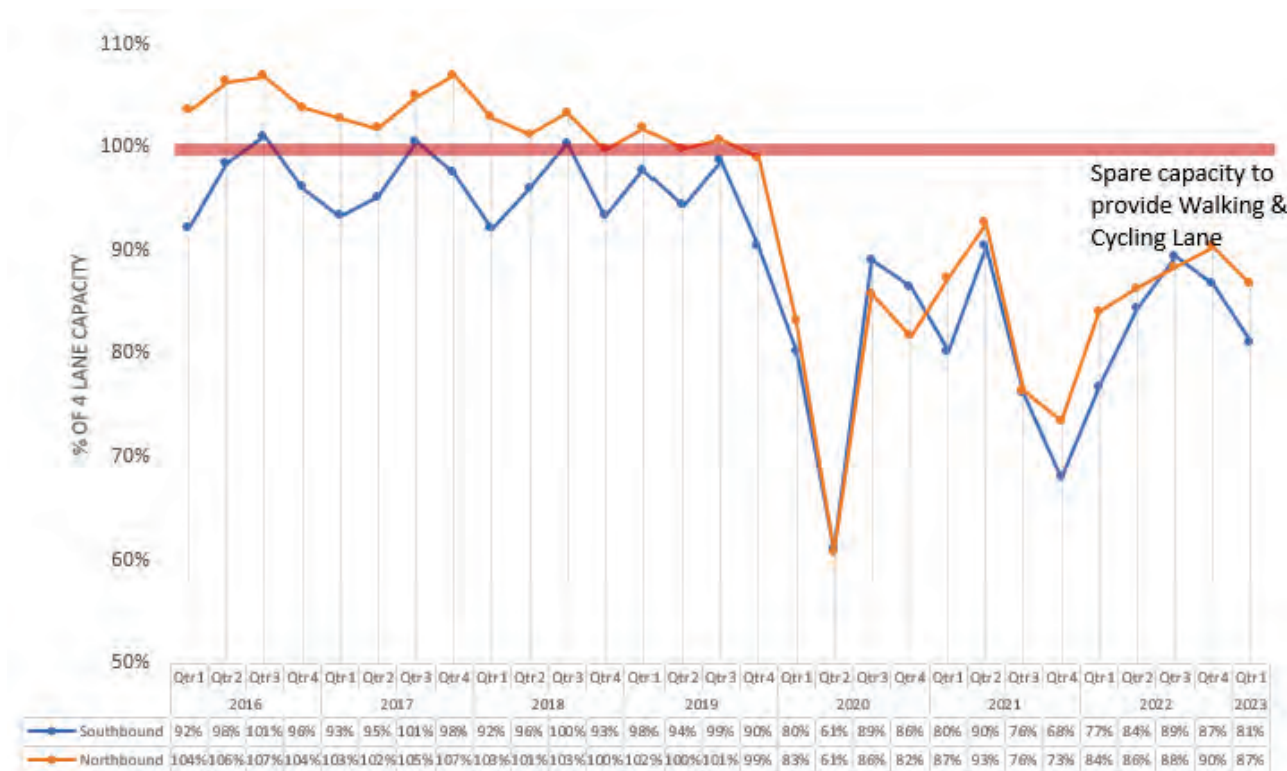


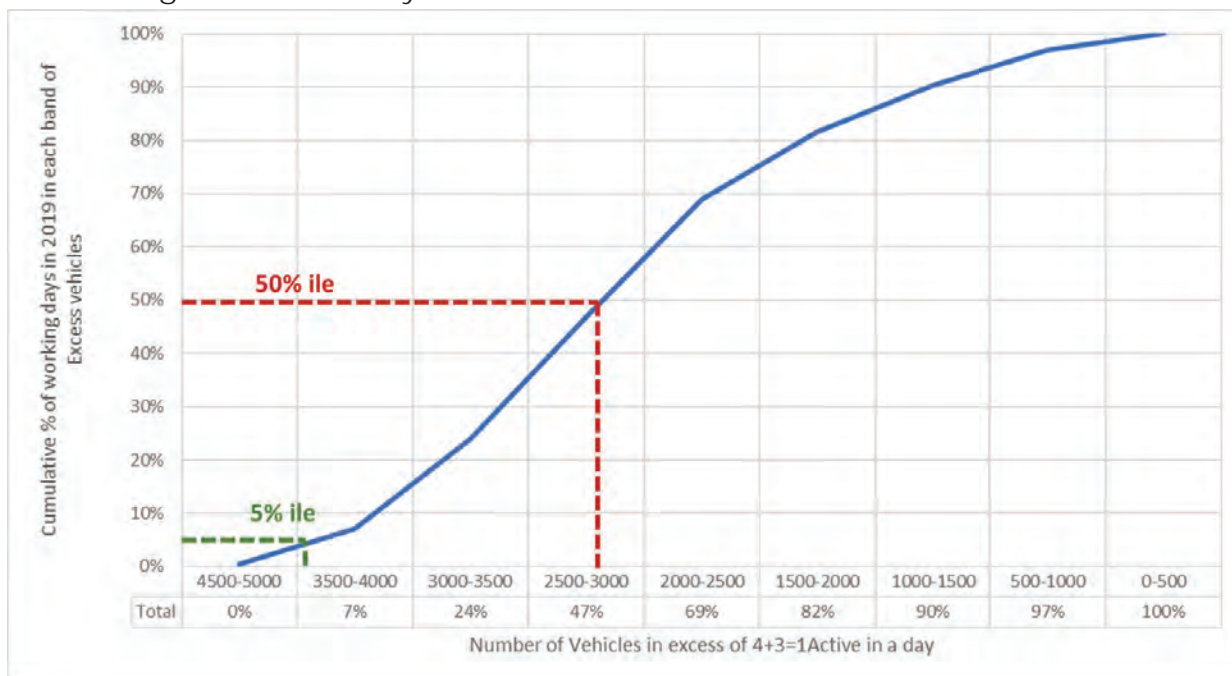
Figure 1 Percentage of 4 Lane capacity used - (M-F Southbound 06:00-9:00, Northbound (15:00-19:00) since 2013 by quarters. (Updated April 2023)

Although the average flows used above are a useful indicator of trends, there are significant day-to-day and seasonal variations in flow and detailed analysis of the data trends show that between 2016 - 2019 (pre-pandemic) traffic volumes were reducing, with a southbound peak period reduction of 1.4% between 2018-19. Trend data from 2016-2019 indicates that (even without the impact of the pandemic) the busiest three-hour peak period southbound traffic flows on the AHB would be fully accommodated with a 4-lane capacity by 2021. Northbound data indicates that (even without the impact of the pandemic) the busiest three-hour evening peak period northbound traffic flows on the AHB would be less than the 4-lane capacity by 2022.

As the data shows that just 1 Lane for people (not in vehicles) is capable of being deployed with minimal overall impact of traffic flows we found no evidence to support Waka Kotahi’s statement¹ that a traffic reduction on the AHB of 17,000 vehicles per day (vpd) would be required to avoid wider traffic disruption in Auckland.

¹ Waka Kotahi I&D Paper 23/11/21 P2 Para 6

Using 2019 data a daily traffic reduction of 3,000 vpd (for 50% of days) or 5,000 vpd (for 100% of days) would have been sufficient to provide sufficient capacity on a 4-lane configuration. In early 2022 these flow reductions were achieved.



To indicate the changes to weekday traffic on the AHB since 2014 Figure 2 has been included here that shows the realistic capacity of providing 4-Lanes of traffic in the morning peak rather than the 5 lanes provided with key points listed below.

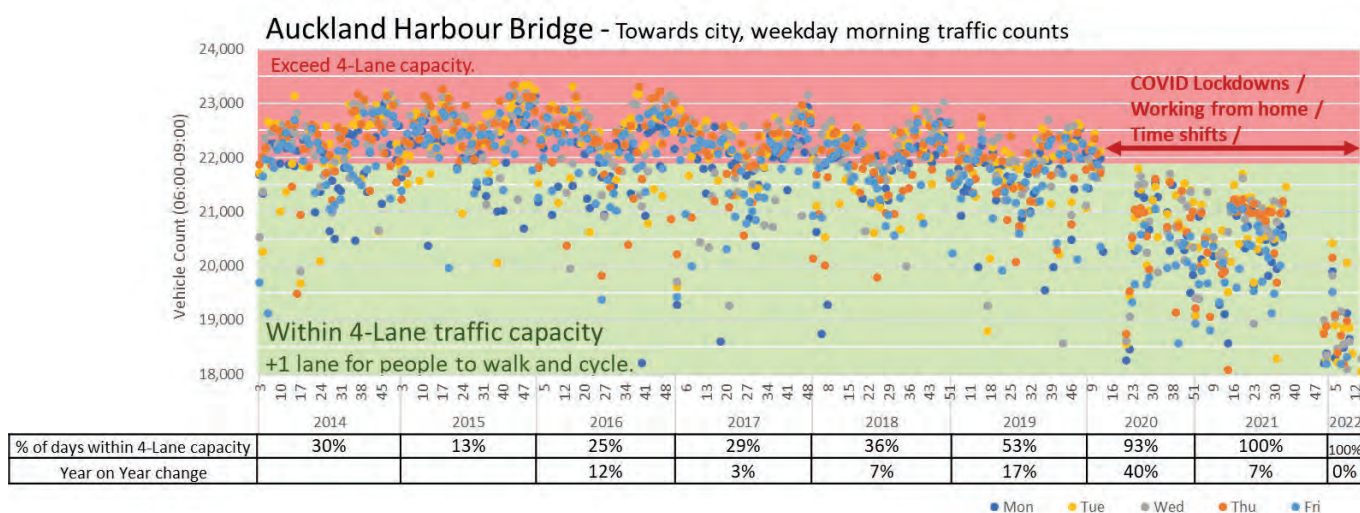


Figure 2 Traffic volumes and capacity Auckland Harbour Bridge 2014-22

- From 2016 to 2019 the morning traffic volumes into the city on the Auckland Harbour Bridge have reduced by around 1% each year.

- The 2020 COVID pandemic accelerated this reduction, with 2020 traffic volumes² being lower than 2019, 2021 volumes being lower than 2020, and 2022 (to date) lower than 2021.
- Morning city bound traffic volumes on the Auckland Harbour Bridge in January-March 2022 were 19% lower than the same period in 2016.
- By 2019 over half the working mornings (53%) could have accommodated a walking and cycling lane without restricting city-bound traffic on the Auckland Harbour Bridge.
- If those trends continued, then by 2022 (even without COVID) a walking and cycling lane on the Auckland Harbour Bridge could have been accommodated without restricting city-bound traffic.
- On every weekday morning since March 2020 the city-bound traffic on the Auckland Harbour Bridge could have been fully accommodated in 4 Lanes, rather than the 5 lanes that were provided.
- 2022 data indicates that peak period traffic flows on the Auckland Harbour Bridge are up to 14% lower than their 2016 peak.
- Even prior to 2020 the off-peak traffic flows rarely exceeded the Proven Capacity of the bridge (6% of the time southbound, 10% northbound) and these occurred between 12:00-14:00 so would be likely to have cleared before the afternoon peak period..
- Since March 2020 a walking and cycling lane could have been installed with without significantly restricting the morning traffic heading to the city.
- These figures are conservative as they take no account of people who would choose to leave their cars at home and take a bus, walk, run, scoot or cycle across the Auckland Harbour Bridge.

A range of other factors have been identified that indicate traffic flows on the AHB are likely to remain significantly below the 2016 peak values. Whilst several of the factors are pandemic related there is evidence that any return to 2016 traffic levels may not occur in the near future, if ever.

² Median values 06:00-09:00 M-F

This report has focused on assessing data that has been made available by Waka Kotahi, and SmartSense wishes to thank Waka Kotahi for their high level of co-operation.

There is strong evidence to demonstrate that whilst converting one lane of the AHB to Active modes would have had some impact on historic traffic flows, in the current environment it's impact would be easily manageable. With some degree of time and mode shift by current low occupancy vehicle occupants the effects could be mitigated.

If March 2022 – April 2023 traffic levels are maintained, then reassigning one lane to Active modes would be no significantly impact on motor vehicle travel times or traffic flows using the AHB. With the June – August period showing the lowest traffic volumes and evidence to date showing 2022 traffic volumes being less than 2018/19 then there is a strong incentive to operate the bridge in a solely 4+4 lane mode. This would save money (no need to move the barriers twice a day) and most usefully provide a wealth of real-world data on the actual impact of running 4 lanes for in the peak flow direction. This should be accompanied by accurate lane specific traffic counts and point-to-point journey time monitoring on the approaches to and across the bridge.

Richard Young,

June 2023.

This report has been prepared exclusively for the Client by SmartSense Ltd for the stated purpose, it contains quantitative data and SmartSense Ltd confirm that as far as practicable it reflects the contractual scope and is an accurate record of what was measured during the survey. © SmartSense Ltd 2022.

Rev	Prepared by	Date Issued	Reviewed	Status
Draft	R Young	10/4/22	Bike Auckland	Initial comments
REV0	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	28/4/22	Viastrada (Dr Glen Koorey PhD, ME(Civil), BE(Hons), BSc, CMEngNZ	Peer review and Bike Auckland Board review
REV1	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	16/5/22		Updated with peer review and Bike Auckland comments
REV2	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	4/7/22		2019 Northbound data added and off-peak capacity assessed.
REV3	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	15/9/22		2022 April – July data added.
REV4	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	27/6/23		August 2022 – March 2023 data added and report updated.
REV4.1	R Young, CEng, MICE, DIC, MBA, MSc, BSc.	29/6/23		Excess vehicle graphs added and modified.

Contents

Executive Summary	2
Contents.....	8
1. Introduction	10
2. Scope of Report.....	11
3. Methodology.....	12
Limitations on Data	12
4. Capacity Analysis	13
Stated Capacity -	13
Proven Capacity.....	14
5. Available Traffic Count Data	17
Variation between years.....	18
Seasonal variation in traffic flow across the Year.....	19
Weekday fluctuations in traffic flows	20
6. Peak Periods – impact of a one lane reduction.....	22
7. Analysis of Peak Period flows since 2013	24
Southbound Morning Peak	25
Northbound Evening Peak.....	28
Assessment of Quarterly Trends	30
8. Off-Peak flows.....	34
9. Sensitivity Analysis of 4-Lane Capacity	37
February - March 2022 4-Lane Operation	38
10. Wider State Highway Network	43
Southbound Flows.....	43
Northbound Flows	46
Both Directions.....	48
11. Other Factors.....	50
Reduced 2022 Traffic Volumes due to changes in behaviour due to pandemic ...	50
Uptake of Active modes	51
12. Conclusions.....	53

13. Completion of Deliverables 56

14. Appendix A..... 57

1. Introduction

The Auckland Harbour Bridge (AHB) carries State Highway 1 (SH1) across the Waitematā Harbour between Stokes Point and Westhaven. The main bridge is around 1,040m between anchorages³ with the elevated highway extending to around 1,800m between Sulphur Beach Reserve and Shelly Beach Road off ramp.

The main bridge comprises of two two-lane structures (clips-ons) that were added onto the original central 4-lane bridge providing a total of eight lanes. There is a movable concrete barrier on the central bridge which enables a tidal flow of traffic with up to five lanes available for traffic in one direction and three in the opposite direction.

Waka Kotahi (New Zealand Transport Agency) is the Road Controlling Authority and responsible for the bridge.

Through a series of Official Information Act requests⁴ and data pro-actively released⁵ ⁶ by Waka Kotahi the 15 minute and 1 hour traffic volumes crossing the AHB have been obtained along with the normal positions and times of the moveable barrier and information on abnormal events including a bridge strike in late 2020.

This report contains the results of analysis of the available data to assess the potential effect of a re-assignment of one traffic lane to active modes on the AHB's ability to carry the traffic flows recorded.

³ Waka Kotahi Suicide Prevention Barrier Feasibility Study Report 18_19 SH1N BSN 4232 Auckland Harbour Bridge 383545-18_19-BR-SH1-4232-RP-FS-001-REV_F1 May 2019

⁴ OIA 10208, 9262, 9593 and 9816

⁵ <https://opendata-nzta.opendata.arcgis.com/datasets/tms-traffic-quarter-hourly>

⁶ <https://www.nzta.govt.nz/resources/state-highway-traffic-volumes/>

2. Scope of Report

The report contains an assessment of the actual flow data (vehicles per hour (**vph**) and vehicles per day (**vpd**)) from 2013 onwards on the AHB. The bridge has a movable barrier so that although it has eight lanes the number of lanes in either direction can be flexed from three to five.

The assessment includes an analysis of the likely impact that a bridge with seven traffic lanes (4+3 or 3+4) and one dedicated to active modes (+1Active) would have on those flows.

The report's scope is limited, and the following items are out of scope:

- Traffic impacts and assessment of any delays off the bridge, including the wider Auckland motorway and road network.
- Assessment of re-routing onto SH16/18/20 associated with the bridge strike in late 2020.
- Detailed design assessment of the converted lane (barriers, on/off ramp provisions).

3. Methodology

Waka Kotahi have made significant volumes of data available. Some data was obtained through Official Information Act requests and other data was downloadable from the Waka Kotahi website. Some questions on the reliability of Waka Kotahi count data was expressed by their consultants at a meeting held between the author and Waka Kotahi⁷, whilst it is accepted that loop-counters may not be perfect they are generally relied on provide reliable and consistent data and for the purposes of this report it is assumed that all data provided by Waka Kotahi is accurate.

Limitations on Data

Whilst Waka Kotahi have provided a large volume of 15-minute interval data there are some gaps which have limited the analysis:

- There is no 2019 Northbound count data available on the AHB counter due to 'equipment and loop failure'.⁸
- This data has been replaced by data that Waka Kotahi have provided since May 2022 based on nearby loops and is considered to be reliable.⁹
- Between January and November 2020 15-minute interval count data was not available and 1-hour data has been used instead.
- No count data is available for the period of a major bridge strike.¹⁰
- Some counts were classified by Light and Heavy, and some were only classified as Light but the volumes appeared to include both Light and Heavy. It is understood that Heavy would include trucks and buses. As only totals of all vehicles have been used this has not been given detailed consideration.
- For peak periods the percentage of Heavy vehicles appears to be around 5%.
- Count data back to 2013 was made available and this has been included in the analysis.

⁷ Meeting Young, Deb Hume (DH) - National Manager Multimodal & Innovation (WK), Andy Hooper (AH) – Technical Advisory Service to Auckland System Management (ASM, WK), Mike Beamish (MB) – Harbour Bridge Structural Engineer (BECA)

⁸ OIA 9262 Reply 8/2/22

⁹ OIA 10208

¹⁰ OIA 9593 incident between 18/9/20-6/10/20

4. Capacity Analysis

To undertake this analysis an assessment of the capacity of the AHB to carry the measured traffic volumes with the introduction of a 'virtual' Active Lane was undertaken. This assessment used data provided by Waka Kotahi to quantify the potential impact that a 7-lane 4+3+1 Active Lane arrangement would have had on the existing 8 lane (5+3 at peak times) structure.

Stated Capacity -

Waka Kotahi provided the following capacities and standard operating hours for different lane configurations in Table 1¹¹. These have been taken as the 'Stated Capacities', we have not sought for Waka Kotahi to justify these capacities.

With the exception of the 3 Lane capacity the Stated Capacities are lane multiples of 1,800 vph. This value is generally accepted as a reasonable value for a typical highway. In the case of the AHB there are some specific environmental factors that could make the adoption of these values conservative: -

- High degree of lane separation and lack of merging/ lane changing. Due to the bridge geometry there are never more than 3 lanes available for traffic to change lanes (even when 5 are open). This limits the ability of traffic to encounter flow break-down.
- Steady speeds. The AHB has an 80kph speed limit and it is observed that traffic using the crossing travels at a steady speed (slightly below 80kph).
- High level of driver familiarity. The bridge is regularly used by many drivers who are familiar with how traffic flows and are less likely to behave in a way to cause flow-breakdown.
- Alignment, gradient and lack of on/off ramps. The AHB is a long straight route with two steady gradients (<5%) and little side friction or other factors that could disrupt flow.

Table 1 Movable barrier and AHB capacity

Times (M-F)	Southbound		Northbound	
	Lanes Available	Stated Capacity (vph)	Lanes Available	Stated Capacity (vph)
5 am to 10 am	5	9,000	3	5,200
10 am to 3 pm	4	7,200	4	7,200
3 pm to 10 pm	3	5,200	5	9,000
10 pm to 5 am	4	7,200	4	7,200

¹¹ OIA 9262

Proven Capacity

Based on Lane Available data provided in Table 1 an analysis of the actual traffic flows recorded for each configuration was determined.

Figure 3 shows the recorded southbound flows did not exceed the Stated Capacity in the 4 and 5 lane configurations but in the 3-lane arrangement the Stated Capacity of 5,200 was exceeded 13% of the time.

The fact that actual traffic flows of the 3-lane arrangement exceeded the 5,200 vph Stated Capacity for a significant proportion of time implies that the Stated Capacity provided by Waka Kotahi is overly conservative. As a result, we have proposed a Proven Capacity of 5,400 vph as the capacity that the actual data from Waka Kotahi supports.

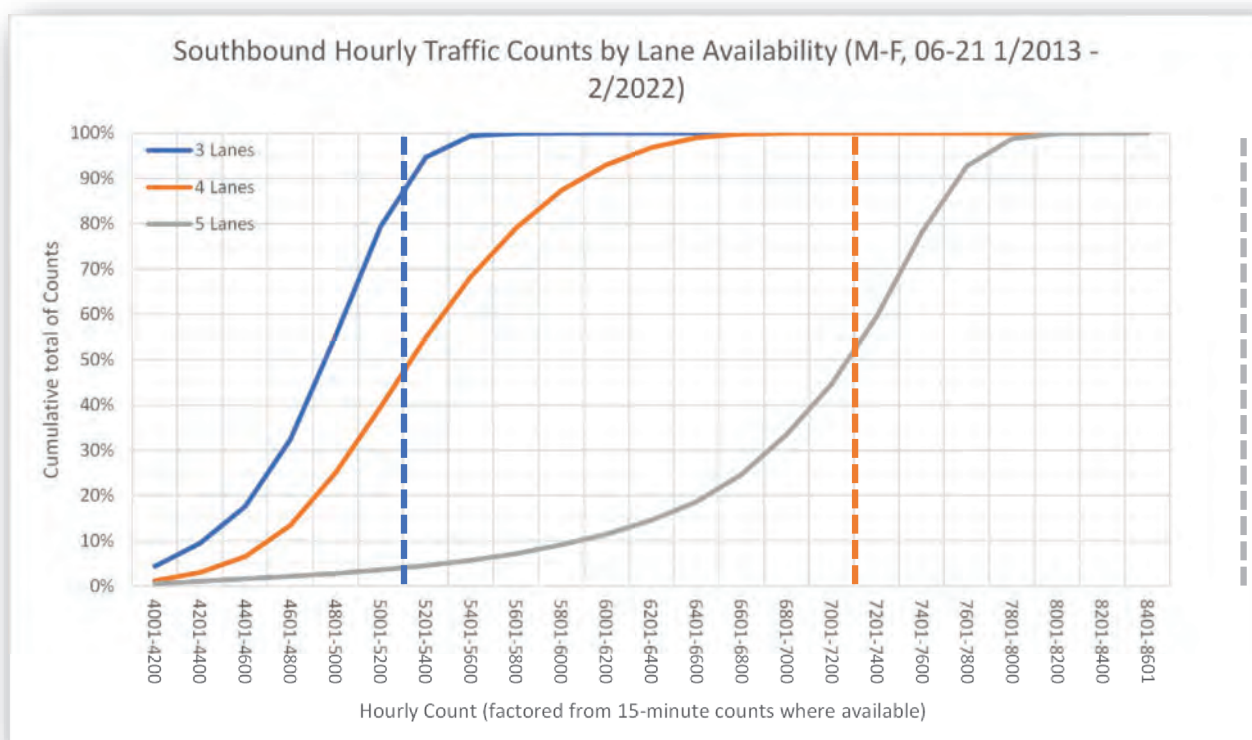


Figure 3 Traffic Counts by Lane Availability (Southbound)

Figure 4 shows the maximum northbound flows did not exceed the Stated Capacity in the 4-lane configuration but marginally did exceed the Stated Capacity in both the 3 and 5 Lane configurations. This demonstrates that when the bridge has 3 or 5

lanes flowing in one direction the Proven Capacity is higher than the Stated Capacity of 5,200 / 9,000vph.

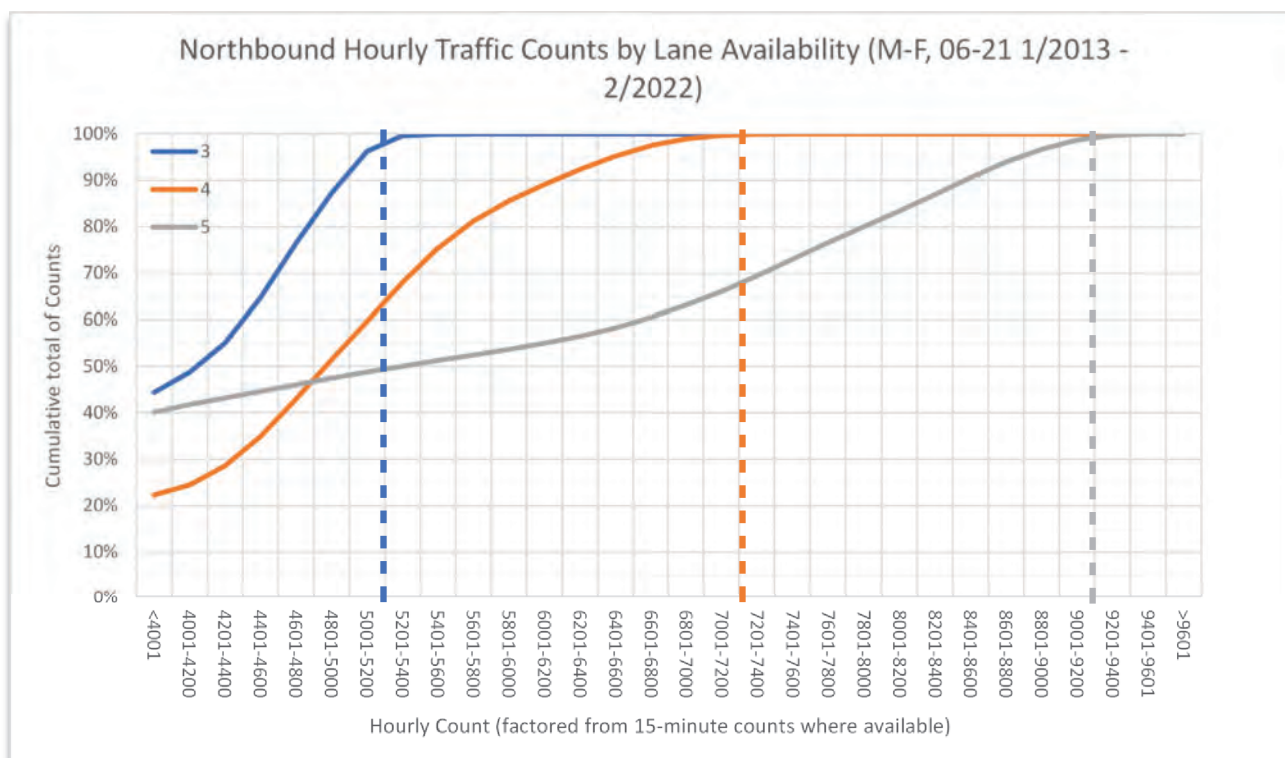


Figure 4 Traffic Counts by Lane Availability (Northbound)

On the basis of the actual hourly flows provided by Waka Kotahi, the 3 and 5 lane configuration capacities can reasonably be increased by 200 and 400 vph respectively.

The 4-lane configuration is generally used at off-peak times when traffic flows may not approach capacity. So, although the Stated Capacity of 7,200 was only rarely exceeded, this does not mean that the Proven Capacity is not higher. The 2013-2020 data supports a 4-lane arrangement having a Proven Capacity of 7,270 (around 1% higher than Stated Capacity), whilst a Proven Capacity increase of 300vph would appear to be a reasonable assumption based on the increases of 200 and 400 for 3 and 5 lane alignments.

In February / March 2022, due to COVID affecting staff availability, the 4-lane arrangement was left in place for several weeks across all periods and this provides more recent information on the Proven Capacity of a 4-lane arrangement. This is described in below.

Any increase in Proven Capacity of a 4-lane arrangement above the Stated Capacity would have a material effect on the potential of the AHB to operate in a 4+3+1Active arrangement. Even a modest increase in the 4-lane Proven Capacity has a significant impact on the analysis.

Using the actual count data from 2013 to 2/2022 it is apparent that even these higher Proven Capacities (Table 2) were exceeded on many hundreds of occasions. This means that Proven Capacities potentially higher than we have used for the analysis may be reasonable. Any higher Proven Capacity, especially above 7,270vph for 4 Available Lanes will materially reduce any negative impact of an Active Lane on AHB peak period traffic flows.

Table 2 Comparison of Stated Capacity and Proven Capacity

Available Lanes	Stated Capacity (vph)	Proven Capacity (vph)	Uplift in Capacity (% above Stated)	Number of 1-hour periods where Proven Capacity exceeded	Comments	Maximum recorded hourly count
3	5,200	5,400	3.84%	1,252 since 1/1/2013	All between 15:00-20:00	5,767 SB15:00-16:00 17/12/21
4	7,200	7,270	0.97%	10	All in November / December 2020	7,417 NB 20/12/2013 14:00-15:00
5	9,000	9,400	4.44%	223, since 1/1/2013	All between 16:00-18:00	9,428 30/9/2013 17:00-18:00

These Proven Capacities (Table 2) were used for analysing the ability of the AHB to operate with 3+4 Traffic and 1 Active Lane. This is reasonable because the data provided by Waka Kotahi demonstrates that their Stated Capacities are overly conservative, unrealistic and represent an understatement of the AHB's proven performance.

5. Available Traffic Count Data

Waka Kotahi have provided a significant volume of data from 1/2013 to 7/2022.

As described elsewhere, data was generally provided at 15-minute intervals but where that was unavailable 1-hour data has been used and the totals are understood to include both Light and Heavy vehicles. In total over 930,000 individual count records and over 514 million classified vehicle counts were provided.

The data (Figure 5) shows that until 2019 that typical daily traffic counts would be around 90,000 in each direction (Wednesday) with some day-to-day fluctuation.

In 2019, the Waka Kotahi northbound counter failed to provide any data, but this was replaced with comparable data from nearby. In June 2022 Waka Kotahi provided alternate data for this location and this has now been included in the report. The Northbound data is significant issue because the evening northbound peak period traffic flows were heavier than the morning southbound volumes and from early 2020 the Covid pandemic significantly affected traffic.

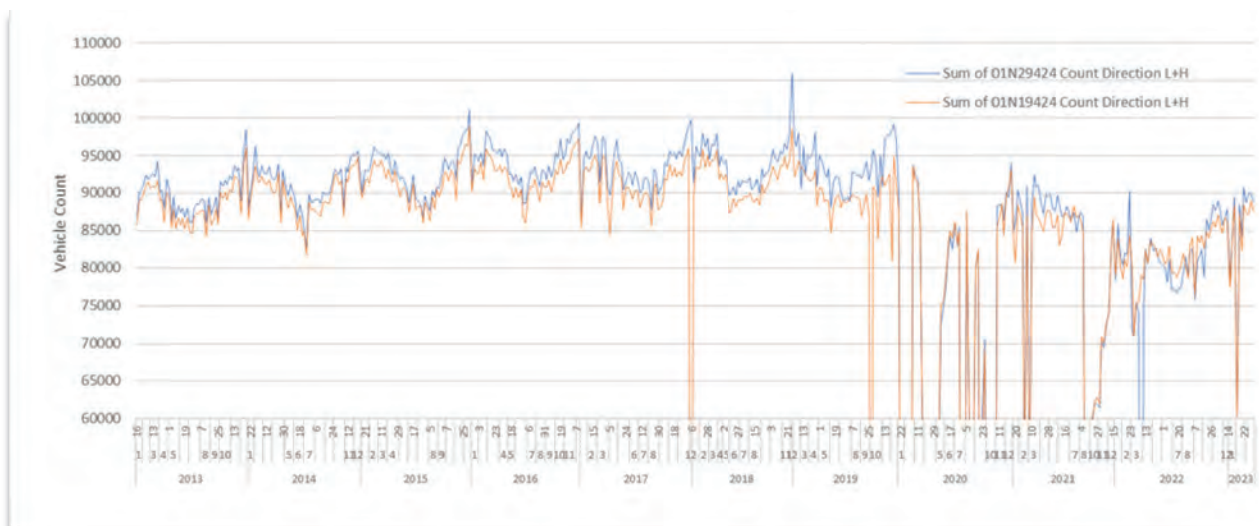


Figure 5 Vertical scale is Daily Traffic Counts Northbound (Blue) and Southbound (Orange) on the AHB. Data for Wednesday, excluding first and last 2 weeks of each year, 0 counts are where no data is available.

Variation between years

From 2016 until 2019 (pre-pandemic) there has been a net reduction in southbound traffic using the AHB. In 2018 this was 2.3% lower than the 2016 peak(Figure 6).¹² This trend of reducing traffic accelerated significantly in 2020 (25% reduction) and 2021(16%) but these two reductions were affected by pandemic lockdowns.

Between 2018 and 2019 the southbound reduction was 1.4%, equivalent to over 2,600vpd on the AHB. Ignoring the pandemic effects, the data demonstrates a measurable and accelerating trend of reducing traffic volumes on the AHB. There is no reason to believe that this trend will cease, especially as work practices have changed, with a likely permanent increase in working from home practices. The 2022 data (weeks 9-31) provided BY Waka Kotahi confirms this trend with Southbound traffic volumes in this period around 14% lower than 2016.

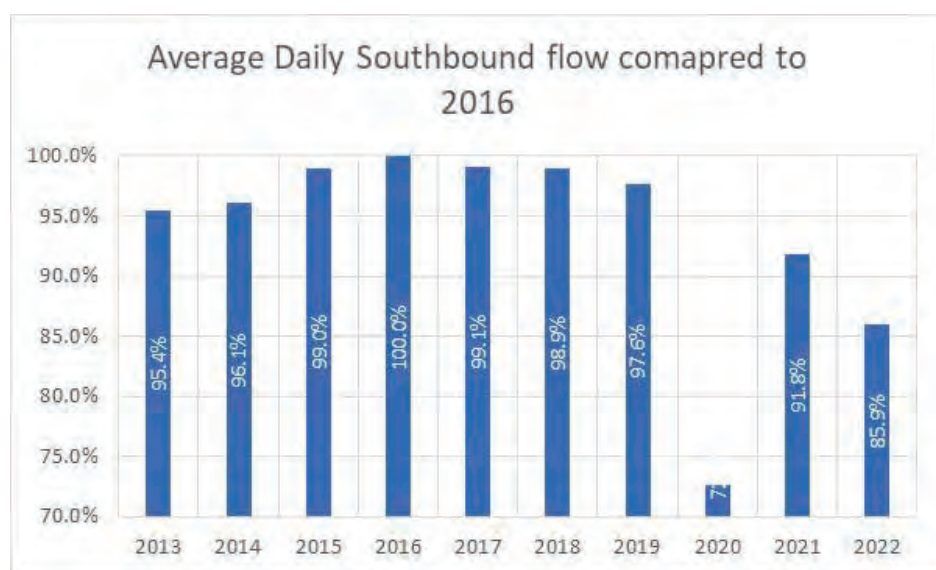


Figure 6 Changes in Southbound weekday volumes 2013-2022 Weeks 9-31, where complete data available

The Northbound daily count (Figure 7) also shows a peak in 2016 with a reduction each year since then and a 1.2% reduction by 2019.

2022 data shows an 16% reduction in traffic volumes since the peak in 2016.

¹² Data based on the 133 days where same Week / Weekday (Mon-Fri) was data available from 2013-2020.

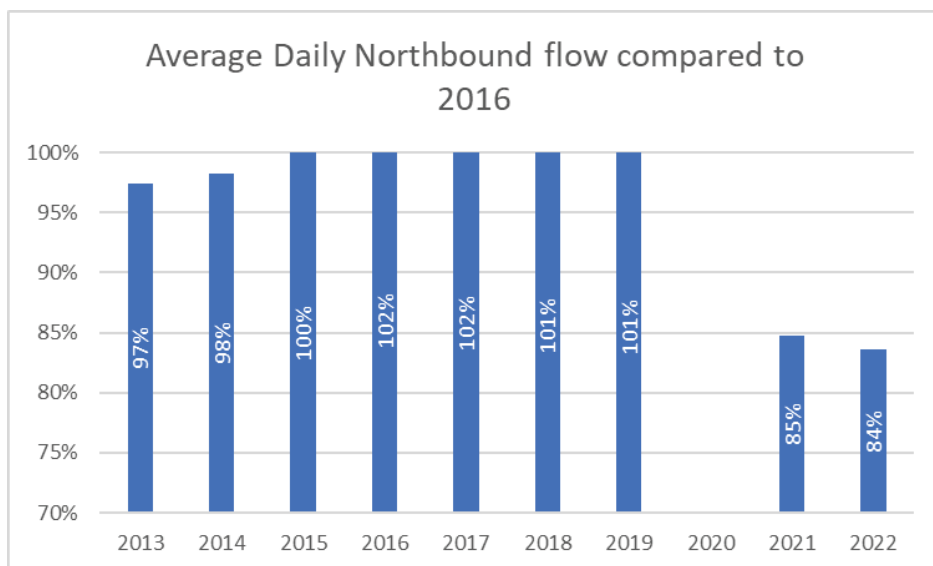


Figure 7 Changes in Northbound weekday volumes 2013-2022

Seasonal variation in traffic flow across the Year

Across the calendar year there is a week-to-week variability in traffic counts. Figure 8 shows the Southbound daily counts.¹³ There is a clear trend of lower counts between weeks 19-35 of the calendar year with peak volumes in February and November/December. This trend is highlighted for 2019 but the graph shows a common trend for 2016-2019.

This analysis demonstrates that the week-to-week fluctuation in traffic flows is likely to affect the impact of any Active Lane has on 4+3 traffic lane arrangements on the AHB. Therefore, any analysis of the impact of an Active Lane would need to take account of the seasonal fluctuation in traffic flows. For example, higher February flows may increase the impact of an Active Lane on traffic compared to June.

¹³ Taken on every Thursday where reliable count data was provided.

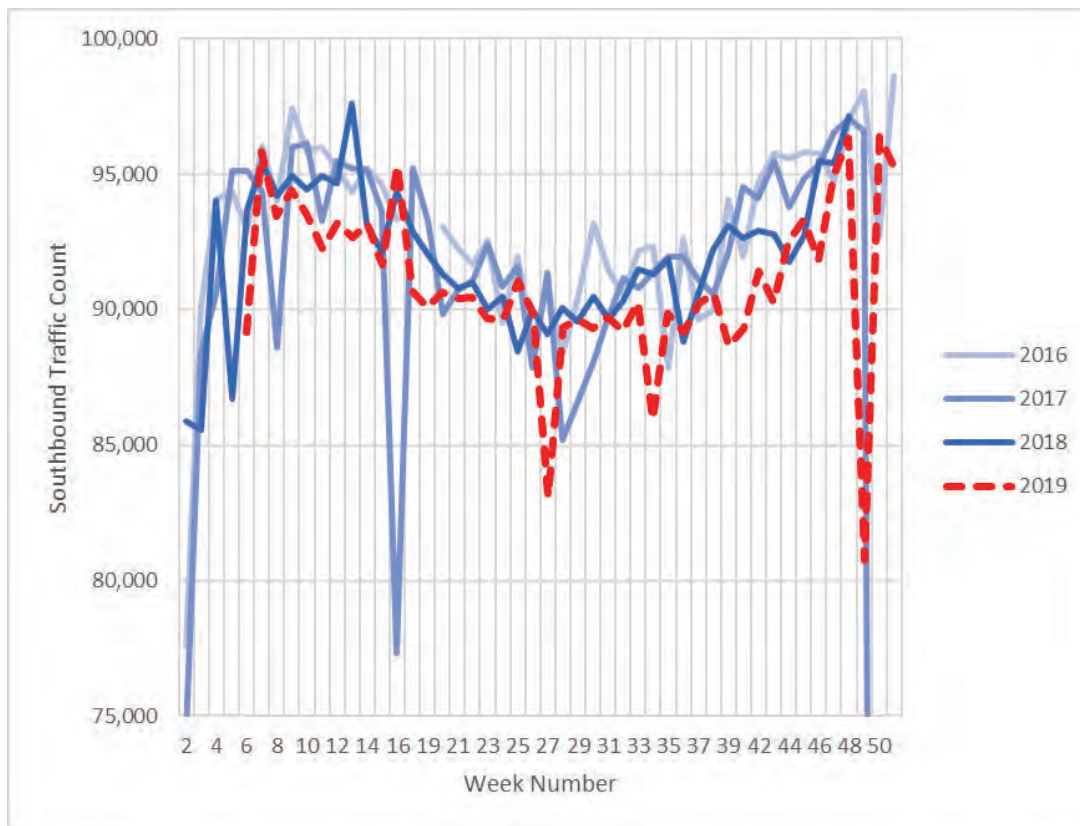


Figure 8 Weekly variability of Northbound (Thursday) volumes on the AHB

Weekday fluctuations in traffic flows

As well as seasonal and year-on-year trends the data also showed a variability in daily flows across the working week. Figure 9 shows the clear upwards count from Monday (Day 1) to Friday (Day 5) for this southbound count data. Across the week the counts would typically vary from the weekly mean by $\pm 5\%$. This day-to-day variability is significant when analysing the impact of an +1Active Lane on 3+4 Traffic Lanes.

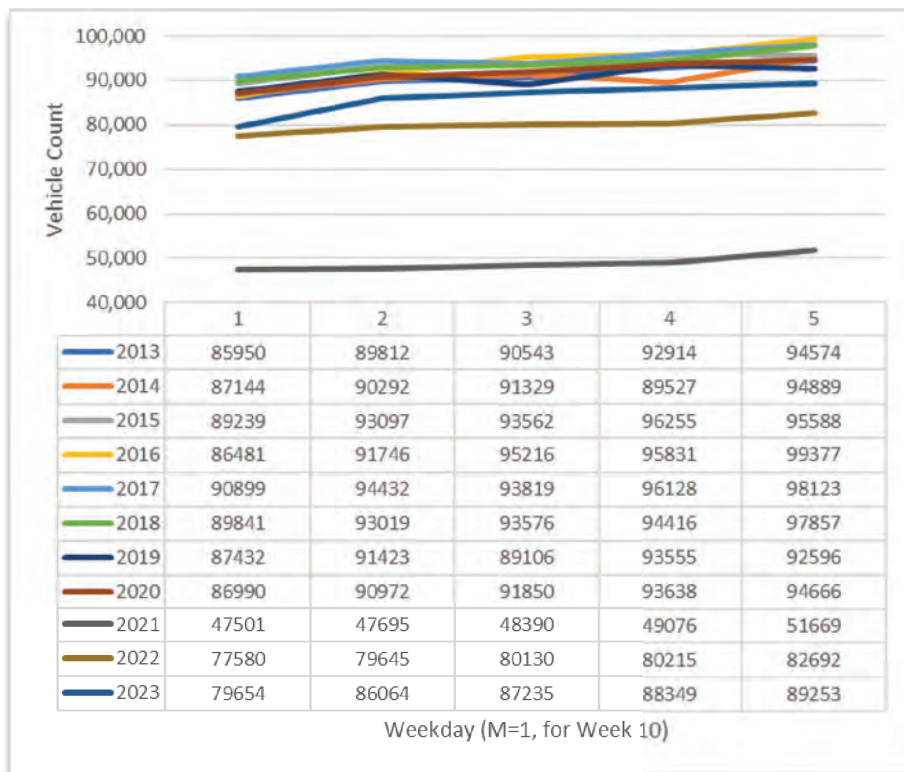


Figure 9 Daily variation in traffic counts across M-F, week 10 2013-2022

As with other variations across time, this day-to-day variation may affect the impact of an Active Lane on traffic, so it is important that any analysis considers a range of flows and undertakes a sensitivity (probabilistic) analysis rather than using a single flow.

Additionally the marked reduction in traffic volumes in 2021 (lockdown affected) and 2022 (not affected by lockdown) are evident. In 2023 the Week 10 counts are still around 5,000 vpd below those of 2020 (pre-pandemic).

6. Peak Periods – impact of a one lane reduction.

To be able to provide an Active Lane, one of the eight existing traffic lanes will need to be repurposed. It is understood that Waka Kotahi's preference is that the outside lane of the eastern clip-on would be most suitable as the Active Lane, this arrangement (Figure 10) provides for 4 lanes in the dominant flow direction and 3 in the opposite direction. Should it be required it can also be configured to enable 5 lanes northbound (away from the CBD).

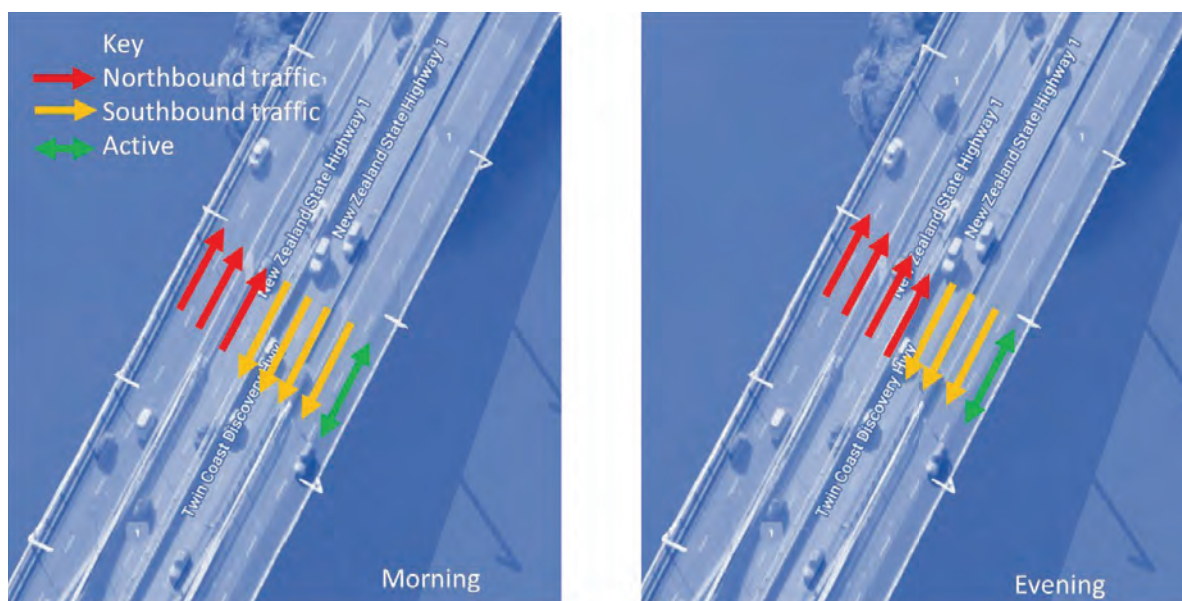


Figure 10 Schematic of Traffic Lane arrangement AHB with Active Lane

As the AHB has a movable barrier this would have the effect of reducing the dominant flow from 5 to 4 lanes at peak times. The alternate option is to maintain 5 lanes in the dominant flow direction and reduce the opposite direction from 3 to 2 lanes.

Our analysis of the theoretical impact on historic traffic flows for these two options shows that the 4+3 option rather than the 5+2 option (combined with optimised barrier movement times) provides the least impact to traffic. This 4+3 Traffic + 1 Active has therefore been used for the analysis.

As described above, the traffic flow on the AHB is not uniform across the day, week or season and year; it varies. It is unclear how Waka Kotahi have undertaken their analysis of the ability of the AHB to cope with an Active Lane. For our analysis we have used the data provided by Waka Kotahi to assess the AHB's capacity to

manage an Active Lane on each hour of each day¹⁴ since 2013. This has allowed a sensitivity assessment to be undertaken of the potential impact of an Active Lane to be performed.

¹⁴ Where data was available

7. Analysis of Peak Period flows since 2013

To investigate the historic ability of the AHB to cater for an Active Lane, an analysis has been undertaken that assesses the percentage of peak periods each year since 2013 in which the AHB historic traffic flows could have been affected by repurposing one lane to active modes.

The analysis in this paper is based on historic data and takes no account of trip reduction / reassignment / time shifting / migration to active transport and public transport that may have occurred over the time period. This is deliberate, in order to be clear that the analysis of this report does not rely on behavioural assumptions or policy levers. Recipients of this report may apply their own assumptions on changes in transport behaviour (i.e. use of the Active Lane by numbers of people), including policy levers such as congestion charging.

However, by assessing the impact of an Active Lane on actual data over many years it has been possible to identify trends showing how often a 3+4 Traffic +1Active configuration would have been inadequate to meet unfettered traffic demand.

The analysis utilises the Proven Capacity (Table 2) to determine the percentage of peak periods since 2013 that the AHB could have accommodated an Active Lane.

Table 3 summarises the existing movable barrier timings and those used for the analysis.

Table 3 Existing and analysed Southbound and Northbound traffic lane configuration

Period (24 hour clock)	Existing Southbound (Weekday)	Existing Northbound (Weekday)	Analysis Southbound with 4+3+1 Active	Analysis Northbound with 4+3+1 Active
00-05	4	4	3	4
05-10	5	3	4	3
10-13	4	4	4	3
13-14	4	4	3	4
14-15	4	4	3	4
15-16	3	5	3	4
16-22	3	5	3	4
22-00	4	4	3	4

Southbound Morning Peak

The busiest southbound period occurs between 06:00-09:00 Monday-Friday. Traffic count data from this period has been analysed to assess the theoretical impact that an Active Lane could have had on the AHB's traffic carrying capacity from 2013-2022 (week 31).

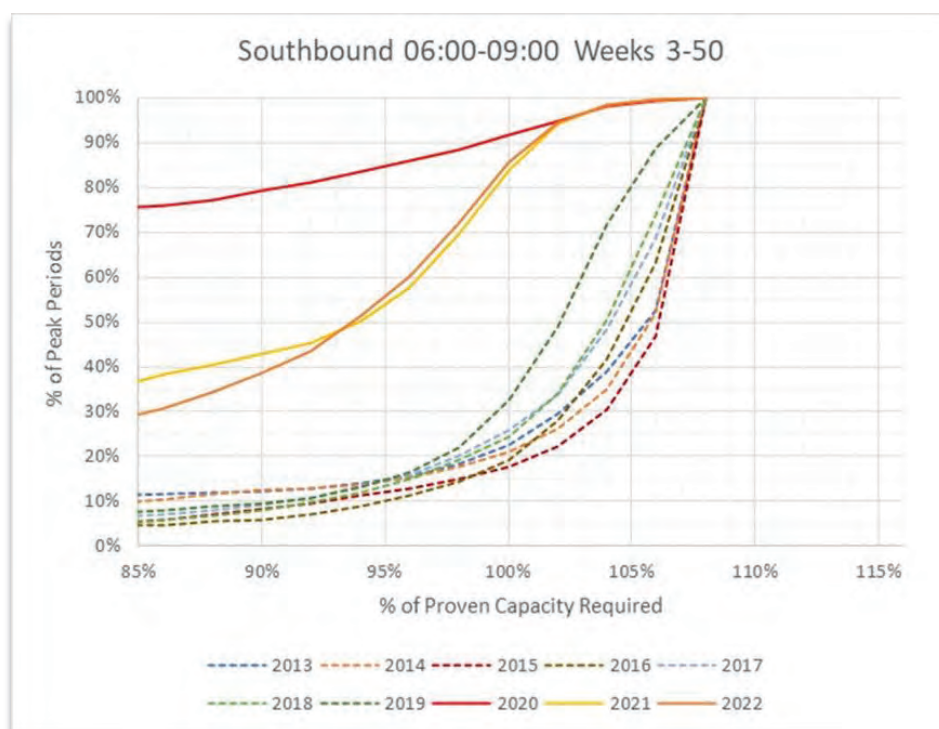


Figure 11 Morning Peak Southbound % of Proven Capacity required with an Active Lane 2013-2022 Weeks 3-50 (2022 to 31)

Figure 11 shows the percentage morning peak Proven Capacity required (for a 4-traffic lane arrangement) for southbound weekday (06:00-09:00) traffic flows. Where the lines cross to the right of the 100% Proven Capacity vertex then that percentage of peak periods in that year can be accommodated with a 4+3+1 Active arrangement with no capacity constraints.

For example, in **2015** only 20% of peak periods could be accommodated with no capacity constraints. Each year after 2015 the number of morning peaks that could accommodate an Active Lane with no capacity issues rises: **2016** - 27%, **2017** - 31%, **2018** - 32%, **2019** - 46%.

This indicates that in 2019 on over 100 of the 240 working days (48 weeks) a 4+3+1 Active Lane arrangement would have provided sufficient morning peak southbound traffic capacity.

If the annual reduction in southbound morning peak traffic recorded between 2018 and 2019 continued at the same rate, then by 2023 a 4+3 Traffic Active arrangement would not impact flows for around 200 weekdays a year (out of a possible 260 weekdays per year). This analysis takes no account of people migrating from vehicles to use the Active Lane, so can be considered conservative.

The dashed data for **2020** (79%), **2021** (93%) and the first part of **2022** (95%) are affected by traffic reductions associated with the COVID-19 pandemic so should not be relied on as being representative. However, from 2015-2019 there is a clear trend that shows that morning peak traffic volumes have been reducing such that a 4-lane southbound arrangement would have provided sufficient capacity for at least half the time.

The latest data for 2022 (weeks 3-31) shows that 85% of weekday mornings would have had no capacity issues.

The **2019** data additionally shows that 95% of morning peak periods could accommodate an Active Lane with just a 5% traffic volume reduction.

Based on 2019 flows just a 5% reduction in southbound morning peak traffic would enable 95% of all weekdays to support an Active Lane with negligible impact on traffic flows.

Based on 2022 flows just a 2% reduction in southbound morning peak traffic would enable 95% of all weekdays to support an Active Lane with negligible impact on traffic flows.

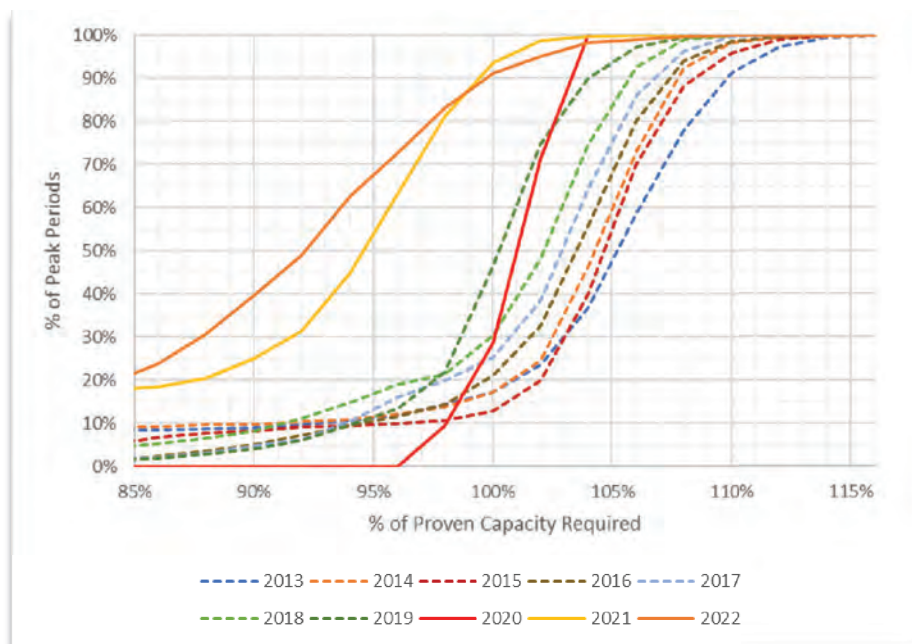


Figure 12 Morning Peak Southbound % of Proven Capacity required with an Active Lane 2013-2022 Weeks 5-11 only

Figure 12 has limited the same analysis to February and the first three weeks of March – this enables the pre-pandemic data from 2020 to be compared to the trend. This indicates that, in the early part of 2020, 95% of southbound morning peak periods could have been accommodated with 4 traffic lanes + 3% capacity (or 4 lanes with 3% less traffic).

Based on early (pre-pandemic) 2020 flows just a 3% reduction in southbound morning peak traffic would enable 95% of all weekdays to support an Active Lane with negligible impact on traffic flows.

If an Active Lane was in place then the reduction in vehicle numbers to enable the AHB to effectively operate with an for each morning between 2016-2022 is shown (Figure 13). By June 2022 the average excess traffic (above a 4-Lane capacity) was under 20 vehicles over four hours. The February 2023 data is the lowest average excess traffic volume of only 22 vehicles a day.

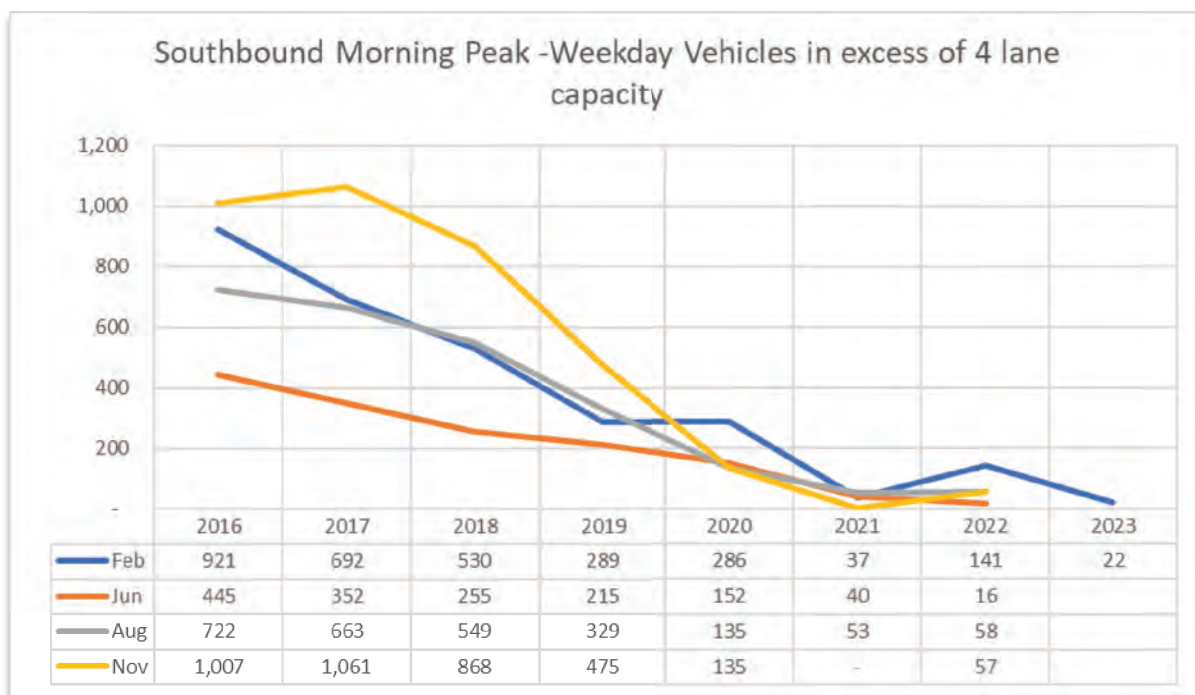


Figure 13 Southbound Morning Peak (06:00-09:00) number of vehicles in excess of 4 lane Proven Capacity

The February data (up to 2020) is considered unaffected by the pandemic and shows from 2016 to 2020 that the number of excess vehicles reduced from **921** (2016) to **286** (2020). This **286** represents only 1.3% of the AHB’s Proven Capacity during that period.

If the traffic reduction trend from 2016-2019 continued, then even without the pandemic, the southbound morning peak period flows on the AHB would have enabled an Active Lane to be installed in 2021 with negligible impact on traffic flows.

Northbound Evening Peak

A similar analysis has been undertaken for the northbound evening peak flows. Figure 14 shows the percentage of northbound evening peak periods that would exceed the 4 traffic Lane Proven Capacity since 2013. The 2019 data indicates that on 29% of weekdays that the three-hour evening peak flow could have been accommodated with a 4 Lane arrangement. With just a 5% reduction in 2019 traffic volumes this would have increased to 44% of weekdays.

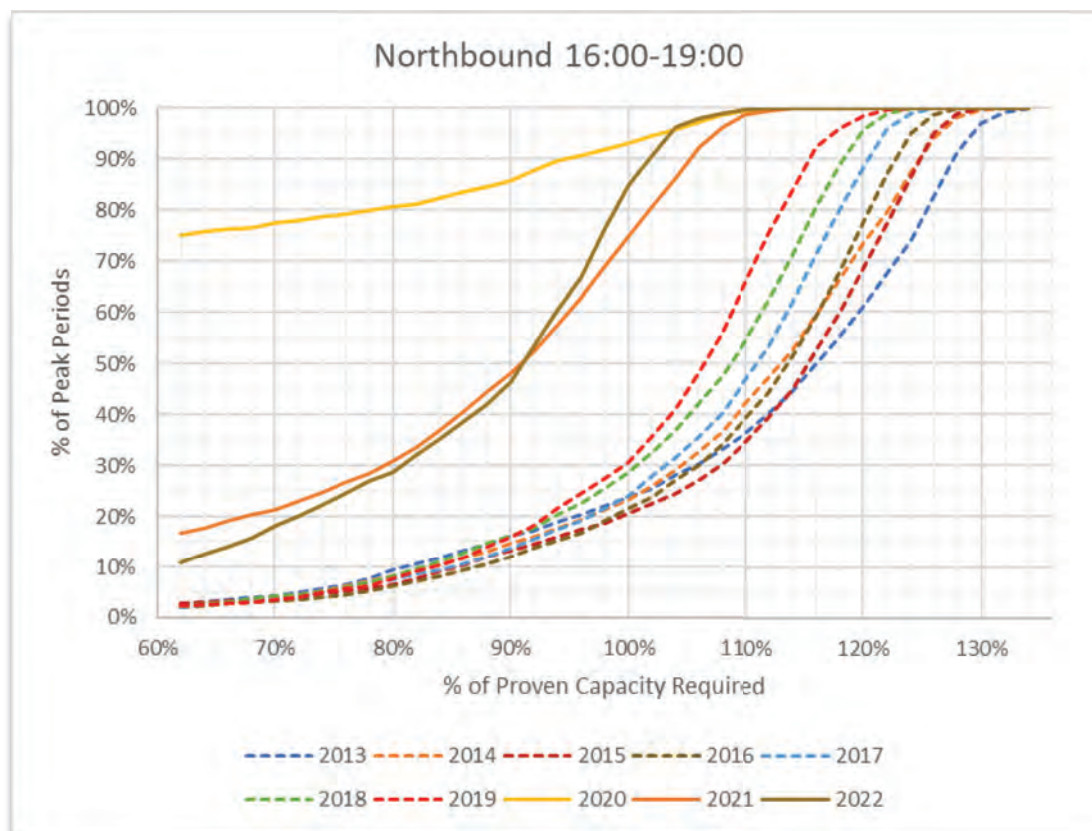


Figure 14 Evening Peak Northbound % of Proven Capacity required with an Active Lane 2013-2022 Weeks 3-50 only (to wk 31 in 2022)

To provide a clear indication on the volume of traffic reduction necessary to facilitate a 4+3+1 Active lane to operate the number of vehicles in-excess of the 4 traffic Lane Northbound capacity across the three-hour evening peak has been calculated of four months for each year 2016-2023 (Figure 15). The February data prior to 2021 is considered unaffected by the pandemic and shows from 2016 to 2020 that the number of excess vehicles reduced from **3,366** (2016) to **934** (2020). This **934** represents 4.1% of the Proven Capacity of a 4 traffic Lane arrangement. The 2022 data (Feb and June) clearly shows that the excess traffic volumes had reduced with June 2022 showing under 100 vehicles more than a four lane capacity. The February 2023 calculation of excess traffic volume has risen to 529 but is still lower than the 934 in pre-pandemic 2020.

The trend from 2016-2018 indicates that even without the impact of the pandemic that three-hour evening peak period northbound traffic flows on the AHB would be less than the 4 traffic Lane capacity by 2022.

The 2022 data (weeks 3-31) shows that on 85% of weekdays that a 4-lane evening peak bridge capacity was not exceeded and 2% reduction in traffic would raise that 85% to 96% of the time.

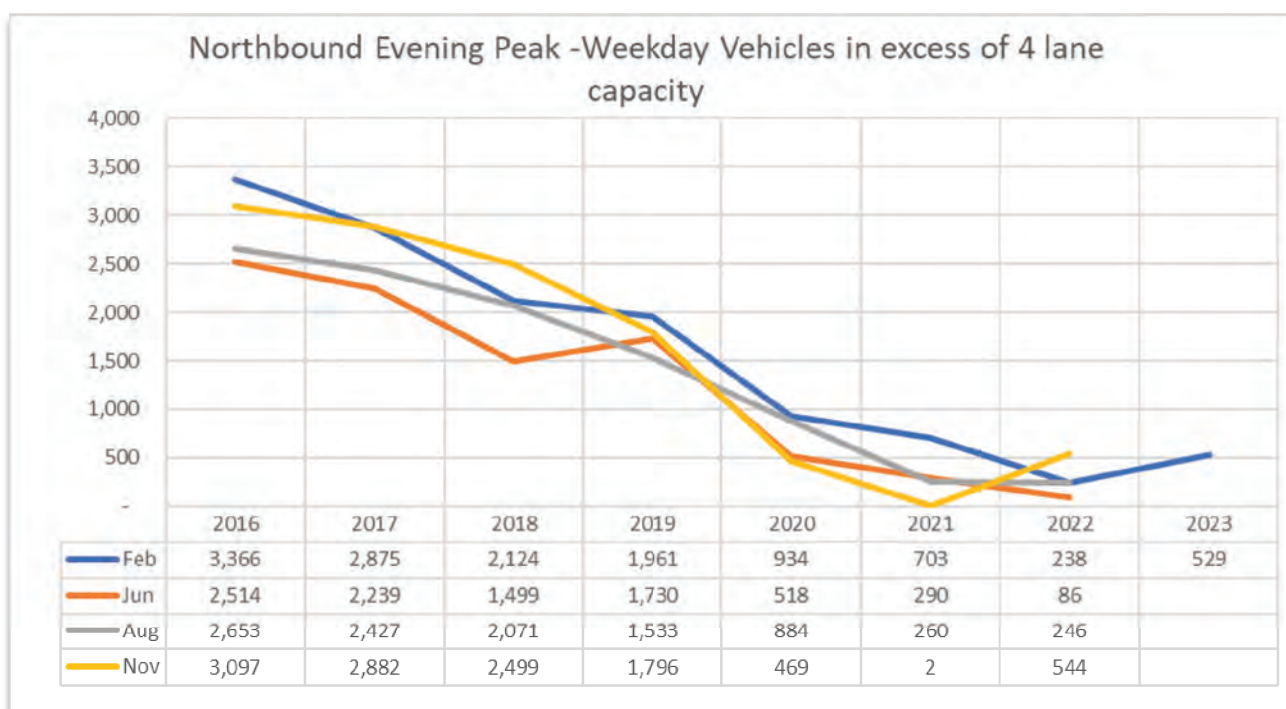


Figure 15 Northbound Evening Peak (15:00-19:00) number of vehicles in excess of 4 lane Proven Capacity

Assessment of Quarterly Trends

For each quarter since 2013 the average weekday peak period flows have been expressed as a percentage of a 4-Lane AHB Proven Capacity (7270 vph) southbound Figure 15, northbound Figure 16.

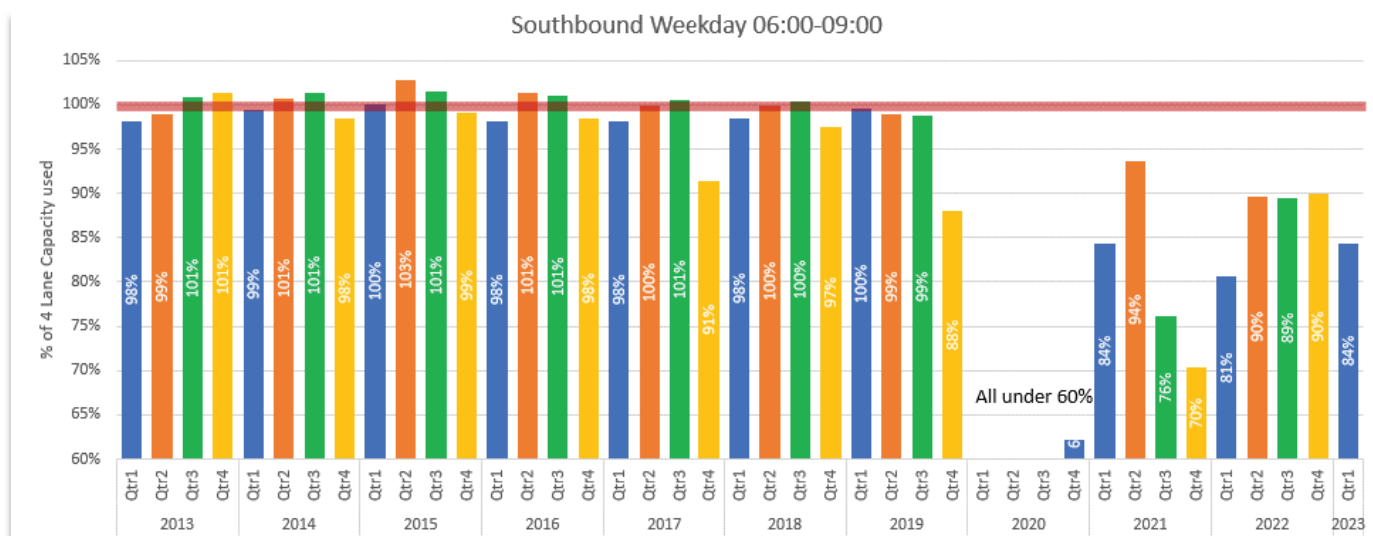


Figure 16 Average quarterly southbound peak (06:00-09:00) flow as a percentage of proven 4-Lane capacity

Since 2016 the weekday morning peak southbound quarterly average traffic flows have reduced. (Figure 16)

In each quarter of 2018 and 2019, the weekday southbound morning traffic flows were within the capacity of a 4-Lane arrangement, enabling one lane to be converted for people who choose to walk, cycle, scoot etc. with limited impact on traffic flows.

The 2022 data shows a traffic peak southbound morning volumes are 10-12% lower than a 4-lane bridge’s capacity.

The latest 2023 data shows a traffic peak southbound morning volumes are 16% lower than a 4-lane bridge’s capacity

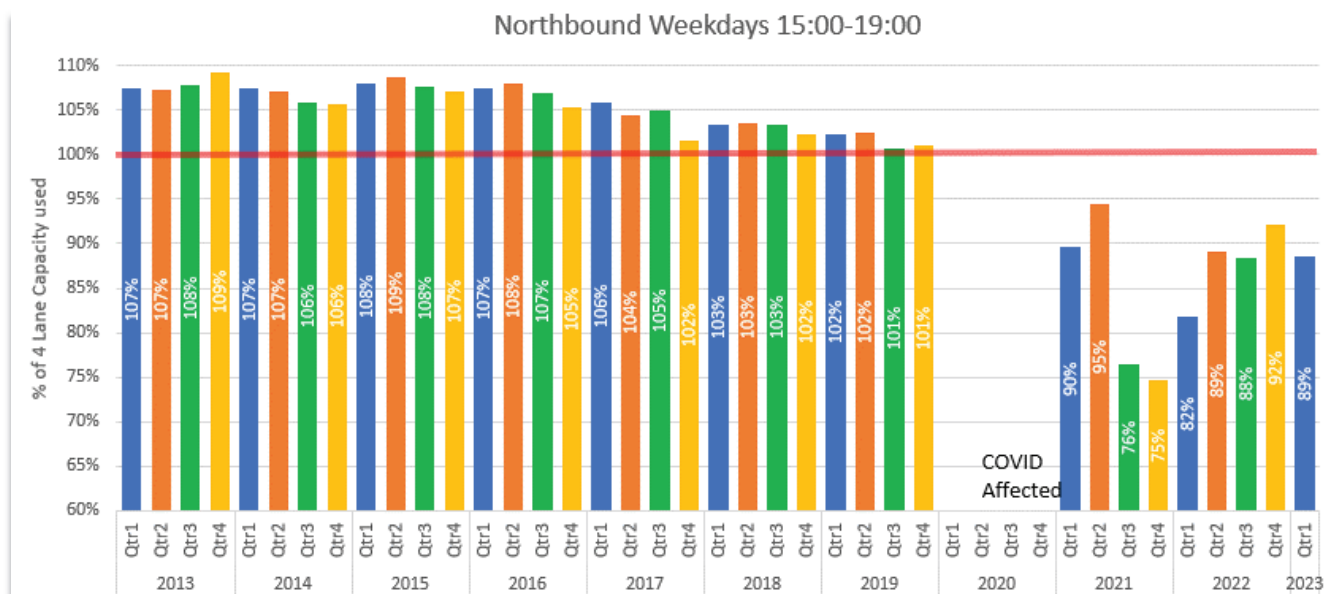


Figure 17 Average quarterly northbound peak (15:00-19:00) flow as a percentage of proven 4-Lane capacity

Since 2016 the weekday morning peak northbound quarterly average traffic flows have reduced. (Figure 17). By 2020, the weekday northbound evening traffic flows were approaching the capacity of a 4-Lane arrangement.

The 2022 data shows a traffic peak northbound evening volumes are 11-14% lower than a 4-lane bridge’s capacity.

The latest 2023 data shows a traffic peak southbound morning volumes are 11% lower than a 4-lane bridge’s capacity

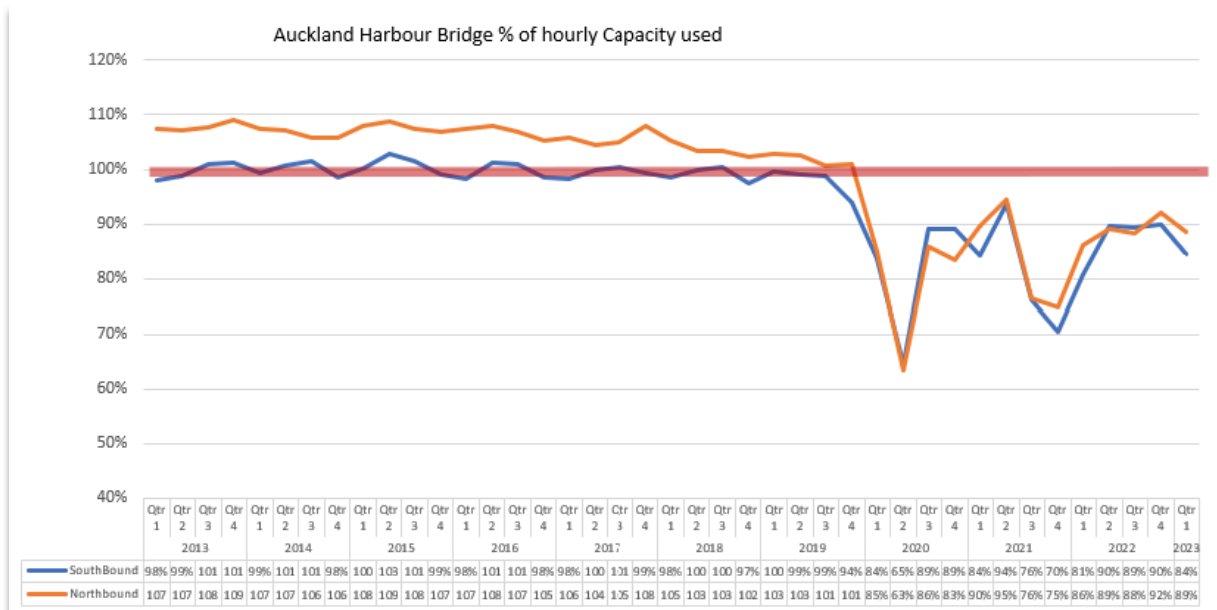


Figure 18 Percentage of 4-Lane Capacity used 2013-2023 Southbound (06:00-09:00), Northbound (15:00-19:00)

When these graphs are combined (Figure 18) the trend of reducing traffic flows prior to the 2020 lockdowns is clearly evident. The late 2021 and all the data for 2022 and the first three months of 2023 is also showing that the peak period traffic can be accommodated on a 4-lane carriageway.

8. Off-Peak flows

Waka Kotahi have expressed¹⁵ concerns that there would be significant risks of the AHB not operating effectively in off-peak weekday periods if one lane was converted for active modes.

To assess the validity of this concern an analysis was undertaken to determine:-

- Frequency of capacity being exceeded each off-peak hour with an active lane installed
- Severity of capacity shortfall each off-peak hour with an active lane installed

Between 2013 and 2022 for each one hour between 09:00-15:00 the number of 15 minute periods when the Proven Capacity was exceeded was calculated. For each quarter year there are 1,560 15-minute weekday off-peak periods.

Figure 19 shows that 2015 Q4 southbound had the largest number of periods (297) when the off-peak liberated capacity was exceeded, this equated to 19.1% of the off-peak period. Over time the frequency and timing of these over-capacity events reduces with capacity exceeded for 104 hours (6.6% of off peak period) in 2019. By 2021 only 54 hours (217 15-minute periods) across the year would have occurred (3% of the time) and predominantly between 13:00-14:00

¹⁵ Meeting between author and Waka Kotahi 27/5/22

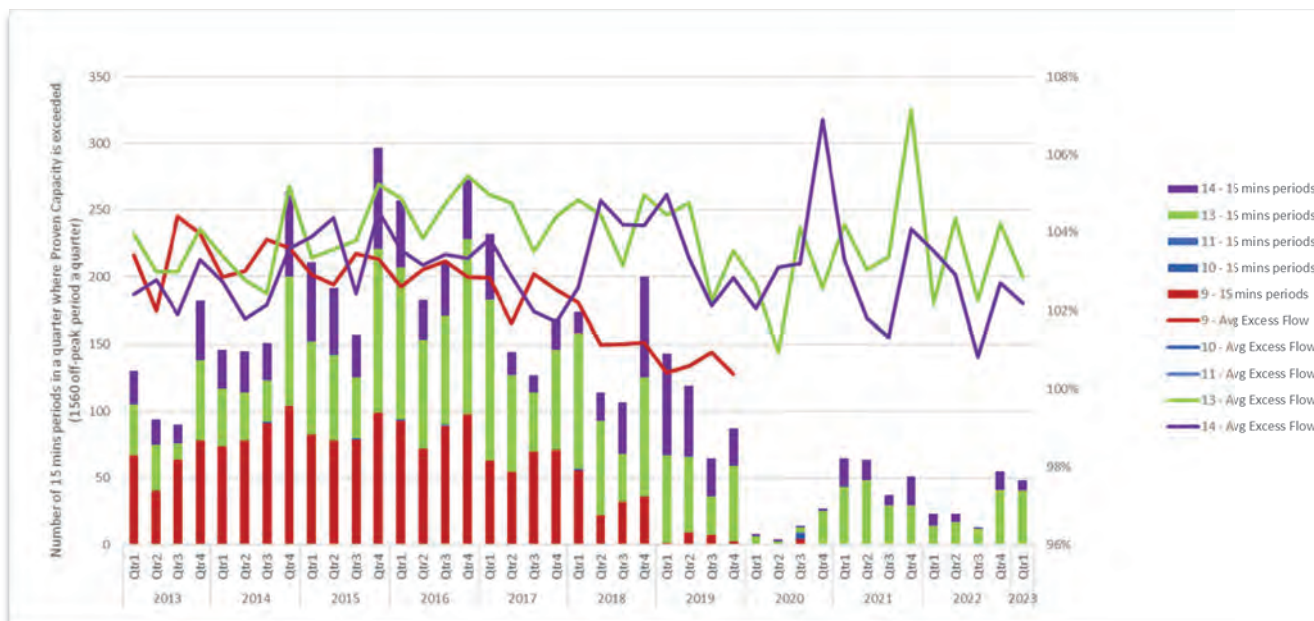


Figure 19 Southbound Off-Peak, frequency and severity of periods where flow exceeds Proven Capacity

Figure 20 shows that 2016 Q4 northbound had a large number of periods (247) when the off-peak Proven Capacity was exceeded, this equated to 15.9% of the off-peak period. With the exception of Q4 2019 and 2020¹⁶, over time the frequency and timing of these over-capacity events reduced with capacity exceeded for 160 hours (10.3% of off- peak periods) in 2020. By 2021 only 107 hours (430 15-minute periods) across the year would have occurred (6.8% of the time) and predominantly between 12:00-13:00.

¹⁶ Unclear why these have such high values, further investigation required.

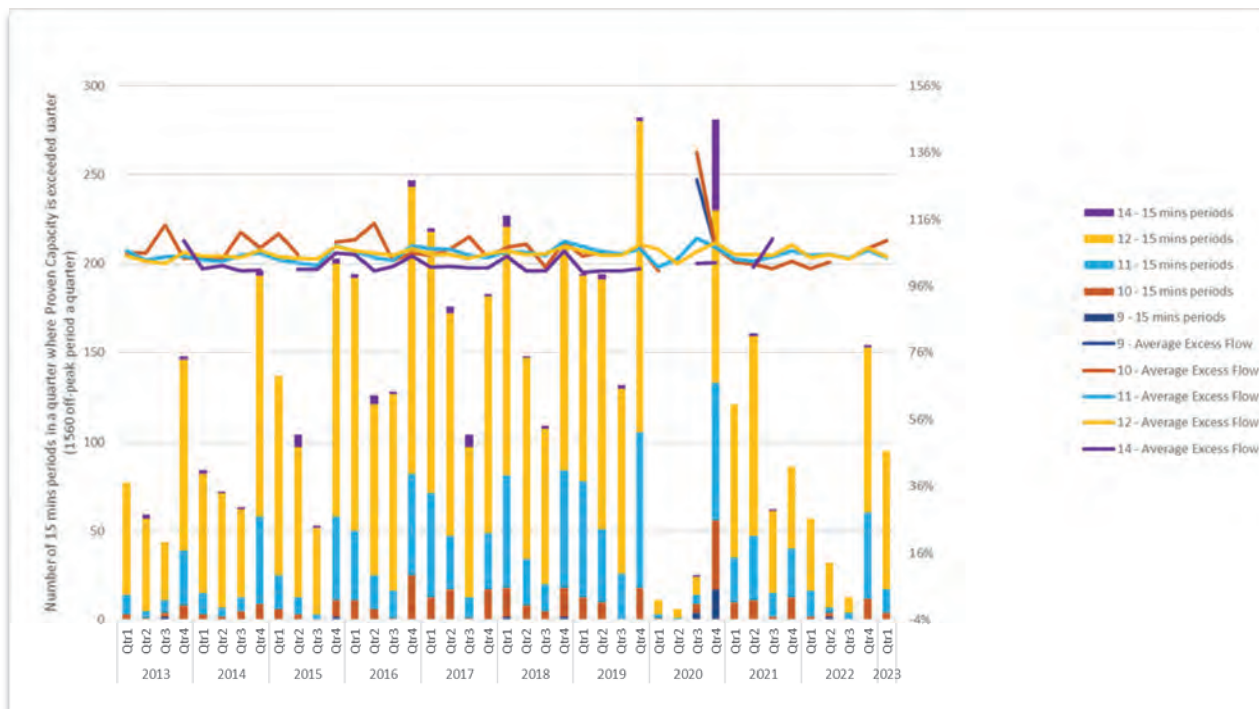


Figure 20 Northbound Off-Peak, frequency and severity of periods where flow exceeds Proven Capacity. (High values in Q4 2019 and 2020 need further investigation.)

The trend of graphs Figure 19 Figure 20 clearly shows that frequency of off-peak capacity exceeding the Proven Capacity is reducing (3-6% of the time) and focused between 12:00-14:00. This indicates a low probability that any off-peak delay would not clear before the evening peak period taken as starting at 15:00.

9. Sensitivity Analysis of 4-Lane Capacity

The 4-Lane capacity of the AHB is the most relevant as in any 4+3+1Active Lane arrangement the 4 Lane direction will take the peak traffic flow.

The AHB does not have any on/off ramps, traffic speed is limited to 80km/h, morning peak period Heavies are 5%¹⁷ and the bridge has a maximum gradient of 5%. It is therefore a relatively controlled environment.

As described above, during peak periods (prior to Feb 2022) the AHB operated in a 3+5 Lane arrangement with the 4+4 Lane arrangement being limited to 10am-3pm (Table 1). During these daytime periods the recorded traffic volumes were lower and therefore are unlikely to represent the maximum flow that the AHB can carry with 4 Lanes.

Additionally, any morning peak 3+4+1Active arrangement will comprise of one lane on the clip-on and three on the main span, this is a different lane format than would occur in a conventional 4+4 lane arrangement.

The NSW Motorway design guide (Capacity and flow analysis)¹⁸ provides some Australian, UK and German examples of ranges flows for differing lanes arrangements.

The 'real world' data from the AHB on 4-Lane capacity is limited and it is therefore reasonable to assess the sensitivity of varying the 4-Lane capacity.

A range of 4 Lane capacities have been assessed (Table 4) for the 2019 southbound morning peak period (06:00-09:00) data to investigate the sensitivity of a 4+3+1Active arrangement. This analysis shows that should a 4 Lane capacity of 7,500 be achievable (300vph higher than Waka Kotahi provided) then for close to 200 out of 240 weekdays¹⁹ in 2019 the morning peak flows could have been accommodated on a 4+3+1Active Lane arrangement. If those 2019 flows in the morning peak reduced by only 2% this would increase the days from 200 to 230, leaving only 12 days where capacity would have been exceeded in 2019.

¹⁷ From 22,694 15-minute interval records from 2013-2021.

¹⁸ Motorway design guide: Capacity and flow analysis – April 2017 Version: 1.0 Table 3

¹⁹ Last two weeks of December and first two weeks of January excluded.

Table 4 Sensitivity analysis for varying capacities of a 4 Lane arrangement (Southbound 2019 mornings)

Capacity	Comment	% of days where Capacity is sufficient to meet demand	Traffic flow reduction % (and approx. count) required to not exceed Capacity on 95% of weekdays
7,200	Provided by Waka Kotahi	39%	6.5% (1,430)
7,270	Proven capacity based on data	42%	6.5% (1,430)
7,300		51%	5.5% (1,200)
7,400		67%	3.5% (770)
7,500		81%	2.0% (440)
7,600	NB with 3 Lanes 5,800, 1 lane 1,800	90%	1% (220)

February - March 2022 4-Lane Operation

Waka Kotahi have provided²⁰ data on the dates and counts when the AHB operated in a 4+4-Lane arrangement. It is understood that this arrangement was necessitated due to staff shortages associated with COVID-19 isolation requirements preventing the movable barrier being relocated. From Monday February 28th to Monday 28th March (20 working days) the AHB operated in this 4+4-Lane Arrangement (Figure 21).

²⁰ OIA 9816



Figure 21 Feb-March 2022 4-Lane Arrangement

Figure 22 shows the 3-hour southbound morning peak period counts on the AHB for March from 2017-2022 with Figure 23 the comparable evening peak period northbound count. These graphs indicate that 2022 volumes were noticeably lower than 2017-2020 (Pre COVID) and also 2021 (COVID affected).

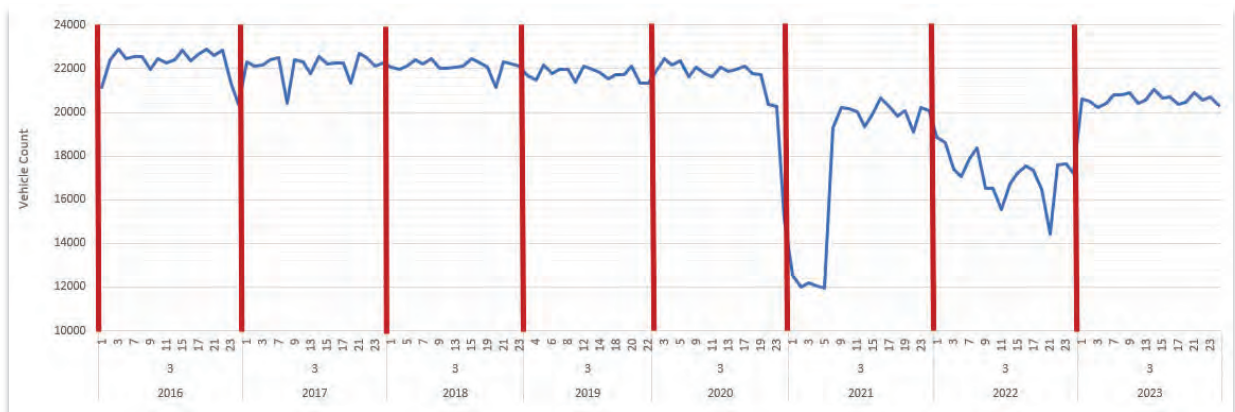


Figure 22 3 Hour weekday count (06:00-09:00 Southbound) March 2017 -2023

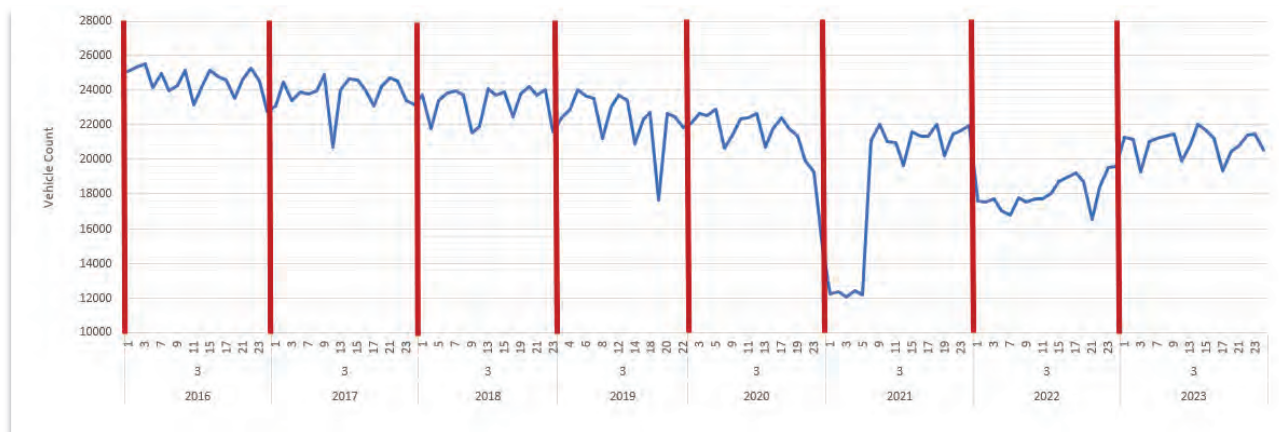


Figure 23 3 Hour weekday count (16:00-19:00 Northbound) March 2017 -2023

It is considered likely that the March 2022 volumes have been affected by COVID related factors but in both directions the traffic volumes recorded were significantly below Waka Kotahi's Stated Capacity for a 4-Lane arrangement (21,600 for 3 hours).

Waka Kotahi data from March 2022 demonstrates that having 4-Lanes available in the predominant flow direction was more than adequate to cater for peak flows during that period.

Whilst the volumes recorded in March 2022 were below comparable periods prior to the COVID pandemic they did provide some evidence to support hourly flow rates in excess of the Waka Kotahi Stated Capacity of 7,200vph (Table 2) for a 4-Lane road. Based on 15-minute counts over the two busiest peak hours in each direction around 1% of hourly flows exceeded 7,300 vph with an absolute peak of 7,452vph²¹. Figure 24 and Figure 25.

The 2023 data for both directions shows an increase on 2022 but still below the 2020 pre pandemic levels.

²¹ 28/2/22 17:30 1,863 vehicles in 15 minutes northbound, equivalent to 7,452vph.

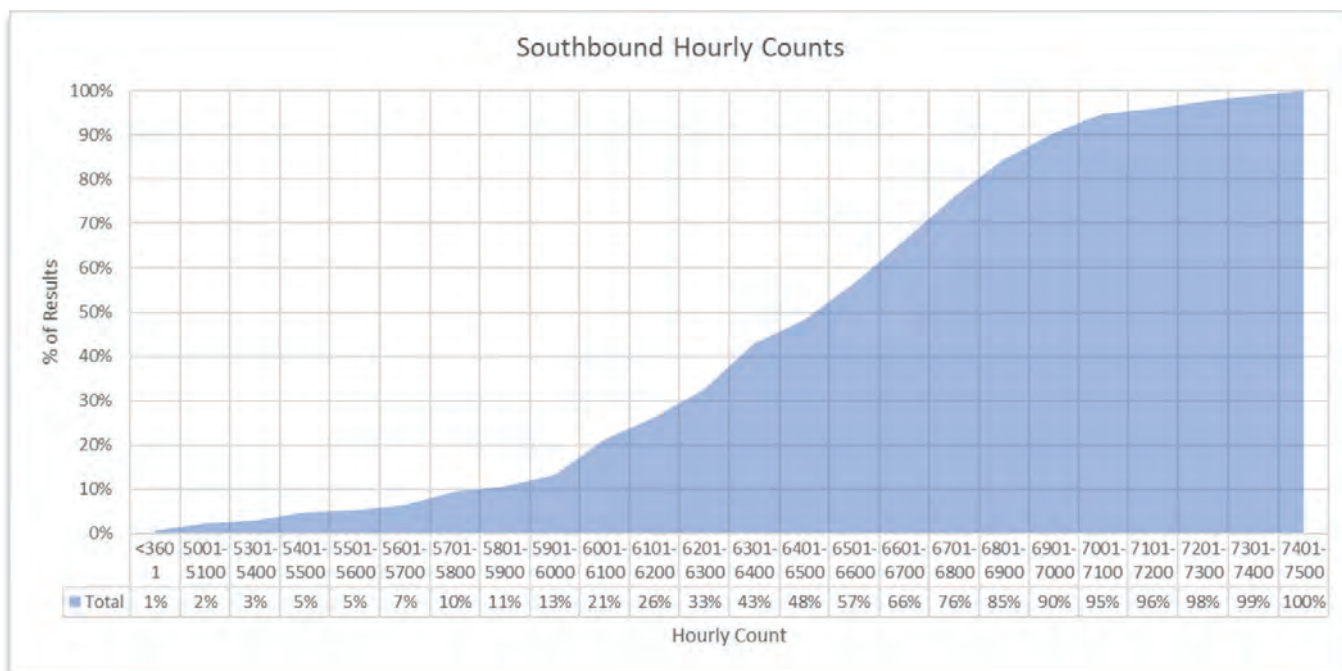


Figure 24 Distribution of Southbound Flow Rates (M-F 07:00-09:00) every 15 minutes March 2022. Maximum 7,444vph, 95%ile 7,240vph

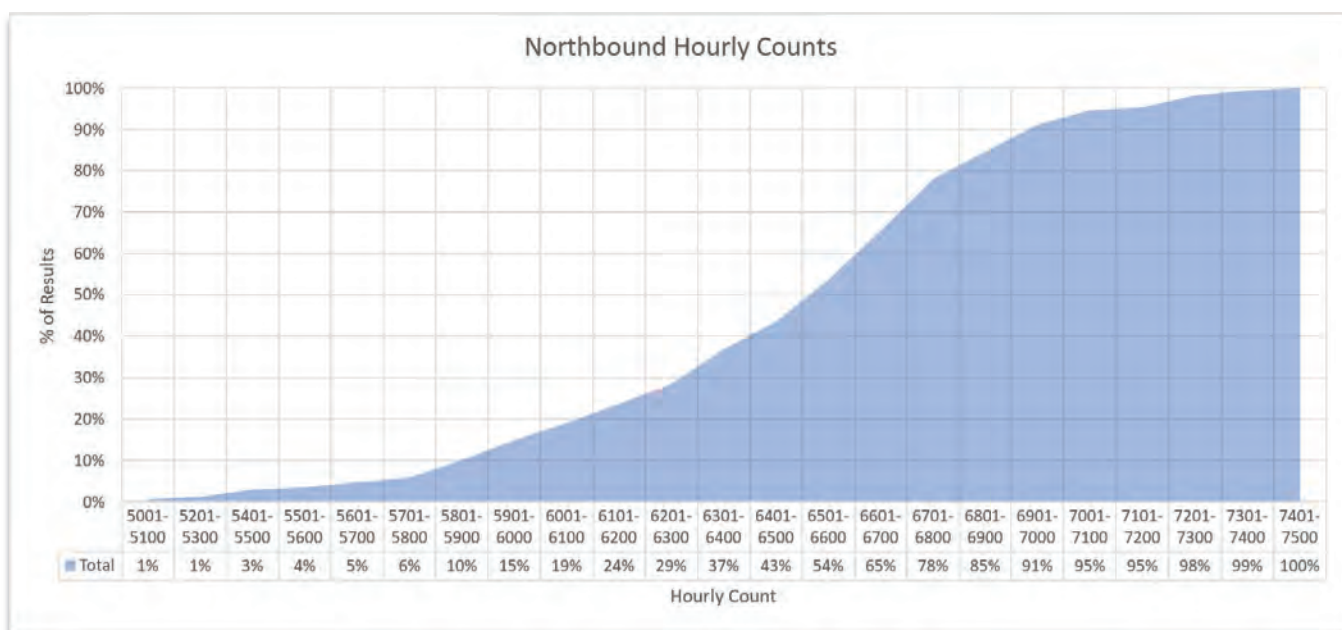


Figure 25 Cumulative Distribution of Northbound Flow Rates (M-F 16:00-18:00) every 15 minutes March 2022. Maximum 7,452vph, 95%ile 7,216vph

These figures support the view that the Waka Kotahi Stated Capacity of a 4-Lane AHB of 7,200vph is conservative and although the Proven Capacity used in the analysis of 7,270vph Table 2 is realistic it to may also be conservative as a capacity of up to 7,400vph has been achieved.

The sensitivity analysis shown in Table 4 shows that at 7,400vph the 2019 morning peak volume would only need to reduce by 3.5% to be within a 4-Lane capacity on 95% of weekdays. This would equate to approximately 770 vehicles across three hours, or one bus full of commuters every 25 minutes²².

²² Based on a notional bus capacity of 100 people replacing single occupancy vehicles.

10. Wider State Highway Network

This analysis has focused on the carrying capacity of the Auckland Harbour Bridge. Waka Kotahi have stated that their modelling '*suggests*' that²³ '*to have a neutral effect on the wider Auckland transport System (greater than 17,000 vehicles per day or a 10% reduction)*' on the AHB is required.

We have identified a steady reduction in traffic volumes across years (and months of the year) so the 17,000 vpd number would require Waka Kotahi to provide an explanation of its source. It would be a reasonable assumption that this 17,000 vpd reduction was split evenly between northbound and southbound flows – resulting in an 8,500 vpd reduction in each direction.

Southbound Flows

The volume of morning southbound traffic (Excess Traffic) above the 4 Lane Proven Capacity are shown in Figure 26(2016 Week 9) and Figure 27 (2019 Week 29) with additional graphs provided in Appendix A. There was negligible southbound Excess Traffic outside of this period.

These southbound figures show that in early 2016 the typical Excess Traffic volume was around 1,000 vehicles over 4 hours, which reduced to under 100 vehicles by mid-2019. Even in a busy month in 2016 the Excess Traffic (1,000) was far below the Waka Kotahi value of 8,500. By mid-2019 the Excess Traffic was less than 1% of the Waka Kotahi stated value.

The mid-2022 data (Figure 28) shows that in week 29 there was no Excess Traffic and all the current morning flow could have been accommodated within 4 lanes.

²³ Waka Kotahi Investment & Delivery Committee Paper 22/11/21' Auckland Harbour Bridge Walking and Cycling Event' P2

2016 Week 9 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity

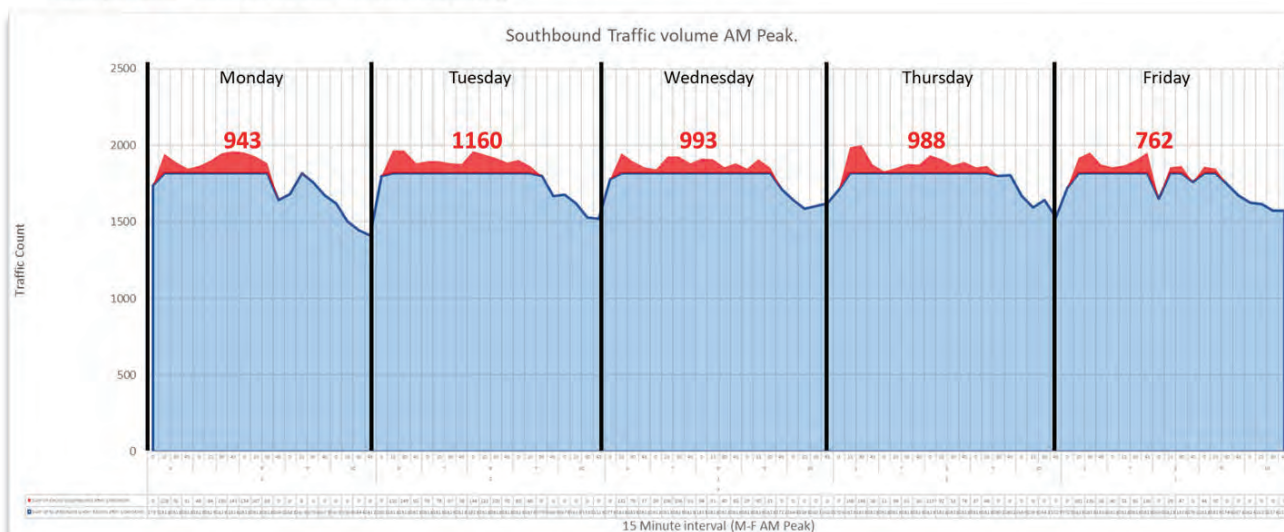


Figure 26 Southbound Excess Traffic 2016 Week 9 Morning Peak

2019 Week 29 06:00-10:00

203 Vehicles above 4-Lane Capacity

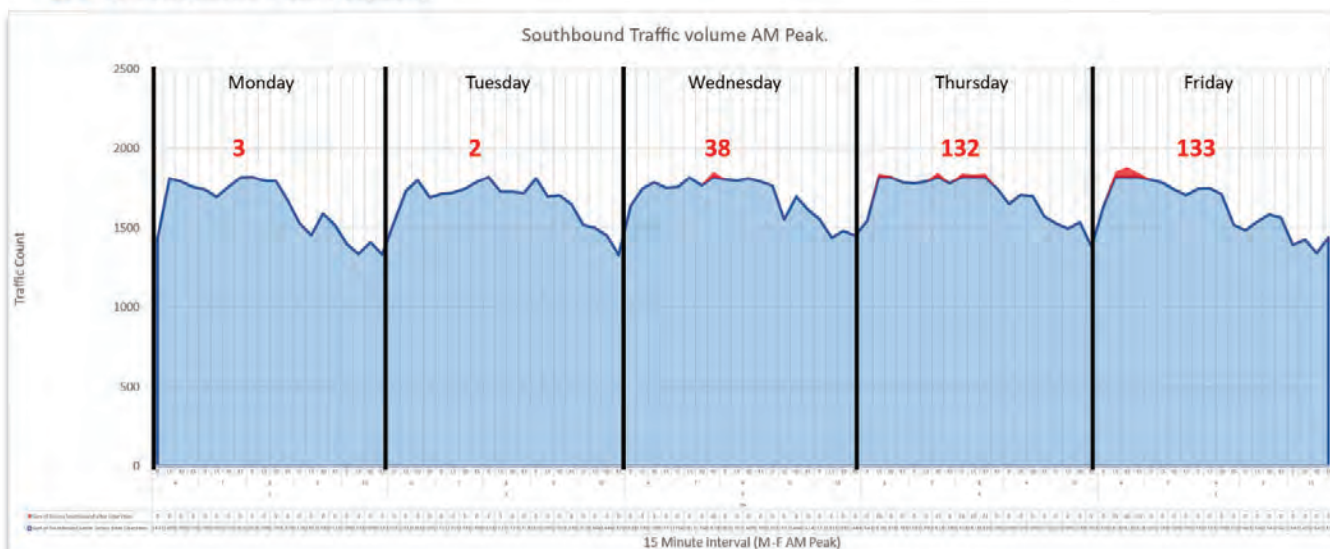


Figure 27 Southbound Excess Traffic - 2019 Week 29 Morning Peak

2019 Week 29 06:00-10:00

Example 0 Vehicles above 4-Lane Capacity

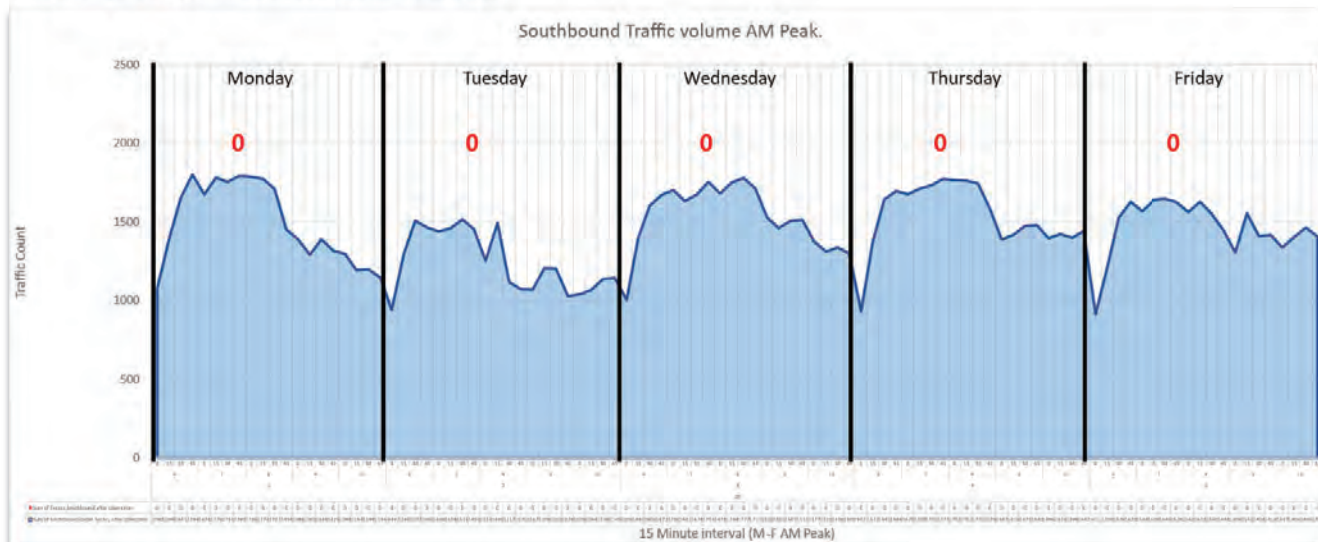


Figure 28 Southbound Excess Traffic - 2022 Week 29 Morning Peak

Northbound Flows

The volume of evening northbound traffic (Excess Traffic) above the 4 Lane Proven Capacity are shown in Figure 29 (2016 Week 9) and Figure 30 (2019 Week 29) with additional graphs provided in Appendix A. There was negligible northbound Excess Traffic outside of this period.

These northbound figures show that in early 2016 the typical Excess Traffic volume was around 3,000 vehicles over 4 hours, which reduced to around 775 vehicles by mid- 2019. In a busy month in 2016 the Excess Traffic (3,000) was only 35% of the Waka Kotahi value of 8,500. By mid-2019 the Excess Traffic was only 9% of the Waka Kotahi stated value.

2016 Week 9 15:00-19:00
Example 203 Vehicles above 4-Lane Capacity

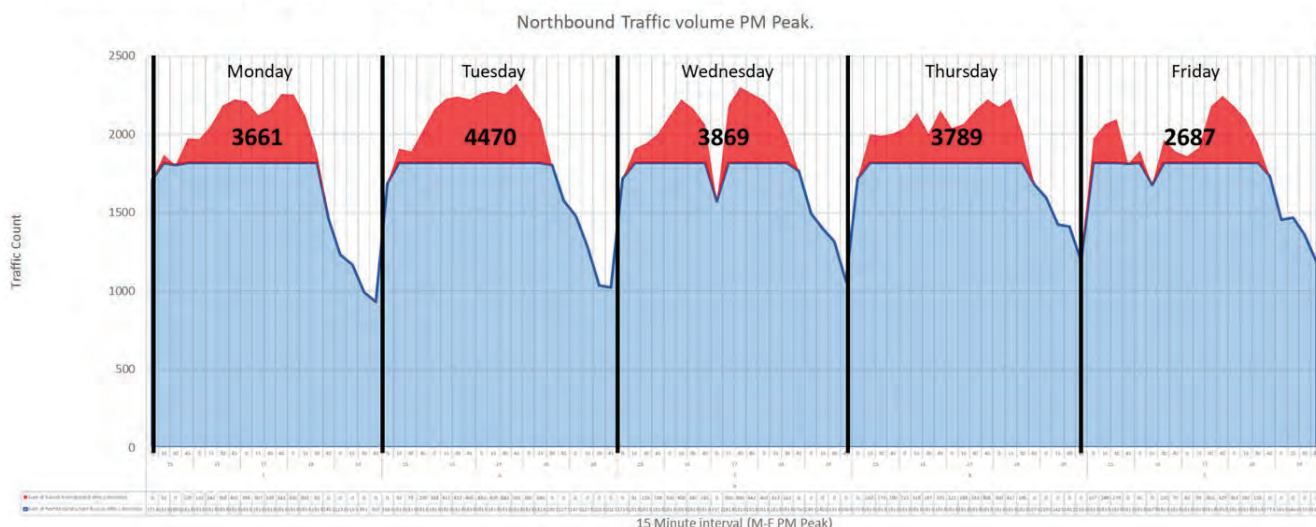


Figure 29 Northbound Excess Traffic 2016 Week 9 Evening Peak

2019 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity

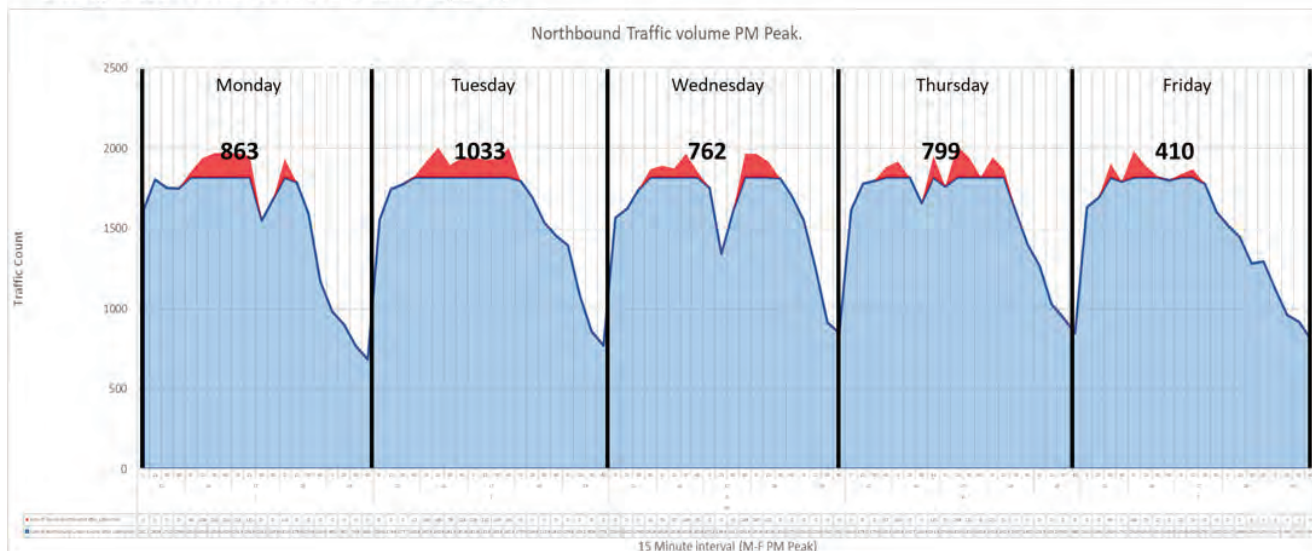


Figure 30 Northbound Excess Traffic - 2019 Week 29 Evening Peak

2022 Week 29 15:00-19:00

Example 0 Vehicles above 4-Lane Capacity

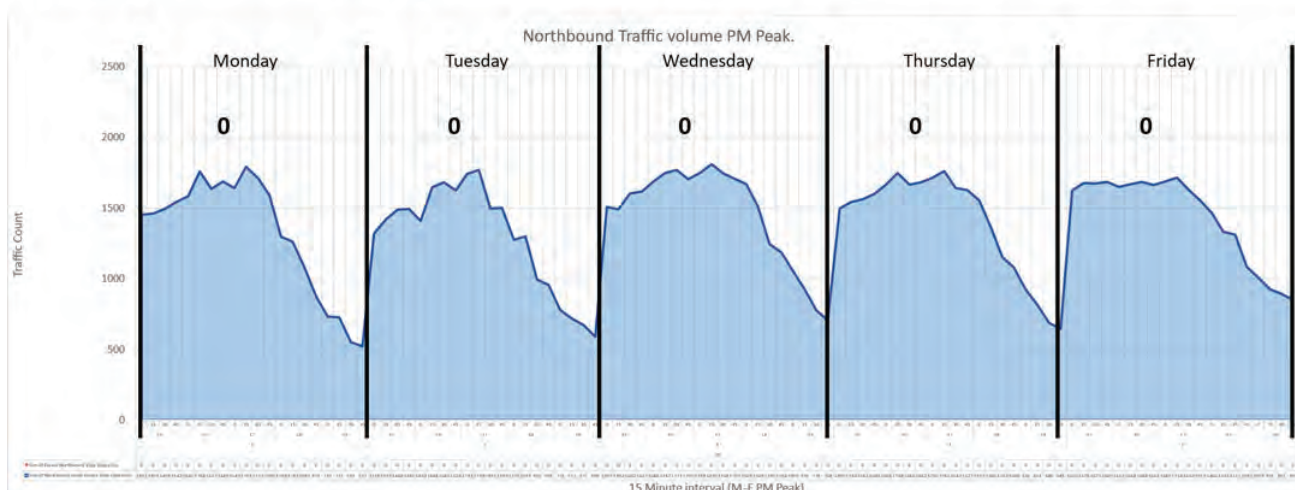


Figure 31 Northbound Excess Traffic - 2022 Week 30 Evening Peak

2022 Week 9 15:00-19:00

Example 624 Vehicles above 4-Lane Capacity

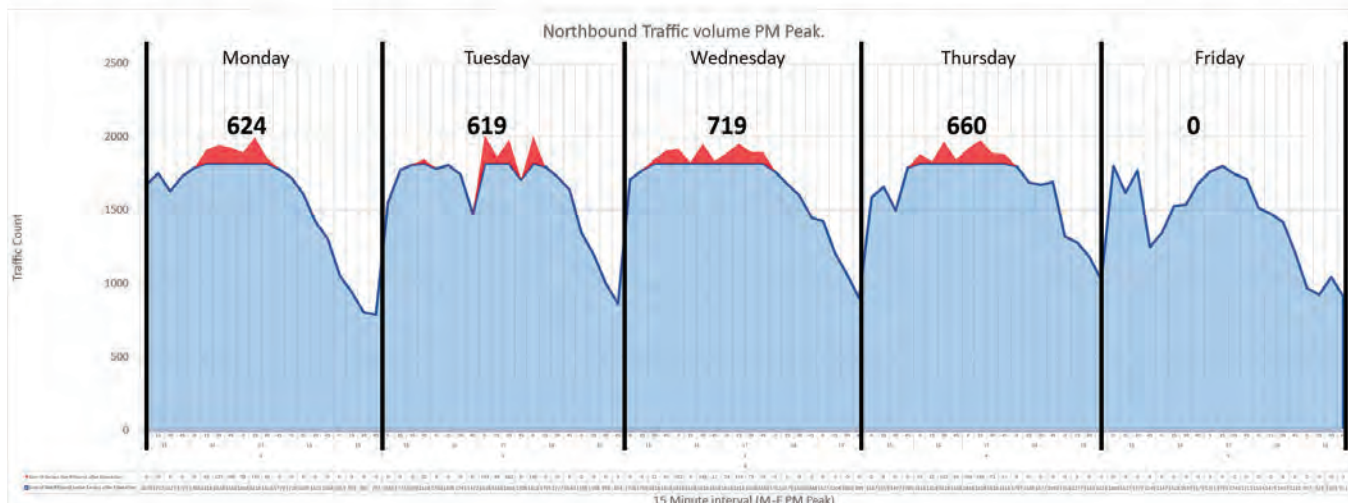


Figure 32 Northbound Excess Traffic - 2023 Week 9 Evening Peak

The mid-2022 data (Figure 31) shows that in week 29 there was no Excess Traffic and all the current morning flow could have been accommodated within 4 lanes.

Both Directions

Based on the analysis above and additional analysis we have undertaken that analysed every working day since 2013 (Figure 33 shows all of 2018), we have not identified a single day since 2013 where a 17,000 vpd reduction would have been required to enable the AHB to operate in a 4+3+1Active arrangement.

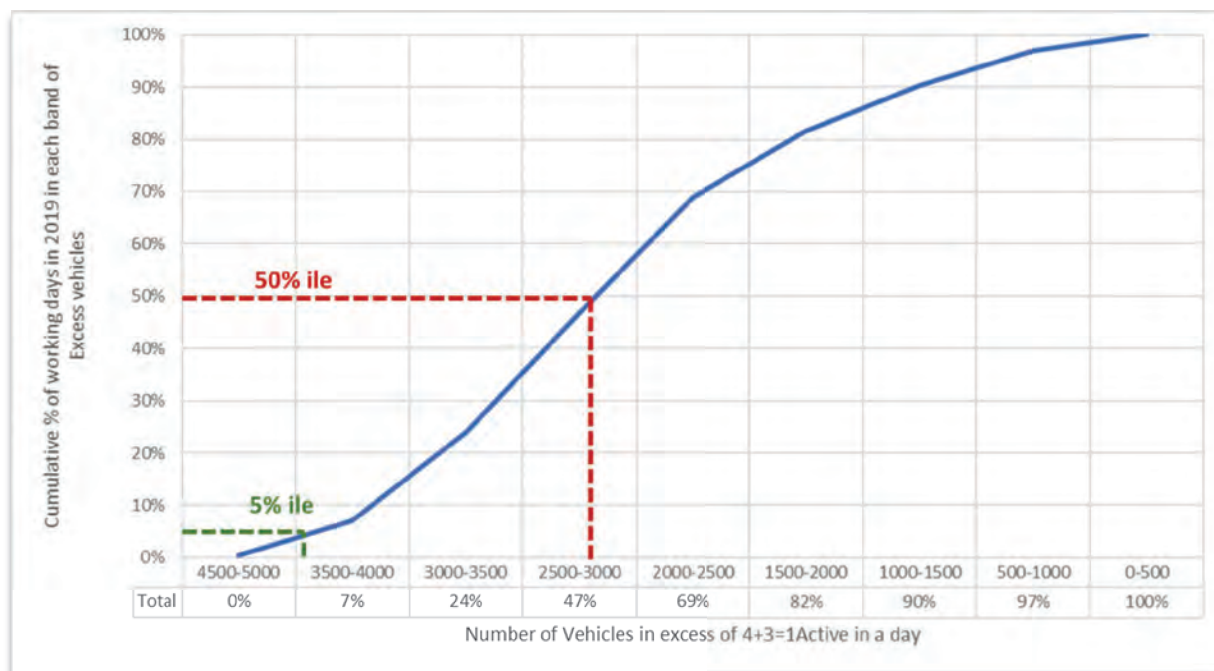


Figure 33 Percentage of Weekdays in 2019 where the number of vehicles (Excess) was above the capacity of a 4+3+1 Active arrangement.

Figure 33 identifies that on only **5%** of weekdays in 2019 did the AHB carry up to 4,000 more vehicles than could have been accommodated by a 4+3+1 Active arrangement.

In 2019 an average reduction of under 3,000vpd was required to enable a 4+3+1 Active arrangement to operate with no impact for at least **50%** of the time, much of this reduction was already occurring prior to the pandemic.

We therefore cannot find any evidence to support Waka Kotahi's statements that repurposing a lane of the AHB for active modes would require a 17,000vpd reduction to neutralize any adverse effects of the wider Auckland network.

11. Other Factors

The analysis undertaken is limited to assessing known data and identifying how effectively a 4+3+1 Active lane arrangement on the AHB could have worked since 2013. There are numerous other factors that are likely to further reduce any potential impacts of the Active Lane on current traffic flows.

Reduced 2022 Traffic Volumes due to changes in behaviour due to pandemic

The data clearly shows that traffic volumes have dropped significantly since March 2020. Some of this reduction has been due to enforced lockdowns, but there is evidence emerging that there are longer term changes to traffic volumes and timings occurring.

Increases in the number of people working from home and split shifts / time shifting will have direct beneficial effects on reducing demand on the AHB. Even small reductions (1-2%) of volumes during peak periods would make significant changes to the expected number of peak periods where a 4+3+1 Active arrangement would operate with little impact. For example, using 2019 flows just a 2% reduction in peak period southbound traffic increases the number weekdays a year by 50 days when a 4+3+1 Active lane would have operated effectively. Based on the downward trend of traffic volumes since 2016 this 2% reduction would have been significantly exceeded by 2022.

Impact of fuel price rises, car tax changes and PT uptake and subsidy

The recent significant fuel price rise and extra tax on many new vehicles, combined with the 50% subsidy to incentivise use of PT are both factors that are likely to reduce vehicles usage of the AHB. Whilst the PT subsidy is currently only announced to last three months the ending of the subsidy is likely to coincide with the reintroduction of an extra 25c/litre in fuel tax. These factors may all result in further and accelerating reductions in vehicle trips over the AHB.

As a bus carries between 80-100 people, only a few extra full buses replacing single or low occupancy vehicles would have significant beneficial impact on the AHB's capacity to add an Active Lane with minimal impact on traffic flows.

The option of using the southbound clip-on as a bus priority lane (along with Ponsonby bound traffic) could further speed up bus travel into the CBD making mode shift more likely. The soon to be opened Northern Busway extension could then become even more attractive. The analysis of how this would affect the morning traffic flows was outside the scope of this report.

Uptake of Active modes

This report does not set out to examine the predictions around predicted patronage usage of an Active Lane. Projected usage figures of 3-5,000 trips a day have been quoted by Waka Kotahi, of which some will be leaving their cars at home. Figure 15 shows that in 2020 and 2021 it only required a few hundred fewer northbound vehicle movements across the four hour peak period to reach a flow where the 4+3+1 Active arrangement can cater for demand.

With no current active modes options available to cross the Waitemata Harbour the comparison of changes to other crossings around the world is difficult to benchmark. Figure 34 shows the recently converted traffic lane on New York's Brooklyn Bridge which has removed cyclists from the wooden shared path above the traffic lanes. This cycle path would not meet NZ Standards but has nonetheless resulted in a 90% increase in cycle usage²⁴.

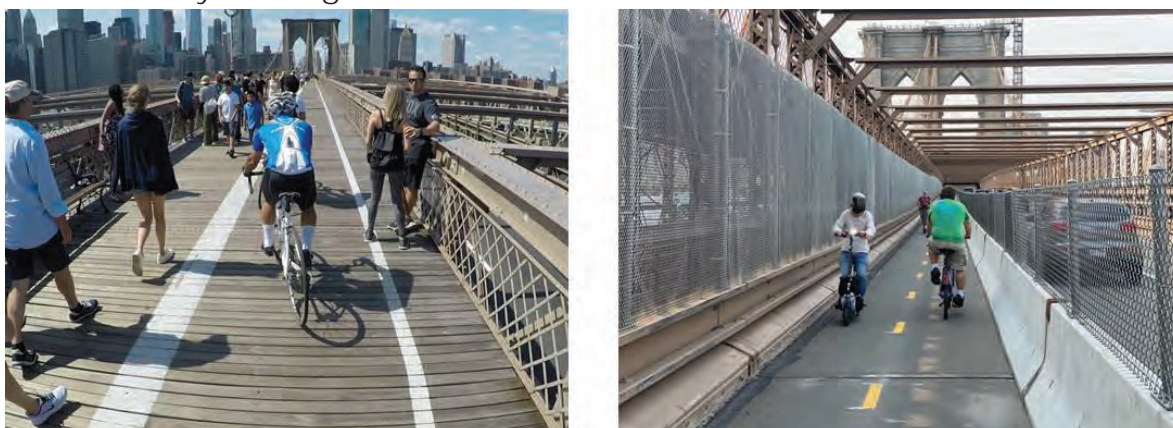


Figure 34 Brooklyn Bridge (left) shared path and (right) new dedicated protected cycle path

One commonly quoted disincentive to cycle on the AHB is the gradient and the proposed 4m width. The bridge has a 5% (1 in 20) maximum gradient and there are many well-used urban cycle paths in New Zealand that are steeper and narrower.

Additionally, the sales of e-bikes (Figure 35) shows that the trend of sales has outpaced even the most extreme predictions, so these combined with e-scooters are

²⁴ 88% increase in cycle usage from Oct 2020 to Oct 2021, <https://www1.nyc.gov/html/dot/html/pr2021/brooklyn-bridge-bike-ridership-skyrockets.shtml#:~:text=Brooklyn%20Bridge%20Bike%20Lane%20Ridership%20Skyrockets,-Report%20shows%20continued&text=NEW%20YORK%E2%80%94NYC%20DOT%20today,lane%20opened%20in%20September%202021.>

likely to increase the number and distance that people will be willing to commute. These factors all are likely to reduce traffic usage on the AHB.



Figure 35 E-bike sales NZ (Via Strada data)

12. Conclusions

From analysing the Waka Kotahi traffic count data on the Auckland Harbour Bridge (AHB) we can conclude.

1. The trend from 2016-2019 indicates that (even without the impact of the COVID-19 pandemic) the morning peak period southbound traffic flows on the AHB would be within the 4 Lane capacity by 2022.
2. The latest 2023 data clearly shows significant traffic flow reductions since 2016 to a point where in July 2022 all existing traffic flows could have been accommodated with 4+4+Active lane arrangement.
3. The trend from 2016-2019 indicates that (even without the impact of the pandemic) the evening peak period northbound traffic flows on the AHB would be less than the 4 traffic Lane capacity by 2023.
4. There is strong evidence to show that prior to March 2020 (the start of the COVID-19 pandemic) peak traffic volumes on the AHB were reducing, with a traffic reduction of 1.4% recorded between 2018 and 2019.
5. Traffic volumes have declined materially during the pandemic and although there has been some recovery towards the end of 2022 there is little evidence or trend data to suggest that these will soon exceed pre-pandemic values.
6. The conversion of one traffic lane to an Active Lane (4+3+1Active) would have impacted the ability of the AHB to carry traffic volumes at peak times in previous years, however this may not occur in the future. The latest 2023 data suggests that traffic volumes are significantly lower than the peak in 2016.
7. The Lane Capacities (vph) provided by Waka Kotahi have been demonstrated to be conservative. Increases of 200-400vph have been shown to be realistic for the 3 and 5 lane arrangements and even a conservative 70vph increase for 4 lanes (from 7,200 to 7,270) has a material beneficial effect on the AHB to operate in a 4+3+1Active configuration. This 4-lane capacity may be even higher, recent data suggested that flows up to 7,400vph are achievable.
8. There are significant traffic flow variations across the week, season and year. Any predictions on traffic should take into consideration the weekday and month and not treat every day and week the same. By applying this

approach to historic data we have been able to assess the percentage of days that a 4+3+1Active Lane arrangement would have provided sufficient capacity.

9. From 2019 data it appears that on over 100 of 240 working days a 4+3+1Active arrangement would have provided sufficient morning peak southbound traffic capacity.
10. A sensitivity analysis shows that if 7,400vph can be accommodated then the 2019 weekday morning peak traffic flow would only need to reduce by 3.5% to be within a 4 Lane capacity 95% of the time. This is equivalent to replacing single occupancy vehicles with one full double decker bus every 25 minutes.
11. In 2019 a net reduction of 3,000 vpd (at the right time of day) would have been required so that a 4+3+1Active arrangement on the AHB would have provided sufficient capacity for half of the working days that year.
12. 2022 data indicates that further modest reductions of traffic volumes of 2-3% would reduce the number of days when peak period capacity was exceeded to under 12 a year.
13. If a reduction of 5000vpd (at the right time of day) in 2019 occurred then the 4+3+1Active arrangement would have provided sufficient capacity on all working days. By 2022 the reduction the reduction required would be around 3,000vpd.
14. No evidence was found to support Waka Kotahi's statement that a 17,000 vpd traffic reduction on the AHB would be required to neutralise effects on Auckland's transport system.
15. Recent fuel price increases, a 50% PT subsidy and reintroduction of 25c/litre fuel tax are unlikely to generate higher traffic volumes on the AHB.
16. The uptake of more active mode travel options, especially e-bikes and scooters has reached extreme growth rate and would likely surprise on the upside.
17. Recent US experience has demonstrated that using a lead infrastructure approach (*build it and they will come*) for an active mode lane on New York's

Brooklyn Bridge has generated unexpectedly and unpredicted higher active mode patronage.

18. With the June – August period showing the lowest traffic volumes and evidence to date showing 2022 traffic volumes in that period being less than 2018/19 then there is a strong incentive to operate the bridge in a solely 4+4 lane mode. This would save money (no need to move the barriers twice a day) and most usefully provide a wealth of real world data on the actual impact of running 4 lanes for in the peak flow direction. This should be accompanied by accurate lane specific traffic counts and point-to-point journey time monitoring on the approaches to and across the bridge.

13. Completion of Deliverables

This report completes the deliverables for the scope of work that the Client has instructed, our Richard Young is available to answer any questions, make a presentation on this material or undertake any further analysis as you may instruct.

Richard Young

Managing Director, SmartSense Ltd

June 2023.

027 839 1961

richard@smartsense.nz

14. Appendix A

This Appendix contains graphs showing the Waka Kotahi provided peak period 15-minute counts on the Auckland Harbour Bridge for weeks (9 and 29) in each direction from 2016 – 2023. The weeks were chosen to show the seasonal range of traffic flows (9 high, 29 low) and the trend across 4 pre-pandemic years (southbound) and 3 northbound (no data provided for 2019).

On each graph the Proven Capacity of a 4-Lane AHB (7,270 vph) is shown and the excess traffic count above this line calculated. Recent 4+4 Lane data (March 2022 data) suggests that this 7,270vph may be conservative as flows up to 7,400vph have been recorded.

These counts are the excess traffic that a 4 Lane arrangement recorded in that 15 minute period. The counts take no account of Time Shifting (filling in gaps when there was unused capacity), mode shift (to PT, Active, Working From Home etc.). These graphs show the day today, season to season and year to year trend, a summary is provided in Table 5.

Table 5 Summary of Excess traffic Graphs 2016-2023 Southbound

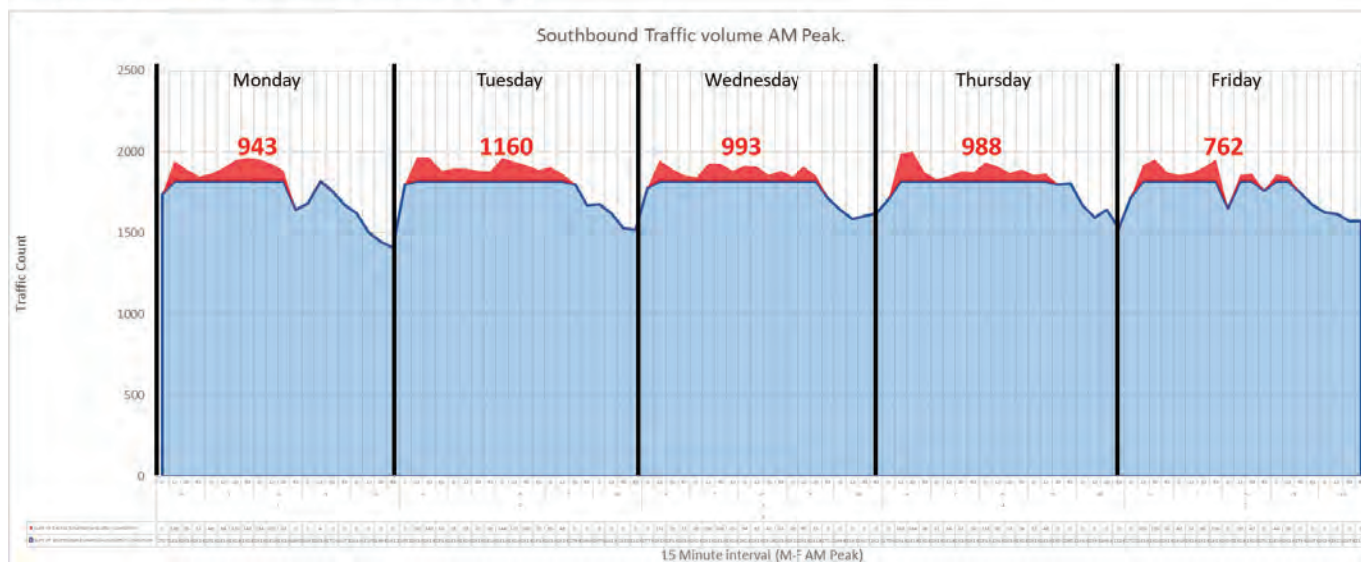
Year	Week	Direction	Peak	Monday	Tuesday	Wednesday	Thursday	Friday	Weekday Total	Weekday Average
2016	9	Southbound	Morning	943	1160	993	988	762	4,846	969
2017	9	Southbound	Morning	457	955	931	949	649	3,941	788
2018	9	Southbound	Morning	527	937	1023	636	640	3,763	753
2019	9	Southbound	Morning	203	346	379	414	114	1,456	291
2020	9	Southbound	Morning							No Data
2021	9	Southbound	Morning							No Data
2022	9	Southbound	Morning	105	497	319	175	0	1096	219
2023	9	Southbound	Morning	19	17	54	5	0	95	19
2016	29	Southbound	Morning	370	603	346	72	54	1,445	289
2017	29	Southbound	Morning	242	301	235	441	130	1,349	270
2018	29	Southbound	Morning	117	225	432	140	19	933	187
2019	29	Southbound	Morning	3	2	38	132	133	308	62
2020	29	Southbound	Morning	0	0	0	0	0	0	0 – No graph
2021	29	Southbound	Morning	0	59	0	99	2	0	32
2022	29	Southbound	Morning	0	0	0	0	0	0	0

Table 6 Summary of Excess traffic Graphs 2016-2023 Northbound

Year	Week	Direction	Peak	Monday	Tuesday	Wednesday	Thursday	Friday	Weekday Total	Weekday Average
2016	9	Northbound	Evening	3661	4470	3869	3789	2687	18,476	3,695
2017	9	Northbound	Evening	2909	3649	3246	3180	2158	15,142	3,028
2018	9	Northbound	Evening	2772	2906	2358	2789	1533	12,358	2,472
2019	9	Northbound	Evening	2143	2359	2240	2322	1166	10,230	2,046
2020	9	Northbound	Evening							No Data
2021	9	Northbound	Evening	793	1634	1000	1271	362	5,060	1,012
2022	9	Northbound	Evening	59	394	1450	254	116	2,273	454
2023	9	Northbound	Evening	624	619	719	660	0	2,653	530
2016	29	Northbound	Evening	3090	1733	2923	2665	1175	11,586	2,317
2017	29	Northbound	Evening	2031	2589	2051	875	1581	9,127	1,825
2018	29	Northbound	Evening	1903	1889	2636	1146	1516	9,090	1,818
2019	29	Northbound	Evening	863	1033	762	799	410	3,867	773
2020	29	Northbound	Evening	238	754	306	273	51	1,376	276 – No graph
2021	29	Northbound	Evening	507	603	432	183	0	1,725	345
2022	29	Northbound	Evening	0	0	0	0	0	0	0

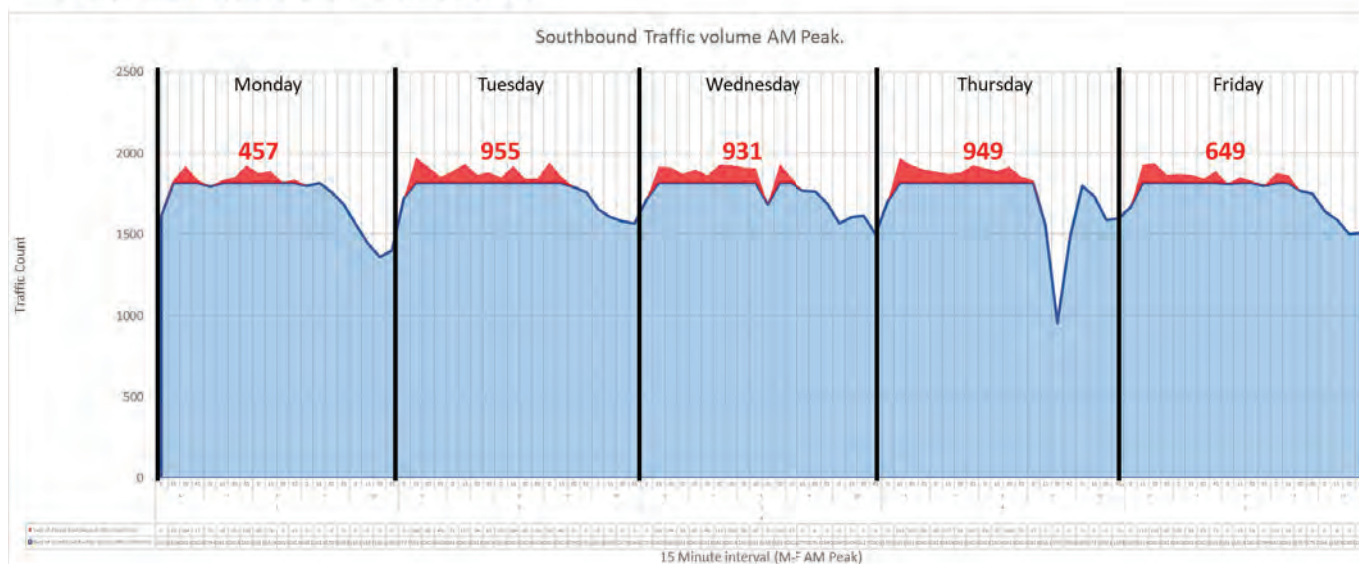
2016 Week 9 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity



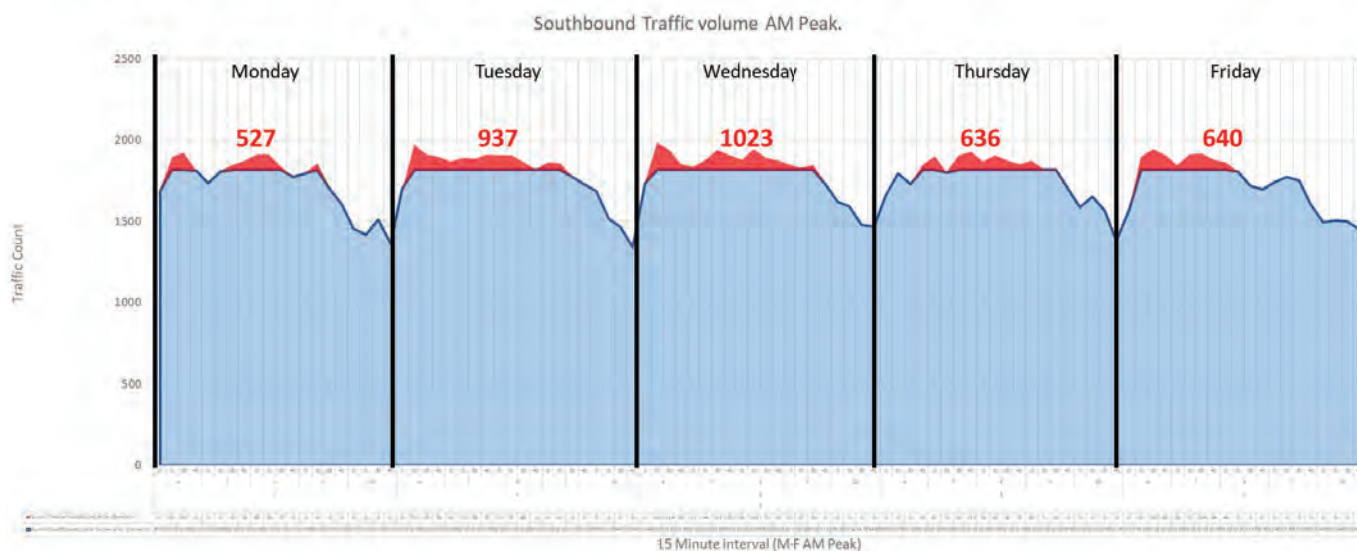
2017 Week 9 06:00-10:00

Example 203 Vehicles above 4-Lane Capacity



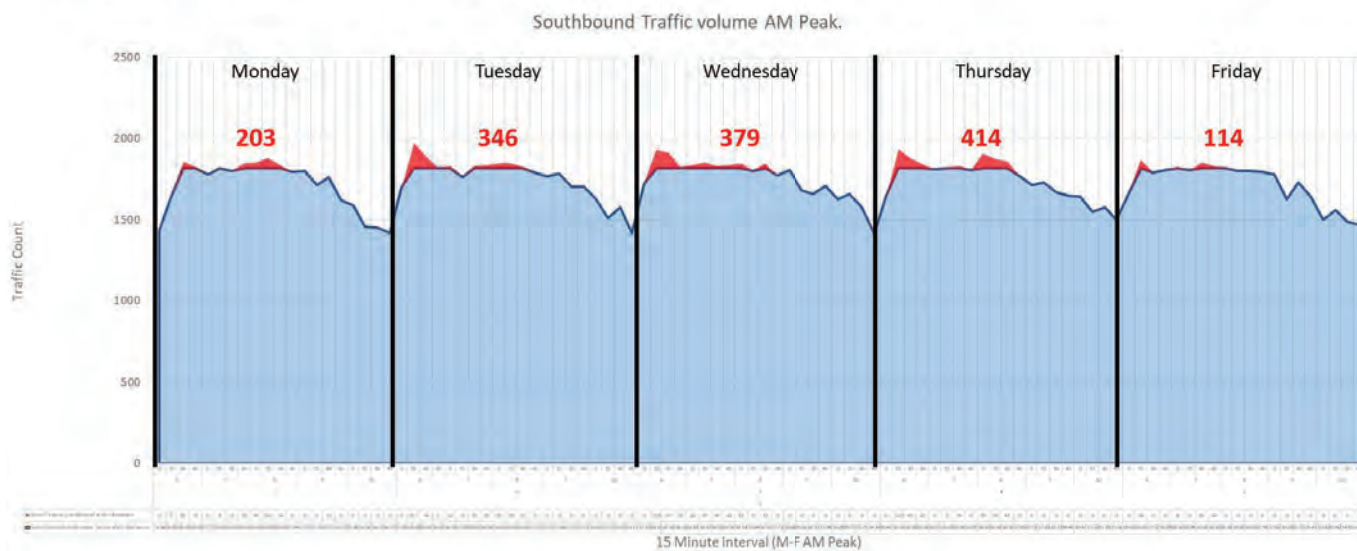
2018 Week 9 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity



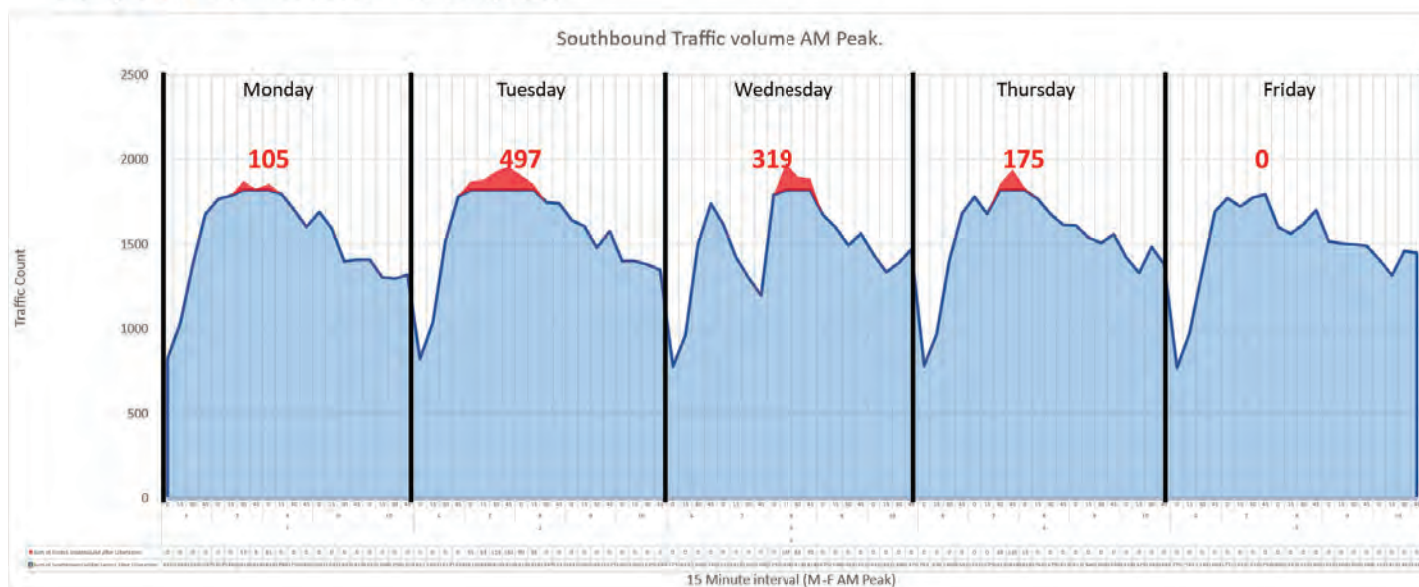
2019 Week 9 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity



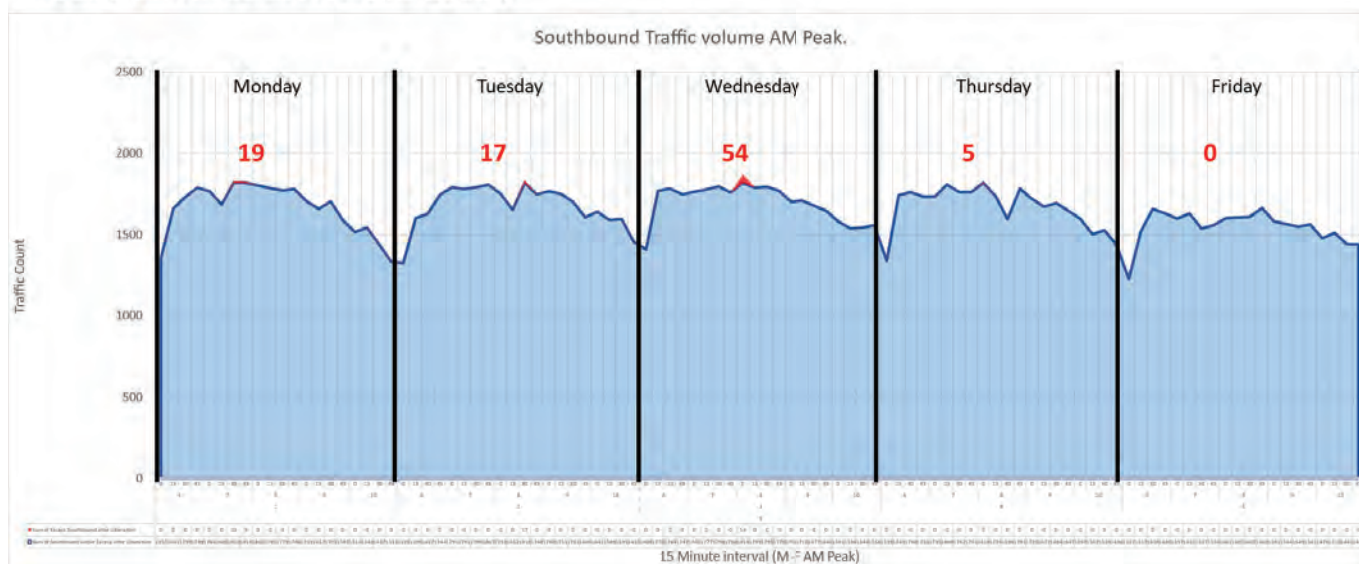
2022 Week 9 06:00-10:00

Example 105 Vehicles above 4-Lane Capacity



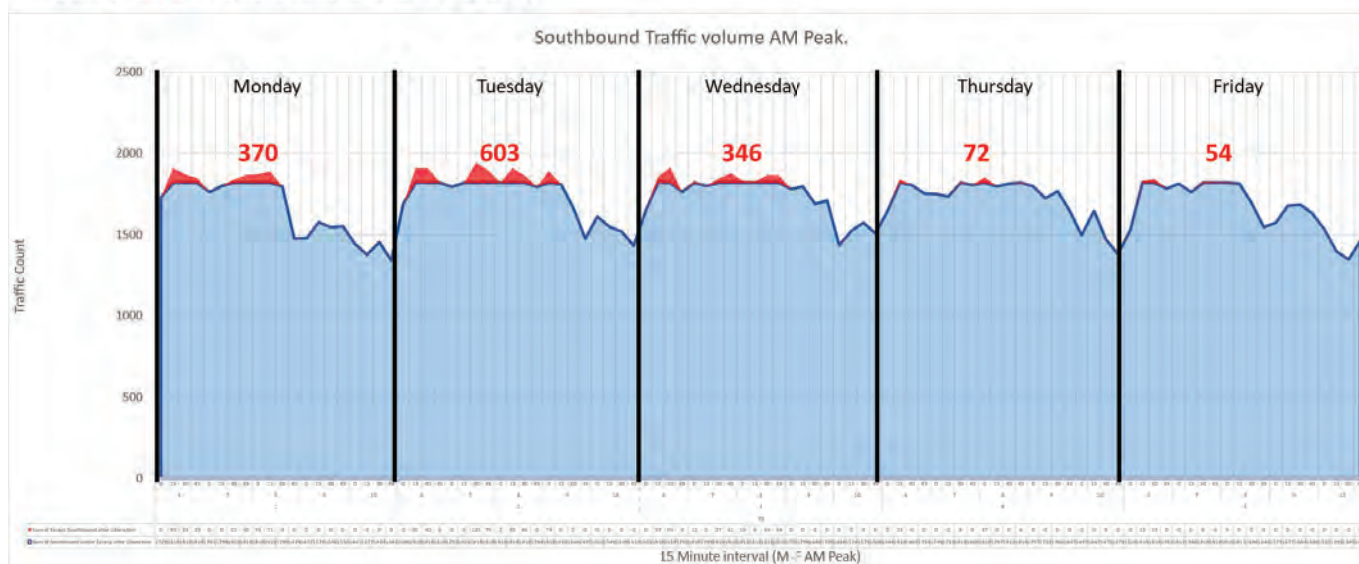
2023 Week 9 06:00-10:00

Example 0 Vehicles above 4-Lane Capacity



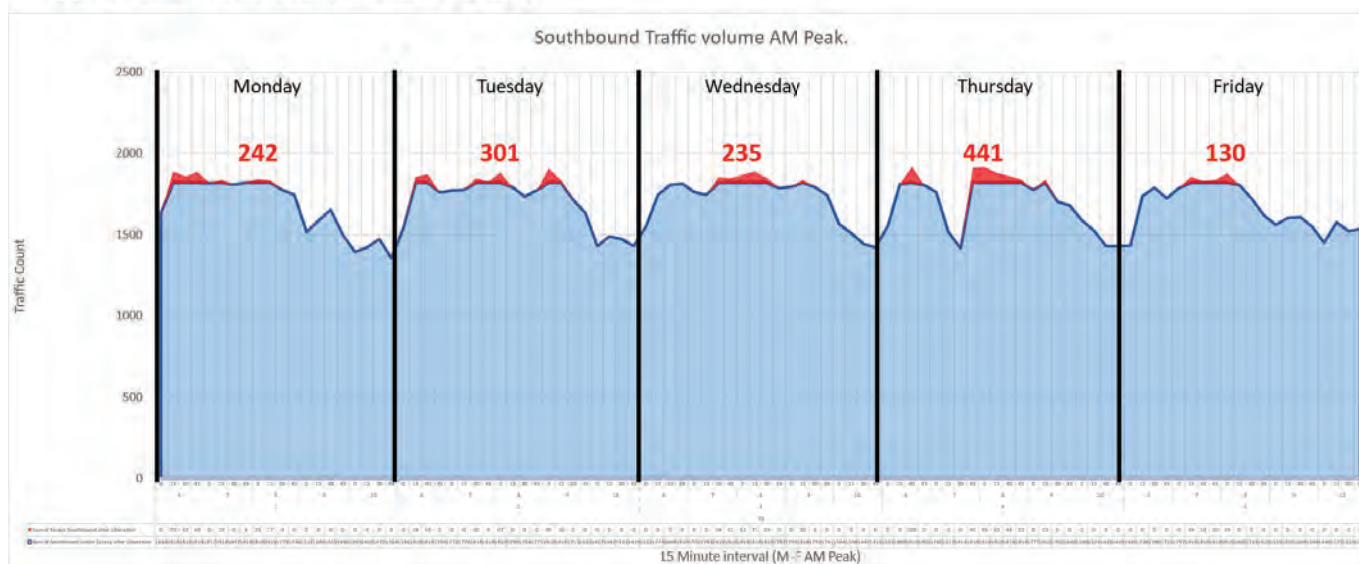
2016 Week 29 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity



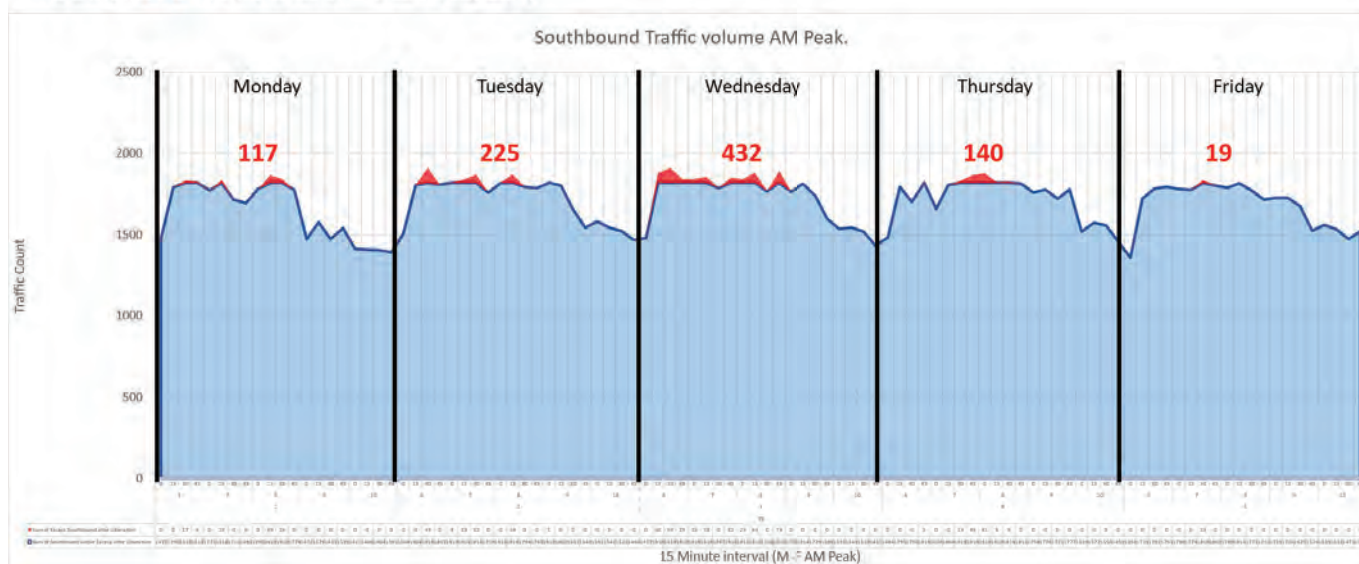
2017 Week 29 06:00-10:00

Example **203** Vehicles above 4-Lane Capacity



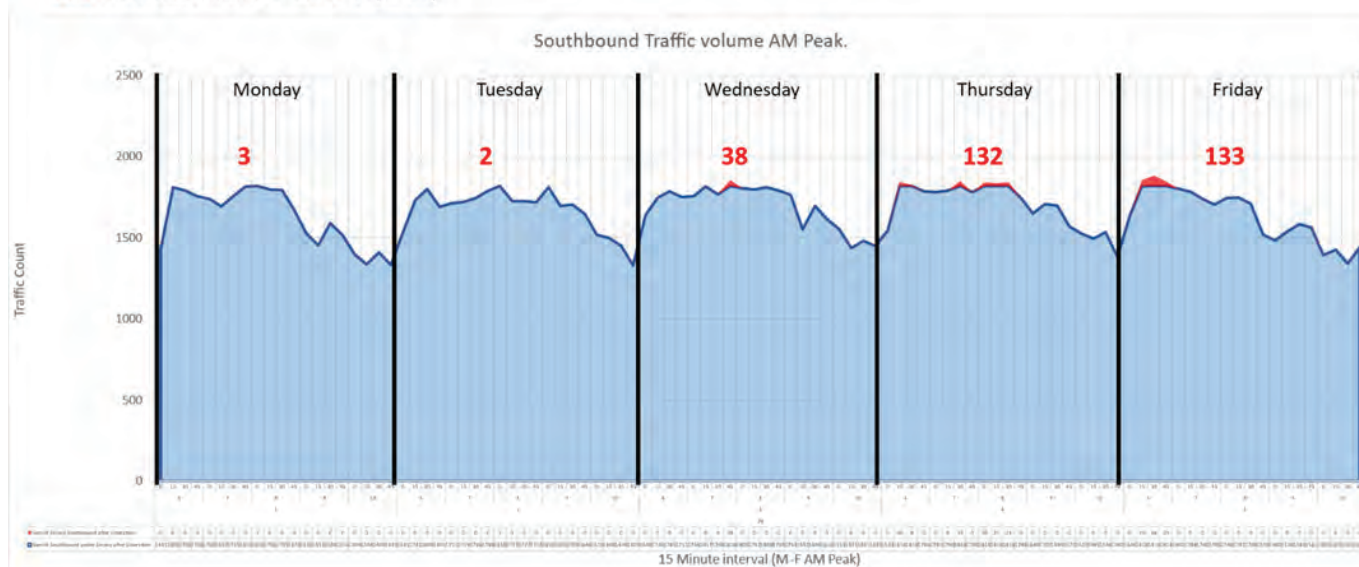
2018 Week 29 06:00-10:00

Example 203 Vehicles above 4-Lane Capacity



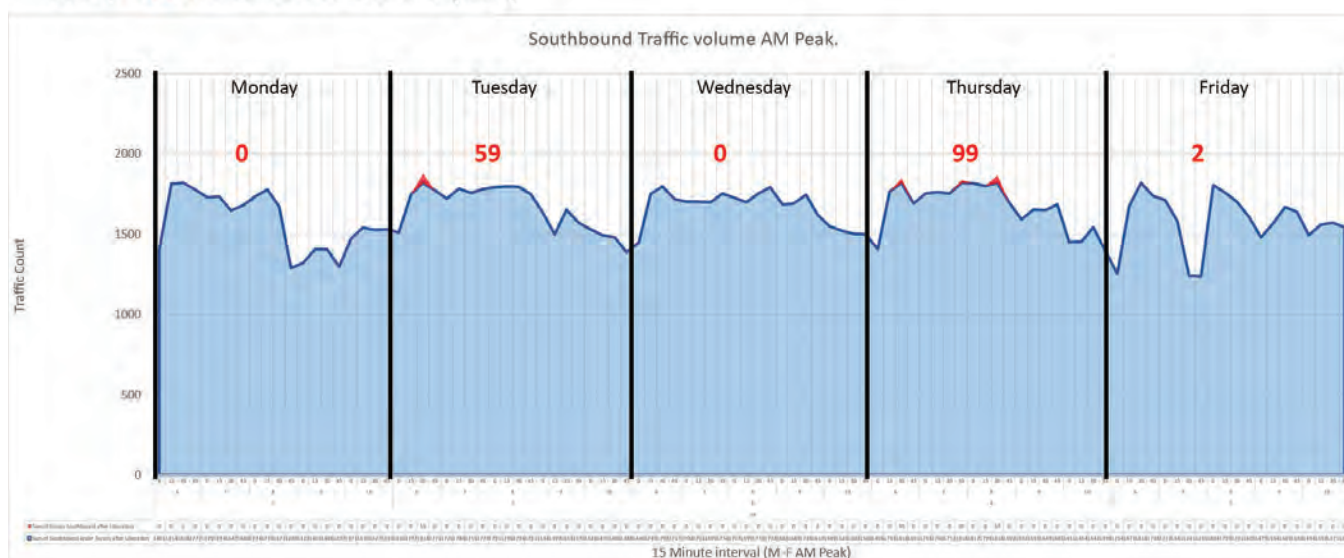
2019 Week 29 06:00-10:00

203 Vehicles above 4-Lane Capacity



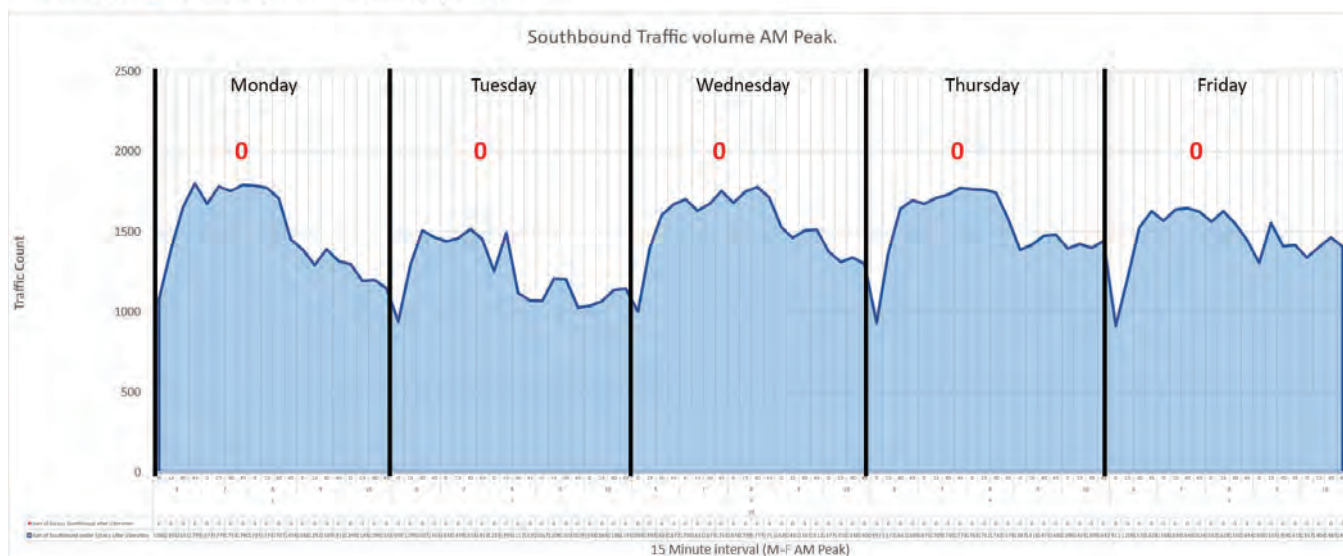
2021 Week 29 06:00-10:00

Example 59 Vehicles above 4-Lane Capacity



2022 Week 29 06:00-10:00

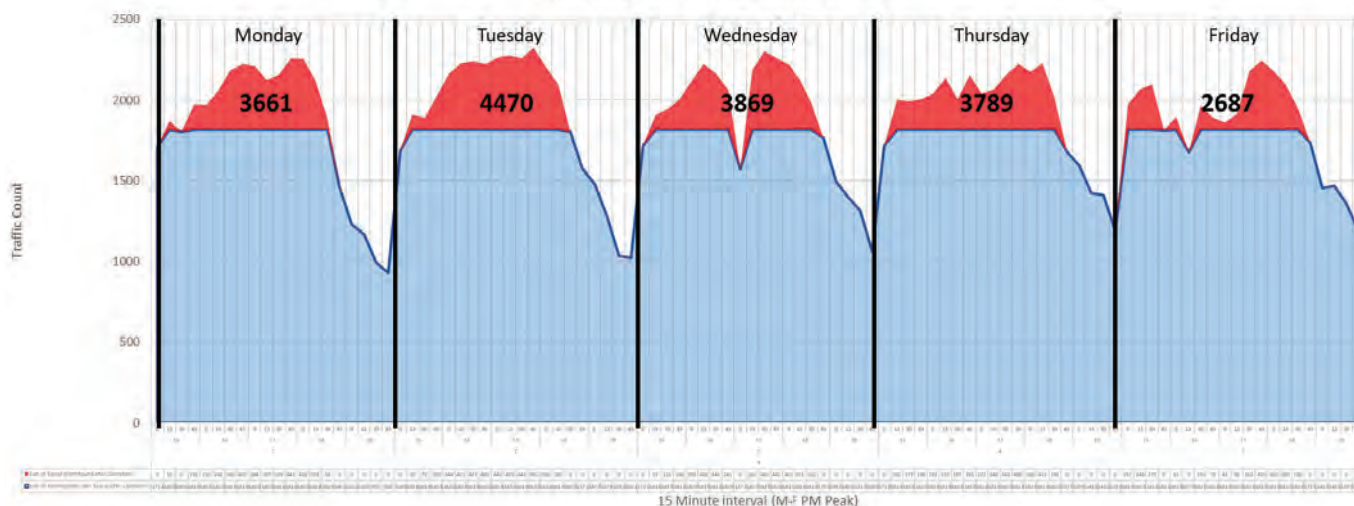
Example 0 Vehicles above 4-Lane Capacity



2016 Week 9 15:00-19:00

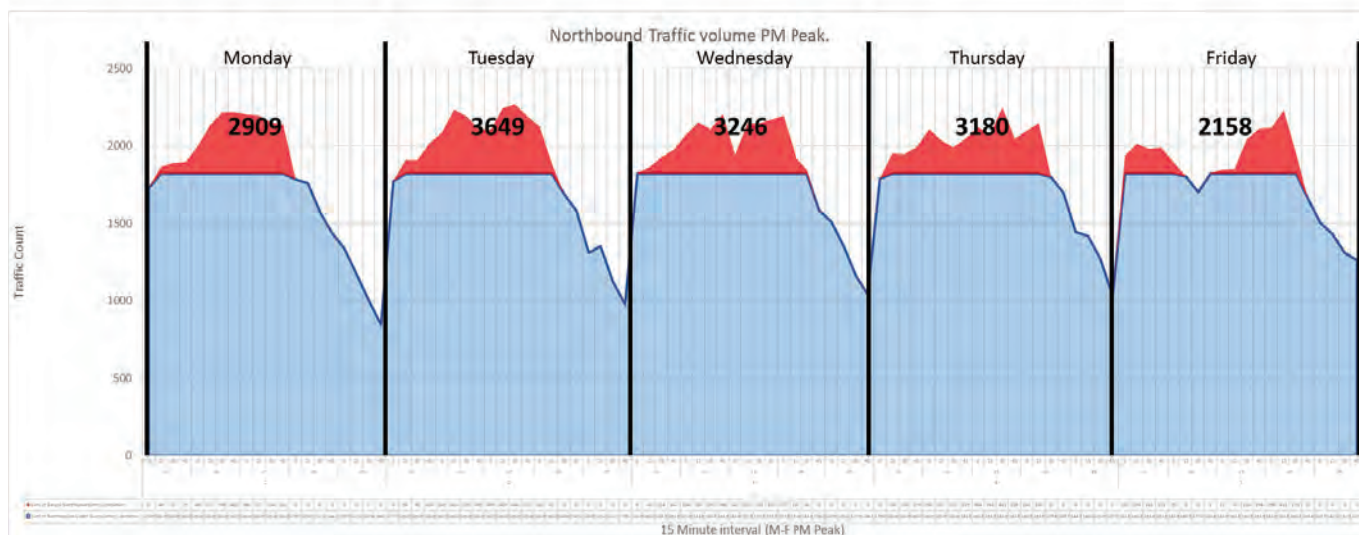
Example 203 Vehicles above 4-Lane Capacity

Northbound Traffic volume PM Peak.



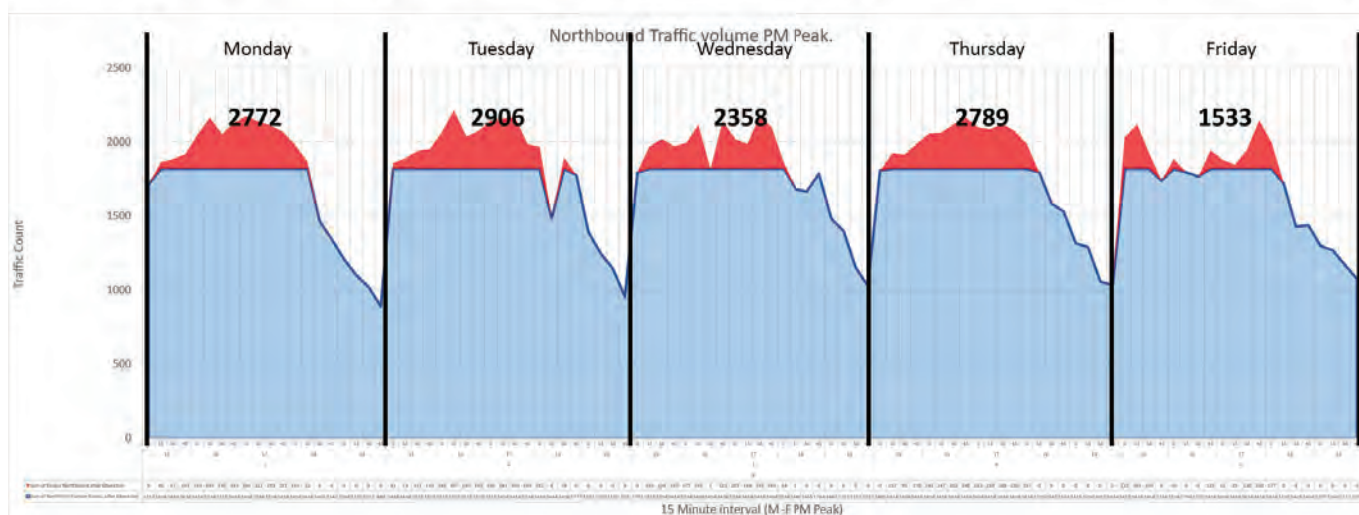
2017 Week 9 15:00-19:00

Example **203** Vehicles above 4-Lane Capacity



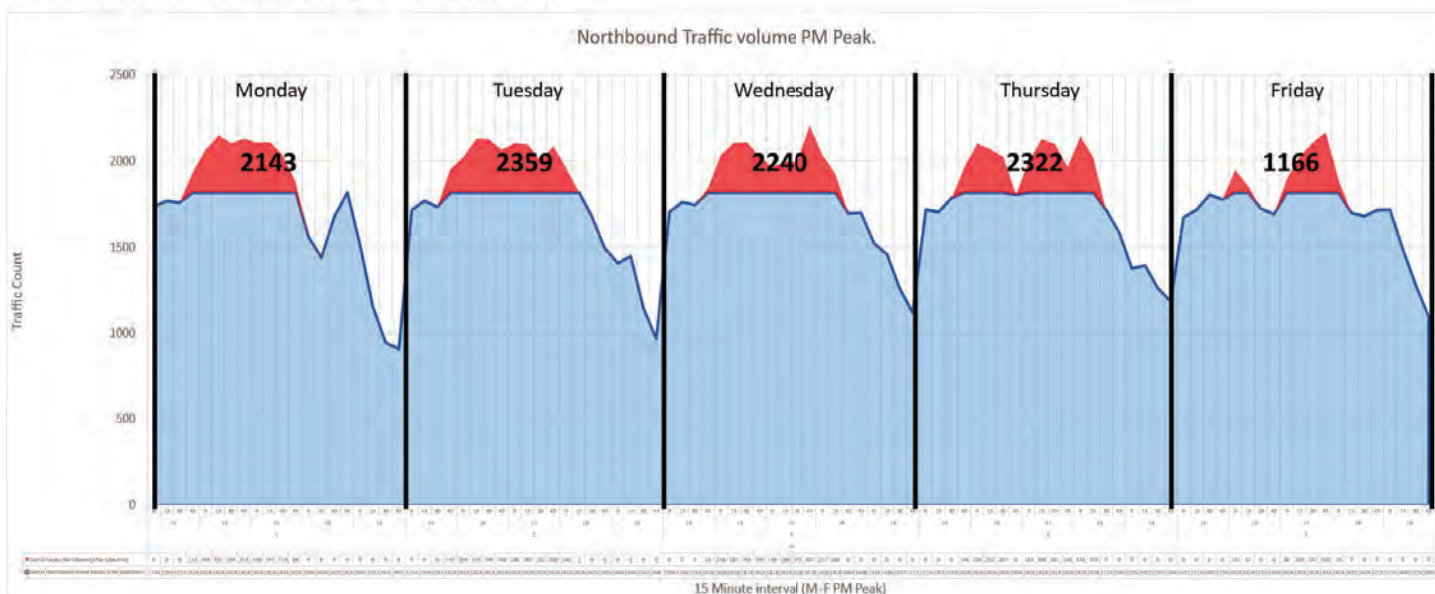
2018 Week 9 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



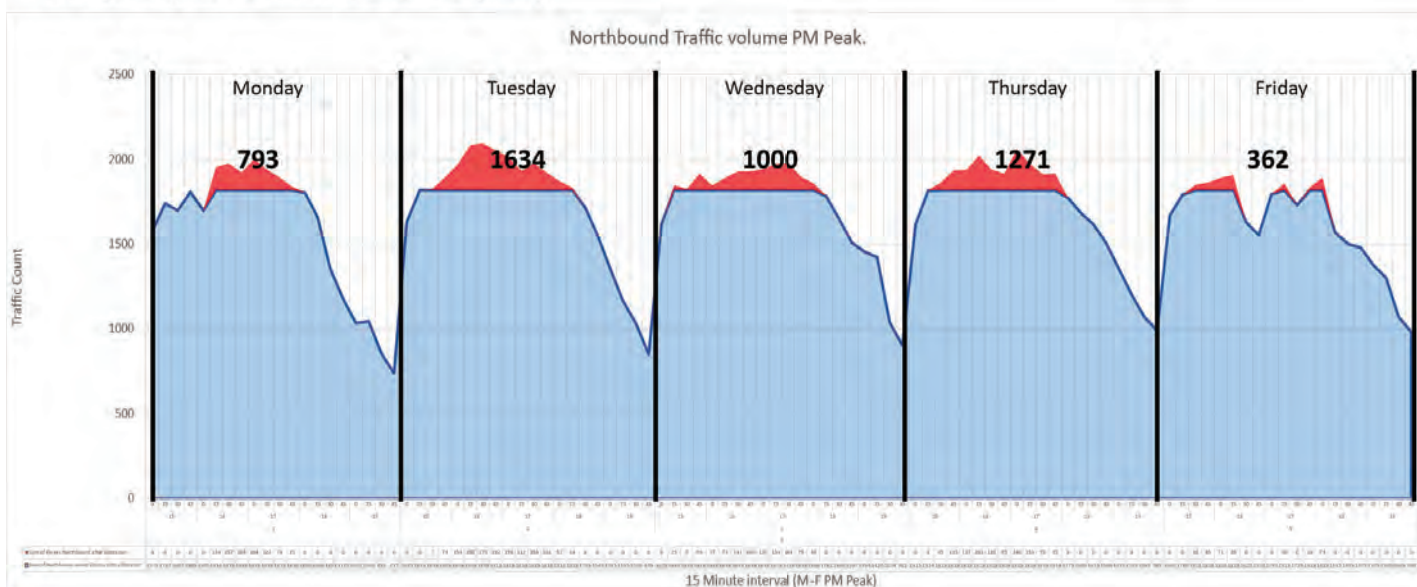
2019 Week 9 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



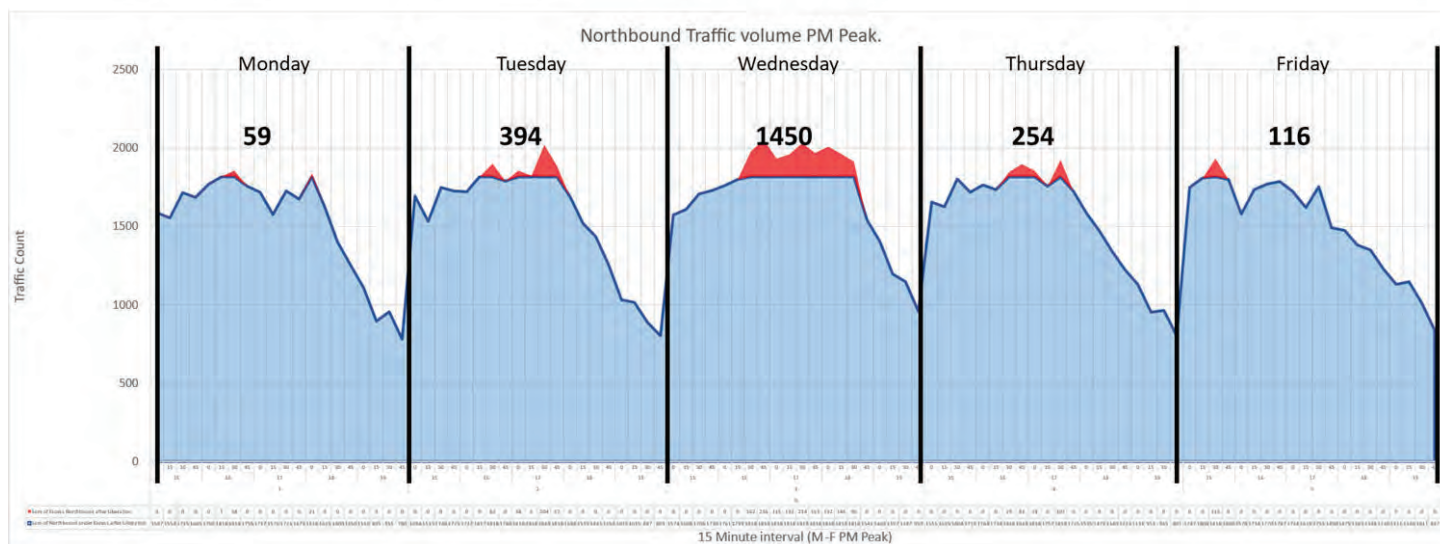
2021 Week 9 15:00-19:00

Example 793 Vehicles above 4-Lane Capacity



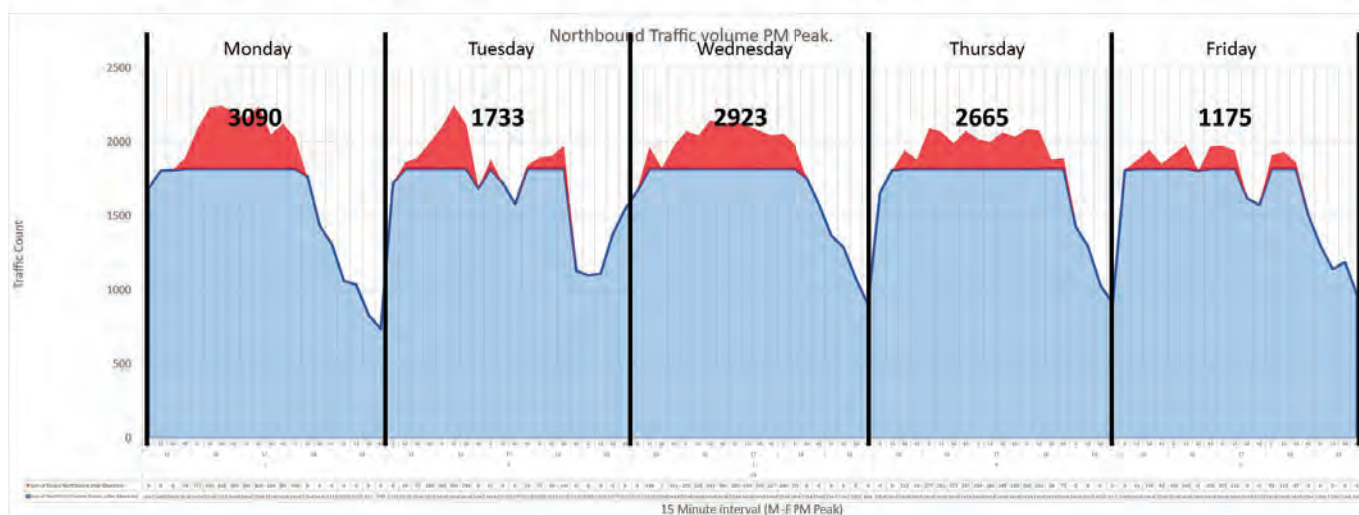
2022 Week 9 15:00-19:00

Example 59 Vehicles above 4-Lane Capacity



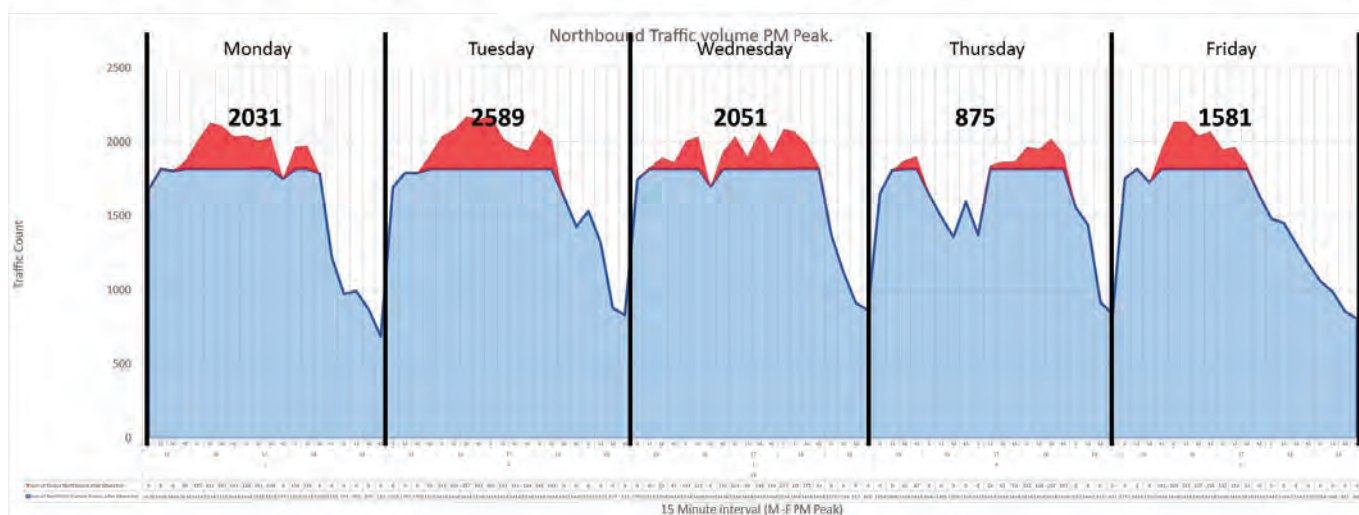
2016 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



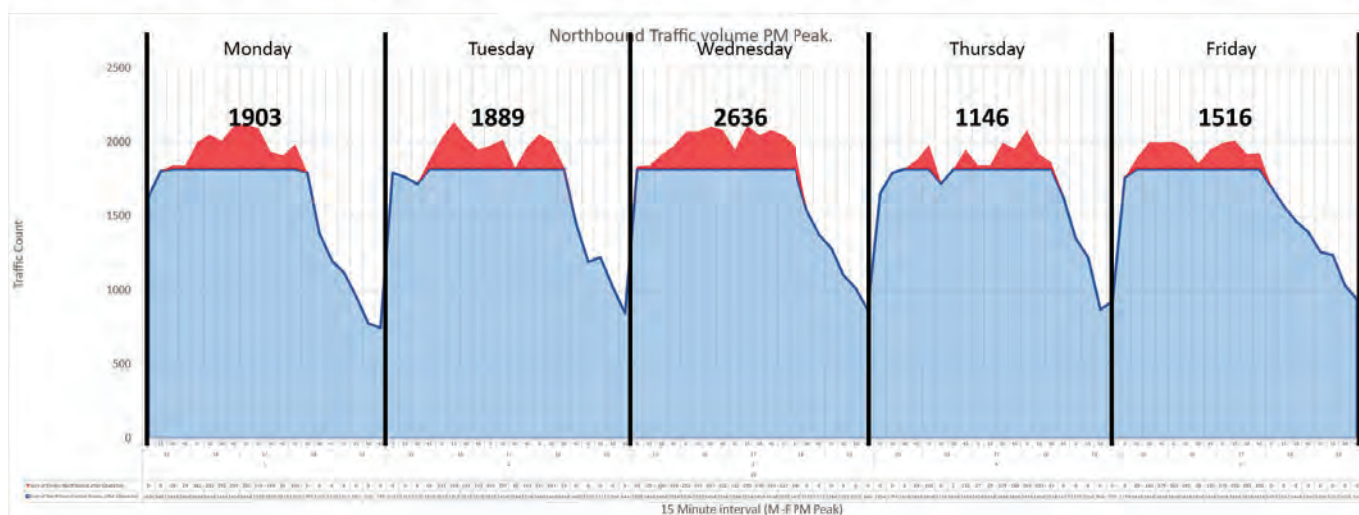
2017 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



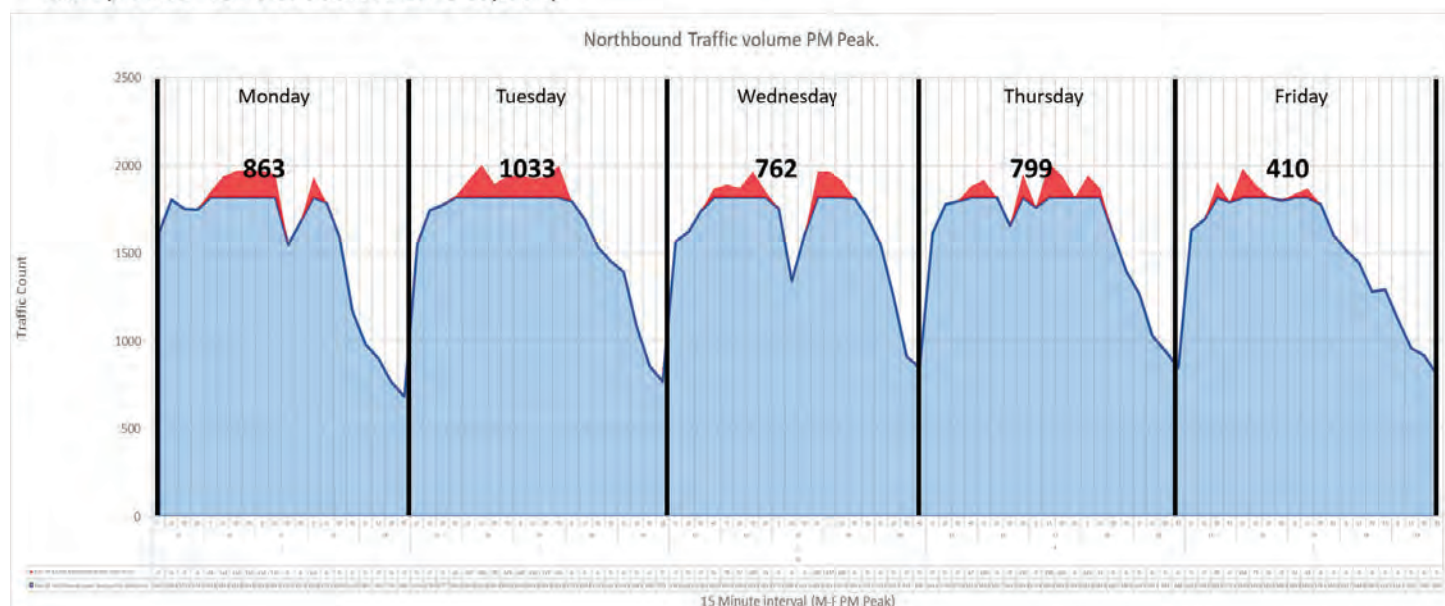
2018 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



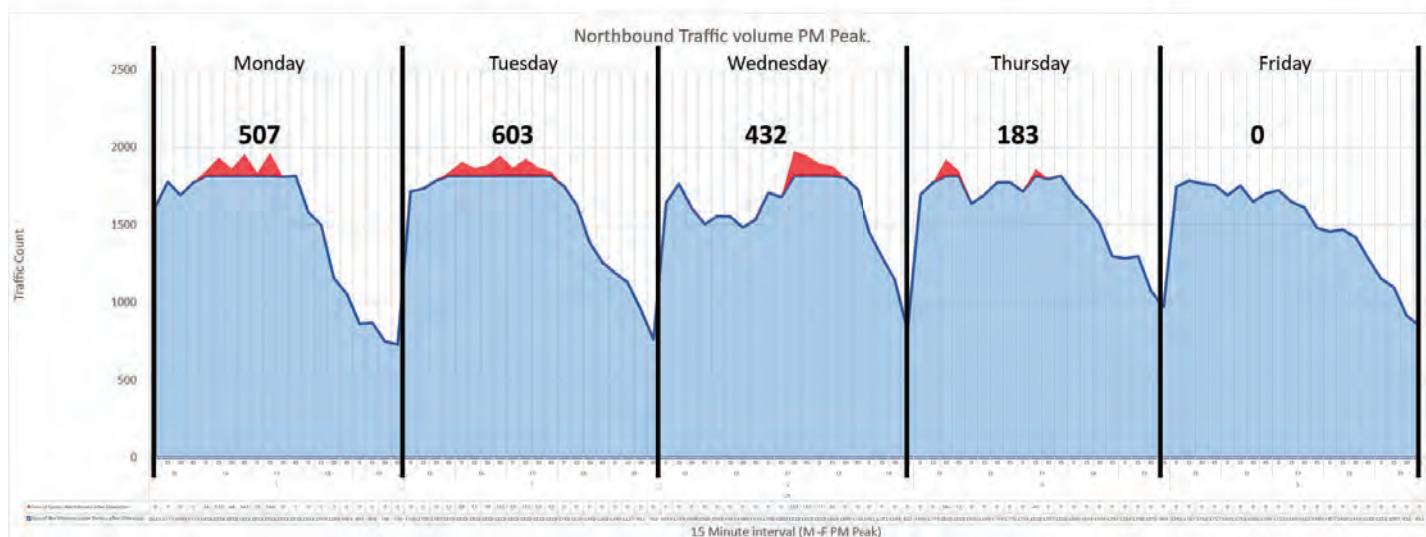
2019 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



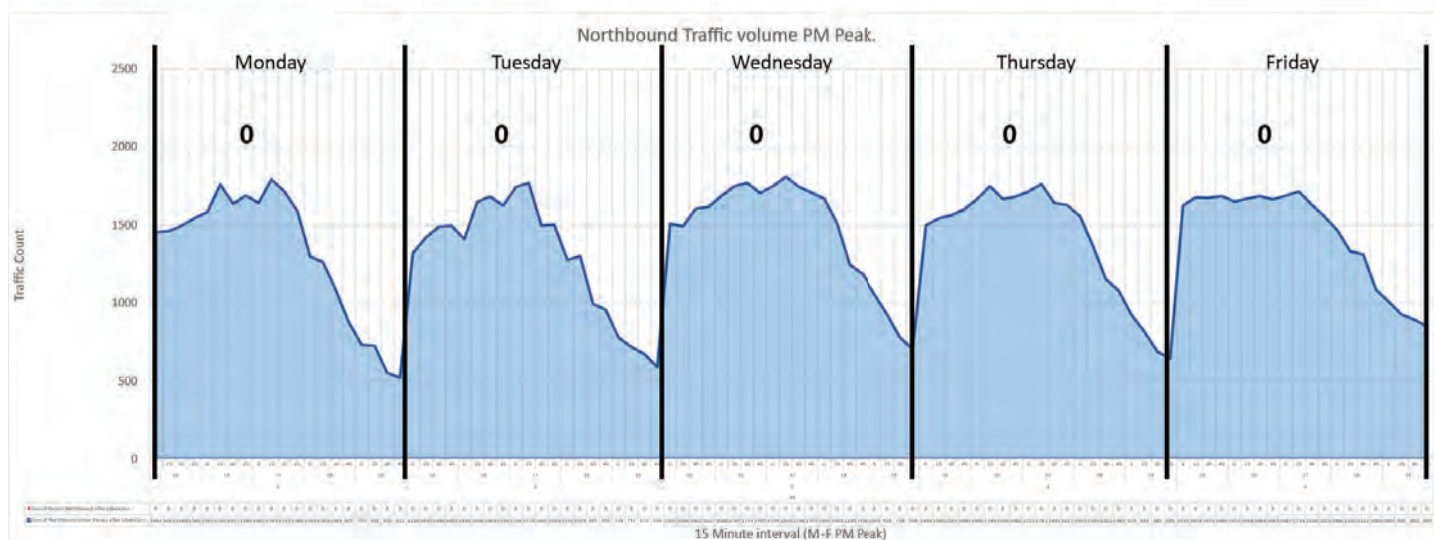
2021 Week 29 15:00-19:00

Example 203 Vehicles above 4-Lane Capacity



2022 Week 29 15:00-19:00

Example 0 Vehicles above 4-Lane Capacity





KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Just skim read it

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

The lack of investment due to years of underinvestment specifically for Birkdale area, footpaths' playgrounds public amenities

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

just skim read it

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Shouldnt tax people for years of underinvestment in natural environment. Should fund more community projects like Kaipatiki Project to protect and enhance naturel areas

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement

	None of the above	
✓	Other, please specify:	playgrounds, footpaths public parks in the Birkdale area

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
✓	Other, please specify: Public transport

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

improve public transport, create some parks, green spaces around Stott Ave, stop with road works on every corner at the same time



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

while I believe that the aspiration and goals are well defined and worthy, I do not think that the entirety of the community will recognise themselves in very lofty goals and the use of so many reference to the Māori world.

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

I think it is very complete, slightly ambitious even...

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

It is well structured once you have read the draft.

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

absolutely.

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

Necessary

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Lots of work to do. Repairs, Kauri dieback protection for reopening Kauri Park and other.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation

	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?

no



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not well at all

Tell us why

It's seems you are only respecting one culture here. I find it over the top and racist n there no solution to anything .

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Proper Solutions and stop wasting money to satisfy one culture (very biased) . There are many people (cultures) in Auckland and NZ that are struggling to get by . Solutions to homeless would be a start.

3. Have we identified the most important opportunities or challenges in our draft plan?

Unsure

Tell us why

Think it's just a bunch of mumble jumble wording with pretty maori pictures and words

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

Its all about the Maori culture , it will be interesting if this actually decreases the crime rate when we hand out all our tax to the cause. Too be honest I feel like you are creating more racism then ever with one culture getting priority over others.

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

No real solutions and it's not made man climate change, read up about the Tongan volcano. Also stop building on cliffs, and in low lying areas, simple. Common sense approach to building , or building a boat house.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

Not sure who we have to pay for that. Could be a waste of money if it's not followed through like most council projects

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows

	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Think it's the worst plan I've read. Where are you getting all these volunteers from by the way to do all this community gardening and planting . Interesting concept (WINZ) Sounds like you are wasting money left , right and centre. There are many people struggling in Auckland , homeless , where have you solved that issue. ?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

It is ambitious, keeping up momentum from previous years. It is good to see a strong inclusion of Te Ao Maori.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

I would like to see more responsibility placed on homeowners and business to contribute to the collective well-being and upkeep of the environment and community. A requirement to maintain properties (some houses are kept empty and garden unmaintained for years during a housing shortage), privately owned properties backing onto bush areas required to remove pest plants and rats whilst planting native trees instead of landscaping exotic plants, better monitoring of residents putting chemicals down storm water drains and not keeping drains clear, the removal of vehicles (including boats) parked/stored on public roads and berms instead of in private garages and driveways to become a hazard for pedestrians crossing roadways and restricting the flow of cars and buses to increase commuter stress. A system for businesses to deposit rubbish and recycling at a central collection point off the main road rather than on the pavement in overflowing bins once a week. More diversity in business with an emphasis on businesses to provide waste-free and quality goods to

customers. If you are going to decorate streets, please stop using cheap plastic decorations that look terrible and degrade into the environment.

I would like to see a move towards encouraging homeowners to put in rain collection tanks and greywater systems to recycle water, especially during summers with water shortages to water gardens and public spaces instead of putting water down the drain. I would like to see new developments putting this planning into their developments to be relatively self-sufficient rather than increase pressure on existing infrastructure.

I would like to see schools actively engage their students with the community, doing community service like tree planting days, picking up trash from public areas, and cleaning graffiti off walls etc to guide young people to becoming better citizens with more appropriate behaviour that considers others and the environment.

I would like to see the Northcote Zerowaste Hub extended into each suburb via the libraries and/or community halls. Northcote is not accessible for those who do not drive, use public transport, or cycle.

Car congestion is still a massive problem, as is driver behaviour, vehicle exhaust pollution levels, and increasing noise levels which impact the well-being of people and wildlife. Put in dedicated cycleways on wide footpaths, I would like to see more advocacy around cheaper bus fares for those not provided with free travel, school kids not paying fares required to give up their seats to paying passengers, buses given the right of way with this signage printed on the back of buses, and buses fitted with cameras to become the "eyes" for poor driver behaviour (drivers blocking intersections, bus stops, and transit lanes to hold-up a bus full of people) to be fined.

Enforcing penalties on drivers using disabled carparks when they are not disabled.

3. Have we identified the most important opportunities or challenges in our draft plan?

Unsure

Tell us why

I need to see tangible actions and measures to visualise tangible outcomes, otherwise it's just another report of aspirations.

4. How well have we reflected the aspirations for Māori in our draft plan?

Not very well

Tell us why

It's a start considering Te Ao Maori is a struggle for many New Zealanders. Developing this strategy has a long way to go which will be difficult without more Maori engaging. More advertisement of hui need to be done, including on old-school billboards an

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

I'm not sure what you mean by "the outcomes for climate change".

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

I 100% support protecting the natural environment. Any development being considered should be built upon the foundation of protecting and enhancing the environment; not finding funds to protect and enhance the environment after an ill-thought out developm

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement

	None of the above	
✓	Other, please specify:	Many of these areas can be "put back" on residents to do as being a community member and/or responsible homeowner. Other areas like water quality, council facilities etc I would expect current rates to cover.

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

I would like to see more Te Ao Maori values clearly stated/tailed and explained in the plan. Values likes kaitiakitanga, manaakitanga and mana etc etc. Not only do these values encompass a lot of power and meaning around rights and responsibilities in a single interconnecting concept which will resonate with many Maori, but it also helps to educate non-Maori.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

I think what the plan said about less engagement than would be desirable with the diversity of the community is probably very accurate. With that caveat, I think it does well. Personally I'm a disabled person who recently moved to the area. I didn't see any specific engagement opportunities with disabled people here.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

There are references to Accessibility in various spots in the plan, which is great, but I think the commitment that if there is ever an upgrade to a facility or a renovation to a building or whatnot, that accessibility considerations need to be absolutely key to that process, could be much stronger. Also, disabled people are not mentioned as a group at any point in the entire plan, "disabled" is not a bad word, this just makes us (disabled people) invisible really, it would be good to at least acknowledge that there hasn't been engagement with the disability community here (at least not that's visible in the plan)

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

I'm Pākehā so not gonna answer that one

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Good but I think the climate justice focus of engaging with specific communities (Maori, Pacific, Asian, disabled people, seniors, farmers...) could be stronger and that that should be key to the resilience planning

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Keen for a local targeted rate for sure. Rather the Board get on with implementing one or keep the investigation scope brief.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
--	------------------------

✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Seems to cover a wide range and shld have something for everyone

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

I'm from the we are all NZers not just Maori....

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Kaipatiki has a lot of natural Bush,waterways,surrounding sea to take care of....maybe more than other boards

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

I hope that local Kauri Kids are being supported and will not be closed as per Ak Council. The future of NZ starts with under 5s having sharing, caring, interactions with adults and other children at daycare. Also how important

Community houses are to the local communities....a must to keep



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Seemed well rounded with actions planned across different areas

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

Advocacy for the things outside LB control is essential

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

For what is in LB control, yes

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

This came through well

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

We have good areas of bush and a large coastline, we can do more

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement

	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

There's not very much improvement in some areas, why have the rubbish bins been removed in public spaces?

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Increase ferry frequency ex Birkenhead point.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

I think more emphasis should be on supporting locals with storm damage repairs, as insurances do nothing and everything to avoid payouts.

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

.

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

More can be done in providing ev power points, ev busses and subsidising paper take away containers to support local businesses and consumers alike

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

No additional money in my bank account with soaring food and transport prices and mortgage rates skyrocketing plus repairing flood and storm damage around our houses in the birkdale neighbourhood

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable

	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Looks rather governance and admin heavy, too much hui too little doi. Sorry, I've been forced to fork out money for flood damage coming down the road and not channeled properly, I'm really frustrated to see the blocked drainages and the damaged water pipes around my area.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Unsure

Tell us why

Although you refer to the environment and transport nowhere do you include climate changes needed. Why is this?

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

I certainly do, we need to build, restore develop with the protection of all from further declining weather conditions. Safer housing, areas of grass for absorbing water and transport that does not run on fossil fuels.

3. Have we identified the most important opportunities or challenges in our draft plan?

Unsure

Tell us why

See above re including approaching deteriorating weather conditions.

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

Consideration needs to be fir all to survive.

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

Already stated

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

No need for me to do so its a given

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting

✓	Waste minimisation	
✓	Water quality improvement	
	None of the above	
✓	Other, please specify:	Restructuring the way we live and move is paramount right now.

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?

We can't change the way we live to a safer, life saving way unless we have the funds to do it with. No time to ask, just do it.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not well at all

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Northcote is transforming and need to include high rise buildings for office to attract business to operate from Kaipatiki instead city centre. Provide enough parking spaces to businesses and close to public transport.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

Not much support seen for families to reduce carbon footprint. Storing rain water, solar panels installation. Growing own veges.

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

Not much support seen for families to reduce carbon footprint. Storing rain water, solar panels installation. Growing own veges.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Revenue generation by converting compostable waste to fertilizers can create sustainable model. Explore sustainable models for revenue generation.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting

✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Explore sustainable model for revenue generation and create employment as well. Charging increased rates not sounds like solution for expenses.

Organize fund raising activities, food stalls, volunteer participation. Selling excess garden veges, fruits. Plant orchards.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

Very innovative

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

I think this was well covered.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

100%! You really have!

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

Very important considerations for Maori.

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

You've really went above and beyond in this regard.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Very good idea.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation

	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?

This plan is just right!



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

It address the issues.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Focus on opening up all of the coastal and bush walking tracks along Beach Haven/Birkenhead coastline

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input checked="" type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Too much emphasis on 10% of the population

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

Most are ok

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

Too much emphasis

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

Leave it to the Government

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input checked="" type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input checked="" type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

This is the core element that goes towards making a good functioning community that works for everyone!

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

Yes - making sure that the waterways around the area are looked after is super important both for biodiversity, but also to mitigate potential infrastructure damage

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

It was great to see the focus on te ao māori front and centre - it's the kind of thing where this approach has the ability to really help guide the community to a better place!



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

The board's aspirations are reflected very well. Great document. Lets hope you achieve some of the aspirations this term.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

There is nothing addressed about your aspiration of keeping the community safe. The break-ins, vandalizing vehicles, thefts, ramraiding. Increased incidents during the last three years have left the immigrant communities such as Indian, Chinese etc feeling unsafe. What steps are you taking to remedy this aspect, even if you need to talk to Central Government People are migrating to NZ for peaceful and tranquil life away from wars, crime and corruption. Why do we feel unsafe. The board should encourage compulsory trade skills training for Youth so that they earn their own money without stealing money.

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

We would like to see Maori whanau supported to find jobs, get off the benefit. If they are provided free housing, they need to be monitored if the house is maintained well, not have too many people in houses and have not be violent and disrupt neighbour

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

More information is required on how the flood victims in the community are supported. What measures have been taken to give the most affected long term housing, counseling and support to whanau.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management

	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

The draft plan is far too long for the average person to read. It should be condensed to about half or one third the size so the average ratepayer can read it at one sitting. Ridiculous if you really expect to get sensible feedback!

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

Not sure what this really means. If it means asking ratepayers to pay more definitely not!

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement

	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

The local board wastes money. Examples are the silly grants it gives away to so called local pet projects of a few individuals and small groups and possibly ignoring the needs of groups which do the most to support the district. All small groups with a pet project should be ignored or at least asked to provide more supporting data before these funds are wasted. There are many other examples where the board supports nice to have but not necessary projects. The overall wastage if we include council contractors doing very little during their daily duties would need a time and motion study by a professional.

I notice during winter months very little maintenance being done to support much used parks. I am not referring to bush tracks which would have little use in winter months.

I also question if it is necessary to have eight board members. Do these board members do a full time job serving in this role? Has this even been addressed by the councillors and the Mayor?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

I think our top priorities in Kaipātiki, and in NZ, right now should be (1) reducing greenhouse gas emissions and (2) providing more social housing for people in need, including much more council housing for the elderly and others in need.

I like the emphasis in the draft local plan on preserving our natural environment, and proposals to encourage more cycleways and public transport. I would like to see much more emphasis on developing safe cycleways and walking paths on and off existing roads, including plans to enable all students to walk, cycle or bus to schools and tertiary institutions such as AUT and Massey.

I think the local board should also work with Haumarū Housing and other charitable funders and social housing providers to vastly expand social housing in Kaipātiki for the elderly and others in need, with the aim of eliminating the waiting list for social housing in this area.

3. Have we identified the most important opportunities or challenges in our draft plan?

No

Tell us why

See above - main challenges and opportunities are to eliminate greenhouse gas emissions and eliminate the waiting list for social housing

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

As above - need much more emphasis on safe cycleways & walking paths, including enabling all students to walk, bike or catch public transport to schools & tertiary institutions.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Protecting our natural environment should not be constrained by limited finance.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
---	------------------------

	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Multi-sports centre at Birkenhead War Memorial

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input checked="" type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input checked="" type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

C



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

No funding should be race based, especially if a greater proportion of funding per capita is going to one race, that is discriminatory towards all other races.

Promotion of culture and language should not be the responsibility of the council, especially n

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

I am not Maori so cannot respond to the question.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input checked="" type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input checked="" type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

The people need to be safe from flooding and landslips.

I agree that more stormwater detention ponds will help, but there is a great need to retreat from the areas which suffered the worst flooding on 27/01/2023, and create open areas ready to be safely flooded. This retreat would allow the water more places to go during future flood events.

The Wairau Valley was an extensive area of wetlands right through to the Milford estuary.

By draining the valley and encasing streams in concrete we created a time bomb which blew up on 27/01/2023.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Auckland in general has lost most of its urban tree cover over the past 5 years.

I want the Board to create constraints on unsustainable population growth and bring back general tree protection.

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

I don't want to speak for Maori.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

My rates have always seemed too low for the amount of work needing to be done.

Kaipatiki has a precious endowment of natural areas needing protection.

I am happy to pay more rates to protect what I value about living in Kaipatiki.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

Bush track maintenance

	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
✓	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Seraj Community Group

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

'- Review public showers' privacy at beaches or swimming pools. Most of the showers have no doors. Moreover, people are getting undressed in the showers and walking around in front of everyone which is inappropriate. There should be notices in each public shower stating that it's unacceptable to be undressed in the common areas, and there should be some consequences.

- Public toilets shouldn't be unisex. There should be separate toilets for men and separate toilets for women. We as Arabs and Muslims don't feel comfortable using public toilets as there's no privacy, especially for women

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting

✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

'- Review public showers' privacy at beaches or swimming pools. Most of the showers have no doors. Moreover, people are getting undressed in the showers and walking around in front of everyone which is inappropriate. There should be notices in each public shower stating that it's unacceptable to be undressed in the common areas, and there should be some consequences.

- Public toilets shouldn't be unisex. There should be separate toilets for men and separate toilets for women. We as Arabs and Muslims don't feel comfortable using public toilets as there's no privacy, especially for women

- dedicate more coal BBQ areas in the parks, so people can enjoy having outdoor BBQs. It could be a dedicated area in the park away from trees, where you can use your own coal BBQ and be able to dispose of the coal after using it.

- Building positive relationships between communities and law enforcement can help create a safer environment for everyone. Training police officers on cultural sensitivity and understanding can be beneficial. Police and law enforcement to participate more in community activities and events. Get involved within the community through meetings or presentations to show people what are the roles of police and law enforcement and how to deal with some serious incidents in New Zealand



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Very well

Tell us why

I see strong advocacy goals around the key areas. The items that are being advocated for that are particularly important for me are

1. Reinstate legislation that provides general tree protection and protection for special ecological areas
- 2 Biodiversity within the board area and park buffer zones to manage invasive plants and kauri dieback
3. funding to maintain community facilities and parks
- 4 Full day clearway on Onewa Road
5. Reduce the use of chip seal on resealing on suburban roads
6. The use of smaller. local businesses and / or not for profit organisations

Other aspects of the plan that are particularly impressive are

1. Improved efficiency of main road corridors and congestion reduction
2. The control of plant and animal pests
3. provision of shade sails in parks
4. Cheaper and more frequent ferries - this is a great way to reduce road congestion

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

The Northcote development is a great initiative. The current government has imposed a generic intensification model on all major cities in New Zealand. This generic plan however does not required urban planning to be taken into account and has limited potential to be customized to local needs. This approach means that the development is put into the hands of developers and real-estate agent who tend to be driven by a goal to maximise profits often at the expense of people and the environment. Council needs to take back control so that we can have planned and sustained intensification rather than the ad-hoc mess that we currently have. The rights of adjoining owners need to be restored so that their privacy and daylight are protected. A clear consultation process between developers and immediate neighbours needs to be developed for the case where buildings of 1 storey or higher are being built next to an existing house so that the neighbours light and privacy are considered and protected. We are building for the future so we should plan and consult before we build

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

A lot of the damage caused by recent weather events happen because of poor design and planning. i have long thought that building of homes on concrete pads at ground level was a

disaster waiting to happen and guess what - it happened. large quantities of impervious areas in new developments, cutting away hills and a failure to take into account runoff and drainage. When damage occurs what happens - they built back in the same way and flooding happens again. We need to make sure we future proof new and existing buildings

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Our rates are high enough as it is - we need to look at other funding or resourcing options

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

Not important

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Not enough focus on the urgent action needed around climate change. We can't necessarily have what we want, such as little shoal bay staying the same. We need proper action.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

If we don't address the climate issue seriously, nothing else will be important

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

It's not enough. You haven't done anything bold or looked at real changes. P

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Because protecting the environment is not just a local issue. It needs to be funded at a much higher level. We're already paying extra rates. That should be going towards if.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation

	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

We are living in urgent times. Yet none of this plan feels urgent



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

North shore islamic association

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

Only a small number of people engage in the political process. Some outcomes appear to be ideologically driven.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

I'd like to see increased investment into spaces and places, i.e. quality walking tracks - which add value to our neighborhood, and promote social wellbeing.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

Very well, but this is an ideologically driven agenda that should be rationalized.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement

	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

A reduction of the people living in the board should be the top priority.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

Time will tell.

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

How well have you reflected the 34% of the Asians in the community?

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

Quite possibly, too little too late.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

As long as it is not wasted on more consultations, surveys reports and committees.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?

Yes, if there was less money tied up in over valued property, people would be able to spend it on what really mattered, something that past governments and councils have chosen to ignore.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

A huge number of the focus areas affect very small numbers of people. Many of the statements made are vague, not measurable, or seem unlikely to make any difference to residents. This plan represents the highly limited influence that the Board has to improve key aspects of life in Kaipātiki.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

But the Board appears to have no influence or ability to drive real change

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

But you're not actually doing much about them. The Transport section essentially says "we'll advocate", but you can't actually commit to doing anything. Same with flooding response.

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

Very vague, weak response. Highly unlikely to make any difference.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

You would need to explain what you would do with that money. Based on this plan the board appears to have no real powers. The list provided here is interesting but I suspect the big stuff like flood management you can't actually do. Stop wasting money on

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows

✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

This plan frustrates me. The Board appears to lack the ability to influence govt, Auckland Council, AT, Watercare, etc so ends up spending money on things of little value. We can't even get AT to make Onewa Rd a clearway so 1 parked car can cause weekend traffic. I love the bush track upgrade programme but there is no transparency over priorities or timelines.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

Seems to be comprehensive, thorough, and thoughtful

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$100-\$149 per year (\$1.92-\$2.87 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Advocate to Auckland Transport to fix the lane marking at the Onewa Rd/Glenfield Rd intersection. Currently traffic travelling straight ahead to Highbury and the Points is disadvantaged and has to join a long queue of traffic that is waiting to turn west into the Highbury bypass. This is a recent and significant disadvantage and only requires lane marking changes to facilitate it. Having multiple cars queuing unnecessarily creates unnecessary congestion and unnecessary pollution.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

I agree in principle but worry that council will take teh opportunity to reduce funding by an equivalent amount. Is there another way to raise money?

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting

	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Juggles diverse needs realistically

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

Not something that I understand well

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Individuals also need to take more responsibility so council should encourage that more

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

I'm financially secure enough to prioritize this

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above

	Other, please specify:	
--	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

For better engagement consider 'drip feed' approach ie instead of an overwhelming 44-page report, separate out the sections so people can start with/look at the one that speaks to their top needs & respond to it, thereby capturing some rather than none.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

the "Ideology" is a bit too "Green-Party" for some of us

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

cant afford it on the pension, and the already too expensive, super-shitty rate hike

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not very well

Tell us why

The draft plan addresses a number of issues in a very broad way and does not provide any detail as to how these issues will be addressed / mitigated. It would be helpful to see a plan that sets out exactly what will be done to address transport, meet our climate targets etc. For example, you talk about improving active transport infrastructure, but I have yet to see any investment in safe cycle infrastructure that would enable travel from Beach Haven / Birkdale to Highbury village.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

More information about available budget and investment priorities.

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

The plan includes a good overview of all the challenges and opportunities but does not give a sense of the priority for addressing the various challenges / opportunities.

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

The plan does not give enough weight to the role car emissions play in contributing to climate change. There should be a much stronger focus on how car emissions can be minimised.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Our household would happily contribute to this initiative, however, care would need to be taken to ensure it didn't burden households that are struggling to make ends meet.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction

	Retrofitting council facilities to be more environmentally sustainable	
✓	Tree planting	
✓	Waste minimisation	
✓	Water quality improvement	
	None of the above	
✓	Other, please specify:	Safe cycle lanes

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?

The plan should be harder-hitting and set more ambitious and detailed targets / actions for addressing climate related issues. I would like for example to see more specificity around how congestion will be addressed. Can we look at localised congestion charging for example? Can we create more T3 lanes? Can we actually put in safe cycleways so kids can cycle to school, people can access cafes, shops and supermarkets without getting in a car? As a newly elected board, it would be great to see you develop a plan that is ambitious, innovative and climate focused.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Most areas covered with thoughtful plans. A lot of advocacy but not so much concrete action is evident

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

Libraries as community hubs?

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

The key areas represent the most pressing needs. Infrastructure a priority - aspiration vs action? Local board vs wider council authority

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

I assume extensive consultation. More than signs - which I support - needed.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

An area wide promotion of solar panels? Council buildings leading the way.

Organic recycling bins - recent council initiative- great!

Council overview of rental - warm homes - meeting requirements

The ferries!! More than talk please. A ferry service to the eastern bays?

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

It behaves all of us to support this work. Though hard if you've little money to feed your children

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows

✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$100-\$149 per year (\$1.92-\$2.87 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

The Wairau Valley proved prone to flooding in January and serious remediation is needed to ensure no recurrence of this. Money will be needed to investigate ways of doing this.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

As previous answer.

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

As per previous answer

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

This will be a necessity.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement

	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Nothing to add



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not well at all

Tell us why

At present there are no plans to introduce ways and means for the community of Northcote , which will include 1,717 new homes within the Cadness st, Tonar st and Fraser Ave, to be able to socialise , workout , be entertained , things that bored teenagers need to keep them from doing harm.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Go back to keeping reserves clean and tidy . There are reserves and areas around my area that are a disgrace. How can people enjoy these areas for bbqs, local volley- ball, picnics and children's activities when there's rubbish and long grass ie, Lindisfarm reserve and the bottom of Woodside Ave .

3. Have we identified the most important opportunities or challenges in our draft plan?

No

Tell us why

We need a cycleway next to the motorway down to Albany like west Auckland people have. And where is our Harbour bridge cycleway . You spent tax payers money on cycleway to northcote point and then nothing .

4. How well have we reflected the aspirations for Māori in our draft plan?

Not very well

Tell us why

You removed most of them when you tore down all their dwellings and have left no areas to garden anymore if any of them stayed . They have made sure that Hato Petera school didn't survive either. Just lip service to the woke mentality

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

We all have to travel in our cars to shop in other communities as there are no decent shops in Northcote. Are there enough e charging sites for the 1717 new houses being built .

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

Not sure what this means

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme

✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Give us a Northcote shopping are something we can be proud to go to . Get rid of this getto type one we have now . We are not all Asian and where is there anything Māori going on here at present .



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

Very comprehensive document,

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Possibly more emphasis on the upgrading of local streams in reserves with attention to scouring and loss of wildlife habitat.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

But, little attention to the problem of rising sea water levels that will impact coastal Kaipatiki severely the near future

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

I have little direct interaction with local Kaipatiki Maori, but I am happy with the intention of the draft plan intentions

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Good with regard to the human population protection from intense storm impacts, but not much with regard to the animal and plant life, and aquatic protection

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Provides the basis for a known annual income from this source, hence allowing for more accurate annual allocation of funds.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable

✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Covered below

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

I don't think the plan goes far enough to address either the current financial position the council is in or the reality of climate change.

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

Support the inclusion of Maori outcomes that are agreed to between local Maori and the local board. I'd love to see initiatives that support more of the Kia Ora Tamaki Makaurau framework 10 priority areas - particularly addressing Kia ora te Rangatahi - r

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

I'd like to see some more aspirational targets on climate change supported by the board. The recent floods and landslips should be a catalyst for what else can we do locally. How about supporting people to do more stormwater planting? Our town centres should be greener - sustainable business practices should already be the norm - how can we be more aspirational - plastic free town centres - reusable takeaway containers rather than single use. I'd also really like to see the Birkenhead plastic bunting stopped. How about reducing cars through centres and encouraging more active modes of transport? I'd love to see traffic free days introduced - how about piloting Sundays as traffic free days and encourage more street based activities like farmers markets and play streets. I'm sure there's a lot more that could and should be done locally.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

We already have targeted rates at a regional level. I don't think any of us want to see our rates go up more than necessary

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn’t be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Under the belonging heading, I support the objectives and am particularly pleased to see the youth panel being reestablished. However, I don't think it's appropriate to be asking for additional funding as outlined in the advocacy section. I would rather see the board decide to sell an asset to generate funding if there are specific areas of work that are seen as a priority. An alternative would be to work with commercial entities for sponsorship or partnership arrangements.

I support the objectives within the Environment section

I largely support the objectives within the Places and Spaces section but believe there needs to be some prioritisation. If we can't afford to maintain and invest in our places and spaces, we need to think about either selling assets off or looking for opportunities to find alternative sources of funding or partnership arrangements that will allow this to happen. As a local volunteer who looks after the Kaimataara space, I have been disappointed in the level of service dealing with litter and human waste in that public area - our volunteers

shouldn't have to feel like they need to clean up human poo! I have also heard that the toilet there is often pretty unpleasant. We need to maintain investment in the little things to reduce the likelihood of bigger antisocial behaviour taking place.

I largely support the transport objectives with the proviso that restoring bush tracks may not always be feasible - for example the waterfall track in Le Roys Bush sounds impractical without substantial funding and engineering works. In the advocacy space I would like to see the board advocate for increased ferry service. It was disappointing to hear that AT are considering abandoning the Northcote Ferry service after a considerable amount of money was spent not only on upgrading the terminal but also installing a safe cycle way. Another harbour crossing is a long way away so we need alternatives that are reliable, frequent and quick. I would like to see AT promote the service better and reassure people before they decide that a service is not being used enough to continue.

I agree that we need better - quicker and more frequent services to other areas such as Takapuna and Albany - our key economic and entertainment hubs on the shore. At the moment, it's too hard to get PT to get there so the city tends to be the default. Please provide a more direct service between the main centres - a tiki tour around the suburbs is so frustrating.

Within the opportunity section I feel that Birkenhead Town Centre needs a major refocus of efforts. While the mall is undergoing a decent upgrade that is making it more attractive to families, the rest of the centre could do with some love and some real rethinking about what would make it a nice place to hang out. We do get a lot of visitors from other parts of the city who come for the evening food options and places like the cafe in Chelsea are a tourist spot, but we need more things that make people want to stay longer or come more often. We have wonderful native reserves - how about an information hub at the wharf or in Little Shoal Bay to educate people and encourage them to get involved? An Arataki visitor centre might be out of reach but something like that to celebrate our local taonga and stories would be amazing - perhaps developed and delivered in partnership with local Maori.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

No

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

The environment is given relatively high priority. However more needs to be done as weeds are rampant in bush areas. There also needs to be more tree planting in public spaces in Kaipatiki (including street trees) to replace the large number of trees being removed from private properties.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Board members to complain to the council about their Park staff continuously mowing over and spraying native plantings!

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

Again the environment is the most important issue in Kaipatiki with our extensive bush area and waterways.

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

One could say too much considering they only make up 9% of the areas population. With such a small population is it money well spent to build a second Marae?

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Rain gardens should be built urgently in areas such as Wairau and other flood prone places, similar to what has been done in Hobsonville Point and Wynyard Quarter. They appear to be effective and are more attractive then grass berms.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Although the first priority should be to ensure Kaipatiki is getting its fair share of the regional environmental targeted rates. It frustrates me when I see one of our local Councillors (Hills) promoting “tree equity” ie more funds going to trees in Sout

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme

✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

No.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

In the plan Birkdale suburb has been not considered.

I'm an homeowner and a mother of two kids.

Before buying our home in Birkdale, I lived around Auckland: Mission Bay first, then Beach Haven and Birkenhead. Changing the place where you live makes you aware of what is really important to you. And as a mother there is nothing more important than the happiness of my kids.

My daughter has grown up (she is 14 years old) and the area is deficient in activities for her age.

It's quite surprising given the number of teenagers in our neighbourhood and that sports facilities are common in all the other suburbs nearby or where I lived before.

I think there should be activities at walking distance for teenagers wherever they live, especially in Auckland. Puberty is a delicate stage and they need to socialize more with

peers (even if they don't think they want to sometimes!), some space where they can spend time and have the occasion to see each other.

In our specific case, we have a beautiful small playground for toddlers just at the top of Verbena Road, Vandeleur Reserve, where there is a nice flat area that can be improved to create something attractive for teenagers too, like a basketball court.

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

It's too generic. I can tell from my experience that natural reserves are not well maintained right now. We live in a house with a creek crossing our garden. This creek during the latest storms has become a river and the area as soon as after our property where water is running though is not free from obstructions.

Sometimes some contractors are in charge to go through the rest of the creek, but they never took real actions to free that up properly. It's full of bamboo and detritus.

We had a huge tree that has fell off from the Inwards reserve and we advised the council. They sent a couple of contractors and they cut only the part that fell on our property. They sliced it in smaller parts and they left everything aside. What do they think will happen when there will be the next storm? all that dirt will be causing obstruction and FLOODING!

And the rest of the huge tree is slowly slipping down towards to the 'creek' causing problems again.

Even if council has been advised of the situation they are not doing what they should do, that is KEEPING CLEAN the natural reserves.

Is it possible that we need those big storms to realize that we need to keep water flows properly cleaned with no obstructions around?

We are committing our personal resources to minimize the risk of this situation but with the latest big events we feel overwhelmed.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

We are already committing our resources in something that council should be doing!

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**





KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

Disappointed there was no recognition of Pest Free Kaipatiki Restoration Society reserve groups now 50+ in number and in the groups such as the Hit Squad - which as well as contributing significantly to the eradication of pest weeds in Kaipatiki has significant social implications, and is not seen elsewhere.

2. Is there anything else you think should be included in the draft plan?

Unsure

Tell us why

In dividing things up into 5 headings you miss highlighting of looking for organisations that provide coverage over a range of your headings. In doing this you remove the excitement, impact and impetus -----Monochrome could be colourful and vibrant , if you like.

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Pleased to see climate change get more prominence

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

More emphasis needed on frequent and excellent public transport and development of active pathways that are not shared with the road.

Very simplistic goals too could be developed eg how many slips were caused by short storm water pipes ending just of a property when the stability of the whole slope should be considered. A local initiative on this would save many slopes and this could be monitored or controlled by the consent process.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

But would this just put another layer of bureaucracy into the local efforts already taking place.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme

	Flood management	
	Park naturalisation e.g. butterfly meadows	
✓	Pest (plant and animal) reduction	
	Retrofitting council facilities to be more environmentally sustainable	
✓	Tree planting	
	Waste minimisation	
✓	Water quality improvement	
	None of the above	
✓	Other, please specify:	Care needs to be taken not to duplicate already well funded areas of council eg waste, storm water

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Excellent Local Board , please keep up the good work.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

More focus on climate risk mitigation

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

It seems well researched and reasonable

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

The focus on climate action is admirable, and integration of ethnicities is great

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

Definitely moving in the right direction, perhaps there should be more urgency to have regular hui with Maori representatives

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

It's hard to pin down exactly what work will be done or needs to be done - there's so much

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

I'd be happy to see my rates directly benefiting the local environment

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement

	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

Because you advocate to reinstate tree protection

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Protecting the environment is costly, but everyone gets the benefits. That’s why everyone should pay a share towards the goal.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$100-\$149 per year (\$1.92-\$2.87 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

The overarching themes are in line with what makes Kaipātiki such a great place to live.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

'Prioritise making a town centres safe and attractive for the most vulnerable transport users (e.g. walking, cycling, PT). Examples of small things we can quickly get done in Birkenhead include the following:

- reduce speed limit to 30km/h, especially in front of the school
- trial the removal of on-street car-parking (there is plenty of parking (Birkenhead Sqaure, under the mall, and the park opposite the library)
- run activations during the weekend (e.g. in the Birkenhead Square carpark)

We could also trial more direct public transport routes, for example an express bus to Newmarket (this could replace one of the 966 services in the morning/afternoon).

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Continue to prioritise walking/cycling/public transport options as these will help the cost of living crisis and climate change

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

Not qualified to comment

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

We need to be going harder and faster from a transport perspective.

We shouldn't have to wait 50+ years for a \$45billion project that will never get across the line to get cycling across the bridge. We can take small + fast actions to trial things temporarily like changing 1 lane on the bridge to cycling.

I only live 10km away from my destination over the bridge yet it takes me 1 hour on the bus to get there. I want a cycling option!

We need to stop prioritising travel by car.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

- 7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:**

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$150+ per year (\$2.88+ per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Franklin

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

You have mentioned the need for strengthening within the community. I would suggest that to support intergenerational opportunities you engage Harbour Sport and their Play lead and ActivAsian Lead. The Play lead has done extensive scoping of Otahu Valley School to engage them with a Neighbourhood Play System to draw the surrounding community into Play opportunities.

Kaipatiki LB doesn't seem to have advocated for slower speed zones throughout the area. It's important that our roads are safer for people o bike, scooter, walk along, especially with children. Advocacy to AT would be of benefit.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

A reminder the māra hūpara does not equal nature play. Māra hūpara requires active iwi involvement and engagement. If this is your plan then great!!!

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation

	Water quality improvement	
	None of the above	
✓	Other, please specify:	Play, recreation, movement, spaces and places for community and families.

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Aktive

Local Board: Hibiscus and Bays

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

AK Have Your Say

Aktive Submission

Kaipātiki Local Board Plan 2023

Overview

Thank you for the opportunity to comment on the draft Kaipātiki Local Board Plan.

Aktive’s vision is Auckland – the world’s most active city: Tāmaki Makaurau – te tāone ngangahau rawa o te ao.

We provide leadership to the Auckland region that encourages, enables and inspires Aucklanders to lead more active lives through play, sport and active recreation. As both Sport NZ’s and Auckland Council’s strategic partner in Auckland, this means investing in organisations and projects that will get more people active, with focuses on tamariki, rangatahi, and identified communities.

We work closely with our System Build Partners, including Harbour Sport, to connect, support and collaborate with organised sport bodies, clubs, schools and community groups in the strategic direction, planning and delivery of programmes and facilities.

Aktive recognises the key role that Local Boards play in the delivery of play, recreation and sport across Tāmaki Makaurau. Local Boards are essential in creating communities where people want to live, work and play and they have a significant impact on the health and wellbeing of Aucklanders through governance and decision making on parks and open spaces, community facilities, grants and support, community leases and many other aspects.

Key Feedback

1. The draft Plan clearly identifies the importance of investing into supporting our communities to deliver activity and to grow their membership.
2. We fully support the aim to makes spaces accessible to everyone and the focus on active transport.
3. The Kaipātiki Local Board has been a great supporter in the development of sport and recreation facilities in the past and we support that this is clearly acknowledged in this draft Plan.
4. We request that the Kaipātiki Local Board continues to provide the Community Facilities Grant as this has delivered significant positive outcomes for the community.
5. We fully support the high priority that the Kaipātiki Local Board Plan has placed on tamariki and rangatahi and the importance of play in their lives.
6. We request the Kaipātiki Local Board add advocacy items to:
 - a. Advocate to the Governing Body for more investment into the Sport and Recreation Facility Investment Fund
 - b. Advocate to the Governing Body for more investment into the Regional Sport and Recreation Facilities Operating Grant.

Kaipātiki Local Board Plan 2023

Aktive commends the vision shown in the Kaipātiki Local Board Plan and the clear priority that has been placed on ensuring that communities have opportunities to access community facilities and open spaces to empower them to live healthy, active lives.

Does the Local Board Plan reflect what you want for this community over the next three years?

The Kaipātiki Local Board Plan does a good job at prioritising play, sport and recreation. We appreciate the focus on ensuring that community facilities and open spaces are fit for purpose and catering for our diversifying communities.

We acknowledge that the role of a Local Board is particularly challenging in that Local Boards need to deliver a wide range of facilities and services without the necessary budget to support their visions. We ask the Kaipātiki Local Board to be very direct in your prioritisation of what is important to the community so that you can make a difference in a number of spaces as opposed to spreading your support too widely.

Sport and Recreation Facilities and Open Spaces

We want to encourage the Kaipātiki Local Board to continue prioritising the health and wellbeing of our population by investing into and advocating for investment into community facilities and open spaces.

We fully support the objectives of:

- *Our aquatic, recreational, art and community facilities are fit-for-service and meet the needs of our growing and changing population.*
- *Our parks, playgrounds, and public spaces are fit for-service, accessible, SunSmart, well maintained, and cost effective.*
- *Quality and sustainable urban development occurs that creates spaces that are safe, healthy, multi-functional and have a low impact on the climate.*

And the key initiatives to:

- *Ensure our playing surfaces, sports fields, aquatic, and recreational facilities meet the increasing recreational needs, are accessible, well maintained, available to the public, and fit for purpose.*
- *Invest in the maintenance of facilities and assets in our parks, public spaces, and along the coast, and when they come up for renewal, consider how we can better meet community needs from our assets on a case-by-case basis, prioritising investment in our well used and loved assets.*
- *Ensure our playing surfaces, sports fields, aquatic, and recreational facilities meet the increasing recreational needs, are accessible, well maintained, available to the public, and fit for purpose.*
- *Consider climate impact when developing places and spaces, to reduce or avoid emissions and adapt to climate change, including avoiding areas or activities that are vulnerable to climate risks such as drought, extreme rainfall, coastal flooding, and heat.*
- *Prioritise investment in assets that are well positioned, multi-purpose, and with good transport links, to reduce its carbon footprint.*

Investment into sport and recreation facilities is extremely important in facilitating positive outcomes in the health and wellbeing of the population. There is evidence emerging that underspending on facilities leads directly to lower participation levels¹ and we know that physical activity brings \$372 million of healthcare savings for Auckland and contributes to 279 fewer deaths annually².

We support the aim to explore alternative ways of generating revenue and delivering facilities and services and would be very interested in being involved in this planning. Please don't hesitate to reach out to us, to enable our support this mahi. We know that there are increased pressures coming on council budgets and that with a diversifying population this makes it harder than ever to cater for the community, particularly in an area such as Kaipātiki with over 200 local parks and 16 significant community facilities.

Community Organisations

We fully support the objectives:

- *Our people and communities feel connected, safe, and well.*
- *Our diversity is celebrated and nurtured.*
- *Opportunities to participate in the arts and sport are available and accessible.*

¹ ACW Auckland City Report 2019, Portas Consulting. Available at: <https://aktive.org.nz/resource-hub/insights-research/>

² ACW Auckland City Report 2018, Portas Consulting. Available at: <https://aktive.org.nz/resource-hub/insights-research/>

And the key initiatives to:

- *Support participation in sport and recreation opportunities, including providing continued access to facilities and local reserves.*
- *Encourage and support a range of accessible and affordable initiatives that celebrate our diverse community and to create a sense of belonging.*
- *Prioritise investment in well-used services that are of quality and meet changing customer expectations and needs.*
- *Support a community-led approach to deliver relevant and diverse services that connect the community.*

We see sport and recreation organisations as a key part of this mahi and know that participation in sport brings 74.3 million hours of meaningful, positive social interaction to Aucklanders each year³ and that sport and recreation operating spend has a direct and significant correlation with participation levels⁴. If the Kaipātiki Local Board is able to continue their history of investing to grow the capability and capacity of sporting organisations, then this will lead to positive outcomes for the entire community.

Play

We acknowledge the focus on play and play spaces in the plan including the objective:

- *Our parks, playgrounds, and public spaces are fit-for-service, accessible, SunSmart, well maintained, and cost effective.*

And the key initiatives to:

- *Implement the actions and priorities identified in the Kaipātiki Play and SunSmart Provision Audit, Kaipātiki Nature Spaces and Trails Feasibility Assessment, Naturalisation of Parks Service Assessment, and the Kaipātiki Local Parks Management Plan.*
- *Invest in nature-based solutions that improve our parks and public spaces including planting specimen trees for shade, reduced mowing for increased biodiversity and natural play items such as māra hūpara.*

Play is an important aspect to acknowledge and plan for as, according to Sport New Zealand research⁵, its benefits include:

- being physically active in a fun way that develops fundamental movement skills
- encouraging self-directed creativity and innovation
- improving social and emotional connections
- improving a young person's understanding of their relationship with the physical environment
- improving resilience, independence and leadership by determining their own outcomes
- aiding better decision-making based around elements of challenge and risk.

Advocacy

We strongly support the advocacy items:

- *Advocate for sufficient funding to maintain facilities and the service provided through our parks, community facilities, and recreational spaces.*
- *Continue advocacy for bringing forward implementation of the Birkenhead War Memorial Park Masterplan, with the first project being to develop a new multipurpose facility and improved aquatic play space.*

We also ask the Kaipātiki Local Board to also consider advocacy items such as:

- Advocate to the Governing Body for more investment into the Sport and Recreation Facility Investment Fund. This is a key regional fund that will enable facilities to be developed that aren't owned by Council and that have a significant impact on physical activity and health for our communities.
- Advocate to the Governing Body for more investment into the Regional Sport and Recreation Facilities Operating Grant to enable communities to deliver facilities and lessen the burden on council.

³ ACW Auckland City Report 2019, Portas Consulting. Available at: <https://aktive.org.nz/resource-hub/insights-research/>

⁴ ACW Auckland City Report 2019, Portas Consulting. Available at: <https://aktive.org.nz/resource-hub/insights-research/>

⁵ Importance of Play 2017. Sport New Zealand. <https://sportnz.org.nz/media/1654/sport-new-zealand-play-principles-nov-2017.pdf>

Conclusion

Aktive supports the work of the Kaipātiki Local Board in creating opportunities for communities to lead active, healthy lifestyles, and we thank you for your commitment to play, recreation and sport as an integral part of life in your community. We also support a continuing and close connection between the Kaipātiki Local Board and Harbour Sport in meeting the needs of our communities.

As noted earlier in this submission, Aktive and Harbour Sport would greatly appreciate you referencing the critical nature of the Sport and Recreation Facilities Investment Fund and the Regional Sport and Recreation Facilities Operating Grant within the Kaipātiki Local Board Plan. Aktive is working with Councillors to ascertain options for increasing the Sport and Recreation Facilities Investment Fund because simply retaining the existing fund level will not be enough to accommodate the demand that exists today, let alone into the future.

As part of these same discussions, Aktive is advocating for the retention of the Regional Sport and Recreation Facilities Operating Grant. This funding is critical to so many sport and recreation facility providers. Without it, costs to users will increase and / or facilities will be forced to close or significantly reduce operating costs which will have an impact on service levels. We are happy to support your Local Board support staff in preparing some further detail for inclusion in your Plan, if you are willing to do so.

We once again thank you for the opportunity to contribute to the Kaipātiki Local Board Plan development process. If you have any questions, or if we can help with any of your ongoing planning, please do not hesitate to contact us.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

Love the call out for cycling in general and in Wairau, I currently do that and it is scary but I need things from the shops there.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Converting a lane on the Harbour bridge for active modes ASAP. I want to be able to cycle to work from Northcote and not have PTSD about being stuck in the city when it floods (every time it rains now I am panicky and I am not the only one. It took many scary hours to get home during the floods with public transport and full busses, but if I could have taken my ebike to work I would have gotten home fast and safe). I am now car free and find relying on public transport on weekends unreliable and infrequent, I want to be able to cycle across to the city centre.

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Active modes and public transit need priority over private, it is currently terrible being in anything other than a car and I am trying, but it is really hard to be responsible and use active modes and public transport without consistency and safety

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

The natural environment is what makes living here lovely, we should protect it and grow it

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input checked="" type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows

	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?

Harbour Bridge active lane ASAP absolutely. More safe cycling infrastructure. Love the shared paths around Northcote and towards Takapuna, but there is a gap between the end of the Takapuna path and Hurstmere Rd that is scary but I like Hurstmere Rd a lot and being able to cycle to the beach. Cycling west is terrible terrible! My parents live that way and I am not comfortable cycling from Northcote to Birkdale. I cycled from Northcote to Glenfield once and it was scary, but it is so close it should feel safe and easy. I want to be able to go to Albany as well, but from Northcote to the end of Albany highway is very scary for cycling!! Please these would be such easy fixes to help people get around without relying on buying a car. Cycling lets me appreciate the community and local nature areas that I never got to appreciate and connect with while I drove everywhere. Thank you!!!



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Pest Free Kaipātiki

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Very well

Tell us why

The plan shows that effort has been made to ask the community what is important to them and listen and respond to their feedback. Priority areas are good.

2. Is there anything else you think should be included in the draft plan?

No

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

Opportunities and challenges identified seem to cover the most significant for each priority area.

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

The draft plan seems to be proactive and supportive of Māori aspirations, but how well it does this is for Māori to say. From an outsider perspective it looks great and we support the attitude of working as a good Treaty partner and supporting and celebra

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

Climate change is a significant threat to our health and wellbeing and the continued survival of our natural environment. We are pleased to see this as a priority and the recognition of the climate emergency. We want to see strong action on this, and are wary of 'good intentions' policy that is more talk than outcome. The Transport section of the Draft Plan identifies half of Auckland's emissions coming from transport and so we want to see every effort taken to provide the transport options people need to be able to make the change away from private vehicle transport. We are pleased to see tree planting included in this section, but would also like to see the recognition that the biggest benefits to mitigating both climate change and its impacts comes from existing large trees, which currently have no protection. This is identified as a point for the local board to lobby on under the Environment section (which we enthusiastically support), but we believe it should be recognised under Climate Change as well.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

We believe our community wants to see continued support for environmental services and values the amazing biodiversity that lives within Kaipātiki. A targeted rate would help provide a more secure funding stream for environmental services.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Under the Environment section we are very pleased to see the Local Board continuing support for the pest free vision of Kaipātiki as well as recognition and support for the extraordinary efforts of the many volunteers who contribute to maintaining and improving our natural environment. We see pest plants and animals as continued significant challenges to overcome in order to protect our natural environment and believe this could be included as an identified challenge to overcome.

We welcome the partnership with Māori and the incorporation of Māori worldviews in the environmental space, as well as the intention to improve Māori place names in our parks and public spaces.

We would like to express a little caution in regards to the plan to investigate storm water detention facilities in parks. This is obviously in response to a need that we do not dismiss, however, our green spaces are shrinking and we do not want to see further pressure put on our biodiversity from loss of yet more habitat to house these facilities.

We strongly support the actions listed under Advocacy in the Environment section, in particular advocacy to lobby Central Government to reinstate general tree protection in legislation. We would like to have more clarity about the use of the word 'delivery' in the first point listed in the Advocacy section. We do not believe that volunteers should be expected to carry out the delivery of Council Contracted services, although we do want to see greater involvement in the setting of these contracts and a more collaborative and open partnership in this area in order to best develop and efficient and supportive relationship between park volunteers, Council contractors, the Local Board and other Council staff. The broad intention of this point seems good, the wording is just a little unclear. Overall the points listed under the Advocacy heading are very much in line with our priorities and the issues that we hear our community struggling with and we are pleased to see the Local Board identifying and including them in their Draft Plan.

Under the Places and Spaces category we are pleased to see the intention to invest in nature-based solutions.

Under the Transport heading we support the intention to prioritise and invest in active and public transport options. As mentioned in our comments about climate change, this area has significant implications for climate change, as well as the health and wellbeing of the people of Kaipātiki. We support investment and advocacy towards better, more frequent, more affordable and reliable busses and ferries.

We also note that the Board suggest advocacy for improved street lighting. We would like to see street light improvements that are safe for our night-flying birds. Sea birds in particular migrate over Auckland at night and can easily become confused and injured due to artificial lights causing them to fly into urban areas. There are good street light options available that significantly reduce this risk and we would like to see this being part of the conversation when advocating for street light improvements.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Shepherds Park Squash Club Inc

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Sufficient investment in aged council leisure facilities.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

Sufficient priority needs to be given to other minority communities within the KPLB area as it is very racially diverse.

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

So many conflicting priorities. There is sufficient budget to meet all outline budget requirements.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
	Water quality improvement

	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?



August 14 2023

Kaipataki Local Board

11 Cambria Rd
Devonport
Auckland 0624
☎ 0274 379 423

By: on line submission

Dear Local Board

LOCAL BOARD PLANS2023

We write in regard to the above local board plan to request that sufficient priority be given to the leisure centres within the community including the **Beach Haven Sports centre** in which both our squash club and the Beach Haven tennis club are situated.

This facility is dated and we are aware that capital expenditure proposed for the building to bring it into the 21st century has been deferred due to recent floods and council budget cuts.

You emphasise in the detailed draft plan that insufficient budget is available to maintain all sports facilities and that the KPLB area has “the perception of being over-serviced by parks and reserves when compared to other parts of Auckland”. We trust that you will act as our advocates to dispel this perception, accentuate the population growth within the area and ensure that local residents have sufficient sporting & leisure opportunities.

We trust that in this regard we will benefit when the time is right from your leading pledge to “invest in the maintenance of facilities and assets in our parks, public spaces, and along the coast, and when they come up for renewal, consider how we can better meet community needs from our assets on a case-by-case basis, prioritising investment in our well used and loved assets”.

The leisure centre was constructed in 1974 and celebrates it's 50th anniversary next year which we fully intend to celebrate in collaboration with appropriate council bodies. We are reliant on your support for its continuation but both clubs are prepared to collaborate with council to drive up utilisation of the centre and further increase its value to the local community.

Best regards



Paul Cornish
Club President



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Big Street Bikers

Local Board: Albert-Eden

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

Please see attached



Big Street Bikers feedback on the draft Kaipātiki Local Board plan 2023

Thank you for the opportunity to give feedback on the draft Kaipātiki Local Board Plan 2023.

Introduction

Big Street Bikers (BSB) is an Auckland-based company with social enterprise accreditation from the Ākina Foundation. BSB is working to increase the access to and use of e-bikes in Aotearoa. We provide secure parking, wayfinding, and charging facilities called Locky Docks that are a free public amenity delivered at no cost to ratepayers.

We work in partnership with mana whenua, community groups, businesses, councils, local boards and government agencies to offer people more transport choices and to promote active and public transport modes. Locky Docks promote the uptake of people riding by removing the security concerns associated with parking bikes at transport hubs, town centres, community facilities and key destinations. There has been a noticeable increase in bike thefts acting as a deterrent to using a bike for short trips.

Big Street Bikers is rolling out a network of Locky Dock stations across Tāmaki Makarau as part of a country-wide programme. The capital expenditure and recurring operational expenditure associated with the delivery and operation of the Locky Dock network is funded by Big Street Bikers through partnerships with Waka Kotahi, Mercury and the sale of third-party advertising content.

Infrastructure investment that gives locals the opportunity to switch short expensive car trips to an easy bike ride is one of the most effective ways of reducing household transport costs for residents. Everyone benefits from a transport system that supports an urban lifestyle where short trips by bike, micro-mobility or on foot are the norm. More trips by active transport leads to cleaner, safer, more business friendly streets, and more resilient communities.

Feedback on the Local Board Plan

Big Street Bikers commends the Local Board for its commitment to transport initiatives to ensure people have more travel choices to get to work, school or go about their daily lives

We would like to suggest that the Local Board include as an initiative in the final local board plan ***installation of secure bike parking, digital wayfinding, and charging stations***





at transport hubs, town centres, community facilities and key destinations as an initiative to be delivered within the next three years.

A local network supports the Local Board's climate action goals and advocacy for investment in active transport modes. The network will help promote and encourage the uptake of active transport in Kaipātiki.

With the funding Big Street Bikers currently has available it will be possible for the local board to achieve a local network of secure bike parking over the next couple of years. Big Street Bikers welcomes the Local Board's assistance to identify suitable locations and advocacy to Auckland Transport for appropriate sites in the road corridor for secure bike parking, wayfinding and charging stations.

Cleve Cameron, co-founder, Big Street Bikers

cleve@bigstreetbikers.com

312 Karangahape Road, Auckland 1010

Phone 021 827706





KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

Climate change and emissions are not the problem, the problem is plastic

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

You should save money, not keep adding new rates

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 - I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

You should stop all the apartments being built in Beach Haven/Birkdale, it should be kept as a suburb, not turned into a city



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Harbour Sport

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

See attachment

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

See attachment

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

See attachment

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

See attachment

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above

✓	Other, please specify:	Sport & Recreation facilities and programmes
---	------------------------	--

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

See attachment

HARBOUR SPORT SUBMISSION

Submission to:

Kaipātiki Local Board Draft Plan

From:

Harbour Sport

Vision: A community physically active for life

Purpose: Inspiring, empowering and strengthening the community through physical activity

INTRODUCTION

Research has shown that increasing community participation in Play, Active Recreation and Sport (PARS) to social capital which acts as the social fabric that binds New Zealand communities. Sports clubs, community organisations, and recreational parks and facilities are important conduits for developing social capital and are good indicator of community strength.

The key benefits of increased social capital through community sport and recreation include:

- Building community sense of pride and belonging
- Increased social inclusion
- Reduced anti-social behaviour
- Educational programmes
- Improved health and well-being
- Economic growth
- Urban and regional regeneration

Kaipātiki has a large number of sport and recreation clubs, regional sport organisations and over 110 sport and recreation facilities which cater for approximately 88,671 of your community residents and approximately 380,000 residents living north of the bridge.

The North Harbour Region has one of the fastest growing populations in New Zealand, with clear indications suggesting that growth will occur around existing town centres in areas such as Kaipātiki. These factors need to be taken into consideration when developing effective community strategies around sport, recreation, parks and facilities in the region.

Harbour Sport endorses the Kaipātiki Local Board outcome areas of access to recreation choices and open space for all including partnering with Harbour Sport to link in groups to make better use of recreation facilities and groups working together to attract funding to realise multi-use clubrooms and spaces. It is important to enhance access and activities on our coastline, parks and reserves and Harbour Sport endorse your commitment to improving park facilities, reviewing reserve management plans to ensure they are fit for purpose and support sport and recreation outcomes and partnering with other organisations.

SUPPORT

We support the following initiatives:

- Support participation in sport and recreation opportunities, including providing continued access to facilities and local reserves
- Ensure our playing surfaces, sports fields, aquatic, and recreational facilities meet the increasing recreational needs, are accessible, well maintained, available to the public, and fit for purpose
- Continue advocacy for bringing forward implementation of the Birkenhead War Memorial Park Masterplan, with the first project being to develop a new multipurpose facility and improved aquatic play space
- Advocate for sufficient funding to maintain facilities and the service provided through our parks, community facilities, and recreational spaces
-

We also ask the Kaipātiki Local Board to also consider advocacy items such as:

- Advocate to the Governing Body for more investment into the Sport and Recreation Facility Investment Fund. This is a key regional fund that will enable facilities to be developed that aren't owned by Council and that have a significant impact on physical activity and health for our communities.
- Advocate to the Governing Body for more investment into the Regional Sport and Recreation Facilities Operating Grant to enable communities to deliver facilities and lessen the burden on council.

CONSIDERATIONS

We kindly request the Kaipātiki Local Board support in addressing the following issues in the Harbour region. We propose exploring options such as:

- Continue advocacy for bringing forward implementation of the Birkenhead War Memorial Park Masterplan, with the first project being to develop a new multipurpose facility and improved aquatic play space.
- Identifying suitable locations for new indoor court facilities.
- Upgrading existing facilities to accommodate a broader range of indoor sports.
- Collaborating with Harbour Sport local sports organisations and community groups to better understand their needs and preferences.

By investing in indoor court facilities, the Kaipātiki Local Board will not only enhance the quality of life for residents in the Harbour region but also promote physical activity, community engagement, and the development of local sports talent.

CONCLUSION

Harbour Sport thanks the Kaipātiki Local Board for the opportunity to comment on the draft local board plan. We hope that the comments and feedback will help support the decision making and provision allocation process.

We believe that the final Kaipātiki Local Board Plan with strong additions regarding the value of play, active recreation, sport and physical activity in the community will be well placed to become a key document for the community for future Sport and Recreation development.

This submission supports the position that Harbour Sport currently holds within the North Shore region and their involvement as a key stakeholder in any future development of sport and recreation in the Kaipātiki communities.

If you would like further clarification of any of the areas raised in this submission, please contact Mike Bishop.

Contact:

Mike Bishop

Chief Executive
Harbour Sport
ceo@harboursport.co.nz

Signature:

Date:



14/8/2023



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

- 1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

I would say that because local parks will be more accessible for children and those in the community by providing somewhere to sit and rubbish bins to put the rubbish in. We also need to take control of animals.

- 2. Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Controlling of animals, such as dogs and cats in the community. More accessible for Children.

- 3. Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

Safety in our community is important for everyone.

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement

	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Henderson-Massey

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0-\$49 per year (0-\$0.95 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0 – I wouldn't be willing to pay any more

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
✓	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

Key initiatives are really good as are how the local board will advocate for these initiatives. However, community and recreational facilities is too broad and should have clarified some assessment of underutilised (places and spaces) is well overdue. We are very well served in Kaipātiki for places and spaces. No new facilities are needed.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

How will some of the initiatives will be funded i.e. rates or government? Who is going to activate these initiatives? Who is going to ensure these are activated?

3. Have we identified the most important opportunities or challenges in our draft plan?

No

Tell us why

Opportunities are identified but challenges are not.

4. How well have we reflected the aspirations for Māori in our draft plan?

Quite well

Tell us why

It sounds good but it is hard to know how much consultation there was with the mana whenua to decide on which initiatives would meet their aspirations. Maybe my response should be not very well.

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

This all sounds good but i'm not sure how these challenges could be extrapolated.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

But will this targeted funding allow for all the other objectives. Protecting and enhancing the wonderful natural environment of Kaipatiki.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows

✓	Pest (plant and animal) reduction	
✓	Retrofitting council facilities to be more environmentally sustainable	
✓	Tree planting	
✓	Waste minimisation	
✓	Water quality improvement	
	None of the above	
✓	Other, please specify:	Community gardens and investigating the possibility of allotments for apartment dwellers as in the UK.

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?

I would like to see an outline of the ways in which the local board members are made accountable for all the expenditure including personal expenses.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0-\$49 per year (0-\$0.95 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Not Supplied

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$50-\$99 per year (\$0.96-\$1.91 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Birkenhead Residents Association Inc

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

We encourage the Local Board to help facilitate (or even require) greater collaboration between community groups to ensure the most efficient and effective use of Board funds.

We would also be keen to discuss how the Board might introduce stronger measures to ensure recipients of its funding deliver the outcomes and value in a way that meets expectations

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Very well

Tell us why

The plan refers to co-management. It would be helpful if this can be clarified so that the scope and implications of this can be considered. The cost of this would also be useful to understand, if there are financial implications.

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

The climate related impacts for Kaipatiki will be significantly increased if housing is intensified to the level envisaged in Plan Change 78. We encourage the Local Board to continue advocating on this matter.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Targeted rates only work if fairly applied across the city for specified or certain types of work. Otherwise, some communities gain benefit from their 'usual' rates that others pay a targeted additional rate for.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management

	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



residents assn.

PO Box 340 374
Birkenhead
Auckland 0746

14 August 2023

To the Kaipātiki Local Board

Submission on the Kaipātiki Local Board Plan

The Birkenhead Residents Association (**BRA**) welcomes this opportunity to provide feedback on the Kaipātiki Local Board's draft 2023 local board plan (**Draft Plan**).

We appreciate the strong working relationship that we have with the Kaipātiki Local Board (**KLB**), and we generally support the intention behind the initiatives in the Draft Plan.

However, as an overall comment, the Draft Plan refers to a large number of initiatives, many of which (while positive) may not have as immediate a benefit to the Kaipātiki area. We would invite the KLB to focus its investment (time and financial) on a prioritised subset of key initiatives that are likely to be the most beneficial and cost-effective.

In our view, the key initiatives should be:

- **Little Shoal Bay** – interventions arising from the Shoreline Adaptation Plan (in association with the Little Shoal Bay Protection Society), in particular protection of the existing wetland area from saltwater ingress, and the parks and reserves (including the cricket pitch) from inundation.
- **Le Roy's Bush** – restoration of tracks affected by the extreme weather events earlier this year. We invite KLB to partner closely with the Friends of Le Roy's Bush in this work, noting the statement in the Draft Plan that the KLB would "*Advocate that the local board and our volunteer groups are more directly involved in the setting and delivery of council-contracted ecological services*".

This involvement of community groups in the setting and delivery of Council contracts is something we strongly encourage across Kaipātiki's service contracts, for example working with CHERPA in relation to the damage in Chelsea Bay.

- **Advocacy** – in particular, to minimise the detrimental effects of Plan Change 78 on the Kaipātiki region's infrastructure (stormwater and transport), sensitive ecological areas, and special character features. The Kaipātiki region will be disproportionately affected by PC78 relative to other areas of Auckland, and we encourage the KLB to focus its efforts accordingly.

In relation to other advocacy initiatives, we would also emphasise (i) advocacy to the Governing Body relating to setting and delivery of ecological services as referred to above; (ii) advocacy to Auckland Transport regarding the importance of reliable public transport, ahead of cycleways, speed variations and signage changes, to enable safe and easy mass movement; and (iii) improved protections for existing trees and bush, particularly where these contribute to mitigating stormwater erosion.

There are other initiatives in the Draft Plan that in our view, while well-intentioned, may not be supported from a cost/local benefit perspective. For example:

- While we support the Draft Plan's objectives relating to reducing climate impacts, we question whether rooftop solar initiatives (as referred to in the Draft Plan) are the only, or indeed the most cost-effective, way for KLB to pursue this objective.

While solar power can add value, we feel that KLB funds should only be spent on it where there is a financial case for doing so. Specifically, (i) the break-even date for the project must occur before the warranty on the installation runs out, and (ii) the IRR (internal rate of return) for the project must exceed the average rate the council is being charged for its debt.

Other initiatives may be more cost-effective ways to achieve climate objectives. These include (i) increased protection of existing bush and support for planting of native trees; and (ii) advocating for power companies to provide better rates for solar panel contribution to the grid. The former will also provide other benefits such as native bird habitats and reducing silting in sensitive catchment areas.

- The Draft Plan refers to support for businesses in the Wairau Valley. In our view, commercial activities should generally "pay their own way". We would prefer to see KLB's resources focused on support for local volunteer groups and community initiatives, in particular local groups who demonstrate clear vision, delivery and value.

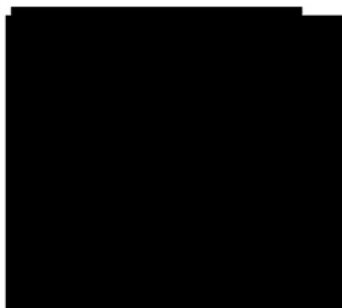
We have also provided further comments on the Draft Plan in the attached copy of the feedback questionnaire. We would welcome the opportunity to discuss these matters further with the KLB.



Gillian Taylor, Chair

On behalf of the BRA Committee:

Gillian Taylor (Chair)





Help shape Kaipātiki Local Board Plan

Feedback must be received by 4pm on 14 August 2023

Local board plans are strategic three-year plans that are developed based on community feedback.

Local boards make decisions about services and facilities in their area. This includes libraries, leisure centres, local parks, community grants and events. However, the cost of living and providing these services has gone up, as well as the costs of borrowing money and doing business.

We want to understand what matters most to you and your community. Before giving your feedback, take a look at the Draft Kaipātiki Local Board Plan.

We encourage you to give feedback online at akhaveyoursay.nz/localboardplans, or you can:

Scan and Email your completed form to:
kaipatikilocalboard@aucklandcouncil.govt.nz

Post your completed form to:
Local Board Plans
Freepost number 190153
Auckland Council
Private Bag 92300, Victoria Street West
Auckland, 1142

Your details

Your name and feedback will be included in public documents. All other personal details will be kept private.

First name:

Last name:

Email address or postal address:

Your local board:

Is your feedback on behalf of an organisation or business? (If yes, this confirms you have authority to submit on the organisation's behalf)

Yes

No

Name of organisation/business: _____

Important privacy information

The personal information that you provide in this form will be held and protected by Auckland Council in accordance with our privacy policy (available at aucklandcouncil.govt.nz/privacy and at our libraries and service centres) and with the Privacy Act 1993. The privacy policy explains how we can use and share your personal information in relation to any interaction you have with the council, and how you can access and correct that information. You should familiarise yourself with this policy before submitting this form.

These questions are optional but will help us understand which groups of the community are engaging with us.

What gender are you?

- Male Female Another gender (please specify):

What age group do you belong to?

- Under 15 15-17 18-24 25-34 35-44
 45-54 55-64 65-74 75+

Which ethnic group(s) do you feel you belong to? (Please select as many as apply)

- Pākehā/NZ European Other European Māori
 Cook Islands Māori Samoan Tongan
 Indian Chinese Korean
 Other (please specify): _____

Which suburb/community do you live in: _____

Would you like to subscribe to any of the following (tick all that apply):

- Kaipātiki Local Board e-newsletter
 People's Panel – to take part in council surveys
 Our Auckland – your weekly guide to what's happening in Auckland

You can also visit AK Have Your Say at akhaveyoursay.nz to find out about, or register to receive regular updates on, other consultation activities happening across Auckland.

Your feedback (all questions are optional)

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

- Very well
 Quite well
 Not very well
 Not well at all
 Unsure

Tell us why

2. Is there anything else you think should be included in the draft plan?

- Yes
- No
- Unsure

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

- Yes
- No
- Unsure

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

- Very well
- Quite well
- Not very well
- Not well at all
- Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

- Very well
- Quite well
- Not very well
- Not well at all
- Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

- Yes
- No
- Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

- Bush track maintenance
- Ecological volunteer programme
- Flood management
- Park naturalisation e.g. butterfly meadows
- Pest (plant and animal) reduction
- Retrofitting council facilities to be more environmentally sustainable
- Tree planting
- Waste minimisation
- Water quality improvement
- None of the above
- Other, please specify: _____

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

- \$0 – I wouldn't be willing to pay any more
- \$0-\$49 per year (0-\$0.95 per week)
- \$50-\$99 per year (\$0.96-\$1.91 per week)
- \$100-\$149 per year (\$1.92-\$2.87 per week)
- \$150+ per year (\$2.88+ per week)

9. Do you have any other feedback on our draft Local Board Plan?

Need more room? You can attach extra pages.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Grey Power North Shore

Local Board: Devonport-Takapuna

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

The Tree Council

Local Board: Waitākere Ranges

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

**Submission - Kaipātiki Local Board Plan 2023
by The Tree Council**

14 August 2023

From: The Tree Council

Contact: Dr Mels Barton, Secretary
PO Box 60-203, Titirangi, Auckland 0642
021 213 7779
info@thetreecouncil.org.nz

Thank you for the opportunity to present The Tree Council's submission on the proposed **Kaipātiki Local Board Plan 2023**.

This submission is made by The Tree Council, an independent, voluntary organisation, a non-profit incorporated charitable society which has been serving the Auckland community since 1986 in the protection of trees and as advocates for the significant benefits and services that our trees and green spaces provide.

Climate Action

The Tree Council comment:

- *We support these objectives and initiatives regarding enhancing tree cover within the local board area. We applaud the fact that one of your flagship projects is to implement the Urban Ngāhere Strategy Action Plan within the local board area to enable opportunities to enhance tree cover to be identified and activated.*
- *We are delighted to see recognition of and your support for the important role mana whenua and community groups play in working to enhance biodiversity and undertake tree planting.*

Our Environment

The Tree Council comment:

- *We acknowledge your environmental work in weed management and the regeneration of your reserves and green spaces.*
- *We urge you to oppose all consents proposing to remove trees in Significant Ecological Areas and Scheduled Notable Trees on both private and public land as these are so important to retain for future generations.*
- *Given that you have kauri in many of your local native bush reserves we strongly urge you to get them tested for kauri dieback as soon as possible and if found to be positive*

then you should consider getting them treated to keep them alive. Kauri dieback has an almost 100% mortality rate so all infected trees will die without treatment. We recommend you work with the Kauri Rescue Trust for the best way to approach this problem. There is an urgency as trees in which the disease has progressed too far will not survive even after treatment, so the earlier this happens the better. Kauri are carbon sinks, we need to keep these trees alive as long as possible.

In addition we would like to see specific support, including financial support, for the following initiatives to protect and enhance existing tree cover:

- *Practical support and (for eg legal) advice for private landowners wishing to provide permanent protection for their trees eg via covenants &/or scheduling to enable protection of existing tree cover on private land to be optimised as most of the mature trees in Auckland are on private land with no legal protection.*
- *Grants for private landowners to maintain Scheduled Notable Trees on their property*
- *Provide permanent and visible physical labels for all Scheduled Notable Trees in the local board area to enable the public to see that this tree is legally protected and tell the stories of our natural heritage.*
- *Look for opportunities to schedule additional mature trees and encourage Council to undertake Plan Changes to the Unitary Plan to regularly add new trees to Schedule 10 (Notable Trees).*
- *Look for opportunities to raise awareness of the value of Scheduled Notable Trees by organising events to celebrate them such as guided walks, community picnics, heritage talks.*
- *We would like to see specific support, including financial support, for mana whenua, local communities and groups undertaking the following types of initiatives within the local board area:*
 - *water quality testing*
 - *monitoring of beaches*
 - *treatment of diseased kauri trees on local parks and private land*
 - *monitoring for myrtle rust*
 - *weed control*
 - *pest control*
 - *protection of rare birds eg dotterel*
 - *restoration planting*
- *Public education on issues affecting biodiversity such as:*
 - *kauri dieback, myrtle rust*
 - *weeds*
 - *animal and insect pests*
 - *other biosecurity threats*
 - *control of dogs in sensitive areas*
 - *conflicting uses eg vehicles on beaches*
 - *rules for marine reserves*



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Not Supplied

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Takapuna Athletic & Harrier Club

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

See attachment.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Hibiscus and Bays

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0-\$49 per year (0-\$0.95 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Eventfinda Stadium

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

Paul Edwards
C/- Kaipatiki Local Board

Via: Email

12/08/2023

Dear Paul,

I am reaching out to you on behalf of the North Shore Events Centre Trust Board (Eventfinda Stadium) to provide feedback regarding the Kaipatiki Local Board Plan currently under consultation. Our intention is to contribute insights that could aid the elected members in their upcoming discussions. It is heartening to observe the extensive effort invested in shaping this document, especially considering the notably "challenging" years that have transpired. I extend gratitude to all members for their diligent work on our collective behalf.

It is highly likely that our feedback will primarily focus on the aspects related to the environment, particularly those concerning climate change, within the draft plan. It is reassuring to note the emphasis placed on this sphere, as it has become a paramount concern for our stadium and the communities we serve.

You might be aware that our venue is surrounded by a Water Care owned open swale on one side, while the other side borders the Takapuna Golf Course. Furthermore, the location is nestled between Sunnybrae Rd and the motorway, with the presence of the Glenfield Substation and the Pumpstation at the eastern end of our carpark. In addition to this, there exists an encumbrance to Vector on the land (leased from Tataki Auckland Unlimited (TAU) to Eventfinda Stadium), allowing the Vector substation feeds to run alongside the open swale. This swale serves to channel storm runoff from Totara Vale, Glenfield, and the adjacent areas to Milford, where it ultimately flows into the ocean through a series of "strainers" designed to trap debris and detritus collected by storm runoff. The complexity of this site both enhances the community and challenges our ability to solve the issues raised by these services.

On the night of January 27th, our premises were completely inundated by the swale to an extent that is previously unprecedented. The swale's capacity to handle the water volume was exceeded, leading to water flowing onto the golf course and inundating our building from multiple sides. During this time, the substation experienced flooding and shut down the neighbouring pumpstation and the congested "strainers" on-site hindered the swale's ability to effectively pass water to the Milford outlet. While these were not the sole contributing factors, they certainly played a critical role in our perspective and must be factored into future stormwater management plans. Notably, the swale's walls have collapsed twice in the past two years.

The flooding resulted in \$3 million worth of damage to the building, forcing a complete shutdown of operations in the community for 12 weeks, in addition to five months of severely limited operations while we assessed, planned, and executed necessary repair work. This has led to unsustainable disruptions to both Gymnastics and Basketball, both of whom I suspect will take 2-3 years to fully recover from the disruption caused to their operations. At a time when the Auckland community is on edge with ski rocketing school absenteeism and issues arising from disengaging youth resulting from Covid-19, facilities like ours are at the forefront of bringing communities together again and stemming the effects of the community fragmentation of the past three years. We need to do better for our Rangatahi especially.

The Stadium was initially constructed in 1992 to accommodate Harbour Basketball and Gymnastics and give those communities a home while also providing a venue for events of community benefit to occur on the shore. Over the years, these communities have had nearly uninterrupted access to this facility, leading to the establishment of two of the country's most successful associations in their respective fields right here on the North Shore. For 31 years, Eventfinda Stadium (previously NSEC) has consistently welcomed more than 1,000 visitors a day, 300 days each year. You would go far to find a resident of the Shore who has not participated in a gym class, watched a basketball game, or attended an event here. We are the North Shore's equivalent of a "town hall," it is a role we deeply cherish and one we are determined to protect into the future. Our stated purpose is *"to provide spaces, places and experiences that foster and enhance the mana of our communities"* it is what we exist to do.

However, we now face a considerable threat from the potential recurrence of a flooding event. Our appeals to the council thus far have met responses along the lines of:

"I would recommend that ensuring resilience for the Event's Centre should be a central consideration in the future asset management planning for TAU and the Trust."

Given the presence of various stakeholders on-site (TAU, Water Care, Vector), resolving this issue needs the involvement and, more crucially, the leadership of Auckland Council. In my view, it is inequitable and unrealistic for Auckland Council to place the burden of resolving these issues, along with the funding of them, on the communities that rely on our facility when the amenity arising from the services (Watercare/Vector) are key to the causation.

With respect, I am seeking the assistance and advocacy of all members of the Kaipatiki Local Board to rectify this stance and help us attain enhanced protection against the adverse impacts of Water Care's open swale. Within the deliberations on the Board Plan we need to find support and solutions to help us achieve (with the stakeholders) an equitable and sustainable defence against future disruptions to this site, and the subsequent damage to our community outcomes.

Sincerely,



Brian Blake
Chief Executive
Eventfinda Stadium

CC: Janet Grant – Harbour Gymnastics
John Hunt – Harbour Basketball
Maree Laurent – Tataki Auckland Unlimited



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Kaipatiki Community Facilities Trust

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



Submission on DRAFT Kaipātiki Local Board Plan:

CONTACT DETAILS

Jill Nerheny QSM
Manager
Kaipātiki Community Facilities Trust

Mobile: 0274 834511
Email: jill@kcft.org.nz

Postal address: P O Box 34344 Birkenhead Auckland 0746:

Static Address: 15 Chartwell Ave Glenfield 0629

Dated August 1st, 2023.

About us:

Kaipātiki Community Facilities Trust (1983) the lead community development organisation for the Kaipātiki Local Board through a partnering agreement. Kaipātiki Community Facilities Trust (KCFT) has been part of a community since 1983, and has been the catalyst for change through collaborative partnering.

Empowering Communities - together to achieve in the `` Kaipātiki Way ``

Our role in Kaipātiki in our communities through the Partnering Model with the Kaipātiki Local Board:

- The KCFT role is to through a community development lens is to deliver key identified programmes and projects, key community events, and ensuring the wider communities of Kaipātiki are well connected.
- We will provide a series of key local networks and connect people with resources and each other.
- As well as working with the community, we work with other agencies to improve the connections between them and the diverse community in Kaipātiki to ensure the needs and priorities of their communities are met.
- Work with community groups to deliver projects or initiatives and support our young people and rangatahi to be the best they can be.

THE SUBMISSION

Kaipātiki Community Facilities Trust.

Support for the DRAFT Kaipātiki Local Board Plan 2023

Ngā huanga Māori

Māori Outcomes

The Kaipātiki Local Board has supported, and continues to support, projects to further the visibility of Māori identity in the area. This includes Te Kete Rukuruku, a programme that showcases the rich Māori heritage and stories of Kaipātiki. One element of this programme is to restore names significant to Māori to our local parks. We have also invested in programme's delivered by community, including weaving workshops, Mau Rākau sessions, school kapa haka festivals and Matariki celebrations. We celebrate and promote te reo Māori and te ao Māori through programmes and events.

Kaipātiki Community Facilities Trust.

- Kaipātiki Local Board to continue to support grass roots delivery model, at the very localised levels. Whilst partnering with mana whenua on existing and new climate action and environmental projects that tautoko (support) kaitakitanga outcomes.

Te Tāruke ā-Tāwhiri

Climate Action

The Kaipātiki Climate Action Plan in March 2023. This action plan sets out how Kaipātiki can transition into a resilient, zero carbon community which is actively adapting to the impacts of climate change. Based on the strategic principles of The Auckland Plan, and Te Tāruke-ā-Tāwhiri: Auckland's Climate Plan, it provides a positive pathway, socially, economically, and environmentally, by focusing on the actions we can take now that are good for our communities.

Kaipātiki Community Facilities Trust

Supports the Kaipātiki Local Board, utilising the community development model of work,

- connecting community members and leaders who are working on climate change, particularly where this can help scale up or amplify innovation.
- Champion opportunities for improved active travel within the forthcoming update to the Kaipātiki Connections Network Plan (for example, Wairau Rd and Albany Highway).

Te Whai Wāhitanga me te Oranga

Belonging and wellbeing

Our people are engaged, connected, healthy, thriving, and are proud to live in Kaipātiki.

Kaipātiki is a culturally diverse place. As of the 2018 census, our European population is the largest represented ethnicity, at 58 per cent, however this is projected to decrease to 47 per cent by 2043. Our Asian population is rapidly growing and the current figure of 34 per cent is expected to reach 57 per cent by 2043. Other ethnicities are expected to remain relatively similar in proportion. Nine per cent of our community identify as Māori, 6 per cent identify as Pacific peoples and 3 per cent from the Middle East, Latin American and Africa.

We have a relatively new community, with 46 per cent of people living in Kaipātiki born overseas, and 21 per cent having lived in their house for less than 12 months.

Our newest residents have brought with them their own sets of beliefs and identities and we believe that we should all, nurture and celebrate these by supporting a range of ethnic and cultural events and activities. We want everyone to have equal opportunity to participate in what Kaipātiki has to offer. This includes attending events, participating in recreational pursuits and other services where we can connect to one another.

Kaipātiki Community Facilities Trust

Kaipātiki has a strong community-led ethos. KCFT support the **Kaipātiki Local Boards** Plan to increase and deliver a wide variety of programmes and community activities that cover interests across art, culture, sport, and recreation. Including meeting the needs of our diverse communities in Kaipātiki.

- To continue to take the strong lead in recognition of the Partnering Agreement that is between Kaipātiki Local Board and Kaipātiki Community Facilities Trust. Acknowledging that Kaipātiki has a strong community-led ethos.
- KCFT send a strong voice, to the **Kaipātiki Local Board** to continue to lead the community development practise, which is Kaipātiki, and continue to ensure that the ` Kaipātiki Way ` continues within the Thriving Community approach, so we encourage the **Kaipātiki Local Board** to continue the community development model we all work and thrive here in Kaipātiki.
- As our diverse community settles into Kaipātiki they also must be included within the `` Kaipātiki Way ` community development model toolbox that nurtures and celebrates all Kaipātiki. Our newest residents have brought with them their own sets of beliefs and identities and we believe that we should all, nurture and celebrate these by supporting a range of ethnic and cultural events and activities.
- We support the **Kaipātiki Local Board** in their vision, to provide the platform for everyone to have equal opportunity to participate in what Kaipātiki has to offer. This includes attending events, participating in recreational pursuits and other services where we can connect to one another.

- The support of the Kaipātiki Local Board - Our children and young people have a voice, is supported and KCFT will be achieved through the partnering model, and I am Auckland, ensuring that all expectations will emerge through the years ahead.

Te Taiao Environment

Our natural environment is restored and protected for future generations to enjoy.

Kaipātiki Local Board has an important role to play in leading and supporting Auckland's response to the climate emergency, including supporting regional climate plan initiatives, and integrating climate awareness into all decisions, including community investment. Kaipātiki Local Board adopted the Kaipātiki Climate Action Plan in March 2023. This action plan sets out how Kaipātiki can transition into a resilient, zero carbon community which is actively adapting to the impacts of climate change.

Kaipātiki Community Facilities Trust

- KCFT congratulate Kaipātiki Local Board for their recognition of the work that has been achieved by Kaipātiki Project and Pest Free Kaipātiki, by showing the financial support needed to meet the expectations of their programmes and increase potential partnering models. For these two organisations represent the environmental challenges ahead, and by working in a community development model this work will without a doubt increase the neighbourhoods' knowledge and learning around their own environment and compliments the KCFT's role in community development in these wider communities of Kaipātiki.

Ngā Wāhi me ngā Takiwā Places and spaces

Our built environment is vibrant, well-maintained, reflects the culture and heritage of Kaipātiki, meets our people's needs, and has a low impact on our climate.

Kaipātiki Community Facilities Trust

- We support the **Kaipātiki Local Board** in investing in nature-based solutions that improve our parks and public spaces including planting specimen trees for shade, reduced mowing for increased biodiversity and natural play items such as māra hūpara.

Whakaotinga whā: Te Ikiiki me ngā Tūhononga

Transport and connections

Our people have many transport options and can easily and safely move around and find their way.

Kaipātiki Community Facilities Trust

We support the **Kaipātiki Local Board** –

People have more travel choices to get to work, school or go about their daily lives.

- Continue the Community Connections Plan
- Supporting the **Kaipātiki Local Board** to work with work with Auckland Transport to identify maintenance priorities for our roading intersections, footpaths, pedestrian crossings, and walkways so that they provide a safe environment for our vulnerable road users – pedestrians, cyclists and bus users of all ages and abilities.

Continue working with Auckland Transport on the identified schools safe to school programme of work.

- Support - Work with Auckland Transport and our school communities on developing better travel options to and from our schools. Congratulations to the **Kaipātiki Local Board and Auckland Transport**, on the installation being currently constructed at Birkdale Primary, from the temporary to the final design these additional safety measure will without a doubt, provide for the school community a safer area for them and the students as well as the daily traffic that moves through that road corridor.

Te Āheinga me te Taurikura

Opportunity and prosperity

Our people are prosperous, and our businesses continue to flourish. We want to see more people working close to where they live and play. We also want to see our local community supporting our local businesses. Although we have a limited role in this space, we will do what we can by working with our town centre associations, and businesses in the Wairau Valley. Beach Haven village and our smaller local precincts such as Bayview, Birkdale, Hillcrest, Sunnybrae and Tōtara Vale are also important to us, providing employment and contributing to the local economy.

Kaipātiki Community Facilities Trust

- Support the **Kaipātiki Local Board** to continue the work around the Wairau Valley and other business areas provide our people with the opportunity to work locally. This will allow the KCFT's Employment Programme for Rangatahi a wider lens on business that may afford a pathway for our rangatahi to gain employment and a new selection of business opportunities for the programme.
- KCFT support the **Kaipātiki Local Board** in its direction to the Northcote and Birkenhead Business Improvement Districts (BIDs) to encourage social procurement and local employment and retail opportunities that contribute to a successful economy in Kaipātiki. KCFT has a long history of community led development support to the Northcote Redevelopment Plan, through many programmes and projects, and we know, that continued support through the **Kaipātiki Local Board** will ensure Northcote will become the thriving community hub of the future.

Jill Nerheny QSM
Manager
Kaipātiki Community Facilities Trust



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Disabled Persons Assembly NZ

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



Disabled Persons Assembly NZ

August 2023

To All Auckland Council Local Boards,

Please find attached DPA's submission on Draft Local Board Plans for all 21 boards

For any further inquiries, please contact:

Chris Ford

Kaituhotuho Kaupapa Here ā Rohe - Regional Policy Advisor (Local Government)

policy@dpa.org.nz

Introducing Disabled Persons Assembly NZ

We work on systemic change for the equity of disabled people

Disabled Persons Assembly NZ (DPA) is a not-for-profit pan-impairment Disabled People's Organisation run by and for disabled people.

We recognise:

- Māori as Tangata Whenua and [Te Tiriti o Waitangi](#) as the founding document of Aotearoa New Zealand;
- disabled people as experts on their own lives;
- the [Social Model of Disability](#) as the guiding principle for interpreting disability and impairment;
- the [United Nations Convention on the Rights of Persons with Disabilities](#) as the basis for disabled people's relationship with the State;
- the [New Zealand Disability Strategy](#) as Government agencies' guide on disability issues; and
- the [Enabling Good Lives Principles](#), [Whāia Te Ao Mārama: Māori Disability Action Plan](#), and [Faiva Ora: National Pasifika Disability Disability Plan](#) as avenues to disabled people gaining greater choice and control over their lives and supports.

We drive systemic change through:

- **Leadership:** reflecting the collective voice of disabled people, locally, nationally and internationally.
- **Information and advice:** informing and advising on policies impacting on the lives of disabled people.
- **Advocacy:** supporting disabled people to have a voice, including a collective voice, in society.
- **Monitoring:** monitoring and giving feedback on existing laws, policies and practices about and relevant to disabled people.

UN Convention on the Rights of Persons with Disabilities

DPA was influential in creating the United Nations Convention on the Rights of Persons with Disabilities (UNCRPD),¹ a foundational document for disabled people which New Zealand has signed and ratified, confirming that disabled people must have the same human rights as everyone else. All state bodies in New Zealand, including local and regional government, have a responsibility to uphold the principles and articles of this convention. There are a number of UNCRPD articles particularly relevant to this submission, including:

- **Article 3 – General principles**
- **Article 9 – Accessibility**
- **Article 19 – Living independently and being included in the community**
- **Article 27 – Work and employment**
- **Article 28 – Adequate standard of living and social protection**
- **Article 29 – Participation in political and public life**
- **Article 30 – Participation in cultural life, recreation, leisure and sport**

New Zealand Disability Strategy 2016-2026

Since ratifying the UNCRPD, the New Zealand Government has established a Disability Strategy² to guide the work of government agencies on disability issues. The vision is that New Zealand be a non-disabling society, where disabled people have equal opportunity to achieve their goals and aspirations, and that all of New Zealand works together to make this happen. It identifies eight outcome areas contributing to achieving this vision. There are a number of Strategy outcomes particularly relevant to this submission, including:

- **Outcome 5 – Accessibility**

The Submission

1.) Introduction and Guiding Principles

DPA welcomes this opportunity to make a generic submission covering all local board areas to raise general principles/themes that every Auckland Council board should take into consideration when considering their local plans from a disability perspective.

This includes that there should be specific references to the needs, issues and aspirations of disabled people in each local area within all plans. The disabled communities of Auckland need to be more visible in these plans as there are few references to disabled people's concerns and issues within many of them.

DPA believes that this points out the need for more consultation specifically targeted to the disabled communities in each board area.

Ultimately, this means that disabled people should be recognised as a specific stakeholder group who should be actively consulted with and have our needs fully referenced within all plans.

Recommendation 1: that all local boards are required to specifically consult with disabled people as a distinct stakeholder group as part of all local board plan consultation processes.

Recommendation 2: that all local boards specifically reference the needs, views and priorities of disabled people (alongside those of other communities) within all local board plan documents.

Significantly, there appears to be either minimal or no data on the number of disabled people in each board area. While DPA notes that the last full Statistics New Zealand Disability Survey figures date from 2013 and that a new survey is about to be undertaken, Auckland Council should collaborate with Statistics New Zealand and other official data stakeholders to estimate the number of disabled people in the region for planning purposes.

Recommendation 3: that Auckland Council collaborate with Statistics New Zealand and other official data stakeholders to estimate the number of disabled people in each board area and across the region for planning purposes.

The fact that disabled people are not specifically targeted or fully included in all board plans has led to the absence of each local plan not being available in accessible formats including New Zealand Sign Language, Braille, Easy Read, Large Print and audio on the Council's website.

Recommendation 4: that Auckland Council ensures all local plans are published and available in accessible formats including New Zealand Sign Language, Braille, Easy Read, Large Print and audio.

Noting all the above factors, this submission focuses on key areas of concern for the disabled community that have been touched upon in our past submissions to the Auckland Council.

Our submission will focus on the need for local boards to plan and/or champion the following principles from a disability perspective. These include the need for:

- Accessible and inclusive communities
- Environmentally and climatically sustainable communities
- Communities that provide economic and social opportunities

2.) Brief background – important disability statistics

One in four New Zealanders have a disability or long-term health condition. Māori and Pacific peoples have an even higher-than-average rate of disability. There were approximately 271,000 disabled people residing in Auckland in 2013¹ (the latest data available), a number which rises to over 300,000 when accounting for current population increases.

¹ Statistics New Zealand. (2013). New Zealand Disability Survey. Retrieved from http://archive.stats.govt.nz/browse_for_stats/health/disabilities/DisabilitySurvey_HOTP2013.aspx#gsc.tab=05

Disabled people in Auckland face an increased risk of living in poverty largely due to lower incomes and extra resource requirements compared to non-disabled people.² Statistics also show that disabled New Zealanders are grossly overrepresented in unemployment rates; two-thirds of disabled people earn less than \$30,000 per annum, increasing the likelihood of poor socio-economic outcomes.³

One of the top issues for disabled people across Tāmaki Makaurau is that of housing.

Disabled people as a group are disproportionately impacted by homelessness. This is due to disabled people being overrepresented amongst low-income earners while also facing higher living costs due to disability. For this reason, disabled people are more likely to be renters; 2018 data from Statistics New Zealand shows that 47% of disabled people rent compared to 35% of non-disabled people.⁴

Disabled people are also at greater risk of homelessness due to the lack of security around ongoing tenancy and this is exacerbated by the low number of accessible houses not only in Auckland but throughout the country.

Another area of ongoing concern for disabled people is access to transport.

Earlier this year we submitted on the Auckland Council's Future Transport Projects plan. In that submission, we outlined how disabled people are highly reliant on public transport given the significant proportion of disabled people who don't drive.

² Wilkinson-Meyers et al. (2015). To live an ordinary life: resource needs and additional costs for people with a physical impairment. *Disability & Society*, 30(7), 976-990.

³ King, P. T. (2019). Māori with Lived Experience of Disability Part II. Commissioned by the Waitangi Tribunal. Retrieved from https://forms.justice.govt.nz/search/Documents/WT/wt_DOC_150473583/Wai%202575%2C%20B023.pdf

⁴ Statistics New Zealand. (2018). The disability gap 2018. Retrieved from <https://www.stats.govt.nz/infographics/the-disability-gap-2018/#:~:text=Home%20life-.Of%20disabled%20New%20Zealanders%20aged%2015%E2%80%93364%20years%20in%202018,p ercent%20of%20non%20disabled%20people.>

Nevertheless, provision needs to be made to accommodate the transport needs of all disabled people and this includes for those who transport themselves via different modes including driving, bussing, walking, cycling, using taxis, ferries and trains.

In 2022, Waka Kotahi published independently commissioned research (in which DPA collaborated) entitled *Transport experiences of disabled people in Aotearoa New Zealand*.⁵

This research illustrated the ongoing accessibility challenges faced by disabled people when using public transport. Disabled people's main challenges included, for example, issues around using Total Mobility (TM), the inaccessibility of bus services, lack of footpaths and safe crossing points, and feeling excluded from the planning of sustainable city centres, as well as the disablist attitudes of some transport planners.

Disabled people also have ongoing issues with accessing train and ferry services in Auckland and the need for improved transport accessibility and affordability.

3.) Accessible and inclusive communities

Disabled people want to live in fully accessible communities which are inclusive of everyone.

DPA believes that disabled people have the right to access all facets of the communities they live in including public buildings, transport networks, schools, housing, playgrounds, parks, community facilities/amenities, and footpaths on an equitable basis with non-disabled people.

This means that disabled people should have the right to go everywhere and anywhere within the local communities in which they live without barriers being placed in the way of their full participation.

That is why DPA stresses the need for Auckland local boards to both champion and build community facilities and infrastructure (or those over which boards have direct responsibility for) to Universal Design Standards (to at least level 5). We also made

⁵ Doran, B., Crossland, K., Brown, P., & Stafford, L. (2022). Transport experiences of disabled people in Aotearoa New Zealand (Waka Kotahi NZ Transport Agency research report 690). Retrieved from <https://www.nzta.govt.nz/resources/research/reports/690>

this call in our recent submission to Auckland Council's Future Development Strategy.

Recommendation 5: that all local boards fully champion and build community facilities and infrastructure (which they have direct control over) to Universal Design Standards (at least to level 5) to ensure accessibility.

All communities in Auckland should be fully accessible for every disabled person to live in too, and this includes people with mobility impairments, blind and low vision people, neurodiverse people (i.e., Autistic people), people with psychosocial disabilities/mental distress and Deaf/deaf communities.

This can be achieved, for example, through having accessible wayfinding signage in public spaces/places, quiet spaces where people can retreat to in busy areas (which are suitable for neurodiverse people and people experiencing mental distress), accessible bus stops, mobility parking and vehicle drop off areas, appropriate seating where people can sit and rest as well as good lighting and security features.

Building infrastructure to UD standards – including, most importantly, housing - should support the building of accessible, inclusive communities.

On housing, local boards should act as an advocate and champion to both central and local government - as well as to private and non-government sector stakeholders - for more accessible housing, especially that built to UD standards which can accommodate the needs of all people who may live in any dwelling across its lifetime, including disabled people.

Accessible housing should be available in all board areas within Auckland and at affordable prices for both rentals and buying.

Recommendation 6: that all local boards champion and advocate for the building of more accessible Universal Design housing in their areas.

Associated with the need for more accessible housing and community infrastructure is the requirement for fully accessible and affordable public transport connections in all areas.

All of Auckland Council's local boards can act as advocates, champions and partners with disabled people in both calling for and working together with Auckland Council and Auckland Transport on transport solutions that will give greater accessibility and choice to disabled Aucklanders.

These solutions include support for the development of Mass Rapid Transit (MRT) systems which are supported by networks of accessible bus, ferry and rail routes which are, in turn, serviced by fully accessible trains, buses and ferries. They should centre around the availability of accessible supporting infrastructure in the form of bus interchanges and stops, ferry terminals and railway stations.

Recommendation 7: that all local boards advocate and champion the extension of accessible transport networks for all Aucklanders, including disabled people.

Local boards should also support calls to make public transport more affordable, including for disabled people, by ensuring that the Total Mobility (TM) Scheme (that provides a 75 percent discount on all taxi and mobility service transport fares for those eligible) remains a key component of the transport system as it gives disabled people the opportunity to fully participate in their communities.

DPA recommends that local boards should advocate to Auckland Council and Auckland Transport to annually adjust the maximum fare cap to keep pace with inflation, so that disabled people can continue accessing journeys throughout the Auckland region.

Recommendation 8: that all local boards advocate for the extension of affordable public transport to all Aucklanders, especially to transport disadvantaged communities which include disabled people.

All local boards should be mindful that the need for fully accessible, inclusive communities must be underpinned by the undertaking of extensive barrier free

accessibility audits in areas where infrastructural improvements are being mooted as part of any local board plans.

Recommendation 9: that local boards either undertake or support the undertaking of barrier free audits in areas where infrastructure improvements are planned.

4.) Environmentally and climatically sustainable communities

The disabled community are already being severely hit by the impacts of climate change and are expected to be one of the hardest hit population groups going forward, locally, nationally and internationally.⁶

DPA found that after reaching out to our members in Auckland following the recent weather events that while many disabled people were resilient and had withstood the floods well, others had not as we heard stories about the impact that weather-enforced homelessness had on disabled people, especially given the lack of accessible housing.

DPA calls upon local boards to ensure that disabled people in every ward area can have their voices heard on all matters pertaining to planning for climate change, especially when developing mitigation strategies.

When it comes to planning climate change mitigation strategies, including the need to develop more 20-minute communities and managed retreat plans, disabled people need to be heard as, for example, planning for the potential red zoning of certain areas from being built on will have considerable ramifications for disabled people going forward including the need to potentially re-locate to less flood prone areas.

Nevertheless, DPA supports the need for red-zoning and building bans in areas where flooding, inundation and sea level rise risks will make it impossible to live in an increasingly warmer world.

⁶ Schulte, C. (2020, March 28). People With Disabilities Needed in Fight Against Climate Change. Retrieved from <https://www.hrw.org/news/2020/05/28/people-disabilities-needed-fight-against-climate-change#:~:text=People%20with%20disabilities%20are%20at,said%20in%20a%20recent%20report.>

All area board plans should specifically note the implications of climate change on marginalised communities, and this includes disabled people and that the voices of everyone, including Māori, Pasifika, disabled people, older people and others need to be clearly elevated and heard when it comes to climate change and environmental sustainability projects.

Recommendation 10: that local boards include disabled people as a key stakeholder group in planning for climate change mitigation and environmental management projects in their areas.

DPA believes that everyone, including disabled people, should have the right to access the increasing number of green spaces which are being proposed all over the city, including within many local board areas.

It is important that the voices of the disabled community are heard when planning these projects as it can make all the difference for walkways, trails and other green spaces to be fully accessible and inclusive, including for disabled people.

DPA asks that all local boards be required to involve disabled people and disability organisations in planning for any new green spaces within local board areas.

Recommendation 11: that local boards be required to involve disabled people and disability organisations in the planning of new green spaces in their board areas.

5.) Communities that provide economic and social opportunities

All local boards should champion, support and facilitate economic and social opportunities for all communities, including disabled people.

Earlier in the submission we pointed out the lower socioeconomic status of most of the disabled community.

While disabled people live in all areas of Auckland and in all communities across the socioeconomic spectrum, there is a higher proportion of disabled people living in lower socioeconomic areas compared to higher socioeconomic areas.

Nevertheless, all local boards in the dispensing of community grants should prioritise the needs of disabled people and community flax roots disability organisations. We would like to see local boards fund more community organisations and initiatives which are run both by and for disabled people on a self-determining basis.

Recommendation 12: that all local boards prioritise criteria for community grants to include funding the needs of disabled people and disability organisations, including those organisations and initiatives run by and for disabled people.

We would also like to see ongoing support given by local boards to initiatives which create employment and training opportunities, especially in the lowest income board areas, and that support be given to programmes and initiatives which are aimed at economically marginalised communities including disabled people, Māori, Pasifika, ethnic communities and rangatahi/youth, amongst others.

Recommendation 13: that local boards, especially in the lowest income wards, support employment and training opportunities aimed at economically marginalised communities, including disabled people.

Lastly, we ask that all local boards play a role in monitoring the impacts of the recent budget cuts imposed by Auckland Council.

We made an extensive submission to Auckland Council on their budget proposals earlier this year and we opposed all cuts to council services and activities. Our submission noted the impact of proposals like cuts to library services and community funding on the ability of disabled people to fully participate in the community.

DPA acknowledges that Council took some of the proposed cuts off the table, but others remain and are currently being implemented.

For this reason, DPA asks that local boards not only monitor the impacts of cuts on residents but also acts to collectively speak out about any impacts on communities.

Recommendation 14: that local boards continue to monitor the impact of cuts decided upon by Auckland Council and speak out on their impacts to communities to Council.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Forest & Bird

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



Forest & Bird

TE REO O TE TAIAO | *Giving Nature a Voice*
Royal Forest and Bird Protection
Society of New Zealand Inc.

34A Charlotte Street, Eden Terrace
Auckland 1021
www.forestandbird.org.nz

Submission on Kaipātiki Local Board Plan.

Wednesday, 02 August 2023

To: Kaipātiki Local Board
Auckland Council
Private Bag 92300
Auckland 1142

➤ Emailed to: kaipatikilocalboard@aucklandcouncil.govt.nz

1. Submitter details

Royal Forest and Bird protection Society of New Zealand Inc. (Forest & Bird)
North Shore Branch

Contacts:

Lynley Hunter (North Shore Branch Working Group Chair),
ljhunter@kinect.co.nz, 021 08197845

Nicholas Mayne (North Shore Branch Working Group Member),
nicholas.mayne.nz@gmail.com, 022 0662963

Carl Morgan (Regional Conservation Manager - Tāmaki Makaurau Auckland),
c.morgan@forestandbird.org.nz, 027 250 9777

2. Trade competition declaration

Forest & Bird would not gain an advantage in trade competition through this submission.

3. Hearing options

We wish to be heard in support of this submission.

Forest and Bird Shore North Shore, North Auckland Local Board Plans Submission.

Kereru don't fly in straight lines!

(A modern Whakataukī by Charmaine Bailie)

When doing restoration planting projects, it is usually seen as important to create consistent spacing between plants; so that each plant has room to grow, gaps don't appear as the canopy develops that allow weeds to grow, and for all this to happen for the least possible cost. In some places around Te Whenua Roa o Kahu, the North Shore of Auckland, this objective has created perverse outcomes. Places where native tree species, of similar age and a narrow collection of species, are lined in rows and columns, like the grid of an ancient Roman city. But when Char taught me to plan community restoration planting events, she pointed out that nature does not work like this. Like the Kereru with a full belly, it dodges and weaves, swoops and dives, around natural features, dispersing a diverse range of native seeds across the whenua. This abundance of diversity dispersed in chaotic fashion is not the redundancy it first appears. Each species has its own ecological niche and helps provide a robust ecosystem community that is resilient to changes in climate. Changes in conditions may favour one species over another, but within the shelter of the forest community all species thrive. It does not respect fences or property boundaries made by people; but rather it goes wherever it chooses. With some species following the path of water, as it travels from the mountain to the sea; and others etching out a living on the harsh clay soils of ridge lines. In doing so it deposits biodiversity in all the places which favour their own prosperity. The wisdom of nature's method, over the attempts people make, is seen in the success of natural ecosystems over millions of years of change and adversity. And so, as we placed plants for people to put into the ground on community planting days, we were encouraged to think like a kereru, by not planting things in straight lines according to our own convenience. Rather we were to scatter a diverse palate of plants across the planting site, being mindful of the landscape and species selection, and adapting where necessary, but mostly trying to mix plants up in uneven spacing in an attempt to imitate the success of nature.

(Nicholas Mayne)

Introduction

Forest & Bird is New Zealand's largest and longest-serving independent conservation organisation, with over 100,000 members and supporters. Its mission is to be a voice for nature, on land, in fresh water, and at sea, on behalf of its members and supporters. Volunteers in 50 branches throughout New Zealand carry out community conservation projects and advocate for nature. Forest & Bird has nine branches and three major projects in the Auckland region alone.

In support of the society's objectives, it has been involved in resource management processes at the national, regional, and district level. It routinely submits on regional and district plan provisions, some resource consents and advocates in the Environment Court in relation to biodiversity, landscape and natural character. It is particularly committed to ensuring that the environmental bottom lines established in national policy statements in order to give substance to the "protective" element of Part 2 of the RMA are given effect to in regional and district plans and achieved through decisions on resource consents.

The North Shore Branch carries out a range of activities, including but not limited to weed control, pest control, restorative planting, education events, content creation and environmental advocacy. The branch covers the North Shore area from the East Coast Bays, south to Devonport, west to Greenhithe, and north to the Weiti River including Okura Bush. Everyone involved in the branch is a volunteer, including the committee, trappers, and supporters.

Approach

We began the process of making this submission by reviewing the Local Board Plans of all four Local Boards in the rohe of our branch, with the intention of making a submission on each plan. However, the more we worked on this the more we realised the similarities between the Local Board plans, and the issues we wanted all Local Boards to address, were more significant than the local issues that we could comment on. These similarities warrant a more cooperative approach between the four Local Boards. For example, both Kaipatiki Local Board and Hibiscus and Bays Local Boards’ plans both propose undertaking work on dog walking rules. Our branch supports this proposal as it could allow for better protection of ground nesting birds, particularly along the foreshore. However, undertaking this in just these two Local Boards would not be good value for money (in terms of development and consultation costs) or achieve rules that are readily understood and applied to all vulnerable habitats across our rohe. Furthermore, we recognise the influence of Local Board’s extends beyond the budgets and work programs they have direct decision making around. Local Board views must be considered in any decision that is made by the Governing Body of Auckland Council, or by a council department or CCO under the delegation of the Governing Body of Auckland Council (section 15.2.c Local Government (Auckland Council) Act 2009). Coordinating advocacy and feedback between the four Local Boards will have a greater impact on the decision making of the rest of Auckland Council. For this reason, we have decided to fly like the kereru, ignoring man-made boundaries, and submit a unified submission for each of the four Local Board plans in our rohe that focuses on achieving a strong common voice for nature.

We are submitting our submission as Objectives, Key Initiatives, and Advocacy items; just as they may appear in a Local Board Plan; along with commentary on some items. We would very much have liked to write a longer commentary on each of the suggestions detailing why we either support or propose them. Unfortunately, time and resources do not permit us to do this as we wanted to provide this submission to our members as early as possible. But we would like to request the opportunity to make an oral submission on the plan, with the Local Board, where we can answer any questions you have around our views.

Local Board Plan submission in structural form

Environment: Nature thrives across our entire rohe.

Objectives	Key Initiatives
<p>Access to indigenous and culturally valued biodiversity is managed to promote enjoyment while protecting vulnerable ecosystems.</p>	<ul style="list-style-type: none"> ● Review and promote the provision of paths and tracks to give people access to forests in local reserves that promote ecological hygiene and conservation of vulnerable habitats. ● Identifying high value ecological areas close to urban populations, such as the shell banks of Oneoneroa and Te Onekiritea, and manage access to prevent damage. ● Work with neighbouring Local Boards to review the Auckland Policy on Dogs, keeping dogs out of high value conservation areas while providing space for

	<p>responsible pet ownership. Dogs off leash are causing significant damage in many reserves and streams.</p> <ul style="list-style-type: none"> ● Preserve and restore the area's horticultural history by planting and maintaining heritage variety plants in parks with public access; including pā harakeke, rongoā gardens, and fruit tree libraries. ● Continue to implement actions from the Urban Ngahere (forest) Strategy
Continue to partner with the community to enhance the biodiversity of local parks and reserves.	<ul style="list-style-type: none"> ● Continue to support community restoration, environmental groups and environmental education initiatives that protect and enhance our natural assets
The lifeforce (mauri) of our harbour and waterways is respected and restored	<ul style="list-style-type: none"> ● Support schools, businesses, environmental groups and community volunteers to carry out stream restoration projects including pollution reduction, stream clean-ups, habitat improvement and native riparian planting on public and private land. ● Prioritise ecological maintenance projects that protect and enhance identified potential inanga spawning sites through bank stabilisation, pest plant control, and pest animal control.
Our communities practise te ao Māori kaitiakitanga (guardianship) principles	<ul style="list-style-type: none"> ● Partner with mana whenua, matawaka and community groups to restore te taiao (the environment), prepare for the effects of climate change and deliver initiatives that increase understanding of matauranga Māori (Māori knowledge)
We will adopt landscape approaches to managing our environment that recognise the connections between different local habitats, such as the North West Wildlink.	<ul style="list-style-type: none"> ● Support initiatives that deliver on current ecological strategies. ● Identify and prioritise restoration planting opportunities, for community and council projects, that create ecological connections by retiring hard to maintain areas within existing parks and reserves.

Advocacy:

- Advocate to the Governing Body that the Local Board and local volunteer groups are more directly involved in the setting and delivery of council-controlled ecological services
- Advocate to the governing body for continued use of the water quality targeted rate, and other funding, to strengthen coastal and marine protection around sedimentation into receiving environments particularly the Hauraki Gulf.
- Advocate for funding and regulation to enhance water retention and reduce stormwater surges in local streams through the use of retention tanks, rain gardens, wetland restoration, tree planting, reduced impermeable surfaces, and the daylighting of streams.
- Advocate for the development of Blue Green Spatial Network plans, especially in future urban zones (like Whenuapai and Dairy Flat) that support the preservation and

enhancement of natural assets (such as streams and wetlands) that will create greater resilience to flooding and improve water quality.

- Advocate for priority development of shoreline adaptation plans including funding to implement actions to ensure a sustainable approach to the management of our shorelines.
- Advocate for strong tree protection (in particular of mature native trees) to support the goals of Urban Ngahere action plans and strategies and maintained canopy coverage
- Advocate for reserve acquisition and planning rules that promote the development of new local habitats that improve ecological connectivity.

Our Communities and Places: Our communities live well alongside nature as climate changes

Objectives	Key Initiatives
We adopt best practices for all council maintenance and projects to maximise the net benefit to community and nature.	<ul style="list-style-type: none"> ● Improve climate impact statements in reports to consider net emissions, biodiversity loss, effect on impermeable surfaces and stormwater, and scarce resource use and recovery.
Our communities are resilient to climate change and care for their surrounding environment	<ul style="list-style-type: none"> ● Support initiatives that build community networks and connections which help communities respond to emergencies and the impact of climate change ● Support community led initiatives and projects that improve wellbeing by promoting a sustainable lifestyle, including waste minimisation and emissions reductions.
Our aquatic, recreational, art, and community facilities are fit-for-service, meet the needs of our growing and changing population, and are resilient to climate change	<ul style="list-style-type: none"> ● Ensure our playing surfaces, sports fields, aquatic, recreational facilities, and community facilities meet increasing social and recreational needs, are accessible, well maintained, available to the public, sustainable, resilient to climate change, and fit for purpose.

Advocacy

- Advocate to the governing body for appropriate long-term planning and associated funding for infrastructure in growth areas that support Auckland climate goals.
- Advocate to the governing body for immediate long term risk scenario planning for the next 50 to 100 years to prepare for and respond to climate changes.
- Advocate for Te Onekiritea / Bomb Point to be vested as a Reserve either in Council or Government ownership.
- Advocate for adequate level of renewals funding to ensure existing assets are well maintained and are adapted to be resilient to climate change.
- Advocate to the governing body for more user-friendly fault reporting mechanisms for assets and infrastructure that have real time responses when the fault is corrected.
- Advocate to Watercare for the use of non-potable water, from Rosedale Treatment Plant, for irrigation of sports and recreation facilities.

- Advocate to the governing body, Auckland transport and Watercare to ensure Aucklanders have a greater understanding about the policies for maintaining and monitoring infrastructure and the importance of reporting faults.

Our Places and Our Economy: Our economy meets the needs of our communities with minimal impact on nature.

Objectives	Key Initiatives
Getting around without a car is attractive because of the seamless and safe network of paths and public transport	<ul style="list-style-type: none"> ● Continue to renew and enhance the path networks to create more direct routes shorter distances and well connected networks for walking and cycling ● Support active modes of transport to be as connected and well maintained as our roading network, through improvements to connectivity and dedicated infrastructure - such as improved path surfaces signage or seating ● Deliver and maintain more healthy street trees and rain gardens to beautify walking areas, while achieving Urban Ngahere plans and strategies, climate emissions mitigation, providing shade, and better stormwater management.
People embrace and enjoy living working playing and travelling locally	<ul style="list-style-type: none"> ● Encourage local business associations to continue to support business development and to continue to meet the changing needs of our residents ● Support an enhanced sense of community through safer, vibrant and connected town centres and neighbourhoods
Our economy grows through sustainable practices by embracing the circular economy.	<ul style="list-style-type: none"> ● Support projects that engage, advise, educate, and assist our community on how to minimise and build the circular economy ● Support the work and expansion of the network of community recycling centres for the benefit of our community

Advocacy

- Advocate to the Governing Body and central government to assist our local economy to become more sustainable by adapting circular economy practices and increase resilience to climate impacts.
- Advocate to the governing body to fund the planned expansion of community recycling centres that will benefit our communities.
- Advocate to the Governing Body for appropriate funding for compliance and education to ensure that minimising construction and demolition waste is a priority for all projects. Noting a local initiative for construction waste compliance education being successfully

piloted through local board funding and we advocate that this be an outcome invested in at a regional level.

- Advocate to the Governing Body for the retention of the Local Board transport capital fund
- Advocate for Auckland Transport to take a more proactive approach by taking the lead in working with new commercial developments that are trip generators, such as supermarkets, to include provision for safe public transport infrastructure, such as bus stops, even on streets with a higher volume of users.
- Advocate to Auckland Transport and Waka Kotahi (New Zealand Transport Agency) for increased funding for walkways and cycleways, and their complementary infrastructure, to ensure a safe, fully connected active transport network, that is integrated with public transport.
- Advocate to Auckland Transport to better accommodate the integration of public transport vehicles, major bus stops and park and rides, with micro mobility and bicycles, to enable people to use these modes to connect with their public transport journey.
- Advocate to the Auckland Council for changes to the procurement policy to allow for greater flexibility to ensure increased use of local businesses, and more consideration of local boards views in decision making over major sub regional contracts – such as the full facilities contracts.
- Advocate to the Governing Body, Auckland Transport and Waka Kotahi (New Zealand Transport Authority) to fund all the elements of public transport networks, including free or low-cost park and rides, efficient and dependable Rapid and Frequent Transport Networks, and dedicated or priority public transport lanes or routes, to keep up with population growth.

Selective Commentary

Environment: Nature thrives across our entire rohe.

While Te Whenua Roa o Kahu is a highly urbanised area it still retains many areas of highly valued ecological habitats. On our East Coast is the Hauraki Gulf, meeting the Waitemata Harbour in the south which stretches along the western boundary of our rohe right up into the northern reaches of Albany. The western half of the rohe contains the largest concentration of urban forest in Auckland including Biodiversity hubs in Albany Heights, Hellyers Creek, and South Kaipātiki, as identified in the North West Wildlink Prioritisation Report (Boffa Miskell 2017). These natural spaces are under strain from their proximity to urban populations, with threats like pollution and sediment, pest species, domestic animals, pathogens, and even just the impacts of human footfall impacting the prosperity of the species that live there. Managing access to these spaces through both positive (development and promotion of hygienic track and trails) and negative (regulation and bylaws) methods is required to retain and enhance the health of these areas.

Connecting these highly valued ecological habitats is an important part of enhancing the biodiversity within them. Most native and indigenous biodiversity is not adapted to crossing sparse urban areas; with many forest birds only willing to fly a couple of hundred metres between forest fragments. As the city continues to intensify, with a corresponding loss of private open space, council needs to invest in developing and maintaining ecological corridors to connect biodiversity hotspots. This involves both the development of existing reserves to host biodiversity (daylighting streams, planting, etc) and acquiring new protected ecological spaces through acquisition and vesting of reserves and planning rules. Maintaining these biodiversity hotspots and ecological corridors requires a landscape approach to council controlled ecological maintenance. This means prioritising investment in ecological and park maintenance in order to manage pest plant and animal populations in specific areas rather than regional elimination of selective pests.

Often there is a correlation between ecological corridors and urban waterways. As such, investment in terrestrial ecology along streams and wetlands has a positive impact on managing stormwater while enhancing freshwater ecology and the health of the Waitematā and Hauraki Gulf. Blue-Green Spatial network plans are an important planning tool for identifying where open space needs to be retained or reclaimed from development in order to manage stormwater, to minimise sediment and pollution entering the harbour and to mitigate the risk of flooding. While not within our rohe, these are particularly important for areas like Whenuapai and Dairy Flat to prevent future damage to the Waitematā from the development of these areas. Within currently urbanised areas opportunities to reclaim open space in flood prone areas should be a priority, alongside reducing the impact of stormwater surge by supporting the use of water retention devices and planting in new development, and retrofitting older developments.

Highly valued ecological habitats are not the only places where biodiversity is valued. Tree cover across the entire urbanised area provides benefits for people and for native fauna; regardless of whether it is native or not. Many of our native plant species have evolved for forest ecosystems and won't grow as well in urban gardens with depleted topsoils. Many introduced or cultivated plants have been selected for horticulture or gardens and are better adapted to urban areas, without becoming a pest plant. Some plants; such as pa harakeke, rongoa gardens, and heritage fruit trees; have special cultural relevance to places that were once horticultural hubs prior to modern development. Including culturally valued plants within efforts to landscape public spaces and increase urban tree cover can add to the attraction of a place and reduce the demand for access to more sensitive ecological habitats, while still supporting native fauna.

Our Communities and Places: Our communities live well alongside nature as climate changes

Within many Local Board Plans, goals for sustainable lifestyles and climate change adaptation are included within the environment outcome. We suspect this has to do with a focus on communities of political interest rather than material outcomes within places and communities. We would advocate that these objectives are better placed in the outcome where they will be acted on more directly. This is of particular importance when considering that the end users of Local Board Plans are not residents and political interests, but rather Council Departments and CCOs with well defined responsibilities that are commonly perceived to relate to one or two outcomes rather than all outcomes holistically. We don't want climate change and sustainability to just be an issue considered by those parts of the council concerned with environment and infrastructure. Rather within this outcome we want it to be at the forefront of mind for those parts of council concerned with community development and resilience, and the provision of sports and recreation facilities. While these actions can have a positive impact on nature and areas of biodiversity, their primary benefit will be in the lives of people and communities.

Our Places and Our Economy: Our economy meets the needs of our communities with minimal impact on nature.

Our communities spend more time involved in economic activity than any other single activity. Economics is not simply the domain of business owners and managers; it is also the interest of workers, customers, and nature. Nature cannot be seen as a limitless resource of materials for economic systems and the mythical "away" to which we throw the waste of economic activity. The resources that fuel our economic activity are scarce and if we waste them for short term profits then we will continue to destroy our natural heritage and rob future generations of economic opportunity.

Adopting a circular economy; where the waste of one economic activity is recovered to fuel another economic activity, is the most responsible form of economic development. Investing in waste recovery, through incentives like community recycling centres, creates employment and new sources of raw materials for local businesses. Community initiatives like repair cafes improve the wellbeing of residents by extending the useful lifespan of the goods they already own.

Transport is another economic activity that requires reform to become more efficient. Not only can Auckland not financially afford to continue to address congestion by investing more in roads for private vehicles; but doing so deprives the city of many other benefits from public and active transport. Public and active transport are both more efficient uses of scarce resources, reducing economic impact on nature. They are also more social means of travel that can build community and make places safer by putting more active eyes onto the streets. Local Boards play an important role in the development of active transport through street to park connections in Greenways plans, Local Board transport capital funds and advocacy to Auckland Transport. Delivering an effective active transport network should not just be about paths though, street trees and facilities play an important part in making these spaces attractive options to meet transport needs.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input checked="" type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input checked="" type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Pinehill Auckland/Greville Road specifically has a rubbish site. It is suggested to upgrade this to a leisure outdoor park



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Senior Focus

Local Board: Hibiscus and Bays

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very well

Tell us why

I liked the detailed innovative ideas eg solar on roofs

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

May be unaffordable for some

Seniors over 80 discount

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input checked="" type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input checked="" type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above

✓	Other, please specify:	Tree planting, waste, water quality eg healthy waters should have other funding
---	------------------------	---

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Just a good start

Resilience needs substantial social infrastructure support

HAVE YOUR SAY

FEEDBACK ON

KAIPAATIKI LOCAL BOARD PLAN

SENIOR FOCUS

CONTACT PERSON

Anne-Marie Coury

027 301 6202

Q4

Yes very well, Signage, Te Reo appearing and story- telling, heritage strengthening are all needed and have value in creating belonging and identity.

A brief statement re Senior Focus


Been broadcasting for 7 years

Covering the whole region.

Supported Age Friendly City

**Movement and consultation for
the Action Plan**

Able to present to the Board



*Anne-Marie Coumy.
027 - 301 6202.*



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Civic Trust Auckland

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

See attached



Submission of Civic Trust Auckland

Draft Local Board Plans - all

Name: Audrey van Ryn (Secretary)

Organisation: Civic Trust Auckland

Phone (daytime): 021 0354431

Phone (evening): 021 0354431

Email: cta@civictrustauckland.org.nz

Postal address: P.O. Box 29002 Epsom Auckland 1344

www.civictrustauckland.org.nz

Introduction

Civic Trust Auckland (CTA) is a non-profit public interest group, incorporated in 1968, with activities and interests throughout the greater Auckland region

The aims of the Trust include:

- Protection of natural landforms
- Preservation of heritage, in all its aspects
- Encouragement of good planning, for the city and region.

We are on Council's list of regional stakeholders and we have previously provided submissions on Local Board plans, as well as on many other Auckland Council plans. Some matters that we have advocated for are now in place, such as smoke-free parks and public open spaces, more resource recovery centres and public education about waste reduction, especially as regards plastic bag use, and Local Boards holding their monthly meetings at varying times.

Other matters are partly in place but we feel the need to submit on them again, including improved public transport: bus, rail and ferry services, better provision for walking and cycling, involving the community more in decision making as well as supporting them as volunteers and improved Local Board engagement with the community, perhaps through a regular clinic at varying times of the day.

CTA has a particular focus on the Environment and Climate Action parts of the Local Board plans.

CTA greatly appreciates the work of Local Boards, and recognises that they know their local communities and are focused on the needs of those particular communities, as well as responding to Auckland Council plans. We are aware of the reduced amount of funding Local Boards have been allocated. We support in general the initiatives in the draft plans. We have highlighted some areas of particular support and particular concern, first making some general points about all Local Board plans as well as particular points for a number of Local Boards, in alphabetical order according to the names of those Local Boards. When quoting from Local Board plans, we use *italics*.

All Local Boards

Climate Action

All the plans address climate action and emissions reduction. We support all Local Boards in the importance they have placed on climate action and commend those that have their own Climate Action Plan, especially those that put them in place even before Auckland Council formulated its Climate Action Plan, such as Puketāpapa. As stated in the Upper Harbour Draft Local Board Plan: “*We recognize that climate change is a complex issue that will require collaborative effort from all of us*” (page 10). The Kaipātiki Local Board articulates that “Climate impact is a key consideration for all decisions made.”

The plans address similar concerns:

- lack of knowledge about how to live low carbon lives
- community preparedness for impacts of climate change
- additional barriers for migrant and ethnic communities
- lack of access to reliable public transport
- road safety concerns, particularly in rural areas
- coastline erosion, sea level rise and flooding
- low tree canopy cover and lack of greenspace to match the rate of urban development and intensification • vegetation, soil and biodiversity loss (urban development, transport, pollution)

We support climate actions across the Local Boards such as

- establishing / supporting EcoNeighbourhoods
- implementing local Urban Ngahere (Forest) Action Plans
- implementing Shoreline Adaptation Plans, especially in collaboration with a neighbouring Local Board
- local waste minimisation, resource recovery initiatives and public education about waste reduction.
- the planting of more trees by Local Boards and support for the community to plant trees
- ensuring Local Board assets will be resilient to the impacts of climate change or severe weather, and reinstating any assets lost or damaged as a result of storm events
- implementing low carbon practices when upgrading or developing Local Board assets
- developing community-led emergency preparedness plans and
- working alongside the community in response to flood events

Furthermore, we support those Local Boards such as Aotea-Great-Barrier that have their own local Emergency Response Team to lead emergency preparedness, response and recovery, and those advocating, as the Devonport-Takapuna Local Board articulates: “*Advocate to and work with Auckland Emergency Management and wider council departments to support community networks and connections that help people to fare better and respond to emergencies and the impacts of climate change*” (page 16).

We are hopeful that communities know about their local Climate Action Plan or are able to find it online or elsewhere.

Māori Outcomes

As stated in all the Local Board plans, *“Māori identity and culture are Auckland’s unique point of difference in the world.”* CTA endorses all the plans listing strong Māori initiatives that build whānau wellbeing and strengthen resilience, such as practicing te ao Māori guardianship (kaitiakitanga) principles led by mana whenua and incorporating mana whenua identity and culture into the design of developments. We support dual naming of parks and other sites to reflect Māori heritage.

We note that the Manurewa Local Board has the highest proportion and highest number of Māori residents across all the Local Boards and that, appropriately, they include in their plan a particularly strong and high number of listed Māori outcomes, including for Māori youth.

Places

All Local Board Plans have included the following in their draft plans under Local Planning and Development:

“We support local town centres and communities to thrive by developing town centre plans and development, supporting Business Improvement Districts (BIDs), heritage plans and initiatives.”

Local boards, sometimes with input from local heritage organisations, arguably have the best understanding of their local heritage and therefore have an important role in its identification, protection and enhancement. We strongly support local boards identifying and assessing the heritage items and places in their area, making use of local knowledge and skills to do so, and consulting with stakeholders in working to preserve and enhance their built and natural heritage.

We observe that the Waitemātā Local Board lists particular heritage buildings in its draft plan, and it seems that only two other Local Board do so (Orakei Local Board – Remuera Library, Waitākere Ranges – Waikumete Cemetery, the rail station house and Playhouse Theatre). We know that communities value their heritage buildings and we know that many of them remain unidentified and unprotected, so we encourage Local Boards to include the identification, protection, enhancement and restoration of built heritage as key initiatives in their plans.

CTA supports local boards to advocate and provide for local board input for improved outcomes for heritage in Council planning documents for the region’s collective heritage.

CTA supports the creation and maintaining of a regional cultural heritage database to provide information to landowners, resource consent applicants and other interest groups. We encourage Council and community groups sharing collective information for the purposes of heritage identification.

CTA encourages all Local Boards to investigate incentives for enhancing low quality designs and to encourage any design solutions that might assist in this. We support Local Boards advocating to the Governing Body for the creation of the position of City Architect.

We support Local Boards reviewing their community-leased buildings, approach and fees to make best strategic use of those spaces.

We support the intention of the Local Boards which are looking at partnership opportunities with local schools, churches, and other organisations to make their spaces available for community use and to ensure programmes and initiatives are coordinated to fill any gaps and to avoid duplication. Schools in particular are a valuable resource for all communities, both their facilities and their populations.

We support the Local Boards working with Kāinga Ora to help provide park and community facility assets, as well as integrate their housing developments with the character of the area, ensure that developments are well planned and that the transport and other infrastructure works well.

Environment

Many of the Local Boards in their plans recognise and celebrate their environmental volunteers who have restored, protected and cleaned up areas over many years and continue to do so. This includes community-led pest control and biodiversity projects. The support of Local Boards for such work is important.

Also important are the actions of those community groups which work to identify and protect built heritage, which is recognised by the Ōrākei Local Board in its intention to work with the Remuera Heritage Society (amongst others) to preserve heritage character in the area (page 25).

Local Board support, including funding initiatives such as planting, pest control, stream and water quality enhancements, and waste minimisation projects is important for healthy ecosystems and sustainability.

Some Local Boards contain Special Ecological Areas within them and we support their work in this regard.

CTA supports the daylighting of streams to divert them from pipes back to the surface, as the Manurewa Local Board and others are planning to do, including Waitematā Local Board with the Waipapa Stream.

We support the use of electric vehicles and the installation of charging stations.

All Local Boards recognise the importance of parks and open spaces in their area and we support those that plan to acquire open space where possible, such as through large-scale developments, and to improve existing parks space through land exchanges, reconfiguration and improving linkages and connections.

We support management plan for local parks, reserves and other open spaces. However, care may need to be taken so that an omnibus management plan does not mean that unique aspects of certain spaces are not protected. Some spaces may need their own separate management plan.

We support more tree planting throughout the city, and, in general, oppose the removal of already planted trees. This includes the removal of exotic trees from the maunga that are demonstrably valued by the people of Auckland. We submit that trees already planted be allowed to live out their natural lives and that any succession planting, where appropriate, should focus on natives.

We suggest that small local projects involving the community, particularly new migrants and/or those for whom language is a barrier, could be to beautify/make use of small areas, including on land which is awaiting development, for planting or for art installations.

Transport

As mentioned in the Devonport-Takapuna Draft Local Board Plan (page 25), *“Local Boards have an advocacy role when it comes to many transport projects.”* All the plans address transport issues, particularly public transport, and they advocate for efficient, affordable and convenient public transport, which CTA has previously advocated for and supported.

CTA supports the Māngere-Ōtāhuhu and Otara-Papatoetoe Local Boards in advocating for free public transport. CTA has suggested in previous submissions that a trial of free transport be held. As pointed out by several Local Boards, transport in Auckland accounts for nearly half of the city's greenhouse gas emissions. Some Local Boards also mention car-dependency and congestion.

We also support safer speeds, especially around schools, traffic calming, road safety and pedestrian and cyclist priority, including cycleways and greenways to encourage healthy activity and less care use, as is advocated for by most, if not all, Local Boards. We caution that when considering implementation of the relevant infrastructure, including bicycle parking, there should be full consultation with affected people, primarily being the residents and/or businesses in the area under consideration. Streetscape character should not be compromised. We suggest that there is already sufficient infrastructure in place for bicycle parking, partly because some Local Board areas have already provided specific bicycle parking and partly because most cyclists like the ability to park very close to where they are going and are happy to use a pole, fence or tree to chain their bike to, and in our experience, this is workable for everyone (in contrast to e-scooter parking). We consider that the negative effect of placing any transport infrastructure in the public realm needs to be recognised and minimised.

CTA encourages Local Boards to support our advocacy to Auckland Council for a Public Transport champion. This could be a youth role and it could involve the telling of good stories about public transport. In our view, many more people would use public transport if they could see other people like themselves using it on a regular basis and get to know more about HOP cards (and the imminent introduction of the use of credit cards to pay for public transport), that public transport can accommodate the frail and disabled, and that there is, in fact, often a bus stop not too far from their house and/or place of work.

We support the Local Boards that are advocating for more bus shelters in their area. In our view, a shelter in addition to a seat at a bus stop is an essential part of the public transport infrastructure, for protection from both sun and rain. Future designs should consider the potential for the seat and the people waiting at the bus stop getting wet from driving rain, as is the case with some of the current shelters.

Community Engagement with Local Boards

The Devonport-Takapuna Draft Local Board Plan under "Challenges" states: "*Low levels of participation and understanding of what local boards do, which weakens local democracy and decision making*" (page 14) and also under "Challenges" the Franklin Local Board includes: "*Limitations to understanding the board's role and mandate, and its influence in regional and national decision-making*" (page 21). The Māngere-Ōtāhuhu Local Board will advocate for "*Auckland Council to communicate in a way that is accessible to all our community leading to a better-informed community and improved engagement with the council*" (page 15).

In our view, engagement with the community is an issue for all Local Boards, as well as the wider Council. We have observed that many members of the community still do not know much (if anything) about local boards, and a very small percentage of them actually engage with their local board. Furthermore, some people find it difficult, for various reasons, to approach their local board and/or to be listened to. We commend those Local Boards which have made their members available through regular open meetings to members of the public, especially those that are at varying times of the day to cater for the availability of different members of the community. We suggest that time could usefully be made available within Local Board workshops for members of the community dealing with certain issues to engage more thoroughly with board members and staff than is possible in public forum appearances at local board business meetings.

The Local Board plans outline what Local Boards do, who the members are and where they are based, so if people in their area read the plans, they would have a better awareness about Local Boards. If all libraries held copies of the relevant Local Board plans in their final version, this would assist in more people becoming more aware of Local Boards.

We suggest that Local Boards share with other Local Boards what has worked in terms of public engagement for them. An idea from a member of the Puketāpapa community is that Local Board members hold their clinics or engagement (where practical) “on the street,” which could mean in a park or square which is used well by the public. We note that when Local Board candidates are campaigning for election, they tend to go to where the people are, and we suggest that this could be part of the answer, rather than expecting the people to come to where they are. We know that many people do not know where their Local Board office is. If practicable, Local Board members could now and again set up tables, chairs and coffee on the footpath outside their office.

We also note that a key initiative of the Devonport-Takapuna Local Board is: *“Community conversations – enabling communities we hear from less to engage more comfortably and on their terms”* (page 15), so perhaps their ideas could be shared with other Local Boards. We also see that this Local Board will *“Advocate for accessible and easy to use engagement methods that suit our residents for both local and regional consultations”* (page 16).

We encourage local board members (and future candidates) to retreat from political affiliations. In our experience, alignment with a political group creates an impediment to good governance and working effectively for the community. Community groups tend to be apolitical and their members tend to work well together. The political affiliations of Local Board members can act as a barrier to community engagement – as well as a barrier for members to engage with and cooperate with each other.

Albert-Eden

We support the Carrington residential development and the way it has developed, including working with the iwi involved, and we also support the bid by the Pt Chev Social Enterprise Trust for there to be a community wellbeing hub incorporated into Building One on this site. We are disappointed that one wing of this building is to be demolished, due to the roading design.

We strongly support the Local Board’s key initiative to: *“Provide feedback on resource consents and planning processes to ensure good design outcomes are achieved and significant views of the maunga (volcanic cones) are protected through the volcanic viewshafts and height sensitive areas in the Auckland Unitary Plan”* (page 12).

CTA strongly supports the establishment of the Waiōrea Community Recycling Centre (opened on 11 August), which seems to be a shared facility between the Albert-Eden and Waitemata Local Board areas, and supports the promotion by these two Local Boards of the facility. We suggest that libraries in all Local Board areas could be a place for dropping off small items such as used batteries (battery collection being something that has taken place in Wellington libraries for some years).

CTA supports this Local Board including as one of the objectives of its plan: *“The history, identity and character of our neighborhoods are celebrated”* and that a key initiative is: *“Protect and celebrate our historic heritage by being good stewards of the assets we own and supporting others to care for theirs”* (page 29). Supporting others could be seen as both support for the people in their Local Board area as regards historic heritage that is privately owned, and supporting other Local Boards to care for historic heritage that are their assets, as well as other Local Boards supporting the protection of other historic heritage sites.

We support the Albert-Eden Local Board for the following (page 31):

- *“Renewal funding for seismic upgrades for heritage assets*

- *Support the Tūpuna Maunga Authority in their aspiration to have the maunga recognised as UNESCO World Heritage site*
- *The Auckland Unitary Plan reflects the importance of built and natural heritage and the contribution that heritage makes to the identity, history and story of Tāmaki Makaurau and its people”.*

We endorse this Local Board’s plan to carry out an audit of the parks in their area to identify where they can get better use, to activate them more and to see if there is the right mix of uses for the future.

A photo of Rocket Park in Mount Albert is featured in the Albert-Eden Draft Local Board Plan. This park no longer functions as it was intended. It was originally a park where children with disabilities could use special equipment such as a Liberty Swing for wheelchairs. We understand that the swing is no longer there, and we submit that it be reinstated, along with the other original equipment for this special park.

We support the Local Board’s intention to *“Encourage green business practices and share information on existing resources such as Sustainable Business Network and Climate Connect Aotearoa”* (page 35).

Aotea-Great-Barrier

This Local Board’s plan states that *“Our community is also incredibly conscious of remote living and the carbon impacts of freighting supplies to the island”* (page 11). The island is very different from the mostly urban areas of Auckland and has some different issues to deal with, which perhaps the rest of the city does not recognise.

CTA supports this Local Board’s intention to *“advocate to the Governing Body for continued staff support to investigate a visitor levy to respond to the impacts of tourism”* (page 14), like the levy that has been introduced to Rakiura / Stewart Island.

The Aotea-Great-Barrier Local Board has been progressing solar power upgrades of council and community buildings and electric chargers, as have several other Local Boards, and we encourage more area to do this, and for Local Boards to learn from each other in this regard.

Another aspect that perhaps other areas could learn from is the island’s *“bespoke approach to education through our Lifelong Learning Strategy, especially for our tamariki and rangatahi with a new early childhood education centre and established Aotea Learning Hub”* (page 13).

We congratulate the island on being designated as an International Dark Sky Sanctuary.

An aspect of the island that needs recognition by Auckland Council is that *“Climate change has led to warmer waters and the arrival of exotic marine pests such as Caulerpa, Asian Paddle Crab, Sea Squirt and Mediterranean Fan worm”* (page 16). We support the Local Board’s call for tighter protections for their coastal waters.

We congratulate this Local Board on its innovative low carbon initiatives, such as Anamata’s Resource Recovery Centre and AoteaOra Trust’s Off the Grid events (page 16) and agree that these should be showcased globally and there should be trials of new sustainable systems. The landfill on the island has closed, which presents a different situation than all other Auckland communities.

We support this Local Board in advocating to central government and the Governing Body to encourage manufacturers to commit to product sustainability (page 17) and encourage other Local Boards to do the same.

We also support this Local Board to:

- *“Advocate to central government and the Governing Body to abolish bottom trawling and dredging in the Hauraki Gulf and to encourage sustainable fishing as directed through Sea Change - Tai Timu Tai Pari*
- *Advocate to central government and the Governing Body for the prohibition of marine dumping” (page 17).*

Aotea-Great-Barrier does not have any public transport and we support the Local Board’s key initiative to *“Investigate a community-led public transport system”* (page 19).

We also support them to advocate to technology providers for stronger connectivity options (page 20).

Devonport-Takapuna

We support collaboration between Local Boards on many matters and so are pleased to see that this Local Board is collaborating with the Kaipātiki Local Board *“on the reduction of litter, sediment, and industry pollutants to improve the water quality of streams and ponds in the catchment of the Wairau Estuary”* (page 11).

This Local Board (and many others) is supportive of the work of environmental groups and environmental education. Amongst other environmental initiatives, we support their aim to *“Prioritise funding and support zero waste initiatives that promote a circular economy”* and the *“focus on native and ecologically appropriate planting and the principle of having the right tree in the right place”* (page 12). Regarding the trees, we would caution that the wrong tree in the wrong place (in any Local Board area) does not necessarily get removed but that it is allowed to live out its natural life, or there is consideration given to transplanting the tree. We understand that there is modern machinery that enables the transplanting of large trees. Perhaps Local Boards, including this one, could advocate to the Governing Body and/or central government for investment in one of these machines.

We see that this Local Board’s intention of *“Working with our neighbouring local boards on joint environmental initiatives to increase the impacts”* (page 18) is something that all Local Boards could do – and we know that many of them are.

We are pleased to see amongst the objectives in this Local Board’s draft plan: *“Natural and built heritage are conserved and both colonial and Māori history and significant places are valued, promoted, and celebrated”* (page 19).

We support this Local Board to *“Advocate to the Governing Body for additional funding to renew our heritage asset portfolio and enable greater protection for our heritage assets”* (page 20).

We also support this initiative (and other similar ones of other Local Boards): *“Utilisation of regional funding to retrofit assets and upgrade them with solar energy saving measures”* (page 22).

On page 25 it is stated that there is: *“Limited capacity for bikes on ferries, zero capacity for bikes on buses, and the absence of infrastructure for active travel modes on the harbour bridge is a constraint on reductions to transport emissions.”* We note that there is not quite zero capacity for bikes on buses, as fold-up bikes can be taken on buses: *“For safety of the passengers on board of buses only compact folding bikes and scooters are permitted on board, and must be folded down before boarding (at.govt.nz).”*

We would encourage the Devonport-Takapuna Local Board to advocate for bike racks on buses that go across the Auckland Harbour Bridge.

Franklin

We support this Local Board's initiative to *"Identify opportunities to reduce Franklin's mowing footprint by replacing lawn with trees"* (page 15) and the other Local Boards that have this same initiative.

We support their advocacy for *"Provision of fit for purpose public transport services that enable underserved communities to access Auckland's public and active transport networks and community service hubs, e.g. through 'AT Local' services or new bus routes"* (page 19).

Franklin, being a rural community, includes in its draft plan, which does need to be recognised by other areas: *"Auckland climate action is focussed on reducing carbon largely by reducing the use of private vehicles but does not constructively support rural residents and farmers in taking action, such as emission reduction"* (page 25).

We support this key initiative: *"Advocate for investment from Auckland Transport and central government in local initiatives that encourage and incentivise modal shift (from private vehicles to active or public transport) or shorten the commute for those reliant on private vehicles (by improving access to public transport)"* (page 26).

We support the following (pages 26 & 33):

- *"Require all public space projects in urban/future urban areas to actively contribute to increases in tree canopy cover and more green infrastructure."*
- *"Advocate for a change of policy and process so that new developments deliver 'urban lungs' i.e. areas to be planted with semimature trees to improve air quality, to lower surface temperatures and to absorb and filter rainfall."*
- *"Increased ferry capacity at Pine Harbour, Beachlands, to address local population growth, acknowledge east Auckland congestion issues and offer off-road commuter options."*

Henderson-Massey

We support this Local Board in the following:

- *"Working with Waitākere Ranges and Whau local boards to take a 'west Auckland' approach to strengthen our capacity"* (page 14)
- *"Continue to prioritise parks and play spaces that are inclusive, accessible to all, and support whānau-focused gatherings and activities"* (page 20)
- *"Progress opportunities for neighbourhood path connections, guided by the Connections Plan"*
- *Prioritise transport projects that improve safety and health and contribute to climate action and social outcomes"*
- *Support initiatives that help remove barriers to cycling e.g. access to a safe affordable bike and learn to ride programmes"* (page 22).

Hibiscus and Bays

On page 12 of this Local Board's plan, their Climate Plan actions are well detailed.

CTA supports this key initiative: *"Take a more proactive approach to ensuring that the different voices in our community are heard to inform our local board decision-making"* (page 16).

We particularly like the way this Local Board has articulated its appreciation for local volunteers: *"Our committed and skilled environmental groups we support have long established histories and have been award winners for their professionalism and work"* (page 19).

This challenge is one that other Local Boards may also face: *"There is increasing demand for council services to be available online, and 24/7 whilst recognising that at other times residents are more comfortable with a face-to-face service provision, all within budget constraints, which will require better integration of locally delivered services with regionally provided online services"* (page 22.)

We are pleased to see this opportunity expressed: *"The rapid increase in residents working from home reduces congestion and adds value to the local economy by keeping commuter spending local"* (page 25).

CTA supports the following: *"Advocate to Auckland Transport to better accommodate the integration of public transport vehicles, major bus stop and park and rides, with micro mobility and bicycles, to enable people to use these modes to connect with their public transport journey"* (page 26).

We would encourage this Local Board to also advocate to Auckland Transport for bike racks on buses.

Howick

This Local Board has a strong focus on youth, as do many of the others.

We note the following key initiative: *"Support the Tūpuna Maunga Authority to protect and enhance Ōhūiarangi / Pigeon Mountain"* (page 20). We do support protecting the maunga but we do not support the removal of trees from any of the maunga in Auckland. It is our position that particular trees that are not wanted on the maunga should live out their natural lives and that replacement planting of other species can be planned for.

We support this Local Board in its intention to advocate to local schools to *"make sustainability, environmental responsibility and waste management part of school values"* (page 20). These values, across Auckland schools should, through school students, extend into the community and become embedded in future generations.

We support this key Initiative (which is shared by other Local Boards): *"Promote healthy living and sustainable lifestyles by supporting community-led initiatives, such as vegetable gardens, and by encouraging and educating the wider community of the benefits of growing your own food"* (page 22).

We support this special initiative and are of the view that it would have region-wide use: *"Establish a sensory playspace that contains areas and equipment that provides stimulation of the senses for children with visual impairment or developmental disorder"* (page 22).

CTA supports this advocacy to Auckland Transport, which could be replicated in other Local Board areas, where appropriate, for:

- “feeder bus services or other innovative on demand services that will enable the community to access the existing bus / ferry networks
- more bus services to serve all areas of the local board
- provision of more ferry and train services” (page 26).

The following is important for all Local Boards: *“Advocate to the Governing Body to: ensure adequate infrastructure is in place before approving housing intensification”* (page 26).

We support the following key initiative: *“Continue to provide operational funding support for the Howick Historic Village”* (page 28).

We support the protection of historic landscapes and in this regard, support efforts to protect in the Unitary Plan the views from Stockade Hill.

Kaipātiki

As with other Local Boards, this one is looking at solar energy, and we support their particular initiatives to: *“Investigate installing solar on facilities with significant solar potential, including: Birkenhead Pool and Leisure Centre, Kaipātiki Local Board Office, Glenfield Pool and Leisure Centre”* (page 14).

We feel that the following key initiative could be adopted by all Local Boards: *“Seek feedback from our student and children’s panels to inform our decisions with a child-lens”* (page 17).

CTA supports the Kaipātiki Local Board’s advocacy to the Governing Body:

- *“to protect and celebrate our built and cultural heritage”* (page 18), and
- *“for the ability for local boards to have direct decision-making authority to make temporary changes to dog access rules to mitigate environmental or social problems, and request the opportunity to be included in the next round of dog access revision hearings Governing Body”* (page 23).

We also support this Local Board advocating to central government:

- *“to reinstate legislation that provides general tree protection”* (page 23), and
- *“for urban planning legislation to adequately protect special ecological areas Central Government”* (page 23).

We see that the challenge expressed by this Local Board that *“An increased number of schools are restricting public access to their grounds outside of school hours which is impacting informal community recreation opportunities”* (page 25) could provide the opportunity to talk to other Local Board about this problem and possible solutions.

We support the following key initiatives, which could be the same for all Local Boards:

- *“Consider climate impact when developing places and spaces, to reduce or avoid emissions and adapt to climate change, including avoiding areas or activities that are vulnerable to climate risks such as drought, extreme rainfall, coastal flooding, and heat*
- *Prioritise investment in assets that are well positioned, multi-purpose, and with good transport links, to reduce its carbon footprint”* (page 27).

We also consider that all Local Boards would similarly advocate for and support the following:

- *“the development of a quality compact, urban form that supports low carbon, resilient development, while ensuring adequate infrastructure to support it*
- *the uptake of sustainable design and construction that will be resilient to the impacts of climate change, including council projects, and encourage residents and businesses to use eco-design and water sensitive design practices” (page 28).*

We think that all Local Boards would want this for their area: *“Advocate for increased resource for compliance enforcement teams so they can respond to all requests and complaints received in the Kaipātiki area” (page 28).*

We support cheaper fares, more frequent ferries, and more buses going to more destinations, more directly (page 29). We view the harbour as an underutilised resource which does not need the sort of maintenance that roads and rail do. We also support electric buses and ferries for Kaipātiki (page 32).

This matter seems to need to become more widely known within Auckland Council: *“Advocate for policy and funding changes to reduce the use of the ‘chip seal’ road resealing method on suburban roads” (page 32).*

We find it useful to have this pointed out: *“Increase in working from home means more of our people are based locally during working hours which can help ease congestions at peak times, enable access to skilled work without the need to travel as frequently, and help increase spending in our town centres” (page 35).*

Māngere-Ōtāhuhu

This Local Board sees as an opportunity, which we think other Local Boards could embrace as well: *“Identify areas where community groups are better placed to deliver services and projects that are currently delivered by council” (page 14).*

CTA supports the **Māngere-Ōtāhuhu** Local Board to:

- *“Revive the Māngere Gateway Heritage plan to enhance Stonefields and promote and regain heritage values (page 17).*
- *Partner with mana whenua to protect significant historical sites and waterways that host an abundance of native plants and animals*
- *Work with Kāinga Ora to encourage tree planting in their housing developments” (page 18).*

CTA supports the entire advocacy list of this Local Board, on page 18 of their draft plan, which focuses on protecting and enhancing their natural environment, including active management of high-value biodiversity features in their area, and achieving zero waste and zero dumping.

Manurewa

The Manurewa Local Board highlights the lack of equitable funding for their area over the last 13 years, and we support them in their advocacy to the Governing Body to confirm its commitment to achieving funding equity for local boards, and to *“Look at ways to achieve funding equity for the most underfunded boards in the shortest timeframe possible with a priority focus on Manurewa, then other southern local boards” (page 23).*

As with some other Local Boards, the lack of decent public transport options is noted. We support their advocacy to increase the frequency, connectivity, and capacity of public transport

services in their area (page 15), and, as with all Local Boards, suggest that car pooling is another option that could be promoted and supported by them.

This Local Board (as with all others) intends to enhance the urban ngahere. They point out that *“there is only so much that can be done on council-owned land.”* We support their call for legislative measures that *“could include adding protection and enhancement of urban ngahere to environmental targets and making it easier for local authorities to protect trees on private land”* (page 16).

We support this Local Board (and the others which have this initiative) in the installation of more solar panels across their community facilities in partnership with power companies. The Manurewa Local Board will encourage households to do this too (page 18), and other Local Boards could encourage the households in their areas to do likewise.

CTA supports this Local Board to *“Advocate to Auckland Transport to increase the frequency and capacity of trains and buses to and from Manurewa and Clendon town centres, linking more of our communities to public transport hubs”* (page 18).

We note the establishment of the Manurewa Community Recycling Centre at the Beautification Trust and their support of the pop-up Repair Cafes operating at the Trust and at Nathan Homestead. We support them to *“continue to advocate for a southern resource recovery park as part of the Auckland Resource Recovery Network”* (page 25).

We are pleased to note under Opportunities: *“increase awareness of the notable tree schedule to protect more of our trees”* and *“using nature-based solutions for flood management”* (page 26), and we support the key initiative to *“Replace every tree removed from the streetscape due to damage, storms and accidents within one year”* (page 27). This practical and clear action could be adopted by other Local Boards.

We also support the following: *“Promote the integration of natural ecosystems into the existing built form, which may include the construction of raingardens, living roofs and swales, mass tree planting, remediation of existing or contaminated soils, rehabilitation of watercourses and wetlands, and opening up buried watercourses and restoring them to more natural conditions (stream daylighting)”* (page 27).

Maungakiekie-Tāmaki

We liked to see the use of different languages in this Local Board’s draft plan in addition to English and te reo.

CTA supports the following advocacy to the Governing Body of the Maungakiekie-Tāmaki Local Board:

- *“to ensure appropriate mechanisms are implemented to limit development where infrastructure cannot support increased pressure from planned growth”* (page 21)
- *“for appropriate staffing resources to enable quality advice for good decision-making, particularly planning, to utilise increased local board decision-making and ensure local services are cost effective and fit-for-purpose”* (page 29)
- *“to increase the regional demolition fund. To maximise the use of our open spaces and oversubscribed maintenance budgets, local boards can look to reduce underutilised and aged buildings on reserves. Regional support will help local boards to realise these strategic long-term opportunities”* (page 29).

Regarding the last one, we caution that these buildings first need to be accessed, especially for heritage and community values.

Ōrākei

This Local Board's intention to use methods such as digital signage, e-newsletters and social media as a way to improve communication with the community could be considered by other Local Boards.

We support the idea to *“Advocate to businesses and philanthropic organisations to seek funding partnerships for community services”* (page 14), which perhaps other Local Boards could also adopt.

We are pleased to see the inclusion in the draft plan of *“Delivering the Pourewa Valley Integrated Management Plan will support the restoration of this significant urban forest”* (page 16).

We strongly support the following (the second of which is also advocated for by other Local Boards):

- *“Continue funding for the Eastern Bays Songbird Project in their valuable work creating and maintaining a sanctuary for wildlife in the Eastern Bays area”* (page 17)
- *“Advocate for a sponge-city approach which promotes water-sensitive design and enables bluegreen networks to manage flooding”* (page 18).

The Ōrākei Local Board states that *“Seismic restoration of Remuera library is undertaken”* (page 23) and we strongly support the restoration of what is not only a heritage building but a community asset.

The Ōrākei Local Board states in its draft plan that theirs is *“an area rich in history. We can use our wonderful built heritage and local history to engender pride in our area and to help protect this valuable heritage”* (page 25). We appreciate the Local Board's acknowledgement of their built heritage and also of the Remuera Heritage Society (page 25).

We support the Local Board advocating to the Tūpuna Maunga Authority to *“protect the War Memorial Seat and daffodil commemorative garden, and for effective pest control on Ōhinerau / Mt Hobson”* (page 27).

CTA also supports the Local Board advocating to the Governing Body, central government and developers *“to ensure the unique character and heritage of our area is retained as development occurs”* and we support the Local Board *“monitoring and assessing proposed legislation that may increase intensification that may have an effect on our infrastructure and character”* (page 27).

Otara-Papatoetoe

This Local Board articulates (as many other Local Boards do, and with which CTA agrees) that it should be the local people who decide what the priorities in their area are: *“Your input on what we need to prioritise for the next three years will help us greatly with this challenge”* (page 3).

Regarding the Local Boards intention to *“Develop a multi-park management plan that manage the use, development and protection of all parks, reserves and open spaces in the area”* (page 21) we would say that the different types and characters of parks need to be considered when developing such a plan or plans.

On page 3 it is stated that *“Public transport hubs and facilities including Manukau Bus and Train Stations, Puhinui Station Interchange, and the Southern Line train provide opportunities for connectivity and public transport use for both residents and visitors”* and the Puhinui Station to attract visitors to the area is a key initiative. As CTA detailed in our submission on [Auckland Council's Future Development Strategy](#):

“This station is an important part of the city’s infrastructure. As Puhinui Station connects to the wider public transport network, you can travel to and from Auckland Airport from anywhere in the city. The bus operates every 10 minutes from 4.30am to 12.40am, 7 days a week and there is a train from Puhinui Station to Britomart every 15 minutes. However, this AirportLink service is not clearly signposted at the airport and we understand that it is used very little. The facility to use public transport without needing a HOP card, as planned within the next 12 months, should help in this regard, but the AirportLink service needs more publicity. For example, the staff at the airport who monitor cars that arrive at the domestic terminal to pick up incoming passengers could hand drivers brochures that describe this service, so people can use it next time.”

We would encourage this Local Board to advocate to AT to publicise this important part of the public transport network.

Papakura

We support this Local Board to: *“Continue work on a heritage trail featuring sites of significance in Māori and European local history, acknowledging Ngāti Tamaoho’s cultural indicators along the southern path overlooking the Manukau Harbour”* (page 8) and we support their aim to highlight and celebrate the area’s history and heritage.

We are pleased that the Manurewa Community Recycling centre is in place and that the Papakura Local Board describes the circular economy that will *“reduce costs and reduce carbon footprint”* (page 11).

We support this Local Board’s advocacy in the following areas of public transport (pages 29 & 30):

- *“Developing AT Local (on demand shared public transport service) in parts of Papakura Local Board area to increase public transport use.*
- *Great South Road –moving the cycle lane to an off-road shared pathway and converting the existing cycle and parking lanes into a combined T2 bus lane.*
- *Increased operating hours of southern train and public transport connections to airport and other employment nodes.*
- *Extending public transport routes to Ardmore and Bruce Pulman Park.”*

We also support the advocacy for *“an increase in the standard street width requirement in new developments”* and *“developers /body corporates to cover ongoing maintenance of green space and enhanced tree canopy in new developments”* (page 30).

Puketāpapa

We congratulate the Puketāpapa Local Board for developing their [‘Becoming a Low Carbon Community’ Action Plan](#) action prior to Te Tāruke-ā-Tāwhiri: Auckland Climate Plan. We would like to see the “local climate champions” mentioned in this plan become more visible and for other Local Boards to adopt this idea.

We support the Local Board’s aim to *“Use existing community hubs, such as the library, to increase community understanding of climate change and promote local climate action initiatives”* (page 16). Schools and libraries are an excellent resource for educating the wider public on climate change.

Rodney

This Local Board states that its residents *“often feel marginalised from Auckland Central and generally do not feel heard by decision makers”* (page 14). We are not sure how the Local Board will address this, but making other Local Boards aware of it should help.

CTA supports the following advocacy by this Local Board to the Governing Body:

- *“for Kawau Island to become pest free, with the support of the community, iwi, the Department of Conservation and stakeholders”* (page 20)
- *“for more investment in regional parks in our local board area and resourcing for service provision, such as with service centres, to meet the pressures of population growth and tourism”* (page 24).

The draft plan states that *“Our large geographical area and low population makes providing public transport difficult”* (page 30). We suggest that with the good community connections described in the draft plan, where *“people look out for each other and feel supported”* (page 3) that carpooling would be a good option that could be promoted and supported.

CTA supports the following advocacy to Auckland Transport:

- *“for the use of the existing rail network to be used for commuter transport to service the North West.”*
- *“for further improvements and the expansion of Rodney bus services particularly in new development areas including park-and-rides, dedicated bus ways, additional routes and stops, weekend services on busier routes, higher frequency on busy routes and greater safety for bus drivers”* (page 31).

Upper Harbour

We support the Upper Harbour Local Board to *“Advocate for accessible, multi-lingual, and easy-to-use engagement methods that enable our diverse residents to participate in both local and regional consultations and engagement processes”* (page 14).

We appreciate that this Local Board has articulated (under Challenges): *“Cost implications of restoration and conservation of our natural environment balanced against the cost of doing nothing”* (page 16) and the huge risks that climate change poses to the environment and people, also the negative impacts of intensification and population growth. In our recent submission on Auckland Council’s Future Development Strategy, we cautioned against too much growth too fast, and that it is preferable to stage intensification, and certainly not to do so in areas that were exposed to potential hazards.

We agree that *“Reducing carbon emissions as individuals can be difficult”* and that *“Communities need support and opportunities to make changes and live more sustainably”* (page 16). We are of the view that Local Boards are making positive differences as regards education about climate change and they can learn from each other’s problems and initiatives, and support each other.

Waiheke

Waiheke is an island sanctuary in the Gulf, as articulated in this Local Board’s draft plan on pg 3.

We commend this Local Board for *adopting “even bolder climate goals through its Waiheke Local Climate Action Plan which includes the aim of achieving a net positive carbon footprint by 2040”* (page 11). This could be a model for other Local Boards.

We recognise this key initiative as important: *“Complete a feasibility study into the equity of year-round access to adequate potable water supply for Waiheke Island residents”* (page 13).

We support this opportunity: *“Waiheke becoming a predator free island”* (page 16) and see the importance of public messaging and the work of predator-free groups.

We support the creation of wildlife corridors (including new and restored wetlands), kauri sanctuaries and seabird and shorebird breeding colonies across the islands, as well as stream restoration projects and academic and citizen science programmes to help understand and support the health of the aquatic environment (pp 16 – 17).

CTA strongly supports advocacy for *“an end to bottom contact fishing such as dredging, bottom trawling and Danish seining”* (page 18).

We also support advocacy to Auckland Transport for a designated Harbourmaster on Waiheke to be sufficiently empowered to address the increasing numbers of abandoned or derelict boats degrading the island’s marine environment.

CTA also supports this Local Board’s key initiative to *“Implement the Waiheke Be Accessible Plan as resources become available to ensure facilities are accessible to all”* (page 21).

Waitākere Ranges

The people of this Local Board area (as with others in the outer areas) often travel long distances to access work and education opportunities. We see that park and ride is an important way of connecting to public transport services (page 12) and that there is anticipation of the benefits that the City Rail Link will bring, as it will mean a 30-minute trip to get into the city.

We support this Local Board’s advocacy in *“Trialling a rural bus service to give our residents and visitors access to public transport”* (page 29).

We support the Local Board also advocating for improved access to broadband and mobile phone services (page 12).

The Waitākere Ranges Heritage Area is a place of national significance which is appreciated by many throughout Auckland, for whom it is a favourite place to visit. There are at least 27 volunteer environmental groups active in the area, recognised in the draft plan. We support the Waitākere Ranges Local Board in its focus on protecting, restoring and enhancing this special area, including making the area a dark sky viewing location (page 15).

CTA supports this key initiative and would like to see it adopted by other Local Boards: *“Ensure universal design for accessibility is embedded into all park development projects to enable everyone to access their local parks”* (page 25).

We also strongly support the following: *“Provide input into council plans, policies and project design, as well as the resource consent decisions relating to heritage”* (page 29).

Waitematā

We support the Waitematā Local Board to *“Promote Waiōrea Community Recycling Centre, local waste minimisation and resource recovery initiatives”* (page 14), and we encourage this Local Board to do this in collaboration with the Albert-Eden Local Board.

We support this key initiative *“Local composting and community gardens network”* (page 14) and would like this Local Board to revisit the application of OMG (Organic Market Garden) to establish a community garden in the Bowen Ave part of Albert Park which has not yet gone ahead, having not received the support it needed.

Under “Our People,” the statement *“We celebrate our diversity and heritage and encourage equitable access to social, economic, environmental and cultural opportunities”* is inspiring (page 16).

We appreciate the following being articulated in the plan: *“We recognise the importance of treasuring and protecting our green spaces, heritage parks and trees”* (page 27).

Under Opportunities: *“Greater value for money through a partnership model which could leverage funding from other sources outside of Auckland Council”* (page 18) is something that other Local Boards could also consider.

We are pleased to see that the problem of air pollution and pollution from shipping is noted (page 21). Pollution as such is not articulated in other Local Board plans. We are also concerned at the proliferation of visual clutter, particularly in terms of lighting, signage and some unnecessary street furniture.

We support the following and consider that other Local Boards could adopt the same: *“Encourage, exemplify and promote the installation of ‘living’ green walls, rooftops, and assets in public buildings and private developments such as the Central City Library living roof, to improve air quality, green space, and biodiversity outcomes in urban environments”* (page 24).

We support the following, and again, encourage other Local Boards to do the same: *“Continue to reduce the use of agrichemicals in our parks and open spaces and advocate for agrichemical spraying to be phased out regionally”* (page 24).

We strongly support this advocacy: *“Auckland Council to advocate for changes to legislation and regulations to enable councils to introduce planning controls on how urban trees are assessed and protected. Auckland Council to simplify and better resource processes for identifying and protecting mature trees”* (page 25).

We encourage the Local Board to consider, when developing concept plans (e.g., Basque Park, Heard Park, 254 Ponsonby Road and Wynyard Point) (page 27) not only the views of the local community and others who will access these spaces but also whether any “revitalisation” works planned are necessary. CTA has previously submitted that Heard Park does not need a makeover, and that the money be better spent elsewhere. Indeed, a couple of paragraphs down (on page 27) in the draft plan it is stated, *“The cost of maintaining our assets and facilities is no longer affordable.”*

CTA strongly supports the key initiative to: *“Deliver seismic strengthening, restoration and modernisation of Leys Institute Library and Gymnasium to preserve as a heritage taonga for future generations”* (page 28).

Regarding the development of a Local Parks Management Plan (page 28), we caution that not all local parks are the same, and their diversity and special circumstances should be catered for.

We support this key initiative: *“Acknowledge the importance of Te Hā (pōhutukawa) and the whenua that sustains it and investigate opportunities to increase the status and protection of Dove Myer Robinson Park including the possible inclusion in the heritage schedules of the Auckland Unitary Plan”* (page 29).

Under “Our Places,” the Local Board states: *“The government’s proposed legislative changes in resource management and urban development enable more intensive development in the city centre and around our neighbourhoods. This creates a challenge to ensure our neighbourhood character and heritage is conserved within our urban landscapes. We will support quality design in new developments that embraces existing heritage to create the heritage of the future”* (page 32). CTA strongly endorses this statement, and also the following one: *“We want to increase understanding, protection and conservation of city centre heritage places, landscapes and*

stories. Our city centre should actively recognise and celebrate Auckland’s historic heritage as a driver of positive change and placemaking” (page 33) and “Valued elements of heritage and special character in the local board area should be recognised formally and informally to assist with its protection and incorporation in the development of our urban landscapes” (page 34).

We support the following key initiatives (page 36):

“Provide feedback on resource consents and planning processes to ensure heritage sites and Special Character Areas are protected

- *Ensure our heritage assets are restored such as Ewelme Cottage and Symonds Street Cemetery monuments*
- *Seek opportunities to promote and celebrate heritage places in Waitemātā including making digital content and place-based stories more accessible*
- *Reflect Māori heritage and whakapapa in our public spaces and facilities incorporating Māori design principles and artwork.”*

We also support this Local Board’s advocacy to the Governing Body as follows (page 37):

- *Advocate to the Governing Body to allocate adequate funds to restore and preserve its heritage buildings, including incorporating seismic strengthening where necessary, to ensure their ongoing usage.*
- *Advocate to the Governing Body to allocate adequate funds to continue identification and formal protection of heritage and character elements of the Waitemātā environment.*
- *Advocate to the Governing Body to retain the council’s Heritage Acquisition Fund to assist with the protection and adaptive re-use of significant heritage buildings in the local board area.*

CTA strongly supports the following initiatives (page 44):

- *Support the preservation of significant heritage buildings such as Carlile House, the former Fitzroy Hotel and St James Theatre.*

We also strongly support advocacy to central government for incentives to encourage the retention of heritage and character elements of the built environment (page 44).

CTA submits that an important addition to the draft plan is the recognition that significant parts of the landscape need protecting, particularly the Waitemātā Harbour and volcanic cones, and other heritage landscapes.

Whau

CTA supports this Local Board advocating to central government for *“Legislation to protect a greater number of significant trees”* (page 23), which other Local Boards have also included in their draft plans.

We also support their concern for sustainable building initiatives and practices (page 23).

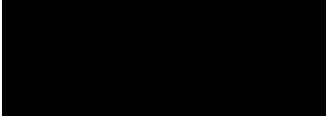
We strongly support the key initiative to: *“Ensure events and services are developed with accessibility considerations to support participation from residents with either physical or intellectual disabilities”* (page 25) and to *“Continue to prioritise and implement actions identified in the previous term to improve accessibility within key parks in the Whau, and do accessibility audits within additional parks”* (page 26).

We support the advocacy to *“Progress on the development of the Avondale-Southdown rail line to support freight movement Kiwi Rail Feedback on relevant policies and plans Central government Auckland Light Rail Group”* (page 31).

We strongly support the following initiative: *“Infrastructure and development that meets future growth, incorporates sustainable urban design principles, and is resilient to climate change, e.g. waste reduction, installation of stormwater retention tanks and permeable surfaces in housing developments”* (page 31).

Date of submission: 14 August, 2023

Signature:



Secretary, Civic Trust Auckland





KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Bike Auckland

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

See Attached.

14.08.2023

Draft Local Board Plans 2023 Feedback

Who is Bike Auckland?

Bike Auckland is a non-profit organisation advocating for safer streets on behalf of everyone who rides a bike in our region - and everyone who would love to give it a go. But more than that: we believe that more people on bikes makes for a better city. Call it a virtuous cycle. Cleaner air, streets with birdsong, neighbours who know each other, elderly people staying active and healthy, children playing on quiet roads. Resilient, eco-friendly multi-modal communities which have long term sustainability. We can create this liveable city by investing in walking, cycling, and public transport. Investing now will save us more money in the future and that's the future we want to live in.

Our audience and members live and work in every local board area, all across the Tāmaki Makaurau Auckland region. This feedback is on their behalf.

Our feedback

We know our members would like to see the following in their local board plans:

- Public transport and active transport modes given priority
- Accessibility for all kinds of disabilities
- Safe streets for kids to walk and cycle to school
- Vibrant town centres with well-designed, people-oriented public spaces
- Low traffic neighbourhoods

Note: People, Environment, Economy, Place, Community are all interlinked focuses and all can be benefited through more transport choice; allowing them to choose to walk, cycle, other micromobility, or use public transport.

People: Diversity includes people who are disabled or who experience mobility difficulties. 28% of Aucklanders do not have a license, and many more do not (or cannot) drive due to illness (seizures, fatigue, anxiety), vision impairments, medication, ADD, or age. These people need the choice to safely walk, cycle and take public transport to get around. Some people find that cycling is easier, and less painful for them than walking (especially if it's an E-bike), and cycling is often recommended by physios as a way to rehabilitate after illness or injury. Providing safe ways to get around by bike creates greater accessibility for this significant portion of our population, and for everyone when they are temporarily injured and are recuperating.

Environment: Cars create run off into our waterways. Giving people the safe option to walk and cycle more will lead to healthier waterways.

Community: When people walk and cycle for their transport they make more eye contact, say hello more, and generally become more connected with their surroundings. This leads to more connected communities, greater community cohesion, greater trust in neighbours, and less loneliness and isolation. This connectedness becomes powerful during times of emergency and can make stronger communities that support each other more and have greater resilience to bounce back from disasters. Bikes can also play crucial role as resilient and versatile transport modes during disasters, especially as they are small, and do not form traffic jams the way that cars do. [More on this here](#).

Key initiatives

Safe speeds

[Safe speeds](#) on residential streets, town centres, and [around schools](#). Permanent speed reductions to 30km/hr not only provide amazing safety outcomes, they are also our fastest and cheapest way of making a safe network for cycling. If our residential areas, schools, and town centres are 30km / hr it feels (and is!) much safer to walk and ride a bike, and many more people will begin to use these transport modes for their everyday local trips.

Modal filters

Another cheap and fast improvement is use of modal filters in residential streets (such as planter boxes or bollards). Modal filters stop cars from cutting through but allow people using bikes, wheelchairs, and pushchairs to get through. Particularly for streets with a large amount of people driving through as a short-cut (rat running) modal filters can help to make the street safer, and quieter, and can also encourage locals to use their bikes and feet for short local trips.

Protected Cycleways on main arterials

Arterial roads usually have higher traffic volumes, higher speeds, and more trucks and buses. Altogether this makes a formidable environment for riding a bike! They are also usually the most direct route to a destination! To give people greater transport choice, there needs to be protected spaces along our main arterials for people to cycle in. This can be done cheap and fast through the use of road reallocation and “pop up” materials like concrete tim tams or tall rubber. We know our members would like to see a network of safer streets sooner, and this means investing in infrastructure which is cheap and fast to roll out.

Greenways

Include safe cycleways through parks, green spaces, and “[blue-green networks](#)”. These provide safe spaces for children and adults to learn to ride a bike and become more confident - they can also form amazing connections to schools, local shops, and public transport.

Safer speeds, modal filters, protected cycleways on arterials, and off road paths through parks all together will form an accessible, safe, efficient, and connected network safe places to ride a bike across our neighbourhoods. These would link people to schools, town centres, and public transport, making local trips by carbon friendly and active modes easier and more appealing.

We also want to see secure bike parking at train stations, transport hubs, town centres, and community facilities (eg. work with your local businesses to get a network of [Lockydocks!](#)), wayfinding to help people find their way around the existing cycleways, and support for community bike hubs and community events which include cycles

We’ve listed some specific transport projects we know our local members would love to see their local boards support. You can find them below in the list of specific local board area ideas.

We would like local boards to advocate for Auckland Transport to:

- Align all projects and renewals with the [Transport Emissions Reduction Pathway \(TERP\)](#) and emission reduction goals
- Support the campaign to [Liberate the Lane](#) on the Auckland Harbour Bridge for walking, cycling, and wheeling
- Advocate for an [E-bike subsidy](#) to help more people replace their car trips with bikes! E-bikes are more accessible for a wider range of people
- Invest more from the financial budget towards walking and cycling. Our emissions reduction and vision zero targets demand a larger monetary allocation towards these modes
- Make streets safer by [reducing speed limits](#) to 30km/h [around schools](#), residential streets, and town centres
- Use modal filters to stop rat running and make residential streets safer and quieter
- Provide an accessible, safe, and efficient local network of pathways and cycleways throughout the region
- Advocate for a local version of Portland's Bike [Disaster Relief Trials](#) to prepare our communities and equip them to respond during emergencies
- Improve cycle and pathway safety, including lighting, physical traffic barriers, non-slip surfaces
- Improve the local public transport network and bus lane priority (bus lanes are often used by more confident cyclists)
- Support local initiatives to reduce emissions by encouraging more use of public transport (eg. secure bike parking at transport hubs, safe cycleways connecting to public transport hubs)

Ideas for specific local board areas:

In alphabetical order...

Albert-Eden Local Board

- Advocate for delivery of the [New North road and Symonds street upgrade](#)
- Advocate for a safe cycle connection along St Luke's road
- Support having a [community bike hub](#) in or near the Mt Eden area

- Ensure the [Pt Chevalier to Westmere Improvements](#) is delivered

Aotea/Great Barrier Local Board

- Advocate for safe cycle connections from trails to airports and ferry terminals

Devonport-Takapuna Local Board

- Support the campaign to [Liberate the Lane](#) on the Auckland Harbour Bridge for walking, cycling, and wheeling
- Support safer speeds, cycle improvements, and raised pedestrian crossings in [Devonport Village](#)
- Support a protected cycleway along Bayswater Avenue (especially so the kids can safely cycle to school)
- Support a protected cycleway along Lake road to connect Devonport safely to Takapuna!

Franklin Local Board

- Support local trail connections throughout the area
- Advocate for, and support, a safer Pukekohe town centre

Henderson-Massey Local Board

- Support protected cycleways through the Henderson town centre
- Advocate for a safe cycle connection from the Henderson town centre to the North Western pathway
- Advocate for a safe cycle connection from Te Atatū town centre to the North Western pathway
- Advocate for improvements for Twin Streams paths ([Opanuku](#) and [Oratia](#)) – especially where they have been damaged by recent weather

Hibiscus and Bays Local Board

- Advocate for a safe cycle connection from Whangaparāoa town centre to Shakespeare Regional Park
- Advocate for a safe cycle connection from Torbay and Browns Bay to the [Northern Corridor](#)

Howick Local Board

- Support the safe cycleway which is part of the extension of the Eastern Busway project
- Advocate for safe cycleway connections to the Eastern Busway and the cycleway beside it

Kaipātiki Local Board

- Support a safe cycle connection from Upper Harbour Drive to Albany Highway
- Support extension of the [Albany Highway](#) cycleway to connect South to the Glenfield road cycleway as was planned a long time ago!
- Advocate and support the continuation of the [Northern Pathway](#) to Westhaven
- Advocate for safe cycle connections to the Northern Pathway
- Support the campaign to [Liberate the Lane](#) on the Auckland Harbour Bridge for walking, cycling, and wheeling

Māngere-Ōtāhuhu Local Board

- Advocate for the [Waikaraka Cycleway](#) to connect all the way around Manukau Harbour to [Norana Esplanade](#) for greater transport choice (especially for people who live or work in Ōtāhuhu) and for a beautiful scenic ride
- Advocate to make the slippery paths in Norana Esplanade safer for people using bikes!
- Support a safe cycleway connection from Māngere Town Centre and the Airport all the way to Nga Hau Māngere Bridge (connecting schools along the way!)
- Advocate for a safe cycle connection from Ōtāhuhu to Sylvia Park

Manurewa Local Board

- Pop up cycleways, such as using concrete tim tams to make a protected on road cycleway, are a fast and cheap way of creating a safe cycle network. Let this board know whether or not you support pop up cycleways for this area
- Support a safe, protected cycleway along Mahia and Roscommon roads
- Support and deliver a [walking and cycling bridge](#) over Papakura stream to connect Wattle Downs and Waiata Shores
- Advocate for an extension of the Southern Corridor further North to connect to the CBD (eg. along Great South road)

Maungakiekie-Tāmaki Local Board

- Advocate for the [Waikaraka Cycleway](#) to connect all the way around Manukau Harbour to [Norana Esplanade](#) for greater transport choice (especially for people who live or work in Ōtāhuhu) and for a beautiful scenic ride
- Advocate for safe cycle connections from the existing surrounding cycleways through Mt Wellington (eg. Waikaraka Cycleway, Eastern Busway cycleway in Panmure)
- Advocate for a safe cycle connection from Panmure through to the Glen Innes shared path (from which you can ride safely all the way to the CBD!)
- Advocate for a safe cycle network [through Onehunga](#) and connecting to Royal Oak roundabout
- Advocate to make [Royal Oak roundabout safer](#) for everyone (there are some glaring gaps in safety for people riding bikes!)

Ōrākei Local Board

- Advocate for and deliver the Gowing Drive connection to Te Ara ki Uta ki Tai, the Glen Innes to Tāmaki shared path
- Support the completion of [Stage 4 of Te Ara ki Uta ki Tai](#)
- Advocate for a space for local mountain bike tracks and skills equipment
- Advocate to make the slippery boardwalks safer for people using bikes!

Ōtara-Papatoetoe Local Board

- Support delivery of a local bike park
- Advocate for an extension of the Southern Corridor further North to connect to the CBD (eg. along Great South road)

Papakura Local Board

- Pop up cycleways, such as using concrete tim tams to make a protected on road cycleway, are a fast and cheap way of creating a safe cycle network. Let this board know whether or not you support pop up cycleways for this area
- Advocate for safe cycle connections to the Southern Corridor

Puketāpapa Local Board

- Continue to support safe cycle connections throughout the area

Rodney Local Board

- Support a safe cycle connection from the NorthWestern shared path through to the Kumeū town centre

Upper Harbour Local Board

- Advocate for a safe cycleway along [Oteha Valley Road](#)
- Advocate and support the continuation of the [Northern Pathway](#) to Westhaven
- Advocate for safe cycle connections to the Northern Pathway
- Support extension of the [Albany Highway](#) cycleway southwards to connect to the Glenfield road cycleway as was planned a long time ago!
- Support the campaign to [Liberate the Lane](#) on the Auckland Harbour Bridge for walking, cycling, and wheeling

Waiheke Local Board

- Advocate for safe, protected cycleways throughout the island!

Waitākere Ranges Local Board

- Support safe cycle connections to public transport hubs
- Support safe cycle connections through Glen Eden and to the Waikumete stream
- Advocate for a safe cycle connection through to Kelston and New Lynn
- Investigate potential for cycle trails through the Waitākere area

Waitematā Local Board

- Continue to support and implement the [City Centre Masterplan](#)
- Connect [Te Hā Noa](#) up to the rest of the Victoria Street cycleway; under current plans there will be a sad gap of a couple of blocks between them!
- Make sure [the Strand](#) has a safe connection for bikes to cross the bridge over the railway (to Tāmaki Drive / Quay st)
- Support safe walking and cycling connections around the [Karanga-a-hape station](#) and to the rest of the cycleway network
- Ensure the delivery of the [Great North road street improvement project](#)
- Support the [Waitematā Safer Routes](#) to get funded and delivered
- Support the campaign to [Liberate the Lane](#) on the Auckland Harbour Bridge for walking, cycling, and wheeling

Whau Local Board

- Support the completion of the next section of [Te Whau Pathway](#)
- Continuing open streets events through the Avondale town centre (eg. regularly on Sundays when the market is on)
- Support safe cycle connections through New Lynn and Kelston
- Support the [New North road and Symonds street upgrade](#) (it connects along the top of Rosebank road)



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Community Groups Feeding the Homeless

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

See Attached.



Submission of Community Groups Feeding the Homeless

Draft Local Board Plans - all

Name: Audrey van Ryn (Secretary)

Organisation: Community Groups Feeding the Homeless Inc.

Phone (daytime): 021 0354431

Phone (evening): 021 0354431

Email: audrey@writeaway.co.nz

Postal address: 5C, 28 Wellesley Street East, Auckland 1010

The focus of Community Groups Feeding the Homeless (CGFH), which has been in existence for 5 years, is on feeding homeless people and supporting them in other ways. We act as an umbrella group that supports a number of groups, including Sunday Blessings (Waitematā) and Waka of Caring (Manurewa).

Homelessness is an issue that affects all of Auckland. Homeless people are amongst the most vulnerable people in our communities. In our view, local boards could help with community initiatives that support homeless people, and we know that some of them do and we appreciate that. Community Groups Feeding the Homeless (CGFH) has received funding from two local boards to support groups that we provide admin and practical support to, for which we are very grateful. We also appreciate the Local Boards that mention homelessness and/or deprivation and inequity in their plans and those that have key initiatives or advocacy that address homelessness.

We would like to see local boards support members of the community wishing to set up Pātaka Kai (<https://www.patakai.co.nz/>) and that local boards publicise, perhaps in their e-newsletters, the locations of these free community pantries in their area. These pantries have been set up around New Zealand and many of them are in Auckland, though there are none in the city centre or on Aotea-Great Barrier.

We are also aware that some local board areas have community fridges in them, and we would like to see local boards supporting the establishment of more of them.

CGFH supports the roll-out of food scraps bins, and once they are established and people know how to use them, we would like the community to be encouraged to place any edible food that they don't need in a Pātaka Kai or fridge rather than in their food scraps bin.

We would also like to inform Local Board that one of the groups we are associated with, Guru Nanak Free Kitchen, has informed us that all the Sikh gurdwaras throughout Auckland are open 24/7 with food available for homeless people and others in need. In this time of cost of living difficulties and food insecurity, we feel it is important that the community know of such facilities and local boards are one way to inform the community.

We know that many of the libraries and community centres throughout Auckland welcome homeless people and some of them have programmes to support them and/or facilities such as showers that homeless people can use. We thank these places for their valuable support.

We note in the following local board draft plans what could and/or definitely are references to support for homeless people and those who are suffering from food insecurity in key initiatives and advocacy, and we endorse your support:

Henderson-Massey

What we will work on:

“Prioritise investment into communities of greatest need to support initiatives led by the community, for the community” (page 14).

“Work with West Auckland Together to support community-led resilience through the kai sovereignty project, including developing māra kai” (page 18).

“F2. Protect our productive soils and move toward regenerative practices to increase food security and carbon sequestration F3. Prevent and reduce waste and maximise the value of surplus food F4. Increase supply and demand for local, seasonal and low carbon food” (page 12).

Hibiscus and Bays

“Advocate for better connections between the local board, our communities and central government agencies working with our communities to build trust, and better outcomes for all, in key areas of concern such as homelessness, housing, transport and marine fisheries (page 16)”

Franklin

“Fund community resilience programmes that: • facilitate locally-led emergency response and resilience planning • support resident associations to embrace new communities • support organisations helping our most vulnerable” (page 18).

Kaipātiki

Flagship project

“Support community-led low carbon food initiatives such as community gardens, markets, cooking lessons, plant-based meal choices, community fridges and urban farm projects, such as those set out in the Kaipātiki Naturalisation of Parks Service Assessment (page 14)”

“We recognise that our physical, mental, social, and spiritual wellbeing is critical in enabling community connection and engagement. We support Te Whare Tapa Whā, a holistic model developed by Māori health expert Sir Mason Durie, which highlights the need to look after all the different aspects of our lives to be well. We are particularly aware of the importance of looking after our mental health. We use the Mental Health Foundation’s Five Ways to Wellbeing as a framework for the decisions we make – Be Active, Give, Connect, Keep Learning, and Take Notice” (page 16).

“Support community-led low carbon food initiatives that also promote kai sovereignty” (page 22).

Mangere- Otahuhu

“Support food scraps, community gardens and māra kai initiatives to improve food security and sustainable living.”

Manurewa

“the Governing Body to address historical and structural inequities in its budget by investing in our most vulnerable communities to keep pace with current economic trends” (page 22).

“In addition, Manurewa residents are more likely than all Aucklanders to mention food security as a key climate change challenge facing our area” (page 15).

“We recognise our community’s concern about food security. We know that as the effects of climate change increase, such as droughts and extreme weather events, crop yields decrease and food prices rise. Already, the cost-of-living crisis is having significant effects on our community. We’ve started work on locally-led food sovereignty and are initiating ways to strengthen community-led food systems in Manurewa” (pages 15-16).

“Continue to support Auckland Teaching Gardens and support other community food initiatives such as local food markets and community gardens” (page 17).

“Continue to fund locally-led food sovereignty and initiating ways to strengthen community-led food systems in Manurewa” (page 18).

“Work with community partners who are targeting harm reduction associated with smoking, gambling, alcohol, drug use and unhealthy diets (page 22).

Ōtara-Papatoetoe

“Promoting and facilitating access to healthy food options for all, including through community and teaching gardens, are simple ways to improve the well-being of all communities” (page 20).

“Explore partnerships with iwi, marae, community organisations including Ngāti Tamaoho and Ngāti Ōtara Marae to reduce the levels of homelessness” (page 27).

Papakura

“The main issues raised by Māori include food security and having healthy options, the importance of youth pathways (e.g. employment/further education), growing Māori economic prosperity, and the need to enhance the green space at Te Koiwi Reserve” (page 8).

“Support māra kai and food security initiatives” (page 16).

“the support and development of resilient local food systems with access to affordable, appetising food/drink. This would be sustainable, locally produced and culturally appropriate. The goal is food security and food sovereignty for all residents in the Papakura local board area” (page 17).

Waiheke

“Support actions and policies aimed at reducing harm caused by alcohol, smoking, drugs, and gambling, including development of a local alcohol policy” (page 13).

“Support the Waiheke Kai Charter, Piritahi Marae Māra Kai model gardens and other community-led programmes that achieve sustainable and lowcarbon behaviour change and self-sufficiency such as composting, vegetable gardens, renewable energy generation and food waste reduction” (page 17).

Waitākere Ranges

“Investigate opportunities to promote and support kai sovereignty” (page 18).

Waitematā

“The board currently supports Māori outcomes through: • taking action in response to people experiencing homelessness.”

“Engage with our vulnerable and diverse communities, such as people experiencing homelessness, elderly, and ethnic communities to better involve them in the planning and programming decisions that affect them” (page 10).

“Support a network of community gardens and food resilience activities” (page 11).

“Continue to support local communities to implement local composting and grow a local low-carbon, resilient food production system, such as through community gardens” (page 23).

“We will support communities to lead their own solutions. We will maintain our commitment to community driven activities and grants that enable positive outcomes for our people. Community groups, organisations, and delivery partners are working every day to provide services that improve community resilience, connection, and wellbeing. Whether this is for vulnerable communities such as those challenged with homelessness, or our rainbow youth, or our elderly living in high-rise residential towers, we want to ensure our funding reaches across the spectrum and continues to provide benefits. Where we cannot directly support with funding, we

will advocate for involvement and programming from other providers, such as central government, and work with delivery partners across Waitemata, such as Auckland Transport, Eke Panuku and Tātaki Auckland Unlimited, to ensure the community is provided for in programming and initiatives” (page 17).

“Engage with our vulnerable and diverse communities, such as people experiencing homelessness, people with disabilities, youth, elderly, and ethnic communities to better involve them in the planning and programming decisions that affect them” (page 18).

“Advocating to Affordable housing solutions for people experiencing homelessness Advocate to the Governing Body and Central Government for increased support to ensure homelessness becomes ‘rare, brief and non-recurring’ including providing short-, medium- and long-term affordable housing solutions” (page 19).

“Support affordable housing provision by iwi, community housing providers and co-housing Advocate to the Governing Body to actively, and appropriately respond to Auckland’s affordable housing crisis; to prioritise and support affordable housing provision by iwi, community housing providers and cohousing projects through advice, raising bonds, guaranteeing loans, empathetic planning rules and consenting. Governing Body, Eke Panuku Council provision of appropriate affordable housing Council to investigate planning for the provision of appropriate affordable housing, particularly Pensioner Housing, in the Auckland Isthmus, where there is no Council housing. Street outreach for mental health and addiction support. Advocate to Governing Body and Central Government to increase funding for assertive street outreach and mental health and addiction support for our communities sleeping rough” (page 19).

“Advocate to Governing Body and CCO’s to enhance provision of city centre public amenities such as drinking fountains, toilets, showers, and lockers which will support travel mode shift and climate change outcomes providing a rich urban experience for our tourists, visitors, families and elderly, and people experiencing homelessness.” (page 19).

“Support a network of community gardens and food resilience activities” (page 29).

Whau

“Grow capacity and network with partners to provide opportunities for kai sovereignty projects”
(page 18).

“Find new ways to engage with our communities and involve individuals and groups, especially those generally underrepresented, in civic initiatives and democratic processes”

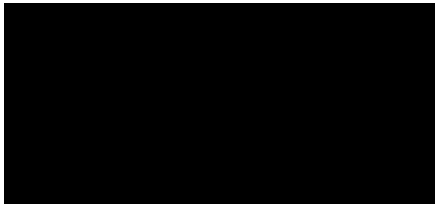
“Positive outcomes for our diverse communities and less prosperous populations (page 13).”

“Improve open spaces, considering public need for shade, shelter, drinking fountains, and toilets” (page 29).

Date of submission:

14 August 2023

Signature:

A large black rectangular redaction box covering the signature area.

Audrey van Ryn,

Secretary,

Community Groups Feeding the Homeless Inc.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Community Cat Coalition Inc.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Tell us why

2. Is there anything else you think should be included in the draft plan?

Tell us why

3. Have we identified the most important opportunities or challenges in our draft plan?

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

Please see attached



secretary@catcoalition.org.nz

PO Box 15903, New Lynn, Auckland 0640

www.catcoalition.org.nz

[Facebook.com/cccauckland](https://www.facebook.com/cccauckland)

The Community Cat Coalition Inc. is committed to the humane management of community (stray, unowned, 'street') cats across Auckland. Established by Bob Kerridge of the SPCA in 2009 to support the work of the many different individuals and groups caring for community cats and working to manage their numbers, CCC is now a registered charity and incorporated society which facilitates and finances around 2500 de-sexings (spay and neuter) of community cats each year, finds responsible homes for as many of these cats as possible, and works to educate the public about de-sexing as the most humane, effective, sustainable and socially-licensed long-term solution to both welfare concerns about too many unwanted cats and biodiversity concerns around cats as predators in urban/suburban areas.

We also work to improve public understanding of the different categories of cat, as defined in the Companion Cat Code under the Animal Welfare Act 1999. The community cats we desex and manage **are not feral** – by definition, feral cats live well away from areas of human habitation, have none of their needs met by people, and their population sizes fluctuate largely independently of companion or community cat populations. Most people never see a feral cat, certainly never in or near Auckland. Community cats are companion cats who have been abandoned, dumped, got lost, are very loosely owned and/or simply the product of people failing to desex their companion animals. They are variably socialised, but by definition they are not 'feral', which in this context does not mean 'hissy' - they have been bred for domesticity and depend on the community to meet their needs. CCC's work is entirely with community cats, not with feral cats. Unlike community and companion cats, feral cats may, in some sparsely populated regions, be defined as pests and be subject to pest management control. Humane community cat management around areas of human habitation (what we do) is an under-funded area of conservation focus and expenditure.

Amongst the introduced animals targeted by biodiversity initiatives, cats are unique in that they are also NZ's most popular companion animal - many people have a great deal of affection for them, and recognise the documented health and life-

enhancing benefits of their companionship, especially for the socially marginalised. The potential for managing community cats as CCC does and seeks to do more of, derives from this historic relationship and regard. In conjunction with initiatives promoting de-sexing of both companion and community cats, TNR, Adoption, and Colony Management programs humanely and effectively address the potential for predation of native fauna by community cats within urban areas, as well as any potential for their recruitment into cat populations within areas identified by Council as ecologically significant.

CCC is seeking recognition by Boards that progress towards the attainment of the biodiversity objectives of their Plans may be advanced by supporting the work of the Community cat Coalition Inc., and/or other groups engaged in this mahi.

Thank you for this opportunity to contribute to the Local Board Plans of 2023.

Community Cat Coalition Inc.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Waitākere Ranges

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not well at all

Tell us why

Restricting and limiting travel and movement and imposing fees and more taxes is not needed. Restructure existing taxes we pay far too many already. But, I don't consent to environmental tax.

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Monorail through motorway specifically to airport, linking to suburbs with way more secure parking facilities at reduced cost. This is council business not just A.T.

3. Have we identified the most important opportunities or challenges in our draft plan?

No

Tell us why

Red tape needs to be cut and responsibility for unsettled flooding cases taken. Council must pay for my costs for family's requirement to pay for a rental on top of a mortgage for a house they are not able to occupy.

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Not well at all

Tell us why

Failed science. I don't see any proof of it existing. Look outside the window in Kelly Tarltons. Water level has not risen. 1st and foremost too many chemicals in aircraft emissions. Audit this publicly.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

No way. Allocate existing rates and cut salaries instead. Rates cost too much already.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows

	Pest (plant and animal) reduction	
	Retrofitting council facilities to be more environmentally sustainable	
	Tree planting	
	Waste minimisation	
✓	Water quality improvement	
	None of the above	
	Other, please specify:	

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Comments on q7. These things should already be sorted currently! On water quality improvement - remove flouride!

Do not penalise classic cars or petrol cars any more than currently! V8s running hydrogen improvements should be compensated via rego and insurance. Stop increasing fees and taxes.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

It reflects the needs of the community as a whole

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

All im interested in as a ratepayer is clean gutters and streets. Rubbish picked up. Enough rubbish bins for rubbish to be deposited and emptied. Thats all.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

I'm not at all interested in aspirations for Maori. We are all one aren't we? Are you people an off-shoot of the Labour Party or Maori Party or Green Party?

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

We don't need as ratepayers more costs loaded upon us. This is just more beurocracy heaped upon us. Ratepayers would have to foot the bill. This sounds like some ones pet project or another bloody "nice to have." Another example of the local board trying

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting

	Waste minimisation	
	Water quality improvement	
✓	None of the above	
✓	Other, please specify:	You people are empire building again and trying to justify yourselves. How crazy are you people. People have enough expenses already.

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Omaruru Reserve. There is a monument, something that looks like a silver tyre that needs pumping up. A Mr Gillon thought it was a great idea. It probably cost the long suffering ratepayer thousands Mr Gillon. I pass this monument twice daily. At this point and time, I have never seen one person standing and looking at it close up. More wasted money. Perhaps the money could have been better spent on something more practical and more cost effective. I don't think the local people are interested. I have never seen anyone showing any interest in this item. I feel the local Kaipatiki Board only want to justify themselves. Perhaps they could think of things like saving money for the long suffering local ratepayers. Just stick to the basics!!!



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

- 1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

Moderate, aspirational yes, does it empower resiliency? Strip away the politics - what is achievable? What can you collectively with others who have the mana do to restore resiliency? I think your team can work closer with grassroots and organisations that have mana e.g. RST's

- 2. Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Transparency, though not really in your domain, economic opportunity and access to housing. Access to green spaces and activities. Connection with young people, diverse communities and elderly post the weather events. Infrastructure plans for population growth. Safety which comes from thriving cohesive communities.

- 3. Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

For the most part yes, I believe so. Advocacy for work opportunities especially in this climate is critical, and transport given the high density of new homes. The structure of local board, community activities are less critical to sustain and less cohesi

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

It's aspirational and an obvious improvement from the previous year with more substance. The 2020 plan has 4 points in regards to Maori. This of course the world had been affected by covid and still in recovery plus economic downturn. 2023 draft report sh

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

Nice aspirational, lofty plans - what are the quick wins that are manageable and realistic now? What are the effects if goals aren't met? Will public be informed if met or not and the barriers?

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Times have certainly changed and our former way of life is not sustainable. The work the local green groups are doing, marakai, green spaces, better water ways and so on is what brings current and new residents here. Also, for food, for our taiao and prot

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
---	------------------------

✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

For q8, comment unsure.

Would like to see updated on community led approach ideas and activity.

Would like to see (i know its out of scope) some support/recognition beyond grassroots of pending poverty and a localised response.

Intergenerational opportunities as mentioned in the draft plan.

Work closer or contract specialists to diversify your plans regarding Asian and Pacific communities. Especially those of us who are long time residents, well experienced and long track record of community service and mana here locally to build and add value to your aspirational plans. Voices from women as well as mothers, grandmas etc will give a different point of view.

Lastly, the community engagement people, who i'm sure are worked closely with, alongside other community leaders would be important to work with. Beyond church leaders for the Pacific - there are many others who hold the mana and insights you need.

Transparency and real visibility.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Not very well

Tell us why

I wanted to see some concept plans for transportation services. Need more recreational spaces in Glenfield, Birkenhead, Birkdale and Beach Haven areas. These suburbs are a bit boring. Look at Botany. They did a fabulous job in creating recreational space. Or look at Catalina Bay in Hobsonville

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Better transport opportunities and ideas. More creative and cultural spaces, recreational spaces for families to get together. Look at Botany or Catalina Bay in Hobsonville.

3. Have we identified the most important opportunities or challenges in our draft plan?

Unsure

Tell us why

Not sure but if AT wants to convert lanes to T2 and T3 then they also need to introduce more buses so people can use them

4. How well have we reflected the aspirations for Māori in our draft plan?

Unsure

Tell us why

I am a recent immigrant and unsure how Maoris can be represented better as I don't understand their culture or values fully yet

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite well

Tell us why

I feel the draft plan adequately covers climate change topic

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

No

Tell us why

Better money management is required

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows

✓	Pest (plant and animal) reduction	
	Retrofitting council facilities to be more environmentally sustainable	
	Tree planting	
	Waste minimisation	
	Water quality improvement	
	None of the above	
✓	Other, please specify:	Creating more recreational facilities for families and community

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Explore with Auckland Transport an option to create a dedicated bus transport service such as Northern Busway. Currently, the T3 & T2 lanes on Lake Rd, Glenfield Rd and Onewa Rd is underutilised. Need to create dedicated bus lanes and increase bus services for Glenfield, Birkenhead, Birkdale and Beach Haven



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?

Quite well

Tell us why

Moderate. Aspirational yes. Does it empower resiliency?

Strip away the politics - what is achievable. What can you collectively with others who have the mana do to restore resiliency? I think your team can work closer with grassroots of organisations that have mana eg RSTs

2. Is there anything else you think should be included in the draft plan?

Yes

Tell us why

Transparency, though not really in your domain.

Economic opportunity and access to funding

Access to green spaces and activities

Connection with young people, diverse communities and elderly post the weather events

Infrastructure plans for population growth

Safety which comes from thriving, cohesive communities

3. Have we identified the most important opportunities or challenges in our draft plan?

Yes

Tell us why

For the most part I believe so.

Advocacy for work opportunities especially in this climate is critical, and transport given the high density of new homes

The structure of local board, community activities will be crucial to sustain and be cohesive with ne

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

It's aspirational and an obvious improvement from the previous year with more substance

The 2020 plans has 4 points in regards to Maori yet of course the world has been affected by covid and still in recovery plus economic downturn.

2023 draft report sho

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

Nice aspirational, lofty plans - what are the quick wins that are manageable and realistic now? What are the effects if goals are not met? Will public be informed if met or not and the barriers?

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Times have certainly changed and our former way of life is not sustainable. The work local green groups are doing, marakai, green spaces, better waterways and so on is what brings current and new residents here. Also, for food, for our Taiao and protector

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

- '- would like to see updates on community led approach ideas and activity
- would like to see (I know it's out of scope) some support / recognition beyond grassroots of pension poverty and a localised response
- intergenerational opportunities as mentioned in the draft plan
- work closer or contract specialists, to diversify your plans re Asian and Pacific communities

Especially those of us who are long-time residents, well experienced and long-track record of community service and mana here locally to build and add value to your aspirational plans.

Voices from women as well - as mothers, grandmas, etc will give a different perspective

Lastly, the community engagement people who I'm sure are worked closely with, alongside other community leaders would be important to work with. Beyond church leaders for the Pacific - there are many others who hold the mana and insights you need.

Transparency and real visibility



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

No

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Devonport-Takapuna

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

✓	Bush track maintenance
✓	Ecological volunteer programme
✓	Flood management
✓	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
✓	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

There is a lack of overall consideration on funding issues (sources) etc.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Extensive collection of information and actionable reports by experts

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
✓	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board:

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board:

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
✓	Waste minimisation
	Water quality improvement
✓	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

Because our cost of living has increased, the public security is poor.

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

Public transport, health care (waiting time too long)

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

Keeping the community going

4. How well have we reflected the aspirations for Māori in our draft plan?

Not very well

Tell us why

Offenders.

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

Education on environmental protection.

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Because it matters our survival and health issues.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation

✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board:

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input type="checkbox"/>	Pest (plant and animal) reduction
<input type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

1. The allocation of community medical resources should be increased. Commonly used electrocardiograms, B-ultrasound, and X-ray machines should be available. 2 To establish entertainment venues for the elderly.

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. How well have we reflected the aspirations for Māori in our draft plan?

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

how to pay attention to the facilities and contact information of the elderly

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$50-\$99 per year (\$0.96-\$1.91 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Unsure

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Unsure

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?

Strengthen law and order measures. Reject discrimination against Chinese. For example, 1. put us back in the queue for medical treatment, 2. Physical examination items that are charged separately for Chinese.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Kaipātiki

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Not very well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Not well at all

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

<input checked="" type="checkbox"/>	Bush track maintenance
<input type="checkbox"/>	Ecological volunteer programme
<input type="checkbox"/>	Flood management
<input type="checkbox"/>	Park naturalisation e.g. butterfly meadows
<input checked="" type="checkbox"/>	Pest (plant and animal) reduction
<input checked="" type="checkbox"/>	Retrofitting council facilities to be more environmentally sustainable
<input type="checkbox"/>	Tree planting
<input type="checkbox"/>	Waste minimisation
<input checked="" type="checkbox"/>	Water quality improvement
<input type="checkbox"/>	None of the above
<input type="checkbox"/>	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?

Strengthen law and order measures. Reject discrimination against Chinese. For example, 1. put us back in the queue for medical treatment.



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board:

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

- 8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?**

\$0-\$49 per year (0-\$0.95 per week)

- 9. Do you have any other feedback on our draft Local Board Plan?**



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board:

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Unsure

Tell us why

Our cost of living has increased, as a result, public security has become worse and worse.

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

Public transport, aged care services, health care (appointments waiting time too long)

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

Ensure that various community activities such as community activities and library opening are carried out in an orderly manner.

4. How well have we reflected the aspirations for Māori in our draft plan?

Not very well

Tell us why

Most of the offenders are teenagers.

5. How well have we reflected the outcomes for climate change in our draft plan?

Unsure

Tell us why

Environmental education remains a cause of climate change

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

Because it is related to the survival, health and daily life of each of us.

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation

✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0 – I wouldn't be willing to pay any more

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Hibiscus and Bays

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Very Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Unsure

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Unsure

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Quite Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Very Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
✓	Flood management
	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

\$0-\$49 per year (0-\$0.95 per week)

9. Do you have any other feedback on our draft Local Board Plan?



KAIPĀTIKI Local Board Plan

Note: this version of the feedback form has been created for the purpose of publishing submissions. As such, contact and demographic information has been removed and handwritten submissions have been transcribed.

Local Board: Upper Harbour

Your feedback

1. **How well do you think our draft plan reflects the needs and aspirations for our community over the next three years?**

Quite Well

Tell us why

2. **Is there anything else you think should be included in the draft plan?**

Yes

Tell us why

3. **Have we identified the most important opportunities or challenges in our draft plan?**

Yes

Tell us why

4. **How well have we reflected the aspirations for Māori in our draft plan?**

Very Well

Tell us why

5. How well have we reflected the outcomes for climate change in our draft plan?

Quite Well

Tell us why

6. Do you support us investigating the use of a local targeted rate to secure dedicated funding for protecting and enhancing the natural environment in Kaipātiki?

Yes

Tell us why

7. A possible local targeted rate to protect or enhance the natural environment in Kaipātiki could cover a few areas. Please pick the area(s) where you would most want to see investment secured for:

	Bush track maintenance
	Ecological volunteer programme
	Flood management
✓	Park naturalisation e.g. butterfly meadows
	Pest (plant and animal) reduction
	Retrofitting council facilities to be more environmentally sustainable
✓	Tree planting
	Waste minimisation
✓	Water quality improvement
	None of the above
✓	Other, please specify:

8. If we were to introduce a local targeted rate to protect or enhance the natural environment in Kaipātiki, how much would you be willing to pay annually on top of your rates bill?

9. Do you have any other feedback on our draft Local Board Plan?

There is no large park in Pinehill Auckland for people to relax. It is suggested that the garbage mountain on Grevile Rd be turned into a park.