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I hereby give notice that a hearing by commissioners will be held on:

**Date:** Monday 18 to Thursday 21 September 2023  
Monday 25 to Thursday 28 September 2023  
Monday 2 to Thursday 5 October 2023  
Monday 9 to Thursday 12 October 2023

**Time:** 9:30am

**Meeting room:** Henderson Council Chamber

**Venue:** 3 Smythe Road, Henderson  
(25 & 26 September in Waimauku)

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## **SUBMISSIONS – VOLUME FIVE**

### **LATE SUBMISSIONS**

## **19 NOTICE OF REQUIREMENTS FOR THE NORTH WEST**

## **THE SUPPORTING GROWTH ALLIANCE (AUCKLAND TRANSPORT AND WAKA KOTAHI NZ TRANSPORT AGENCY)**

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### **COMMISSIONERS**

**Chairperson** Richard Blakey  
**Commissioners** Mark Farnsworth  
Vaughan Smith

**Patrice Baillargeon**  
**KAITOHUTOHU MATAAMUA WHAKAWĀ/  
SENIOR HEARINGS ADVISOR**

Telephone: 09 890 4692 or 027 338 5383  
Email: [patrice.baillargeon@@aucklandcouncil.govt.nz](mailto:patrice.baillargeon@@aucklandcouncil.govt.nz)  
Website: [www.aucklandcouncil.govt.nz](http://www.aucklandcouncil.govt.nz)

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**Note:** The reports contained within this document are for consideration and should not be construed as a decision of Council. Should Commissioners require further information relating to any reports, please contact the Hearings Advisor.

## **WHAT HAPPENS AT A HEARING**

### **Te Reo Māori and Sign Language Interpretation**

Any party intending to give evidence in Māori or NZ sign language should advise the hearings advisor at least ten working days before the hearing so a qualified interpreter can be arranged.

### **Hearing Schedule**

If you would like to appear at the hearing please return the appearance form to the hearings advisor by the date requested. A schedule will be prepared approximately one week before the hearing with speaking slots for those who have returned the appearance form. If changes need to be made to the schedule the hearings advisor will advise you of the changes.

Please note: during the course of the hearing changing circumstances may mean the proposed schedule may run ahead or behind time.

### **Cross Examination**

No cross examination by the requiring authority or submitters is allowed at the hearing. Only the hearing commissioners are able to ask questions of the requiring authority or submitters. Attendees may suggest questions to the commissioners and they will decide whether or not to ask them.

### **The Hearing Procedure**

The usual procedure for a hearing is:

- **the chairperson** will introduce the commissioners and will briefly outline the hearing procedure. The Chairperson may then call upon the parties present to introduce themselves. The Chairperson is addressed as Madam Chair or Mr Chairman.
- **the Requiring Authority** (the applicant) will be called upon to present their case. The Requiring Authority may be represented by legal counsel or consultants and may call witnesses in support of the application. After the Requiring Authority has presented their case, members of the hearing panel may ask questions to clarify the information presented.
- **Submitters** (for and against the application) are then called upon to speak. Submitters' active participation in the hearing process is completed after the presentation of their evidence so ensure you tell the hearing panel everything you want them to know during your presentation time. Submitters may be represented by legal counsel or consultants and may call witnesses on their behalf. The hearing panel may then question each speaker.
  - Late submissions: The council officer's report will identify submissions received outside of the submission period. At the hearing, late submitters may be asked to address the panel on why their submission should be accepted. Late submitters can speak only if the hearing panel accepts the late submission.
  - Should you wish to present written evidence in support of your submission please ensure you provide the number of copies indicated in the notification letter.
- **Council Officers** will then have the opportunity to clarify their position and provide any comments based on what they have heard at the hearing.
- The **requiring authority** or their representative then has the right to summarise the application and reply to matters raised. Hearing panel members may ask further questions. The requiring authority's reply may be provided in writing after the hearing has adjourned.
- **The chairperson** will outline the next steps in the process and adjourn or close the hearing.
- **The hearing panel** will make a recommendation to the Requiring Authority. The Requiring Authority then has 30 working days to make a decision and inform council of that decision. You will be informed in writing of the Requiring Authority's decision, the reasons for it and what your appeal rights are.

**19 NOTIFIED NOTICE OF REQUIREMENTS TO THE AUCKLAND COUNCIL UNITARY PLAN BY THE SUPPORTING GROWTH ALLIANCE (AUCKLAND TRANSPORT AND WAKA KOTAHI NZ TRANSPORT AGENCY)**

**VOLUME FIVE – LATE SUBMISSIONS**

**W5 - NORTH WEST LOCAL NETWORK: ALTERATION TO DESIGNATION 1437 HOBSONVILLE ROAD (AUCKLAND TRANSPORT)**

Notice of requirement lodged by Auckland Transport for an alteration of the existing Hobsonville Road designation 1437 to provide for the widening of the Hobsonville Road corridor between Oriel Avenue and Memorial Park Lane, including provision of separated active mode facilities.

Project W5 in North West Local Network package lodged by Te Tupu Ngātahi Supporting Growth Alliance. The North West Local transport projects are eight upgrades to existing roads in Whenuapai and Red Hills. The Supporting Growth Alliance has lodged six Notices of Requirement for designations with Auckland Council for route protection which are to be constructed at a future date.

The works described for the Projects could be carried out in stages as urban development occurs surrounding the Project area.

**LATE SUBMITTERS**

Page 5-6	Ross Thomas
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**S2 - NORTH WEST STRATEGIC NETWORK: ALTERATION TO DESIGNATION 6766 STATE HIGHWAY 16 MAIN ROAD UPGRADE (WAKA KOTAHI NZTA)**

Notice of requirement lodged by Waka Kotahi to alter Designation 6766 to provide for the upgrade of the corridor, including provision of active mode facilities and realignment of the Station Road intersection with SH16.

Project Highway 16 Main Road Upgrade (“S2”) in North West Strategic Network package lodged by Te Tupu Ngātahi Supporting Growth Alliance. The North West Strategic are six individual transport projects in Auckland located in Whenuapai, Kumeū, Huapai and Red Hills. The Supporting Growth Alliance has lodged six Notices of Requirement for designations with Auckland Council for route protection. These projects include an Alternative State Highway, an upgrade to the current State Highway 16, and a new Rapid Transit Corridor with two new Rapid Transit Stations. Access Road is also being upgraded.

The works described for the Projects could be carried out in stages as urban development occurs surrounding the Project area.

**LATE SUBMITTER**

Page 7-48	Kumeu Central Limited
Page 49-78	Tahua Partners Limited

**W1 - NORTH WEST LOCAL NETWORK: TRIG ROAD (AUCKLAND TRANSPORT)**

Notice of requirement lodged by Auckland Transport for a new designation, for the upgrade of the Trig Road corridor to an urban arterial with separated active mode facilities.

Project W1 in North West Local Network package lodged by Te Tupu Ngātahi Supporting Growth Alliance. The North West Local transport projects are eight upgrades to existing roads in Whenuapai and Red Hills. The Supporting Growth Alliance has lodged six Notices of Requirement for designations with Auckland Council for route protection which are to be constructed at a future date.

The works described for the Projects could be carried out in stages as urban development occurs surrounding the Project area.

**LATE SUBMITTERS**

Page 79-84

Yvonne and Gayo Vodanovich



**From:** [NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz](mailto:NoticeOfRequirementOnlineSubmissionForm@donotreply.aucklandcouncil.govt.nz)  
**To:** [Unitary Plan](#)  
**Subject:** [ID:808] Notice of Requirement online submission - Ross Thomas  
**Date:** Tuesday, 30 May 2023 8:00:08 pm

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The following customer has submitted a Notice of Requirement online submission.

### **Contact details**

Full name of submitter: Ross Thomas

Organisation name:

Full name of your agent: Ross Thomas

Email address: ross5thomas@gmail.com

Contact phone number: +64274128428

Postal address:  
8 HOBSONVILLE RD  
WESTHARBOUR  
AUCKLAND 0618

### **Submission details**

Name of requiring authority: Auckland Transport

The designation or alteration: Alteration to designation 1437 Hobsonville Road (NoR W5)

The specific provisions that my submission relates to are:

Time frame and lack of due process relating to acquirement of the property.

Do you support or oppose the Notice of Requirement? I or we oppose the Notice of Requirement.

The reason for my or our views are:

We are in the middle of a development and are working to a plan that started over two years ago. We have Resource Consent and are about to lodge Building Consent for two additional houses and a subdivision.

I or we seek the following recommendation or decision from Auckland Council:

I need an agreement to either complete development if the timeline is long or agreement to purchase at a price that will put us in the same position.

Submission date: 30 May 2023

### **Attend a hearing**

Do you wish to be heard in support of your submission? Yes

Would you consider presenting a joint case at a hearing if others have made a similar submission?

No

### **Declaration**

I accept and agree that:

- by taking part in this public submission process that my submission (including personal details, names and addresses) will be made public,
- I or we must serve a copy of the submission on the person who gave the notice of requirement as soon as reasonably practicable after submitting to Auckland Council.



**Form 21**  
**Submission on requirement for alteration of a designation for State Highway 16 Main Road Upgrades**  
**(NOR S2) that is subject to notification**  
*Section 169 of Resource Management Act 1991*

To: Auckland Council

### SUBMITTER DETAILS

**Name of Submitter:** Kumeu Central Limited (KCL)

1. Kumeu Central Limited makes this submission on the North-West Strategic Network: State Highway 16 Main Road upgrade NOR S2 ("**NOR S2**") lodged by Waka Kotahi New Zealand Transport Agency (**NZTA**) to the Auckland Unitary Plan - Operative in Part in accordance with Section 169 of the Resource Management Act 1991 (**RMA**) as follows.
2. KCL could not gain advantage in trade competition through this submission.
3. KCL **wishes to be heard** in support of their submission.
4. If any other submitters make a similar submission, KCL will consider presenting a joint case with them at the hearing.

### OVERVIEW OF KCL

5. KCL owns the land located on the corner of Putaki Drive and Main Road Kumeu. The property extends through to adjoin Papatupu Way. The address is 102 – 104 Main Road Kumeu. The land contains a Burger King and associated drive through activity; an Anytime Fitness; two commercial office tenancies, a medical centre, dog grooming business and an Indian restaurant. A copy of the consented plans are **Attachment A**.
6. KCL has an interest in NOR S2 that is greater than the interest of the public generally because they own and have developed the land.

### SCOPE OF SUBMISSION

7. This submission relates to NOR S2 as a whole but specifically the portion of the Notice of Requirement (**NOR**) showing a proposed designation over the front portion of the KCL Land. NOR S2 seeks to alter the existing designation applying to State Highway 16 Main Road Kumeu to enable Main Road to be upgraded to a 24m wide urban corridor with provision for two lanes

(one each direction) with walking and cycling facilities. A copy of the General Arrangement Plan for the NOR is **Attachment B**.

8. KCKL supports in principle the need to upgrade roads and provide for various transport modes. However, KCL opposes NOR S2 in terms of the impacts on its land which are not justified.
9. **Attachment C** is a Technical Note prepared by Flow Transportation Specialists. This Note explains the existing Main Road designation #6768 existing over the frontage of the property in the location of the car parks fronting Main Road Kumeu. The proposed designation extends a significant distance further into the site; extending past the existing vehicle entrance off Putaki Drive that is also the entrance to the Burger King drive through.
10. The extent of designation proposed in the NOR would prevent access from Putaki Street and restrict circulation on the site. The only remaining vehicle access would be from Harikoa Street.
11. 70 – 80% of the Burger King business is via the drive through. The proposed designation renders the drive through unusable and consequently will have catastrophic impacts for the Burger King business.
12. There is insufficient information or detail provided in the NOR to demonstrate that the proposal represents an outcome with an acceptable level of impact on the local environment.
13. KCL considers there are alternative options that would better achieve the outcomes sought and have lesser impacts on the site and business operations.
14. The NOR as it impacts the KCL site, is not in keeping with the relevant objectives and policies in the Unitary Plan relating to infrastructure namely the proposed infrastructure is adversely impacting on established businesses in a manner that will render the businesses unviable. This part of the infrastructure will not contribute to the economy of Auckland in a positive manner. There is insufficient information, and evaluation of alternatives to make an informed decision that the NOR represents the best, or only solution for this location.

## RELIEF SOUGHT

15. KCL seek that Auckland Council recommend NOR S2 be refused, specifically in relation to the impacts on the land and established activities on KCL land.
  - a. Any other relief required to achieve the outcomes sought in this submission.

Yours sincerely



Burnette O'Connor  
Director | Planner  
The Planning Collective Limited

Date: 16 August 2023

**Address for Service:**

The Planning Collective Limited  
Attn: Burnette O'Connor  
PO Box 591  
Warkworth  
0941  
Ph: 021-422-346  
Email: [burnette@thepec.co.nz](mailto:burnette@thepec.co.nz)

**Attachments**

- A** – LUC60339040 Decision and Approved Plans
- B** - NOR S2 general arrangement plans – Sheet 2 of 8 and Sheet 3 of 8
- C** – Technical Note prepared by Flow Transport Specialists

## Attachment A:

LUC60339040 Decision and Approved Plans



# Decision on an application for resource consent under the Resource Management Act 1991



## Discretionary activity

**Application number:** LUC60339040  
**Applicant:** Matvin Group Limited  
**Site address:** 102-104 Main Road Kumeu  
**Legal description:** Lot 3 DP 81928 & Lot 4 DP 81928  
**Proposal:**

To construct two commercial buildings for a number of tenancies including a drive through restaurant, and associated signage and earthworks, 57 carparking spaces, bicycle parking, and vehicle crossings, servicing, landscaping.

Resource consent is required for the following reasons:

## Land use consent (s9) – LUC60339040

### Auckland Unitary Plan (Operative in part)

#### **District land use (operative plan provisions)**

##### *Stormwater Management Area – Flow 2*

- E10.4.1(A4) Development of new or redevelopment of existing impervious areas greater than 50m<sup>2</sup> within Stormwater management area control – Flow 1 (being 1758m<sup>2</sup>) that do not comply with Standard E10.6.1 or Standard E10.6.4.1 - **Discretionary activity.**

##### *Land Disturbance – District Rules*

- E12.4.1(A6) Land disturbance greater than 2,500m<sup>2</sup> (being 3,345m<sup>2</sup>)- **Restricted discretionary activity.**

##### *Signage*

- E23.4.2(A53) Comprehensive Development Signage - **Restricted Discretionary activity.**

##### *Business – Town Centre Zone*

- H10.4.1(A9) The establishment of a drive through restaurant - **Restricted Discretionary activity.**
- H10.4.1(A35) Any new buildings - **Restricted Discretionary activity.**



## *Kumeū Precinct*

- I516.4.3(A32) Retail greater than 600m<sup>2</sup> of GFA, with 786m<sup>2</sup> proposed - **Restricted Discretionary activity.**
- I516.4.3(A40) and I516.6.4.5(A46) New buildings - **Restricted Discretionary activity.**
- I516.6.4.5(A48) Earthworks over 500m<sup>2</sup>, with 3,345m<sup>2</sup> - **Restricted Discretionary activity**

The development fails to meet the following core standards and under rule C.1.9(2) - **Restricted Discretionary activity.**

- The plinth sign on the New World Road has a total height of 5m and is considered to be a building. This sign encroaches into the 3m front yard setback as it is placed up to the boundary which is required by I516.6.4.
- Glazing at the ground floor does not consist of 70% as required by I516.6.6(a) with 69% for Putaki Drive and 64% (Papatupu Way) proposed. The building façade for the New World road is set back more than 5m (refer to the below) and therefore does not adjoin the road frontage.
- For the third street frontage, the building is set back more than 5m as required by I516.6.6(b), with an outdoor seating for the food and beverage tenancy provided instead, in accordance with I516.6.6(c).

The reasons for consent are considered together as a **discretionary** activity overall.

I have read the application, supporting documents, and the report and recommendations on the application for resource consent. I am satisfied that I have adequate information to consider the matters required by the Resource Management Act 1991 (RMA) and make a decision under delegated authority on the application.

Acting under delegated authority, under sections 104, 104B and Part 2 of the RMA, the resource consent is **GRANTED**.

## **Reasons**

The reasons for this decision are:

1. In accordance with an assessment under ss104(1)(a) and (ab) of the RMA, the actual and potential effects from the proposal will be acceptable as:
  - a. The layout of the site has been well considered and the buildings are accessible on all sides and appropriate uses and design ensure useful and effective overlook and safety of the car parking area, while vehicle and pedestrian circulation is well considered.
  - b. The scale of the built form is acceptable and will provide a rhythm along the street which will assist in supporting a strong sense of enclosure to the Putaki Drive western edge.
  - c. A strong retail frontage is on all sides. Overall street amenity is concluded to be acceptable, and shelter from weather is adequately provided by a continuous verandah over the footpath.

- d. Landscaping helps define the edge of the Main Road (SH16) and will visually help narrow this section of road, which can assist in slowing traffic and protecting the shared space from general vehicle movements.
  - e. The signage is proportioned and will complement the scale of the building.
  - f. The provision of car parking and access is sufficient to meet anticipated demand, and the proposed activity is not considered to impact on the wider transport network which would adversely affect the wider environment.
  - g. The subject site contains no flood plains or overland flow paths, and all works to clear and prepare the site were consented in the underlying subdivision consent. Adverse effects of natural hazards on the environment are less than minor.
  - h. Stormwater devices will be installed to manage flows and stormwater quality meet Council standards, so that adverse effects on in this regard are less than minor.
  - i. Connections are to be provided to reticulated wastewater and water systems with sufficient capacity to service the development and meet council standards. Overall, serving effects are less than minor.
  - j. There are no known sites of cultural significance on the subject site, and no significant changes to the landform, water systems, or native vegetation are proposed. No issues raised by iwi. Adverse cultural and heritage effects will be less than minor, and no iwi groups are considered to be adversely affected.
  - k. In terms of positive effects, office space, food and fitness outlets are provided by the proposal, providing a range of employment and service opportunities for the area.
  - l. With reference to s104(1)(ab), there are no specific offsetting or environmental compensation measures proposed or agreed to by the applicant to ensure positive effects on the environment.
2. In accordance with an assessment under s104(1)(b) of the RMA the proposal is consistent with the relevant statutory documents, in particular the following relevant objectives and policies:
- a. Stormwater Management Area: E10.2(1), E10.3(1), E10.3(2), E10.3(3) which seeks to protect high value rivers, streams and aquatic biodiversity from further adverse effects of stormwater runoff, and where possible enhance these waterways.
 

The stormwater system has been designed in line with the Kumeū Huapai Network Discharge Consent requirements. In terms of stormwater quality treatment, a Stormwater 360 stormfilter proprietary device meeting Council standards will be installed to treat the water from the parking and access areas.
  - b. Land Disturbance – District: E12.2(1), E12.3(2), E12.3(3), E12.3(4), E12.3(5), E12.3(6) which seek to manage land disturbance so that the safety of people is protected, and potential adverse effects on the environment are avoided, remedied or mitigated.
 

The subject site contains no flood plains or overland flow paths, and all earthworks to clear and prepare the site were consented in the underlying subdivision consent. The scale of the proposed earthworks, being a total of 710m<sup>3</sup> of cut over an area of 3,345m<sup>2</sup> is considered reasonable given that there are no natural hazards on site and the works will be carried out in accordance with Auckland Council GD05.
  - c. Signage - E23.2(1), E23.2(2), E23.3(1), E23.3(2), E23.3(3), E23.3(4), E23.3(5) which seek to manage billboards and comprehensive development signage to contribute to the social

and economic well-being of communities through identifying places, providing information; including for convenience and safety purposes, and advertising goods and services, and maintaining traffic and pedestrian safety, historic heritage values and the visual amenity values of buildings and the surrounding environment.

The signage has been designed to cater for visual amenity in the surrounding environment, being of an appropriate scale when considered cumulatively, as well as providing for traffic safety.

- d. Town Centre Zone: H10.2(1)- (9), H10.3(1), H10.3(3)- (7), H10.3(9)- (19) which seek to create a strong network of centres that are of an appropriate design, form and scale to provide for the communities economic and social needs and provide a good urban design outcome.

The application is consistent with these objectives and policies in terms of site development, built form and the activities proposed. The application is appropriate in this location and will assist in consolidating the town centre.

- e. Kumeu Precinct: I516.2(1)- (3), I516.2(5)- (12), I516.3(1)- (2), I516.3(4), I516.3(7)- (12) which seek a comprehensively designed and co-ordinated town centre providing a range of activities and recognising the presence of the Kumeu River, the adjoining State Highway and the potential for reverse sensitivity. The provisions also refer to managing stormwater flows, safe and efficient transportation options and connections and ensuring good urban design principles are implemented.

The application is consistent with the underlying CDP, which ensures an integrated and co-ordinated development of the wider area. Stormwater management is in accordance with higher level documents and expectations in this location. The design of the development, including end uses of the building, ensure that a good urban design outcome is achieved.

The application does not include any residential component, but nonetheless provides for a mixture of activities that will be busy at different times of the day, contributing to a vibrant centre.

3. In accordance with an assessment under s104(1)(c) of the RMA, no other matters are considered relevant.
4. In the context of this discretionary activity application for land use, where the objectives and policies of the relevant statutory documents were prepared having regard to Part 2 of the RMA, they capture all relevant planning considerations and contain a coherent set of policies designed to achieve clear environmental outcomes. They also provide a clear framework for assessing all relevant potential effects and there is no need to go beyond these provisions and look to Part 2 in making this decision as an assessment against Part 2 would not add anything to the evaluative exercise.

## Conditions

Under sections 108 and 108AA of the RMA, this consent is subject to the following conditions:

1. This consent shall be carried out in accordance with the documents and drawings and all supporting additional information submitted with the application, detailed below, and all referenced by the council as resource consent number LUC60339040.

- Application Form and Assessment of Environmental Effects prepared by Nick Rae of Transurban Limited, dated 16/5/19.

Report title and reference	Author	Rev	Dated
Engineering Infrastructure Report, Ref. 40421	Paul Culley, MSC Consulting Ltd.	Final	10/05/19
Traffic Impact Assessment	K.S. Bell & S. Goodwin Traffic Engineering & Management Ltd	Final	10/05/19
Urban Design Assessment	Transurban Limited	0	13/05/19

Drawing title and reference	Author	Rev	Dated
Landscape Planting Plan, LA-01	Shafer Design	02	09/05/19
Site Locality Plan, Project Number 19001 Drawing No. 100-00	Gel Architects	A	16/05/19
Site Plan, Project Number 19001 Drawing No. 100-01	Gel Architects	B	28/06/19
Building 01 Floor Plans, Project Number 19001 Drawing No. 200-01	Gel Architects	B	28/06/19
Building 02 Floor Plans, Project Number 19001 Drawing No. 200-02	Gel Architects	A	16/05/19
Roof Plan - Building 1, Project Number 19001 Drawing No. 200-03	Gel Architects	A	16/05/19
Roof Plan - Building 2, Project Number 19001 Drawing No. 200-04	Gel Architects	A	16/05/19
Roof Plan - Building 2, Project Number 19001 Drawing No. 200-05	Gel Architects	B	19/06/19
Elevations 1, Project Number 19001 Drawing No. 300-01	Gel Architects	B	19/06/19
Elevations 2, Project Number 19001 Drawing No. 300-02	Gel Architects	B	19/06/19
Elevations 3, Project Number 19001 Drawing No. 300-03	Gel Architects	B	19/06/19
Elevations 4, Project Number 19001 Drawing No. 300-04	Gel Architects	B	19/06/19

External Cladding & Signage Plinth, Project Number 19001 Drawing No. 300-05	Gel Architects	1	16/05/19
Section 1, Project Number 19001 Drawing No. 300-06	Gel Architects	A	16/05/19
Perspective View 1, Project Number 19001 Drawing No. 400-01	Gel Architects	A	16/05/19
Perspective View 3, Project Number 19001 Drawing No. 400-02	Gel Architects	A	16/05/19
Perspective View 3, Project Number 19001 Drawing No. 400-03	Gel Architects	A	16/05/19
Other additional information	Author	Rev	Dated
E-mail, ref 104 Main Road	James McQuaid, Matvin	-	2/7/19
E-mail, ref 104 Main Road	James McQuaid, Matvin	-	28/6/19
E-mail, ref 104 Main Road- WSL approval in principle	James McQuaid, Matvin	-	28/6/19
E-mail, ref: 104 Main Road, Kumeu - LUC60339040	James McQuaid, Matvin	-	13/6/19
Letter, ref Network Utility Operator	Auckland Council (Paula Vincent)	-	8/7/19

2. Under section 125 of the RMA, this consent lapses five years after the date it is granted unless:
  - a. The consent is given effect to; or
  - b. The council extends the period after which the consent lapses.
3. The consent holder shall pay the council an initial consent compliance monitoring charge of \$675 (inclusive of GST), plus any further monitoring charge or charges to recover the actual and reasonable costs incurred to ensure compliance with the conditions attached to this consent.

**Advice note:**

*The initial monitoring deposit is to cover the cost of inspecting the site, carrying out tests, reviewing conditions, updating files, etc., all being work to ensure compliance with the resource consent. In order to recover actual and reasonable costs, monitoring of conditions, in excess of those covered by the deposit, shall be charged at the relevant*

*hourly rate applicable at the time. The consent holder will be advised of the further monitoring charge. Only after all conditions of the resource consent have been met, will the council issue a letter confirming compliance on request of the consent holder.*

4. All earthworks shall be specifically designed to the “Standards for Engineering Design and Construction” and NZS 4431 by a Chartered Professional Engineer experienced in soil mechanics.
5. Prior to the commencement of earthworks activity, all required erosion and sediment control measures on the subject site shall be constructed and carried out in accordance with Auckland Council publication GD05.
6. All earthworks shall be managed to ensure that no debris, soil, silt, sediment or sediment-laden water is discharged beyond the subject site to either land, stormwater drainage systems, watercourses or receiving waters. In the event that a discharge occurs, works shall cease immediately and the discharge shall be mitigated and/or rectified to the satisfaction of the Council’s Team Leader, Compliance Monitoring NW.
7. There shall be no deposition of earth, mud, dirt or other debris on any public road or footpath resulting from earthworks activity on the subject site. In the event that such deposition does occur, it shall immediately be removed. In no instance shall roads or footpaths be washed down with water without appropriate erosion and sediment control measures in place to prevent contamination of the stormwater drainage system, watercourses or receiving waters.
8. Prior to the commencement of the earthworks activity, the consent holder shall obtain the approval of Auckland Transport for a Temporary Traffic Management Plan (TMP) specifically addressing control of construction access to the site and traffic control adjacent to the site, and the protection of the public. No earthworks activity shall commence until a copy of the approved TMP is provided to the Council’s Team Leader, Compliance Monitoring, NW. A copy of the Traffic Management Plan, together with the verification letter shall be kept on the site at all times. All measures for the protection of the public and other personnel set out in the verified Plan shall be maintained and complied with at all times until such time as the works are completed.
9. All earthworks activity on the subject site shall comply with the New Zealand Standard 6803:1999 for Acoustics – Construction Noise.
10. There shall be no obstruction of access to public footpaths, berms, private properties, public services/utilities, or public reserves resulting from the earthworks activity. All materials and equipment shall be stored within the subject site’s boundaries.
11. There shall be no airborne or deposited dust beyond the subject site as a result of the earthworks activity that in the opinion of the Council is noxious, offensive or objectionable.
12. There shall be no damage to public roads, footpaths, berms, kerbs, drains, reserves or other public asset as a result of the earthworks activity. In the event that such damage does occur, the Team Leader Compliance Monitoring NW is to be notified within 24 hours of its discovery. The costs of rectifying such damage and restoring the asset to its original condition will be met by the consent holder.

13. Prior to the commencement of trade all access, parking and manoeuvring areas shall be formed, sealed with an all-weather surface, marked out, sign posted and drained in accordance with Council Standards to the satisfaction of the Council's Team Leader, Compliance Monitoring NW1.
14. Prior to lodging a building consent application, the consent holder shall provide a final landscape plan for the proposal for the written certification of the Council's Team Leader, Compliance Monitoring NW. The plans shall include:
  - o Pavement plans and specifications detailing materiality, finish and colour throughout the development site, including access and manoeuvring areas
  - o Location and design of any structures, furnishings, sculptures, planters and boundary fencing
  - o Details of all tree and plant species with their sizes, locations and spacing, including specific maintenance for a 5 year period.
  - o Lighting plan (including any lighting of signage). No flashing signage shall be permitted.

Once approved, this plan shall be implemented (with planting occurring no later than the planting season following the completion of construction) and maintained thereafter.

15. Prior to the erection or installation of any signage, the consent holder shall provide a signage plan containing details of all signs to be erected or placed on the building or within the site. The plan shall be submitted for the certification of Council's Team Leader, Compliance Monitoring NW.

The two 3.7sqm Type 3 backlit signs will need to be of a high quality and use cut back lit lettering to complement the vertical pre-cast concrete panels and avoid a dominant light box stuck to the panels.

Once certified, any signage shall be erected or installed in accordance with this plan and be maintained.

16. No film or signs, including opaque film, shall be placed on retail windows that would cover more than 25% of the glazed area, and no screens or appliances of any form will be placed within .5m of the window pane.

## Advice notes

1. *Any reference to number of days within this decision refers to working days as defined in s2 of the RMA.*
2. *All activities on the site shall meet noise rules and Standards of the AUP(OP) or a separate resource consent will be required.*
3. *For the purpose of compliance with the conditions of consent, "the council" refers to the council's monitoring inspector unless otherwise specified. Please email [monitoring@aucklandcouncil.govt.nz](mailto:monitoring@aucklandcouncil.govt.nz) to identify your allocated officer.*
4. *For more information on the resource consent process with Auckland Council see the council's website: [www.aucklandcouncil.govt.nz](http://www.aucklandcouncil.govt.nz). General information on resource*

consents, including making an application to vary or cancel consent conditions can be found on the Ministry for the Environment's website: [www.mfe.govt.nz](http://www.mfe.govt.nz).

5. *If you disagree with any of the above conditions, and/or disagree with the additional charges relating to the processing of the application(s), you have a right of objection pursuant to sections 357A and/or 357B of the Resource Management Act 1991. Any objection must be made in writing to the council within 15 working days of your receipt of this decision (for s357A) or receipt of the council invoice (for s357B).*
6. *The consent holder is responsible for obtaining all other necessary consents, permits, and licences, including those under the Building Act 2004, and the Heritage New Zealand Pouhere Taonga Act 2014. This consent does not remove the need to comply with all other applicable Acts (including the Property Law Act 2007 and the Health and Safety at Work Act 2015), regulations, relevant Bylaws, and rules of law. This consent does not constitute building consent approval. Please check whether a building consent is required under the Building Act 2004.*
7. *Details of the off-line configuration bypass for the stormwater filtration shall be provided following the detailed design process at the building consent stage.*
8. *A drainage plan that shows the location of the stormwater treatment device clearly marking the area of the site for which it drains shall be provided in the building consent stage.*

Delegated decision maker:

Name: Helen McCabe

Title: Team Leader, Resource Consents

Signed:



Date: 12 July 2019





ARTIST IMPRESSION ONLY REFER TO DRAWINGS FOR ALL BUILDING INFORMATION

PROJECT NUMBER 19001  
 DATE 16/05/2019

Drawing Set  
 RESOURCE CONSENT  
 Revision A



**DRAWING LIST**

<b>00 OVERVIEW</b>	
00-01	COVERSHEET
<b>100 SITE PLANS</b>	
100-00	SITE LOCALITY PLAN
100-01	SITE PLAN
<b>200 PLANS</b>	
200-01	BUILDING 01 FLOOR PLANS
200-02	BUILDING 02 FLOOR PLANS
200-03	ROOF PLAN - BUILDING 1
200-04	ROOF PLAN - BUILDING 2
<b>300 ELEVATIONS &amp; SECTIONS</b>	
300-01	ELEVATIONS 1
300-02	ELEVATIONS 2
300-03	ELEVATIONS 3
300-04	ELEVATIONS 4
300-05	EXTERNAL CLADDING & SIGNAGE PLINTH
300-06	SECTION 1
<b>400 3D IMAGES</b>	
400-01	PERSPECTIVE VIEW 1
400-02	PERSPECTIVE VIEW 3

**PROJECT TEAM**

Architects  
**GEL ARCHITECTS**  
 Tel: 09 388 4403

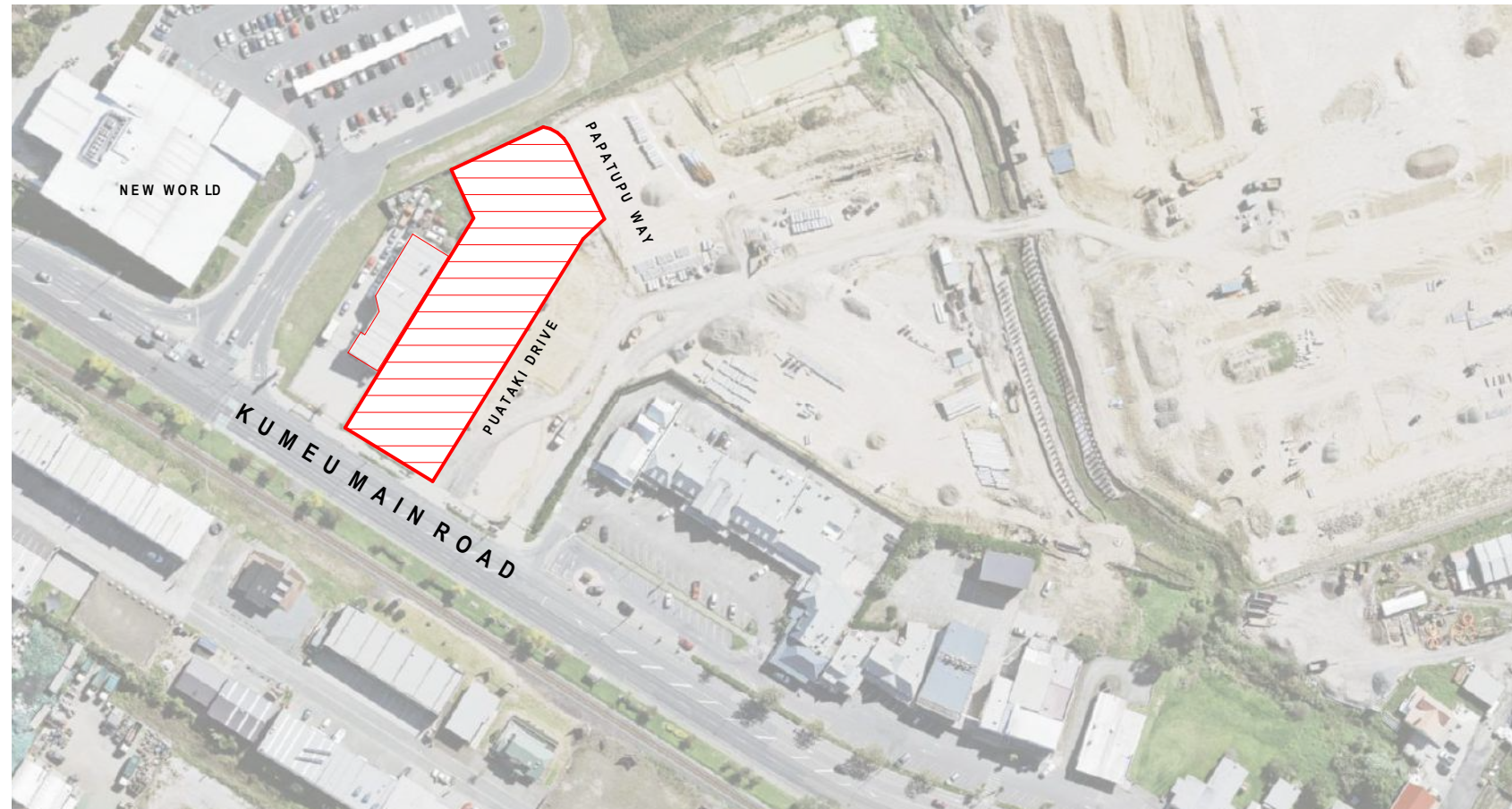
Town Planner  
**B & A URBAN & ENVIRONMENTAL**  
 Tel: 021 422 346

Traffic Engineer  
**TEAM TRAFFIC**  
 Tel: 09 838 3888

Civil / Drainage Engineer  
**MSC CONSULTING GROUP**  
 Tel: 09 486 2210

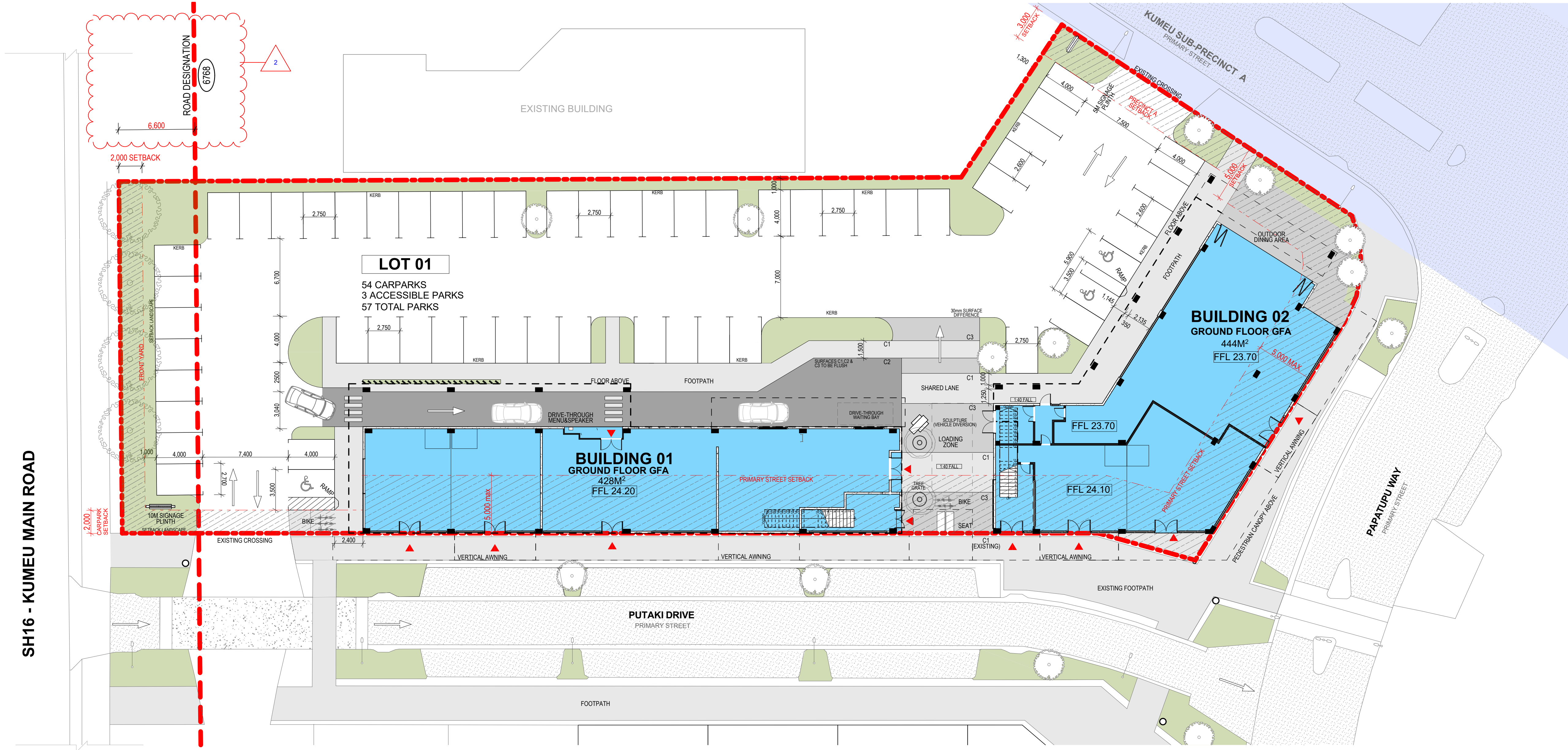
Urban Designer  
**TRANSURBAN**  
 Tel: 09 309 2555

Landscape Architect  
**SHAFAER DESIGN**  
 Tel: 09 421 1412



Location  
NTS





**SITE PLAN**  
 1:200@A1

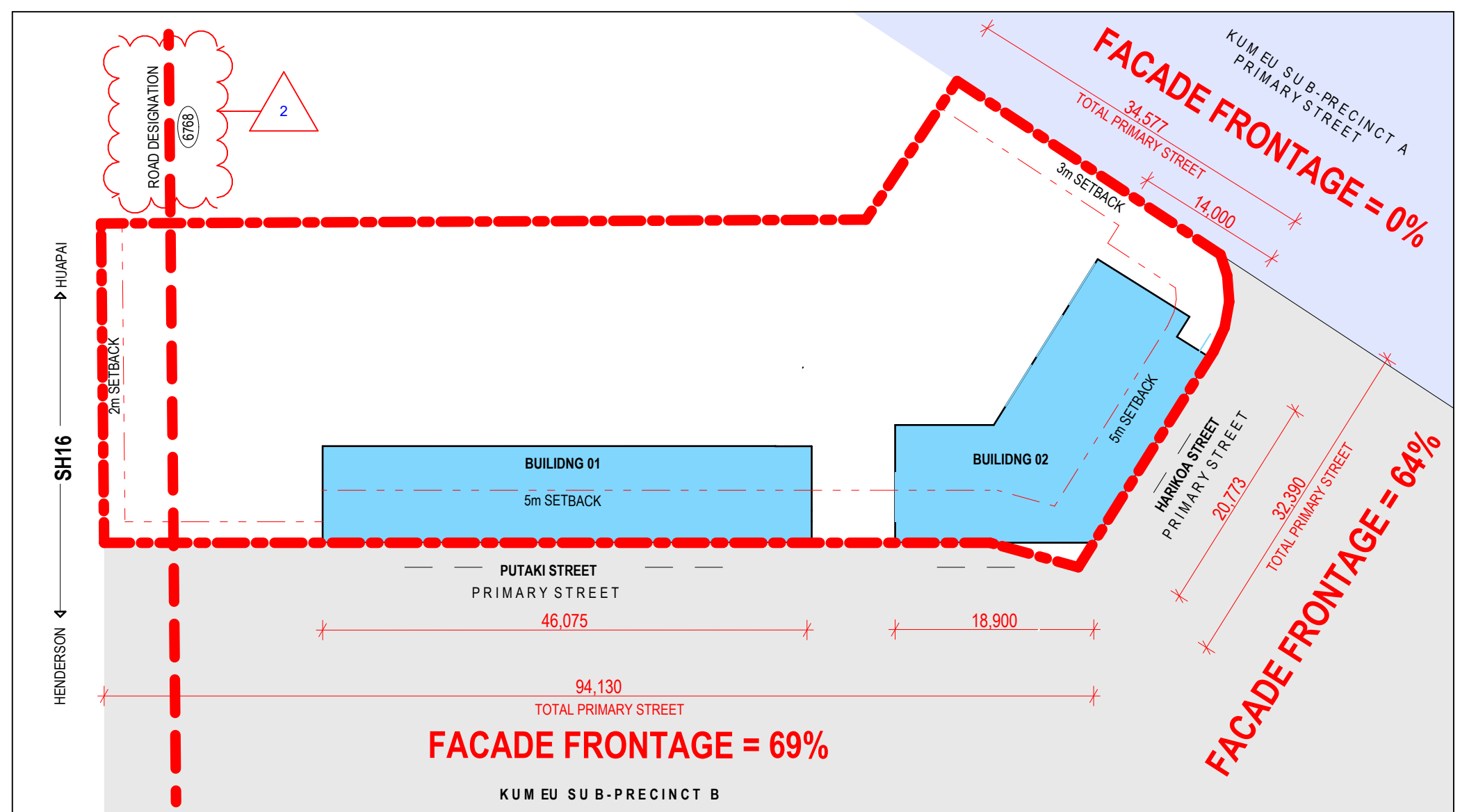
REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR PLANTING PLAN

**SITE INFORMATION**

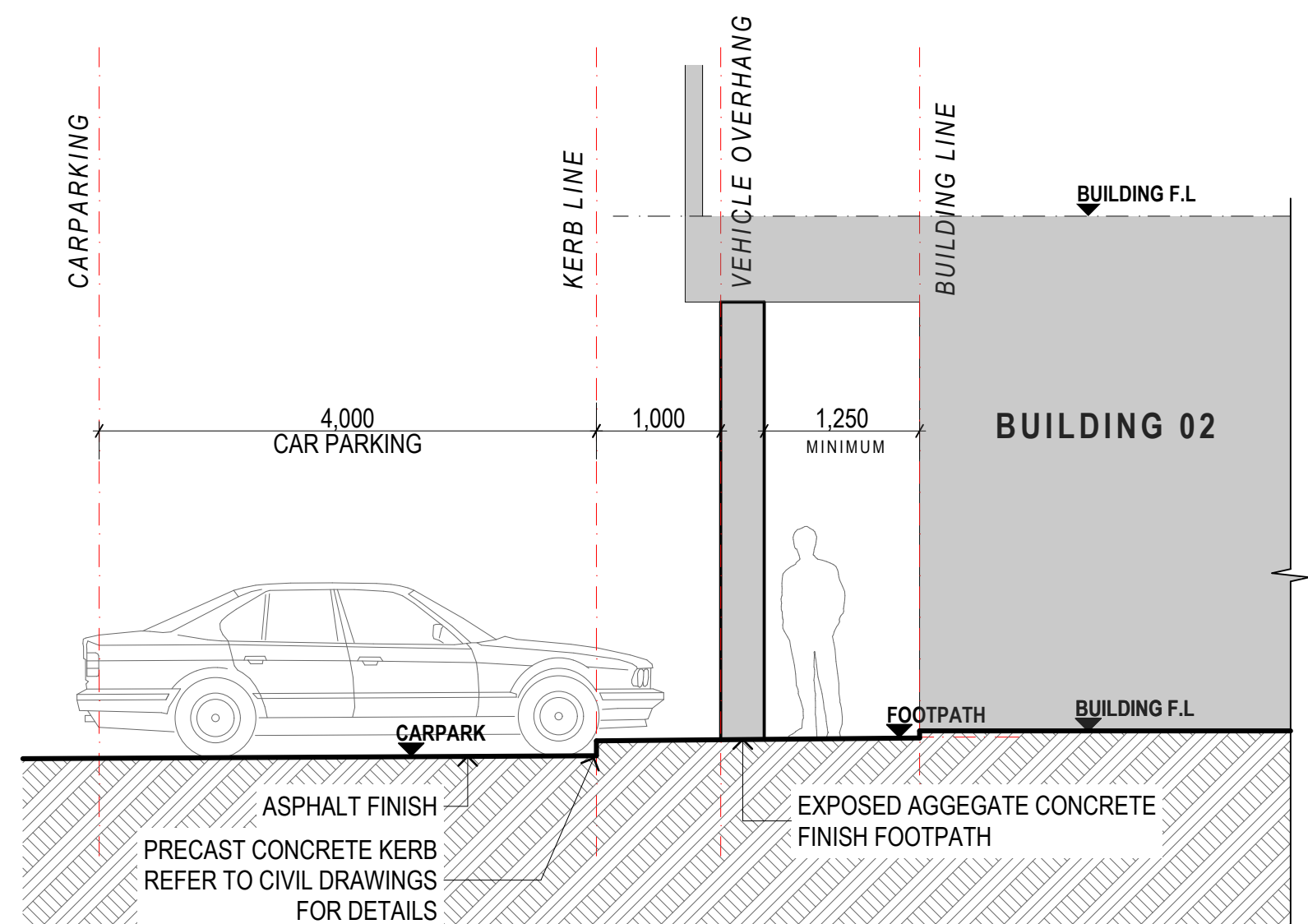
LEGAL DESCRIPTION:	LOT 3, DP 81928
WIND ZONE:	HIGH WIND ZONE
ZONE:	TOWN CENTRE MIXED USE, KUMEU SUB-PRECINCT B
SITE AREA:	3300m <sup>2</sup>
<b>CARPARKING</b>	<b>TOTAL 57</b>
SITE CARPARKS:	54 (3 ACCESSIBLE CARPARKS)
<b>BICYCLE PARKING</b>	<b>5</b>

**SITE AREAS**

SITE AREA	3345m <sup>2</sup>
<b>BUILDING COVERAGE</b>	
BUILDING 01	548
BUILDING 02	561
	1,199m <sup>2</sup> <b>36%</b>
<b>IMPERMEABLE PAVING &amp; CARPARK</b>	1,641m <sup>2</sup> <b>49%</b>
<b>PERMEABLE LANDSCAPE</b>	460m <sup>2</sup> <b>14%</b>



**FACADE FRONTAGE**  
 1:500@A1



**CARPARKING FOOTPATH DETAIL - BUILDING 2**  
 1:50@A1



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RevID	ChID	Issue Date	Notes
A	1	28/06/2019	RETROSPECTIVE ISSUE 16-05-2019: ISSUED FOR RC
B	2	28/06/2019	S92 RC RFL: ROAD DESIGNATION ADDED

**NZIA PRACTICE**



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
 For: **MATVIN GROUP LTD**

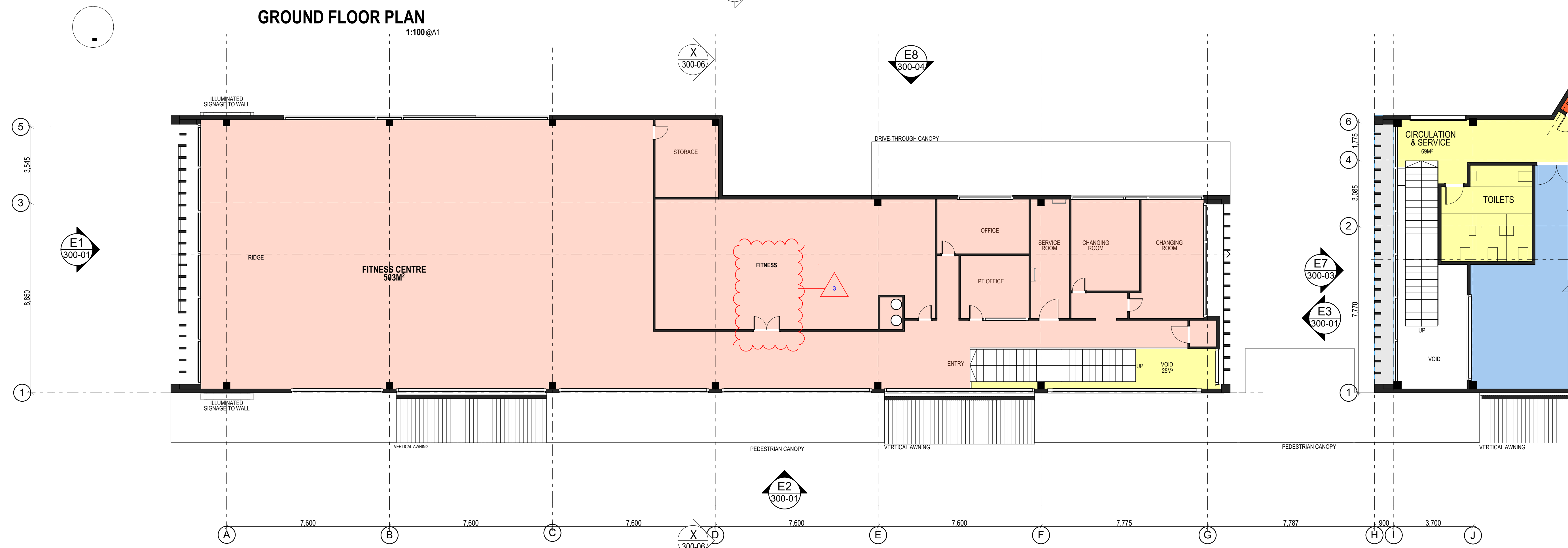
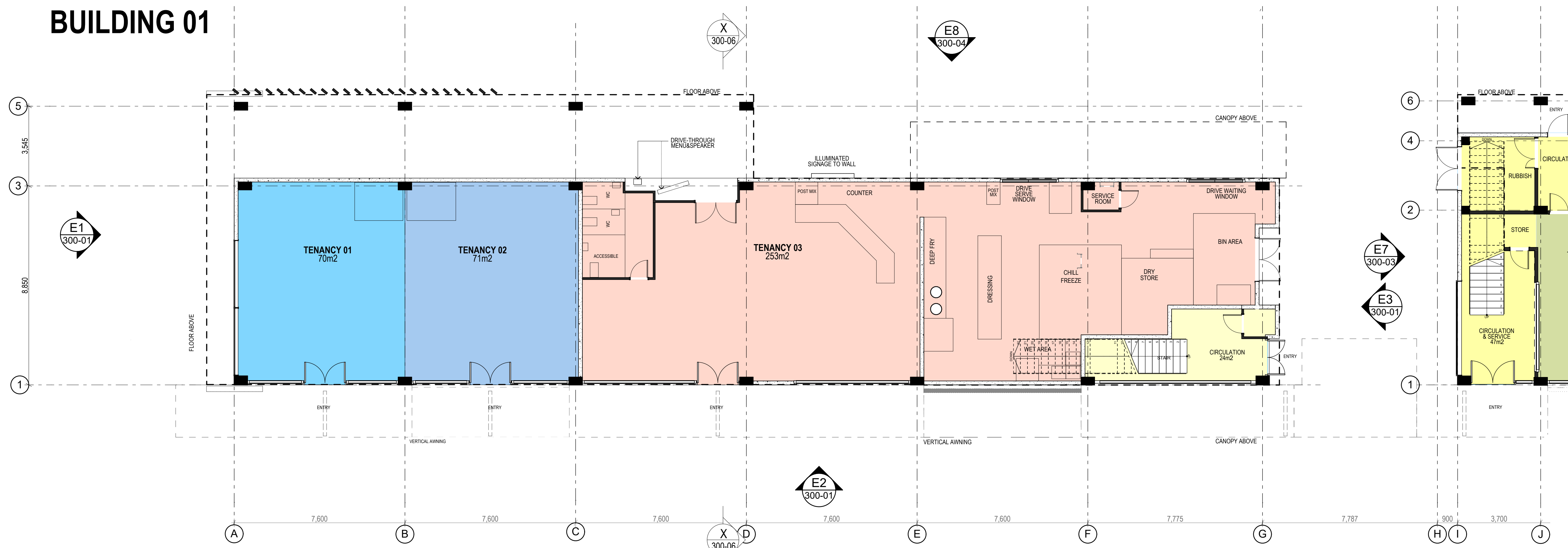
Drawing Title: **SITE PLAN**

**RESOURCE CONSENT**

Project No:	<b>19001</b>	Drawing No:	<b>100-01</b>
Scale @ A1:	1:200, 1:500, 1:50		
Scale @ A3:	1:400, 1:1000, 1:100		
Date:	28/06/2019	Revision:	<b>B</b>



# BUILDING 01



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**NZIA PRACTICE**

RevID	ChID	Issue Date	Notes
A	1	28/06/2019	RETROSPECTIVE ISSUE 16-05-2019; ISSUED FOR RC
B	3	28/06/2019	S92 RC RFI; ROOM NAME REVISED



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**

For: **MATVIN GROUP LTD**

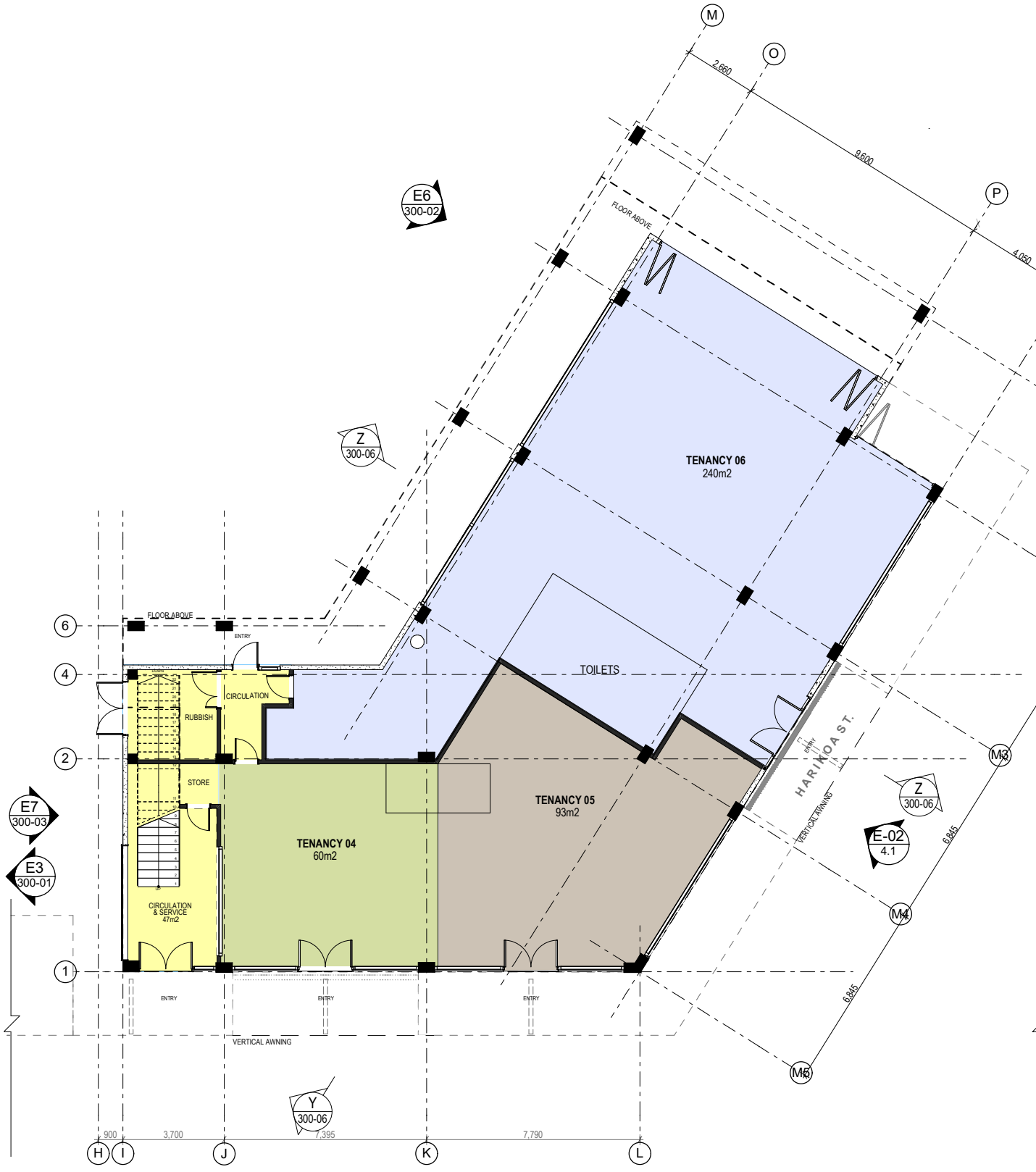
## BUILDING 01 FLOOR PLANS

RESOURCE CONSENT	
Project No:	<b>19001</b>
Scale @ A1:	1:100
Scale @ A3:	1:200
Date:	28/06/2019
Drawing No:	<b>200-01</b>
Revision:	<b>B</b>

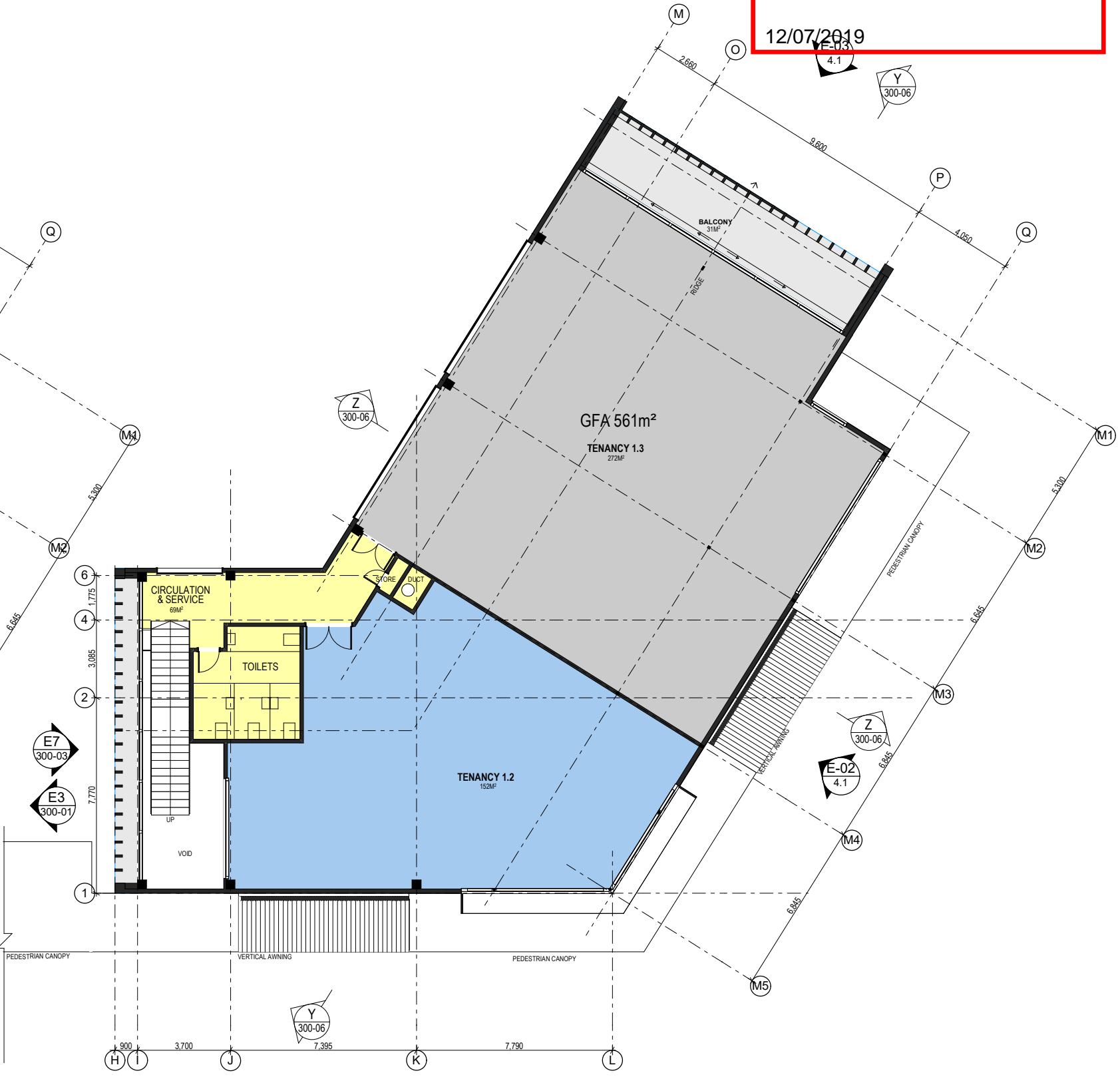
# BUILDING 02



**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019



**GROUND FLOOR PLAN**  
 1:100 @A1



**FIRST FLOOR PLAN**  
 1:100 @A1



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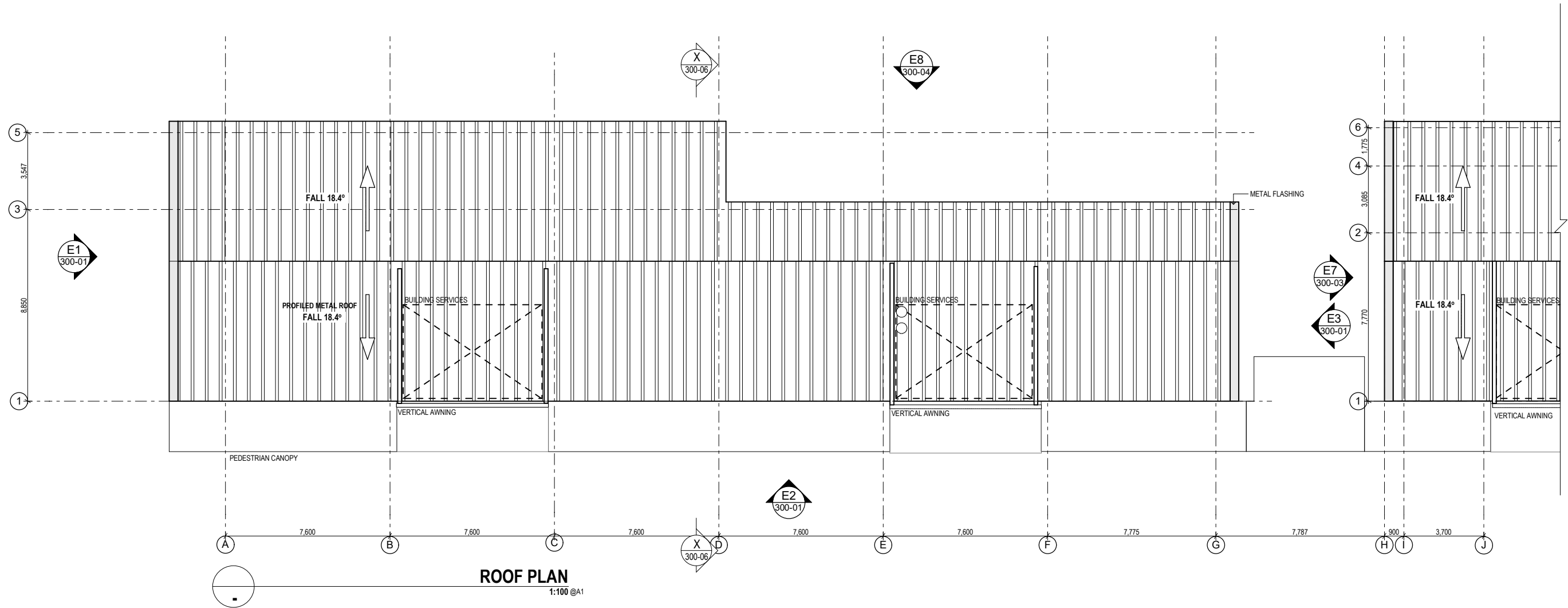
Revisions:  
 Rev: Date: Notes:



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
 For: **25 MATVIN GROUP LTD**

Drawing Title: **BUILDING 02 FLOOR PLANS**

RESOURCE CONSENT	
Project No: <b>19001</b>	Drawing No: <b>200-02</b>
Scale @ A1:	1:200
Scale @ A3:	
Date: 16/05/2019	Revision: <b>A</b>



ROOF PLAN  
1:100 @A1



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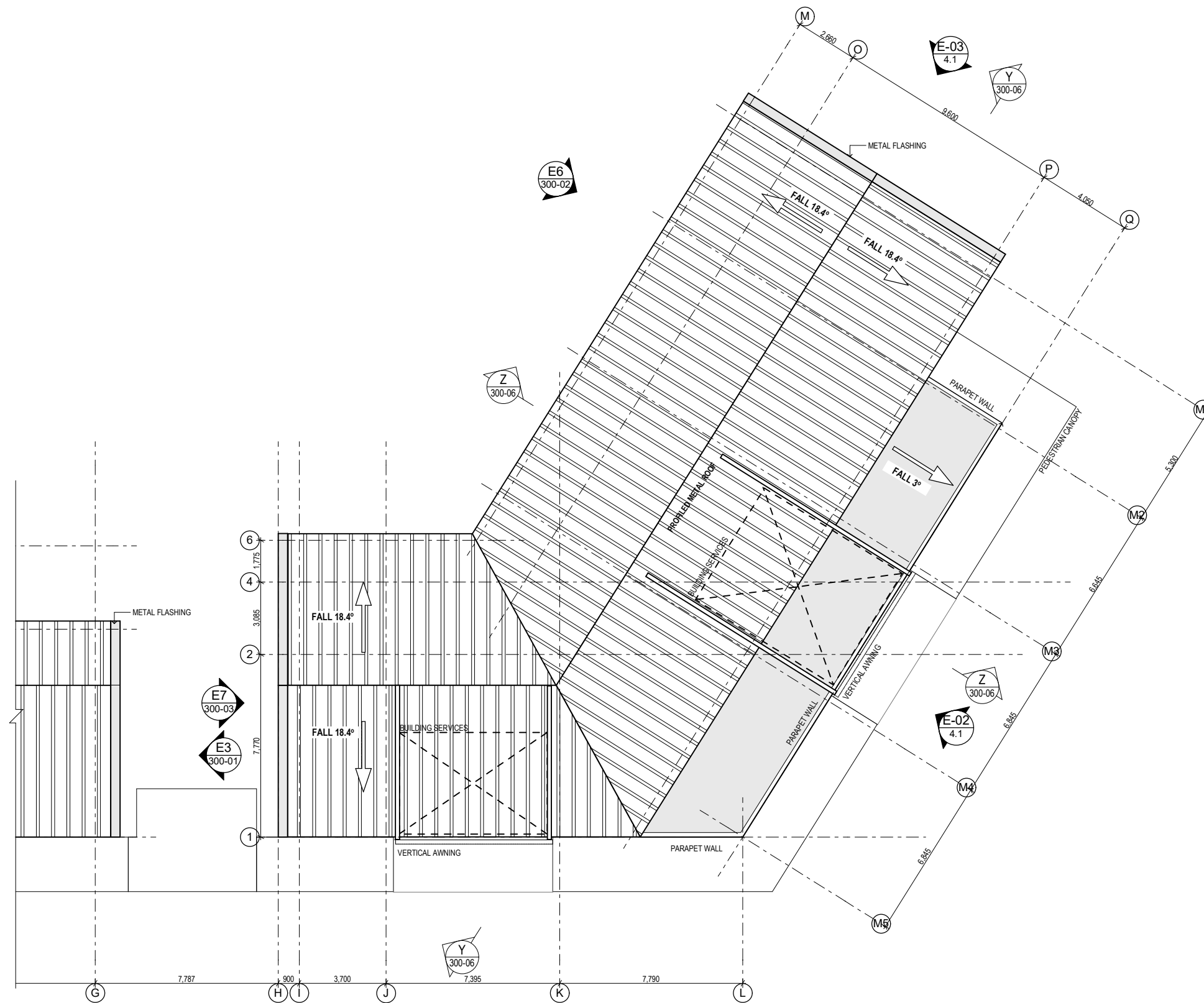
Revisions:  
Rev: Date: Notes:



Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
For: 26 MATVIN GROUP LTD

Drawing Title: ROOF PLAN - BUILDING 1

Project No: 19001		Drawing No: 200-03	
Scale @ A1: 1:100		Scale @ A3: 1:200	
Date: 16/05/2019	Revision: A		



ROOF PLAN  
1:100@A1



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Revisions:  
Rev: Date: Notes:



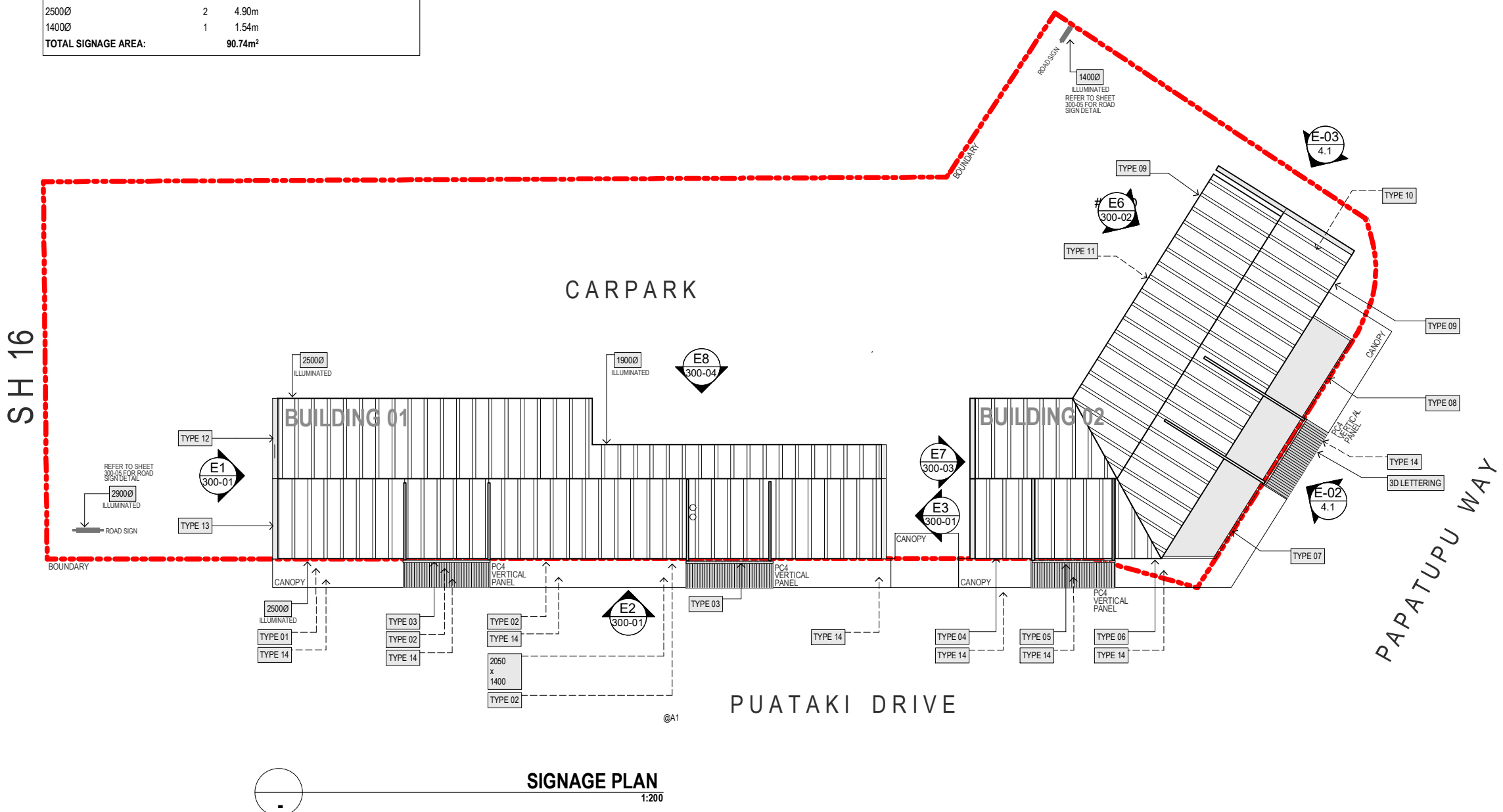
Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
For: 27 MATVIN GROUP LTD

Drawing Title: ROOF PLAN - BUILDING 2

Project No: 19001		Drawing No: 200-04	
Scale @ A1: 1:100		Scale @ A3: 1:200	
Date: 16/05/2019	Revision: A		



SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 8 6200 x 600(mm)	1	3.7	NON ILLUMINATED (SHOP FRONT)
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	LIGHT BOX (UNDER CANOPY)
2900Ø	1	6.5m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
2500Ø	2	4.90m	
1400Ø	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	





**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019

**SIGNAGE BUILDING 1:**

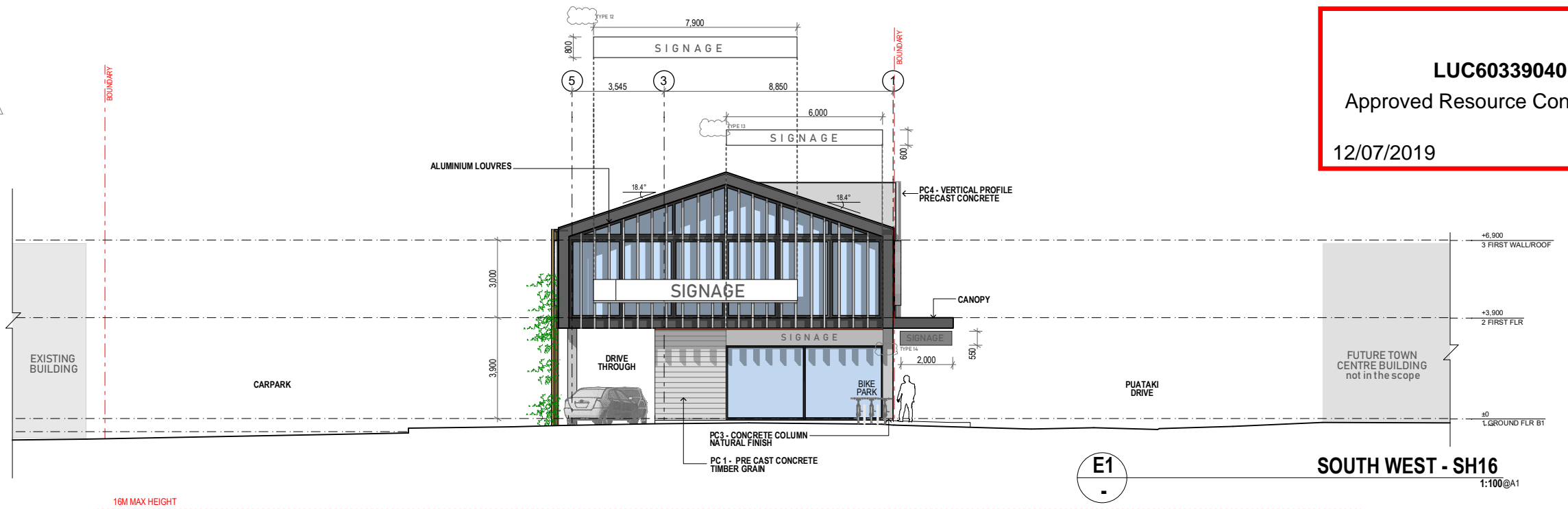
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	

**SIGNAGE BUILDING 2:**

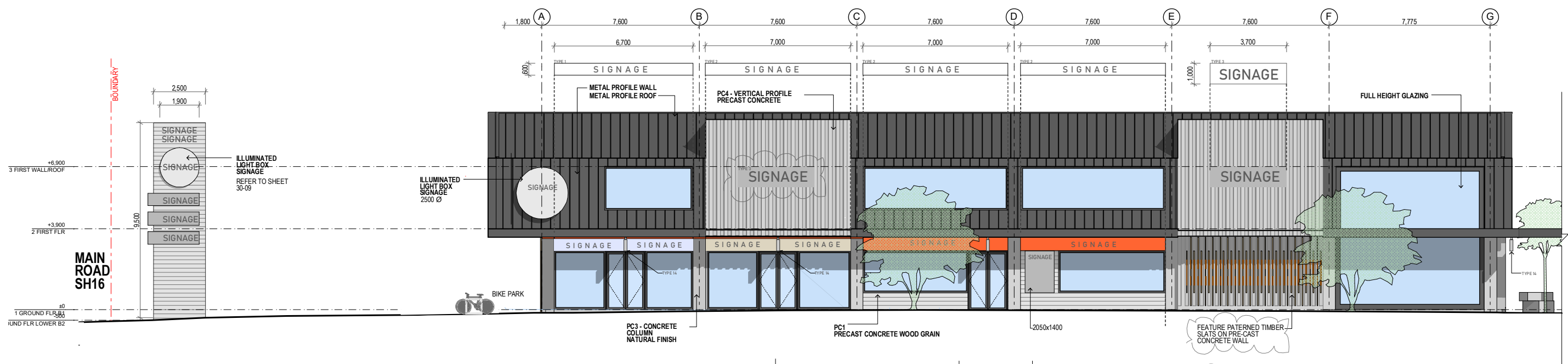
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 8 6200 x 600(mm)	1	3.7	NON ILLUMINATED (SHOP FRONT)
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	LIGHT BOX (UNDER CANOPY)

2900Ø 1 6.5m<sup>2</sup> NON ILLUMINATED (SHOP FRONT)  
 2500Ø 2 4.90m<sup>2</sup> NON ILLUMINATED (SHOP FRONT)  
 1400Ø 1 1.54m<sup>2</sup> NON ILLUMINATED (SHOP FRONT)

**TOTAL SIGNAGE AREA: 90.74m<sup>2</sup>**



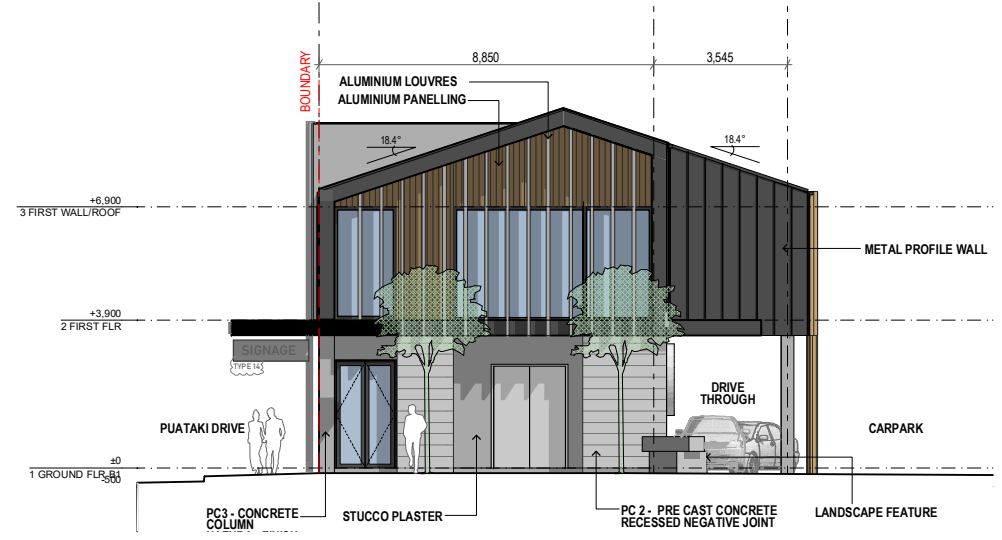
**E1 SOUTH WEST - SH16**  
 1:100@A1



**E2 SOUTH EAST ELEVATION- Puataki Drive**  
 1:100@A1

**CONCRETE FINISHES**

<b>PC1</b> PRECAST CONCRETE HORIZONTAL TIMBER GRAIN	<b>PC3</b> NATURAL PRECAST CONCRETE
<b>PC2</b> PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT	<b>PC4</b> VERTICAL AWNING RIB PRE CAST CONCRETE



**E3 NORTH EAST**  
 1:100@A1



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Revisions:  
 Rev: B Date: 19.06.19 Notes: RC - SECTION 92



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
 For: **29 MATVIN GROUP LTD**





Drawing Title: **ELEVATIONS 1**


**RESOURCE CONSENT**

Project No: <b>19001</b>	Drawing No: <b>300-01</b>
Scale @ A1: 1:100, 1:50	
Scale @ A3: 1:200	
Date: 19/06/2019	Revision: <b>B</b>

SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (PC4 CONCRETE PANEL)			
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (UNDER CANOPY)			
NON ILLUMINATED (SHOP FRONT)			
29000	1	6.5m <sup>2</sup>	
25000	2	4.90m	
14000	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	

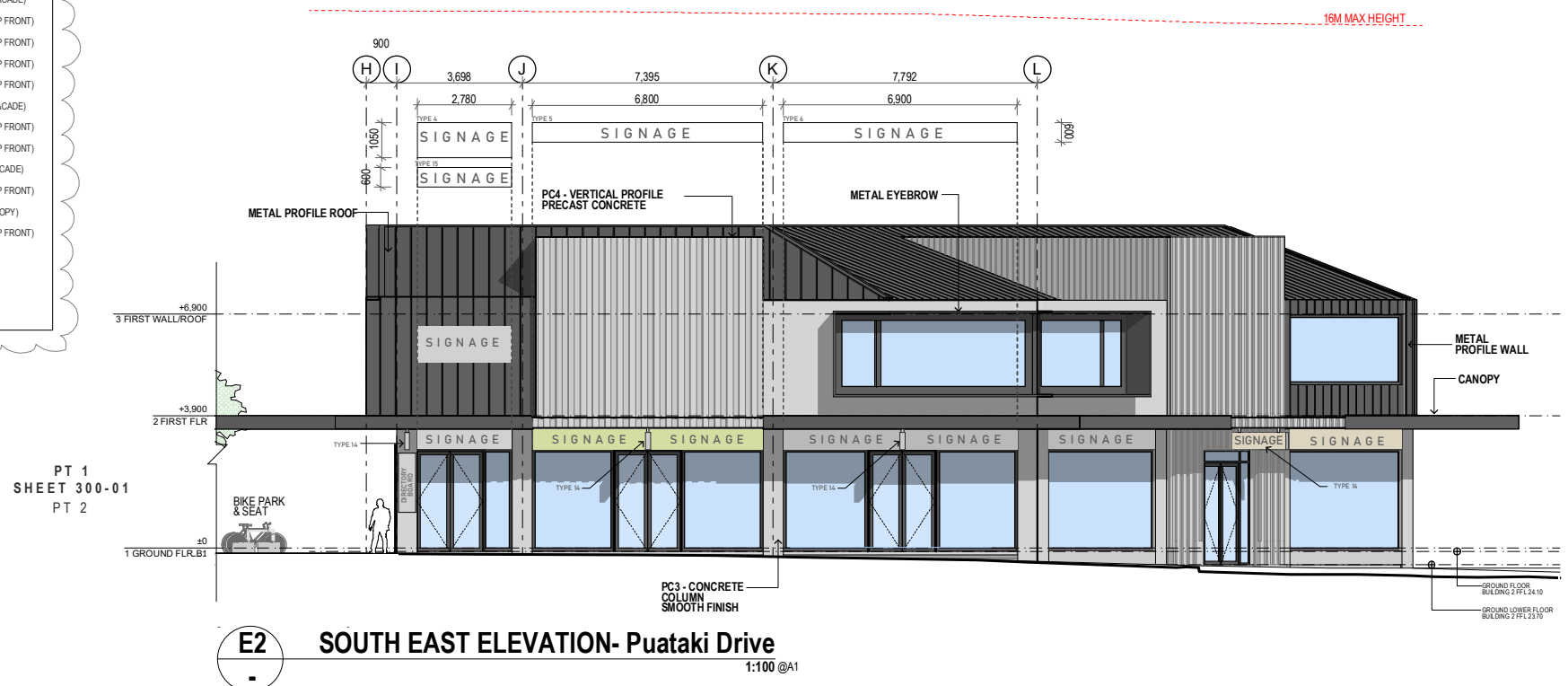
CONCRETE FINISHES

 <b>PC1</b>	 <b>PC3</b>
PRECAST CONCRETE HORIZONTAL TIMBER GRAIN	NATURAL PRECAST CONCRETE
 <b>PC2</b>	 <b>PC4</b>
PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT	VERTICAL AWNING RIB PRE CAST CONCRETE

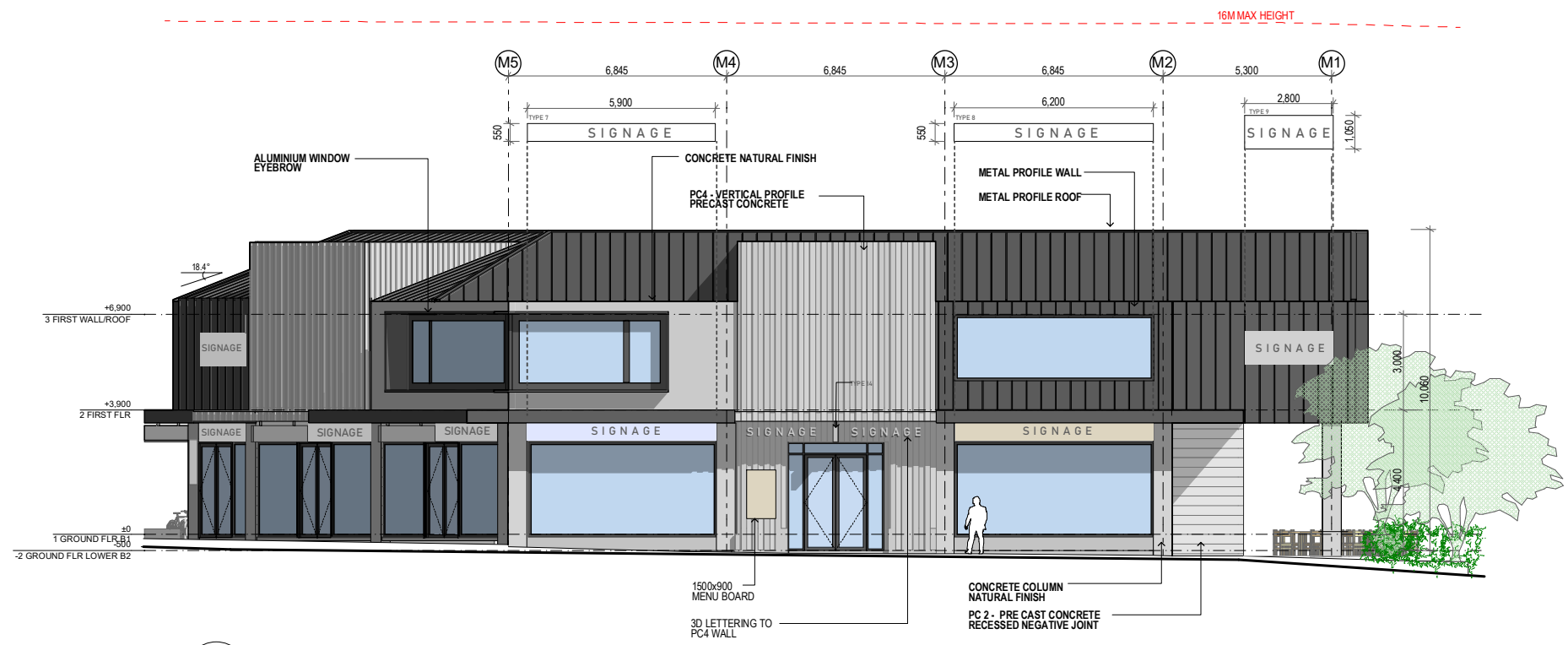


**LUC60339040**  
Approved Resource Consent Plan

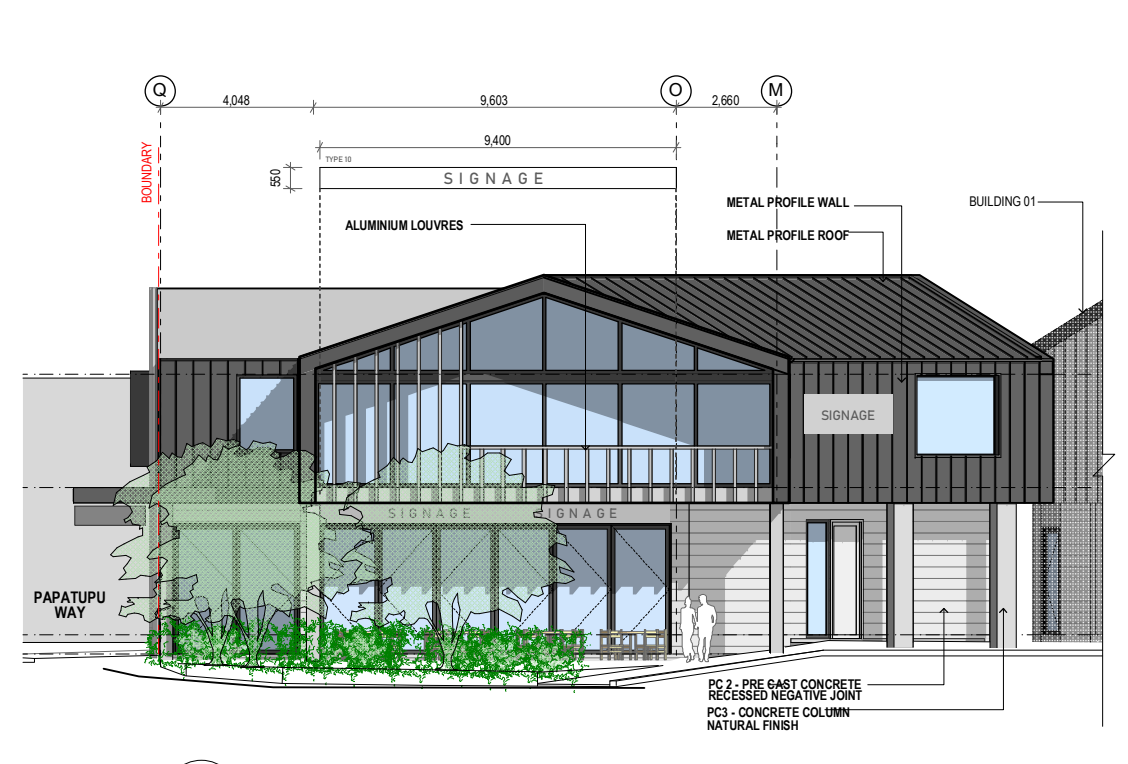
12/07/2019



**E2 SOUTH EAST ELEVATION- Puataki Drive**  
1:100 @A1

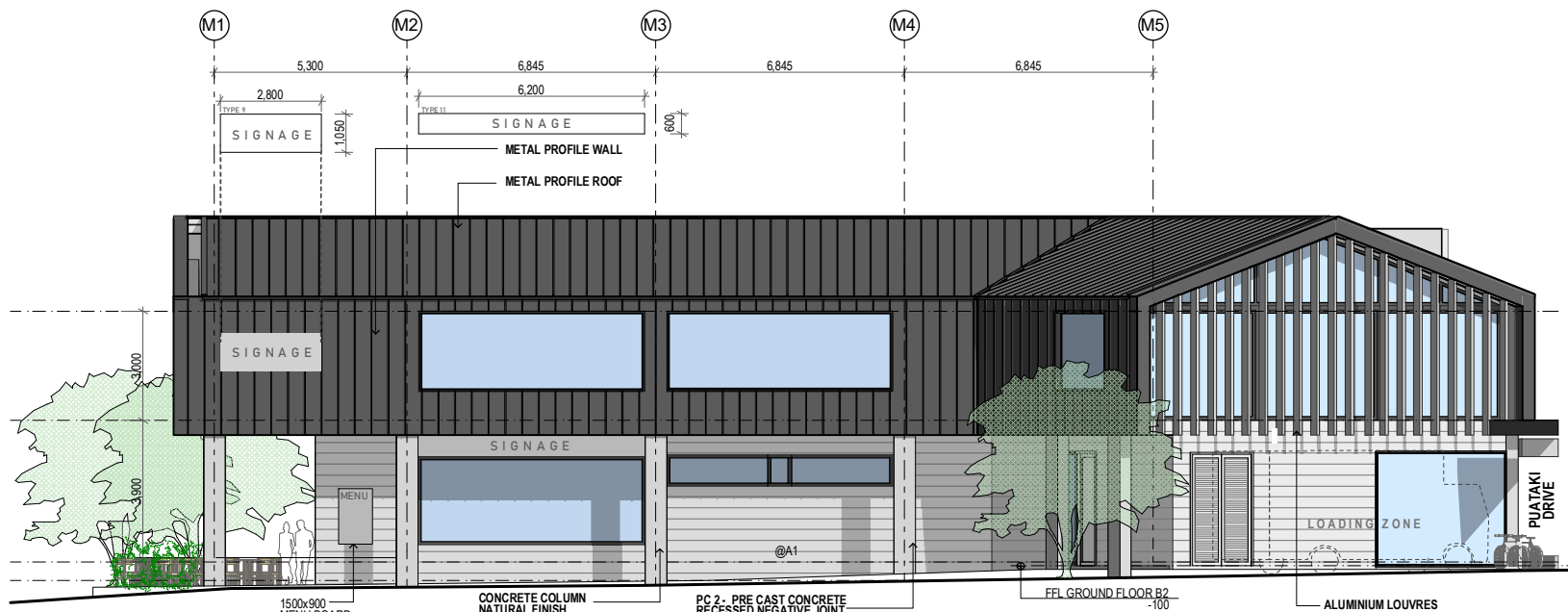


**E4 NORTH EAST Papatupu Way Elevation**  
1:100 @A1

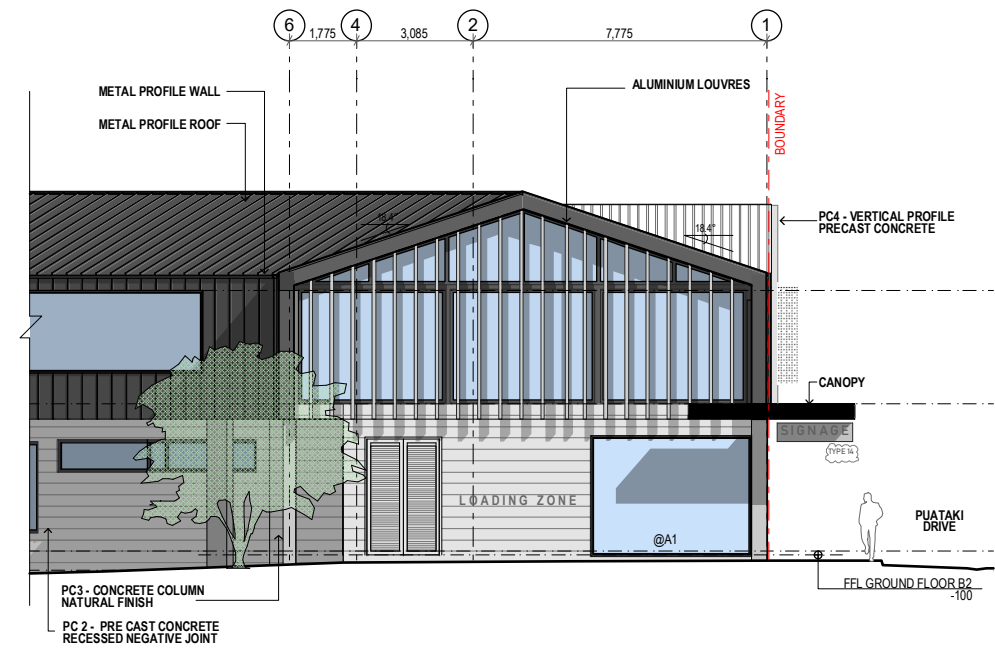


**E5 NORTH WEST**  
1:100 @A1

SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
29000	1	6.5m <sup>2</sup>	
25000	2	4.90m	
14000	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	



E6 SOUTH WEST 1:100 @A1



E7 SOUTH WEST 1:100 @A1

CONCRETE FINISHES

- PC1**  
PRECAST CONCRETE HORIZONTAL TIMBER GRAIN
- PC2**  
PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT
- PC3**  
NATURAL PRECAST CONCRETE
- PC4**  
VERTICAL AWNING RIB PRE CAST CONCRETE

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Revisions:  
Rev: B Date: 19.06.19 Notes: RC - SECTION 92



Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
For: 31 MATVIN GROUP LTD


Drawing Title: ELEVATIONS 3

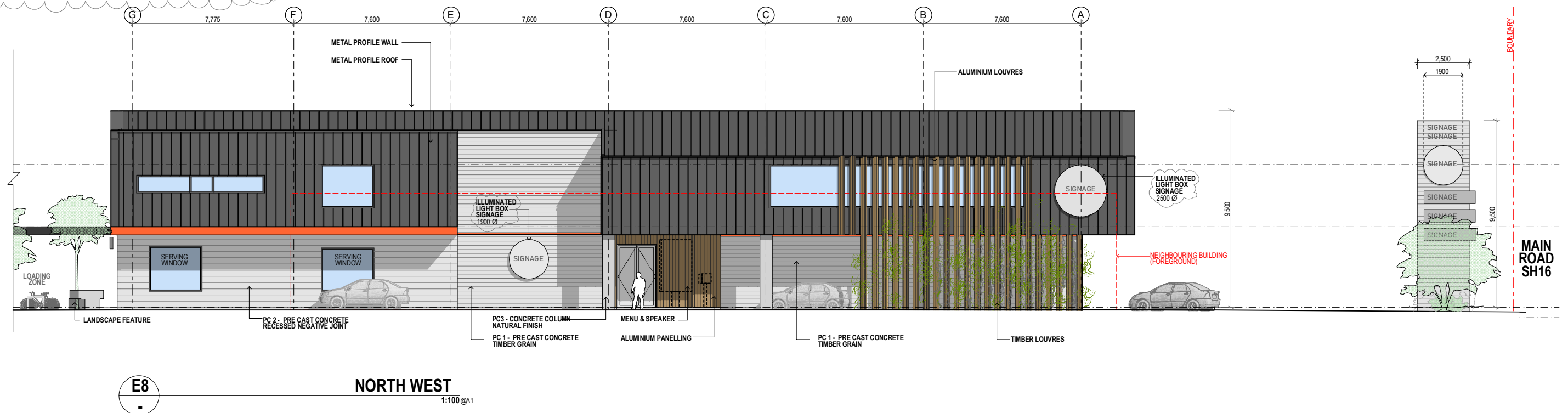
Project No: 19001		Drawing No: 300-03	
Scale @ A1: 1:100, 1:50			
Scale @ A3: 1:200			
Date: 19/06/2019		Revision: B	







**BUILDING 01**



SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (PC4 CONCRETE PANEL)			
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
NON ILLUMINATED (SHOP FRONT)			
2900Ø	1	6.5m <sup>2</sup>	
2500Ø	2	4.90m	
1400Ø	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	

  
**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019

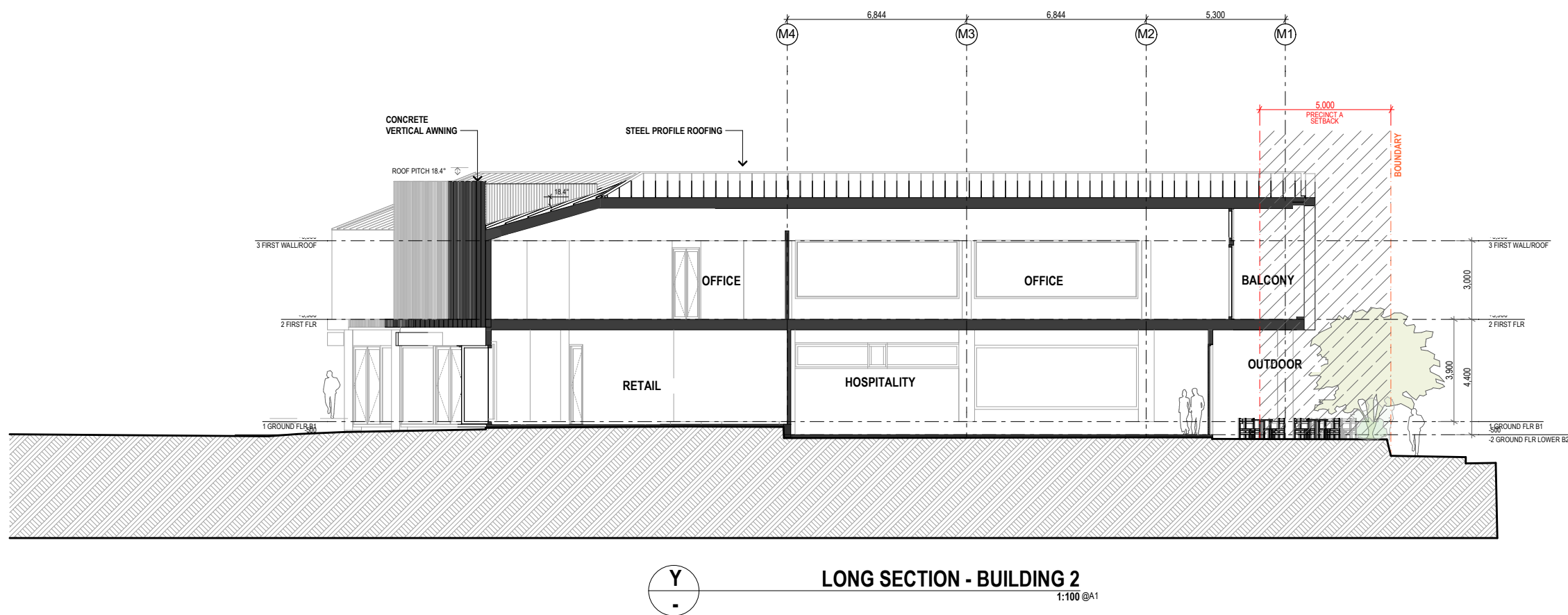
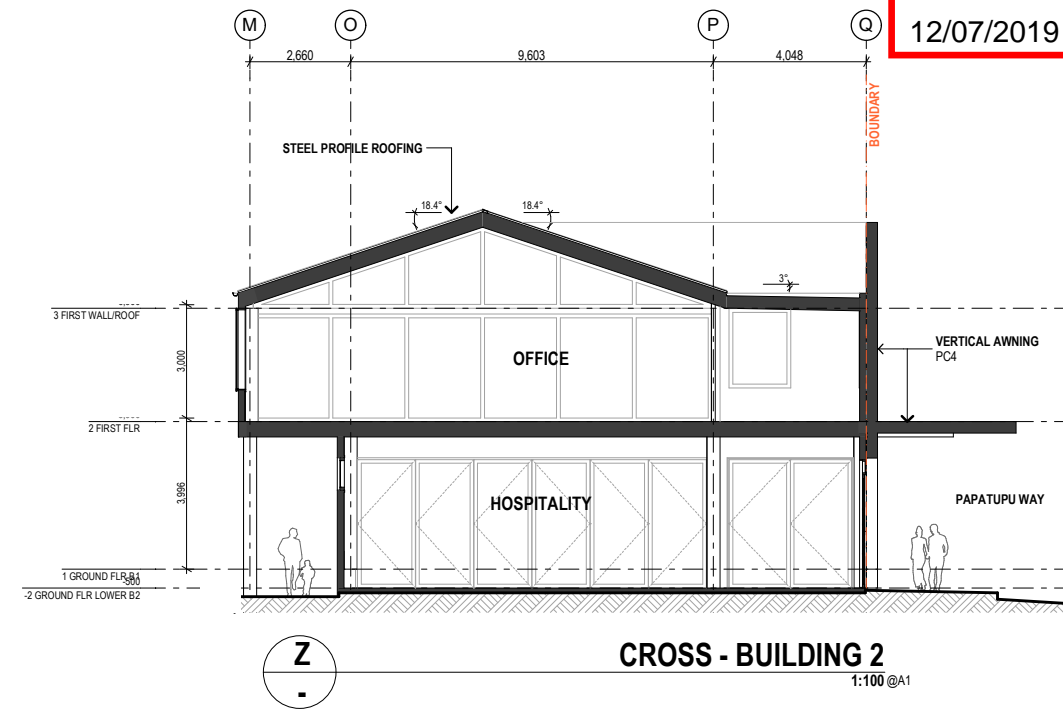
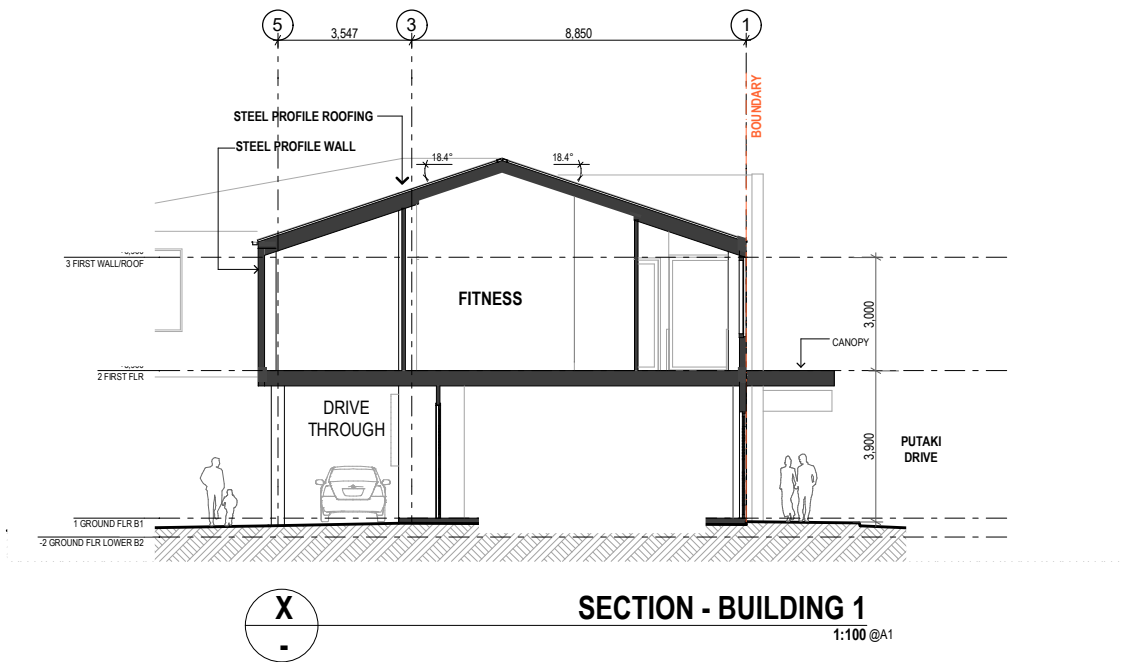


**CONCRETE FINISHES**

 <b>PC1</b> PRECAST CONCRETE HORIZONTAL TIMBER GRAIN	 <b>PC3</b> NATURAL PRECAST CONCRETE
 <b>PC2</b> PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT	 <b>PC4</b> VERTICAL AWNING RIB PRE CAST CONCRETE

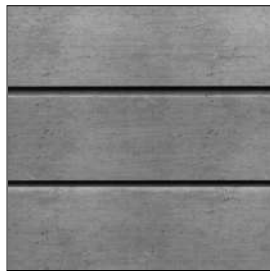



	Do not scale drawings. Verify all dimensions on site before preparing Shop Drawings or commencing work. Any doubt, ambiguity or omission on any part of the drawings or specifications should be referred to the Architect for clarification prior to commencement of work. This drawing is COPYRIGHT and remains the property of Gel Architects Ltd.	Revisions: Rev:    Date:    Notes: B    19.06.19    RC - SECTION 92		Project: <b>Lot 1 - 102-104 MAIN ROAD KUMEU</b>  For: <b>32 MATVIN GROUP LTD</b>	Drawing Title: <b>ELEVATIONS 4</b>	Project No: <b>19001</b>	Drawing No: <b>300-04</b>
						Scale @ A1: 1:100, 1:50 Scale @ A3: 1:200 Date: 19/06/2019	Revision: <b>B</b>

RESOURCE CONSENT





**EXTERNAL CLADDING LEGEND**


**CONCRETE**

		 <b>PC1</b> PRECAST CONCRETE HORIZONTAL TIMBER GRAIN
		 <b>PC2</b> PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT
		 <b>PC3</b> NATURAL PRECAST CONCRETE
		 <b>PC4</b> VERTICAL AWNING RIB PRE CAST CONCRETE
		 PROFILED METAL ROOFING & WALL (WRAPPED) (CHARCOAL GREY)

**CLADDING PRODUCTS**

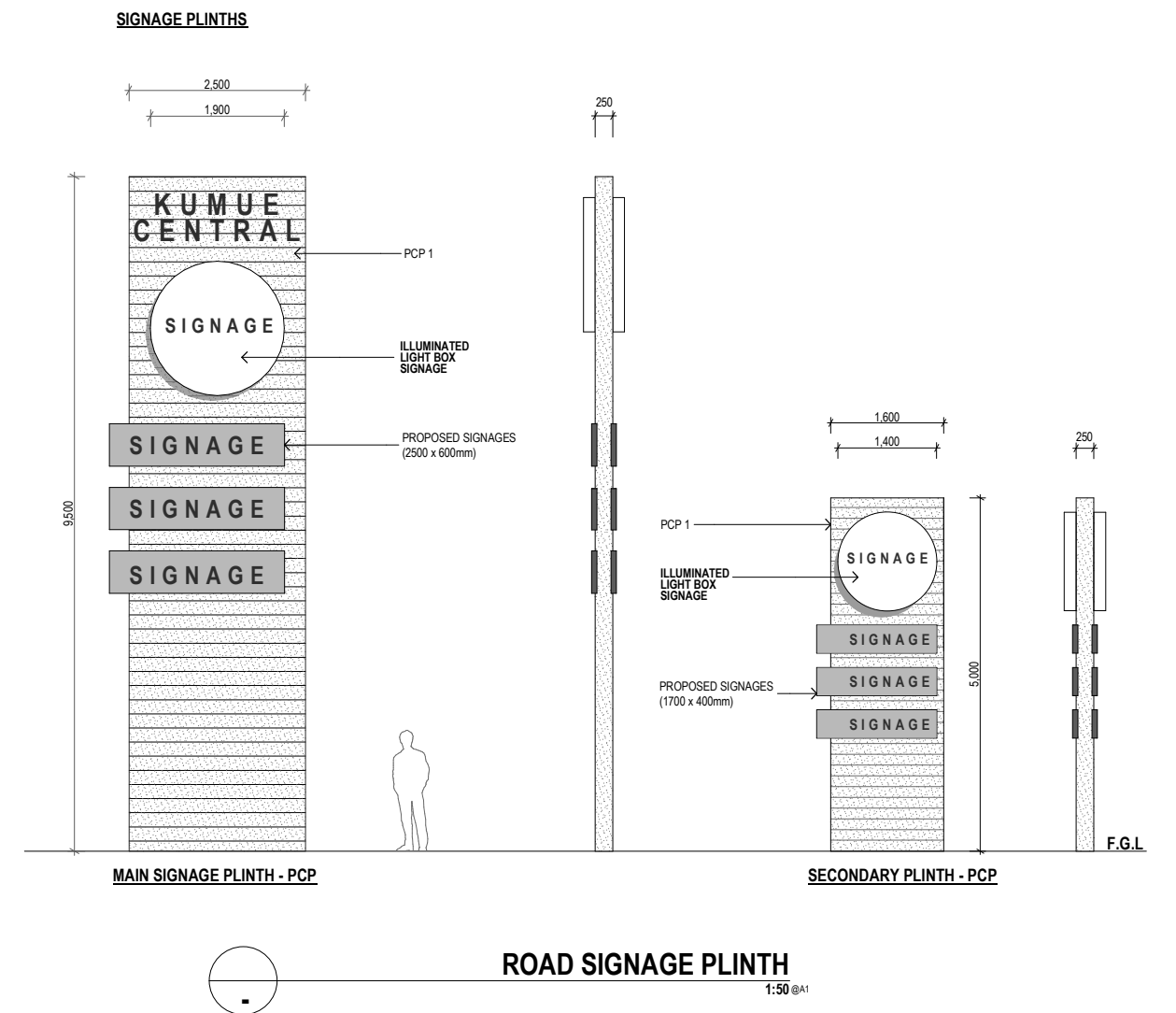
	 VERTICAL ALUMINIUM - COLOURED
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**SCREENS, LOUVRES**

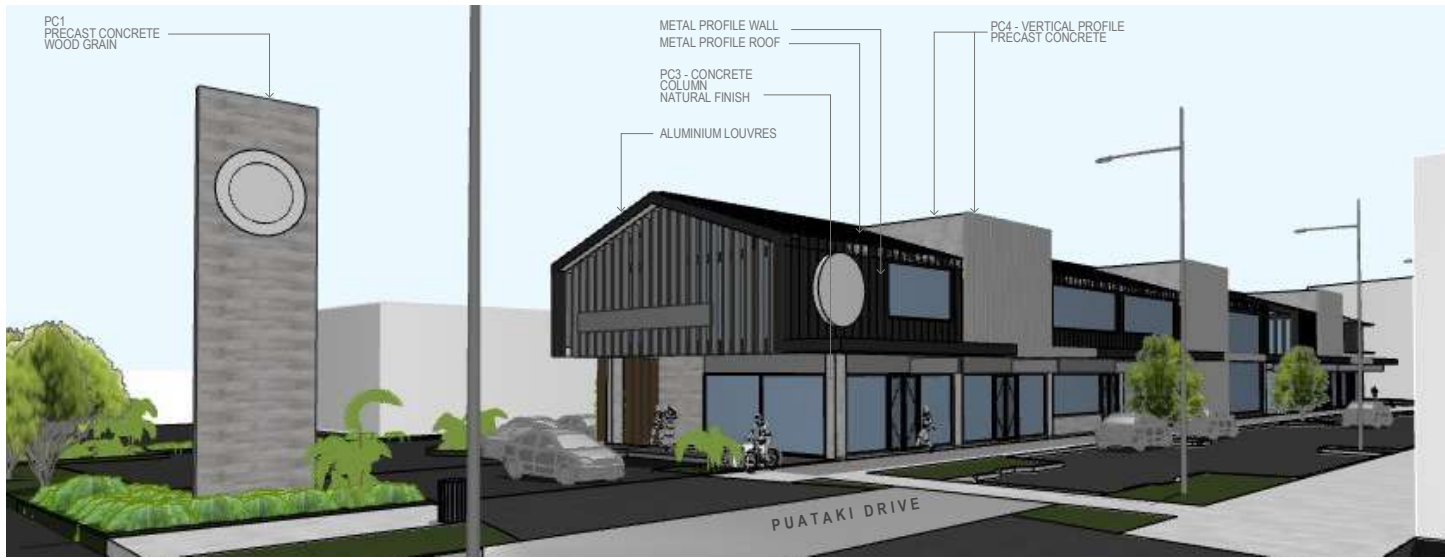
	VERTICAL ALUMINIUM LOUVRES (DARK GREY OR BLACK)
--	--

**PEDESTRIAN CANOPY & SOFFITS**

	 FIBRECEMENT FLUSH PANEL SOFFIT
---	--







○ ————— **PUATAKI DRIVE**  
1:77.4369



○ ————— **FROM SH16**  
1:100



○ ————— **PUATAKI DRIVE & PAPATUPU WAY**  
1:62.9800



**PAPATUPU WAY**  
 1:75.3345



**SECONDARY CARPARK ENTRY**  
 1:100





AERIAL 1 PUTAKI DRIVE  
1:85.3391



AERIAL 02 PUTAKI DRIVE & PAPATUPU WAY  
1:58.1387



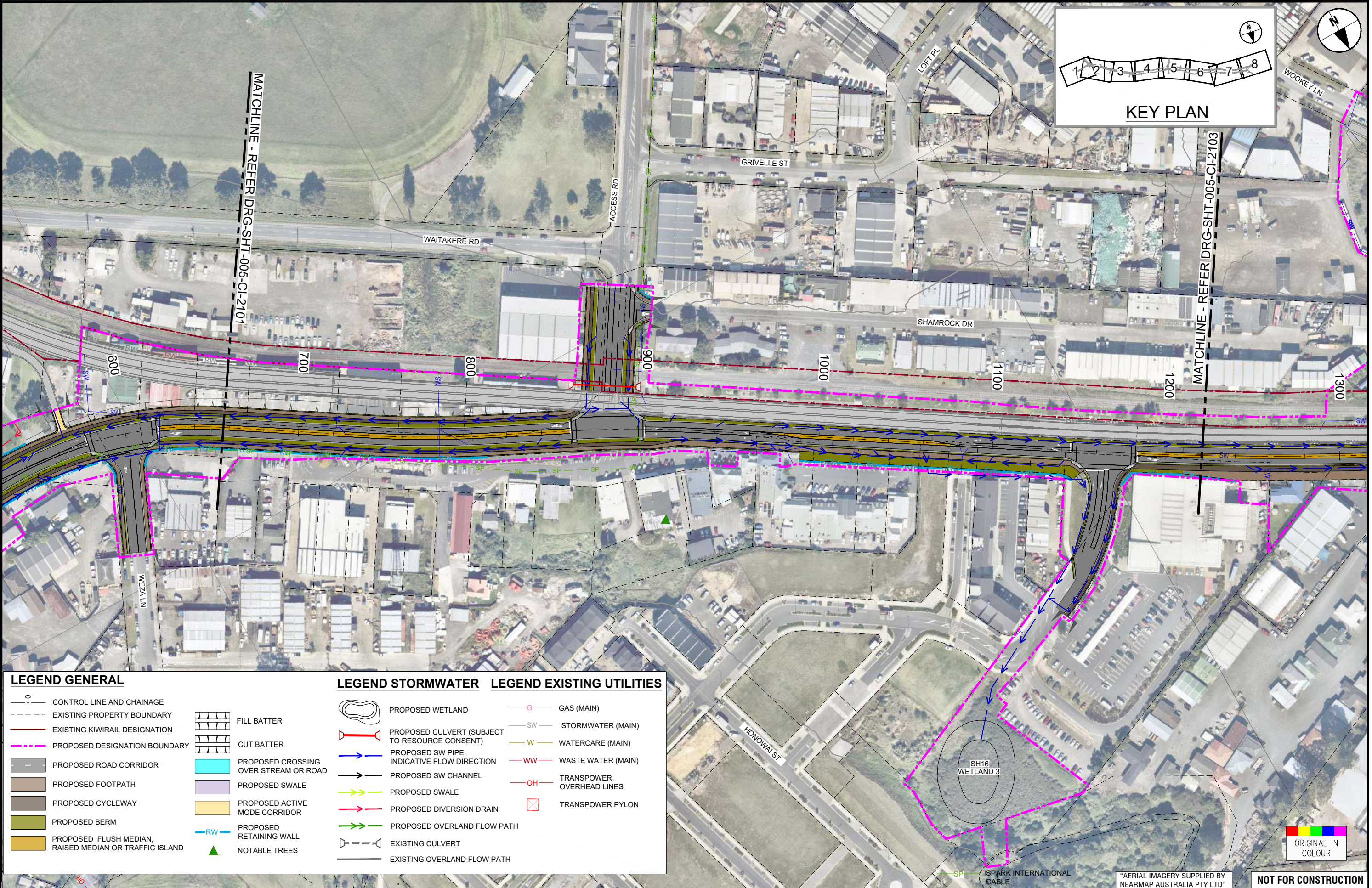
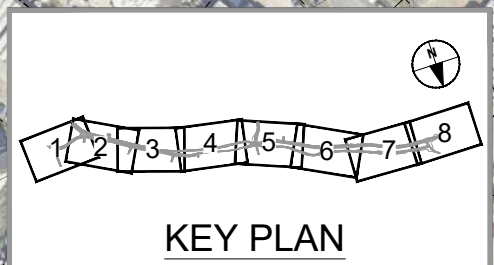
## Attachment B:

NOR S2 general arrangement plans – Sheet 2 of 8 and Sheet 3 of 8





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**LEGEND GENERAL**

- CONTROL LINE AND CHAINAGE
- EXISTING PROPERTY BOUNDARY
- EXISTING KIWI RAIL DESIGNATION
- PROPOSED DESIGNATION BOUNDARY
- PROPOSED ROAD CORRIDOR
- PROPOSED FOOTPATH
- PROPOSED CYCLEWAY
- PROPOSED BERM
- PROPOSED FLUSH MEDIAN, RAISED MEDIAN OR TRAFFIC ISLAND
- FILL BATTER
- CUT BATTER
- PROPOSED CROSSING OVER STREAM OR ROAD
- PROPOSED SWALE
- PROPOSED ACTIVE MODE CORRIDOR
- PROPOSED RETAINING WALL
- NOTABLE TREES

**LEGEND STORMWATER**

- PROPOSED WETLAND
- PROPOSED CULVERT (SUBJECT TO RESOURCE CONSENT)
- PROPOSED SW PIPE INDICATIVE FLOW DIRECTION
- PROPOSED SW CHANNEL
- PROPOSED SWALE
- PROPOSED DIVERSION DRAIN
- PROPOSED OVERLAND FLOW PATH
- EXISTING CULVERT
- EXISTING OVERLAND FLOW PATH

**LEGEND EXISTING UTILITIES**

- GAS (MAIN)
- STORMWATER (MAIN)
- WATERCARE (MAIN)
- WASTE WATER (MAIN)
- TRANSPOWER OVERHEAD LINES
- TRANSPOWER PYLON

ORIGINAL IN COLOUR

ORIGINAL SIZE A1

REV	ISSUED FOR NOTICE OF REQUIREMENT LODGEMENT	JDT	DEC 2022	DESIGN REVIEW	R. KUBALA	16.07.2021
A	DRAFT ISSUE FOR REVIEW	VDLT	JULY 2021	DESIGN CHECK	J. DELA TORRE	16.07.2021
B				DRAWING CHECK	V. DELA TORRE	16.07.2021
				DRAWN	B. BUSNARDO	07.12.2022

SURVEYED	N/A	16.07.2021
DRAWN	V. DELA TORRE	16.07.2021
DRAWING CHECK	J. DELA TORRE	16.07.2021
DESIGN	R. BELTRAN	16.07.2021
DESIGN REVIEW	R. KUBALA	16.07.2021
APPROVED	B. BUSNARDO	07.12.2022



41

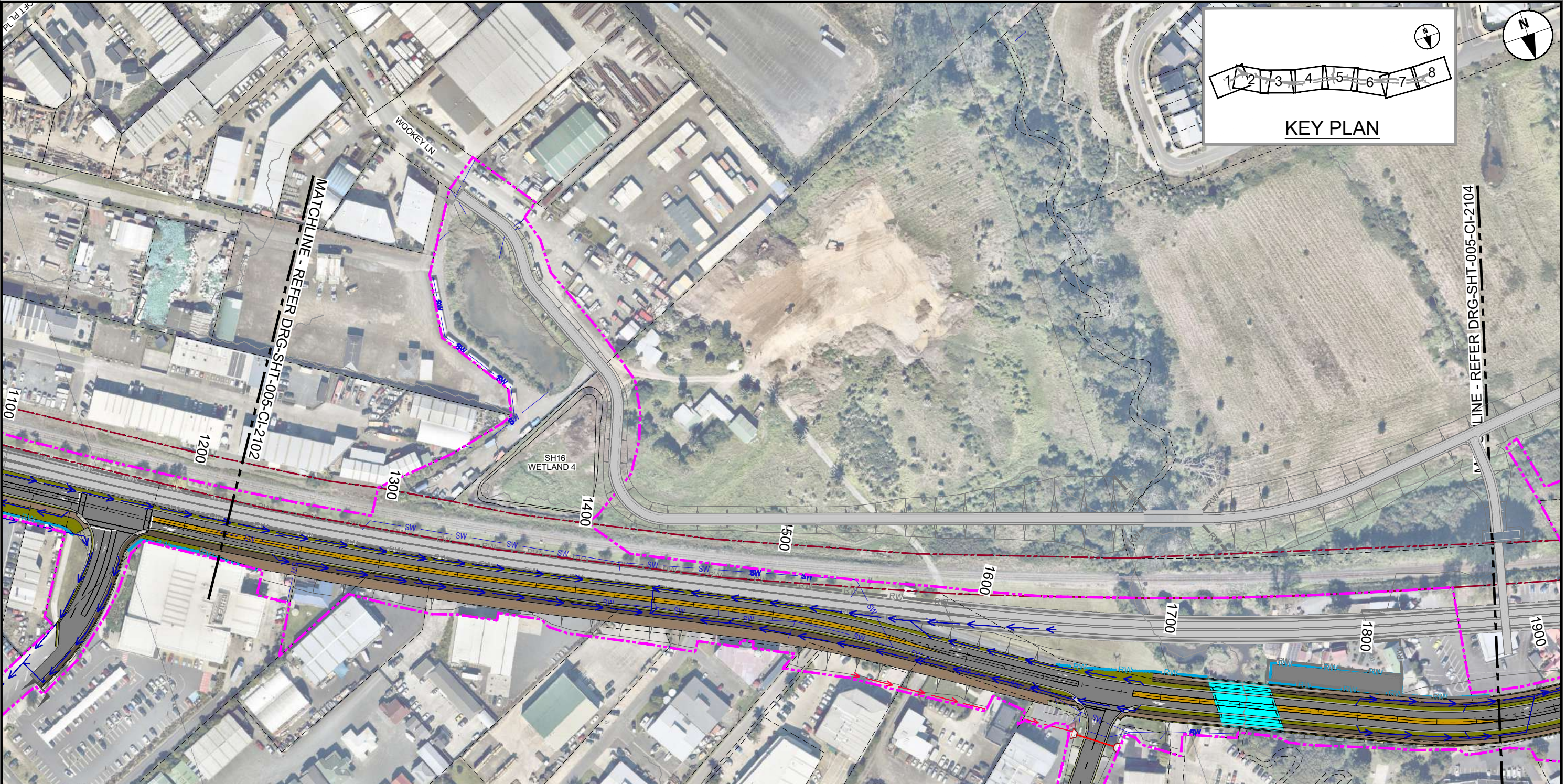
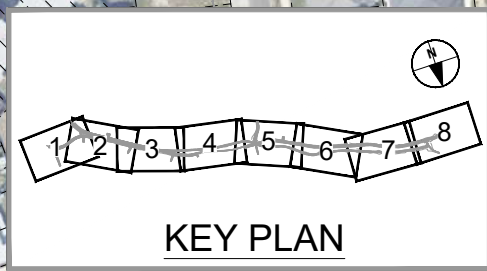
**SUPPORTING GROWTH PROGRAMME**  
**NORTH WEST STRATEGIC - SH16 MAIN ROAD UPGRADE (NoR S2)**  
 Drawing Title: GENERAL ARRANGEMENT LAYOUT PLAN  
 SHEET 2 OF 8

**FOR LODGEMENT**  
 Drawing Date: 07.12.2022  
 Drawing No: SGA-DRG-NWE-005-CI-2102  
 Discipline: CIVIL  
 Scale: 1:1000  
 Revision: B

NOT FOR CONSTRUCTION



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**LEGEND GENERAL**

- CONTROL LINE AND CHAINAGE
- EXISTING PROPERTY BOUNDARY
- EXISTING KIWI RAIL DESIGNATION
- PROPOSED DESIGNATION BOUNDARY
- PROPOSED ROAD CORRIDOR
- PROPOSED FOOTPATH
- PROPOSED CYCLEWAY
- PROPOSED BERM
- PROPOSED FLUSH MEDIAN, RAISED MEDIAN OR TRAFFIC ISLAND
- FILL BATTER
- CUT BATTER
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- PROPOSED SWALE
- PROPOSED ACTIVE MODE CORRIDOR
- PROPOSED RETAINING WALL
- NOTABLE TREES

**LEGEND STORMWATER**

- PROPOSED WETLAND
- PROPOSED CULVERT (SUBJECT TO RESOURCE CONSENT)
- PROPOSED SW PIPE INDICATIVE FLOW DIRECTION
- PROPOSED SW CHANNEL
- PROPOSED SWALE
- PROPOSED DIVERSION DRAIN
- PROPOSED OVERLAND FLOW PATH
- EXISTING CULVERT
- EXISTING OVERLAND FLOW PATH

**LEGEND EXISTING UTILITIES**

- GAS (MAIN)
- STORMWATER (MAIN)
- WATERCARE (MAIN)
- WASTE WATER (MAIN)
- TRANSPOWER OVERHEAD LINES
- TRANSPOWER PYLON

ORIGINAL SIZE A1

REV	ISSUED FOR NOTICE OF REQUIREMENT LODGEMENT	JDT	DEC 2022	DESIGN REVIEW	R. KUBALA	16.07.2021
A	DRAFT ISSUE FOR REVIEW	VDLT	JULY 2021	APPROVED	B. BUSNARDO	07.12.2022
B						

SURVEYED	N/A	
DRAWN	V. DELA TORRE	16.07.2021
DRAWING CHECK	J. DELA TORRE	16.07.2021
DESIGN	R. BELTRAN	16.07.2021
DESIGN REVIEW	R. KUBALA	16.07.2021
APPROVED	B. BUSNARDO	07.12.2022



**SUPPORTING GROWTH PROGRAMME**  
**NORTH WEST STRATEGIC - SH16 MAIN ROAD UPGRADE (NoR S2)**  
 Drawing Title: GENERAL ARRANGEMENT LAYOUT PLAN  
 SHEET 3 OF 8

Drawing Date: 07.12.2022		FOR LODGEMENT	
A1 Scale: 1:1000	A3 Scale: 1:2000		
Discipline: CIVIL			
Drawing No: SGA-DRG-NWE-005-CI-2103		Revision: B	

ORIGINAL IN COLOUR

"AERIAL IMAGERY SUPPLIED BY NEARMAP AUSTRALIA PTY LTD" NOT FOR CONSTRUCTION



## Attachment C:

Technical Note prepared by Flow Transport Specialists





**PROJECT** 102 – 104 MAIN ROAD KUMEU  
**SUBJECT** NOTICE OF REQUIREMENT (NOR) FEEDBACK  
**TO** MATTHEW ELLINGHAM (MATVIN)  
**FROM** TERRY CHURCH (DIRECTOR)  
**DATE** 6 APRIL 2023

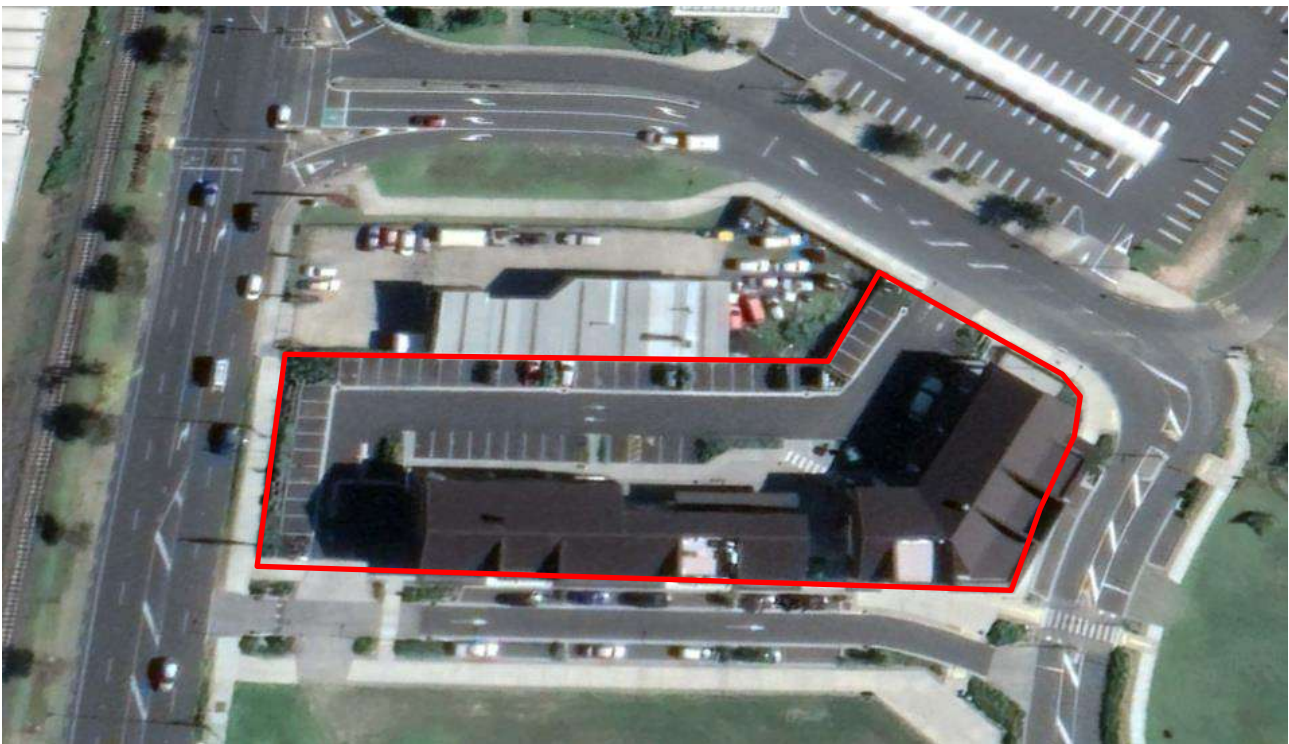
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Email: [matthew@matvin.co.nz](mailto:matthew@matvin.co.nz)

This technical note provides high level feedback on the Notice of Requirement that seeks to alter the existing Designation along Main Road, Kumeu to allow for future road widening. The Site, being 102-104 Main Road, Kumeu takes access from Putaki Drive and Harikoa Street.

The site is shown in Figure 1.

**Figure 1: Subject Site – 102-104 Main Road (indicative site boundaries)**



The Supporting Growth Alliance is altering the existing Designation to allow for a dedicated Rapid Transit Network corridor (2 lanes), 3 lane carriageway (allowing for a central median or turning lanes), cycle infrastructure, footpaths and berms/stormwater treatment. A plan outlining the proposed upgrade is shown in Figure 2.

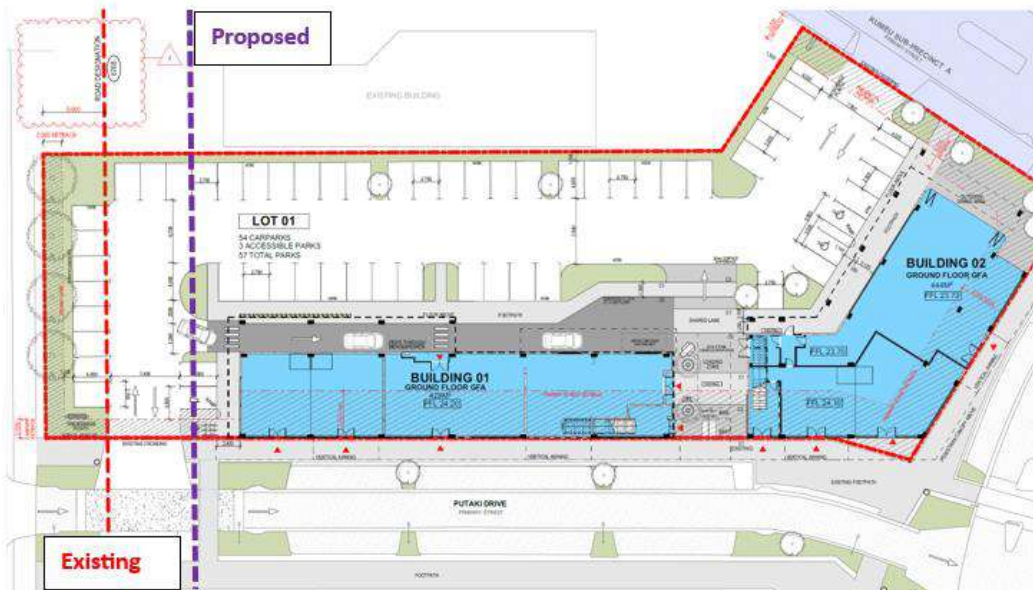
Figure 2: Main Road General Arrangement Plan



Source: <https://www.aucklandcouncil.govt.nz/UnitaryPlanDocuments/09-s2-general-arrangement-plans-state-highway-16-main-road-part%20of-5.pdf>

The drawings do not include specific details on the design, with regard to gradients and dimensions. We have therefore assumed what these may likely result in, in terms of effects and have provided what we consider are the potential impacts of the proposed designation on your property.

- ◆ Road designation 6768 (New Zealand Transport Agency) already exists over the frontage of the site, as per the above plan (red dotted line) and is setback some 6.6m from the current front boundary.

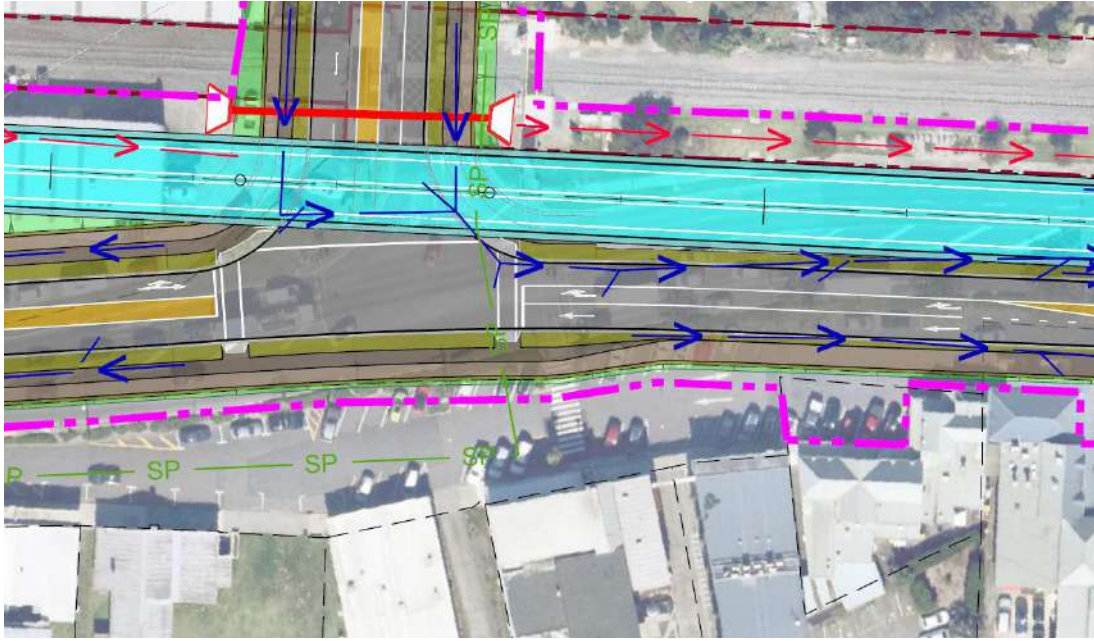


- ◆ Based on the recent NOR lodged, the intent is to provide a RTN corridor on the western side of SH16 (Main Road), with all road widening occurring to the east given the fixed location of the rail line.
- ◆ It appears that the designation seeks to extend the designation a further 9.25 to 9.50 metres into the site.
- ◆ The effect of the proposed designation, when compared with what Designation 6768 (existing) already allows includes
  - Prohibit two way access/any access into and out of the site via Putaki Drive, requiring all access to the site to be via Harikoa Street
  - Prevent any access into the Burger King drive through, ie the drive through would be unworkable as vehicles will not be able to turn into the entry. You cannot reverse the drive through operation, as such it would be closed
  - Remove five, potentially six parking spaces, in addition to those located along the frontage that sit within the existing designation...plus
  - Servicing the site would be problematic, as the closure of the front access removes the current through route which assists larger vehicles. Several additional parking spaces would need to be removed in order to provide sufficient space for a delivery truck to turn around within the site, unless service provision was made available on Putaki Drive.
  - In terms of compliance with Unitary Plan parking, following the removal of six parking spaces, it would be quicker for me to refer to the Traffic Assessment that supported the consent. If you could provide that, I can then provide feedback on parking provision.

#### Recommendation

- ◆ Undertake vehicle tracking to confirm the space needed to allow for the continued operation of the Burger King drive through
- ◆ Undertake tracking and confirm the servicing requirements of the site
- ◆ Review the original ITA and Resource Consent conditions to check in with the parking requirements of the existing development
- ◆ Question the need for the wide rear berm. While I suspect it is required to assist with stormwater, it hasn't been shown further to the east, about Access Road. This may allow sufficient room to allow the Putaki access to remain open (one way in) and allow the drive through to continue operating. This would also allow servicing of the site to remain as existing. Is there an opportunity to use the berm available on Harikoa Street, at the intersection with Main Highway.





Reference: P:\flow\024 Proposals\Development - public and private\NORs\Matvin Kumeu\TN1A230405\_Kumeu Main Road NOR.docx - Terry Church



THE PLANNING  
COLLECTIVE

## Form 21

### Submission on requirement for alteration of designation for State Highway 16 Main Road Upgrades NOR S2 that is subject to notification

*Section 169 of the Resource Management Act 1991*

To: Auckland Council

#### SUBMITTER DETAILS

**Name of Submitter:** Tahua Partners Limited ("TPL")

1. Tahua Partners Limited makes this submission on North West Strategic Network: State Highway 16 Main Road upgrade NOR S2 ("**NOR S2**") lodged by Waka Kotahi New Zealand Transport Agency (**NZTA**) to the Auckland Unitary Plan - Operative in Part in accordance with Section 169 of the Resource Management Act 1991 (**RMA**) as follows.
2. TPL could not gain advantage in trade competition through this submission.
3. TPL **wishes to be heard** in support of their submission.
4. If any other submitters make a similar submission, TPL will consider presenting a joint case with them at the hearing.

#### OVERVIEW

5. TPL is a New Zealand retail; and hospitality investment company with a portfolio that includes Starbucks, Burger King and Number One Shoes.
6. TPL own the Burger King located on the corner of Main Road and Putaki Drive, Kumeū. The site address is 102 – 104 Main Road Kumeū. The Burger King is situated within a retail complex, which also includes Anytime Fitness, a medical centre, a dog grooming business and an Indian Restaurant. There is on-site parking, a drive thru, and a road frontage sign associated with the Burger King. An image of the street view is included in **Attachment A**
7. The land is owned by Kumeu Central Limited (KCL), who obtained consent for the development. A copy of the consented plans is **Attachment B**.
8. TPL has an interest in NOR S2 that is greater than the interest of the general public as they own and operate the Burger King at 102 - 104 main Road, Kumeū.

## SCOPE OF SUBMISSION

9. This submission relates to NOR S2 as a whole but specifically the portion of the Notice of Requirement (**NOR**) showing a proposed designation extending over the front portion of the land at 102 - 104 Main Road, Kumeū (the **Site**). NOR S2 seeks to alter the existing designation applying to State Highway 16 Main Road Kumeu to enable Main Road to be upgraded to a 24m wide urban corridor with provision for two lanes (one each direction) with walking and cycling facilities. A copy of the General Arrangement Plan for the NOR is **Attachment C**.
10. TPL supports in principle the need to upgrade the roading infrastructure to provide for various transport modes. However, TPL **opposes** NOR S2 due to the extent and nature of impacts on the land at 102 - 104 Main Road, Kumeū and the significant adverse effects it will have on the continued operation of the Burger King. The effects on the Burger King business have not been assessed and are not justified. Because the effects are significantly adverse alternatives need to be considered.
11. The proposed designation extends a significant distance onto the Site, extending past the existing vehicle access off Putaki Drive. This vehicle access provides access to the carparking on-site as well as the drive thru for Burger King. Vehicles enter the Burger King drive thru off Putaki Drive and then exit the drive thru onto Harikoa Street.

The extent of the proposed designation will prevent access from Putaki Drive, consequently the only access would be from Harikoa Street. Whilst the Harikoa Street access provides for two-way traffic, the Site does not provide sufficient manoeuvrability to accommodate all traffic having to turn around as opposed to travelling in one access and exiting via the other access. The singular access will restrict circulation of traffic through the Site and result in congestion.

12. 70-80% of Burger King's business is via drive thru. Rendering the use of the drive thru unusable will have catastrophic impacts on the continued operation of this Burger King store.
13. Currently, as provided by the Putaki Drive access, there is access directly into Burger King from the main highway. Currently, left turn and right turns into Putaki Drive are available, providing direct access to Burger King, which the proposed designation will remove.
14. The proposed designation would result in the removal of at least 10 carparks, resulting in considerably less carparks on site. This will also impact on the viability and operation of the Burger King business, particularly in relation to the removal of the drive thru access this is a double and unjustified impact.
15. The pole sign will need to be removed and relocated under the proposed designation. Burger King relies heavily on good visibility and signage to draw in customers. Removal and relocation of the sign will decrease the visibility of the restaurant to passing vehicles, resulting in fewer customers. The current pole sign location is indicated within the approved plans attached as **Attachment B**, shown on Drawing Number 100-01.
16. Overall, the physical impacts on the Burger King business are severe and will render the business unviable.

17. During construction of the State Highway 16 Main Road upgrades, there will be roadworks outside Burger King, and under the proposed designation, will extend almost completely up to the restaurant front. This will be highly disruptive and will adversely affect the day-to-day operations of the business during this period due to noise and vibration, dust and could impede access.
18. Overall, the NOR has significant adverse effects on the Burger King business and will result in the business being entirely unviable, consequently forcing the closure of the Burger King.
19. There is insufficient information or detail provided in the NOR to demonstrate that the proposal represents an outcome with an acceptable level of impact on the local environment.
20. TPL considers there are alternative options that would better achieve the outcomes sought and have lesser impacts on the site and business operations.
21. The NOR as it impacts the TPL (and the Burger King), is not in keeping with the relevant objectives and policies in the Unitary Plan relating to infrastructure namely the proposed infrastructure is adversely impacting on established businesses in a manner that will render the businesses unviable, contrary to policies B3.3.2(4) and B3.3.2(7) in particular. This part of the infrastructure will not contribute to the economy of Auckland in a positive manner. There is insufficient information, and evaluation of alternatives to make an informed decision that the NOR represents the best, or only solution for this location and that it achieves a quality compact urban form consistent with objectives B.2.2.1.

## RELIEF SOUGHT

22. TPL seek that Auckland Council recommend NOR S2 be **refused for the following reasons:**
  - a) The proposal will have significant adverse effects on established activities on the site at 102-104 Main Road, including whether the proposed NoR avoids, remedies or mitigates those adverse effects associated with the construction and operation of the transport infrastructure on the environment and on community health and safety (Policy B3.3.2(7)); and
  - b) The indicated design does not achieve integration with adjacent land uses, taking into account their current and planned use, intensity, scale, character and amenity (Policy B3.3.2(4)(a)); and
  - c) The indicated design does not contribute to achieving a well-functioning urban environment that includes achieving a quality compact urban form which includes:
    - a higher quality urban environment (B2.2.1 (1) (a));
    - enabling better use of existing infrastructure and efficient provision of new infrastructure (B2.2.1 (1) (c)); ;
    - greater social and cultural vitality (B2.2.1 (1) (e));
    - reduced environmental effects (B2.2.1 (1) (g));
23. TPL further seek any other relief required to achieve the outcomes sought in this submission.

Yours sincerely



Burnette O'Connor  
Director | Planner  
The Planning Collective Limited

Date: 16 August 2023

**Address for Service:**

The Planning Collective Limited  
Attn: Burnette O'Connor  
PO Box 591  
Warkworth  
0941  
Ph: 021-422-346  
Email: [burnette@thepc.co.nz](mailto:burnette@thepc.co.nz)

**Attachments**

- A – Aerial and Street views of Burger King
- B – Consented Plans 102-104 Main Road, Kumeū
- C - General Arrangement Plan for the NOR S2



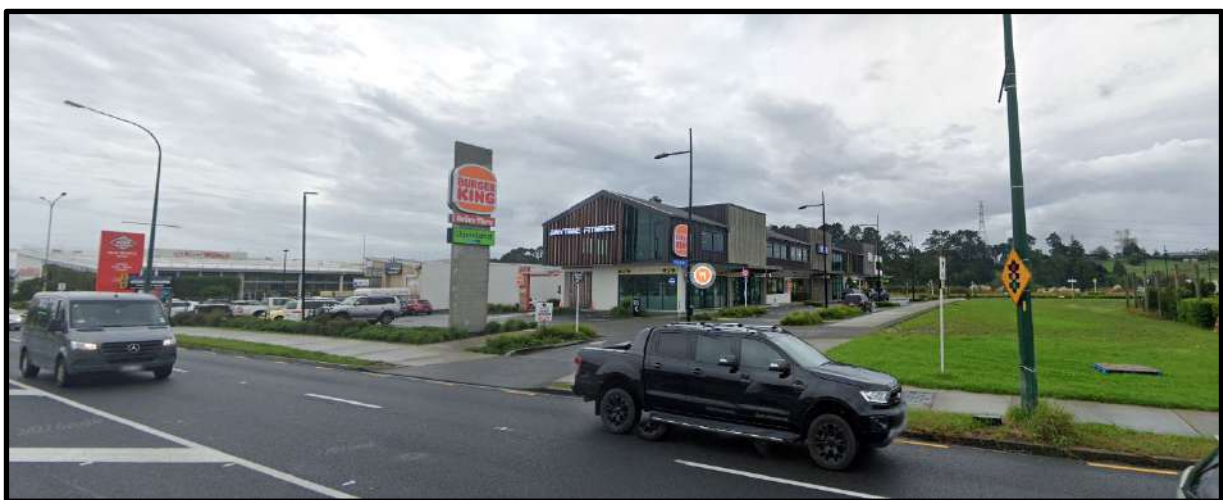
## Attachment A: Aerial and Street views of Burger King



*Aerial view of Burger King, Kumeū (Source: Google Maps – accessed 20 April 2023)*



*Street view of Burger King, Kumeū (Source: Google Maps – accessed 20 April 2023)*



*Access to Burger King, including Drive Thru from Main Road, Kumeū (Source: Google Maps – accessed 20 April 2023)*



## Attachment B:

Consented Plans 102-104 Main Road, Kumeū





ARTIST IMPRESSION ONLY REFER TO DRAWINGS FOR ALL BUILDING INFORMATION

PROJECT NUMBER 19001

DATE 16/05/2019

Drawing Set  
 RESOURCE CONSENT  
 Revision A



**DRAWING LIST**

<b>00 OVERVIEW</b>	
00-01	COVERSHEET
<b>100 SITE PLANS</b>	
100-00	SITE LOCALITY PLAN
100-01	SITE PLAN
<b>200 PLANS</b>	
200-01	BUILDING 01 FLOOR PLANS
200-02	BUILDING 02 FLOOR PLANS
200-03	ROOF PLAN - BUILDING 1
200-04	ROOF PLAN - BUILDING 2
<b>300 ELEVATIONS &amp; SECTIONS</b>	
300-01	ELEVATIONS 1
300-02	ELEVATIONS 2
300-03	ELEVATIONS 3
300-04	ELEVATIONS 4
300-05	EXTERNAL CLADDING & SIGNAGE PLINTH
300-06	SECTION 1
<b>400 3D IMAGES</b>	
400-01	PERSPECTIVE VIEW 1
400-02	PERSPECTIVE VIEW 3

**PROJECT TEAM**

Architects  
**GEL ARCHITECTS**  
 Tel: 09 388 4403

Town Planner  
**B & A URBAN & ENVIRONMENTAL**  
 Tel: 021 422 346

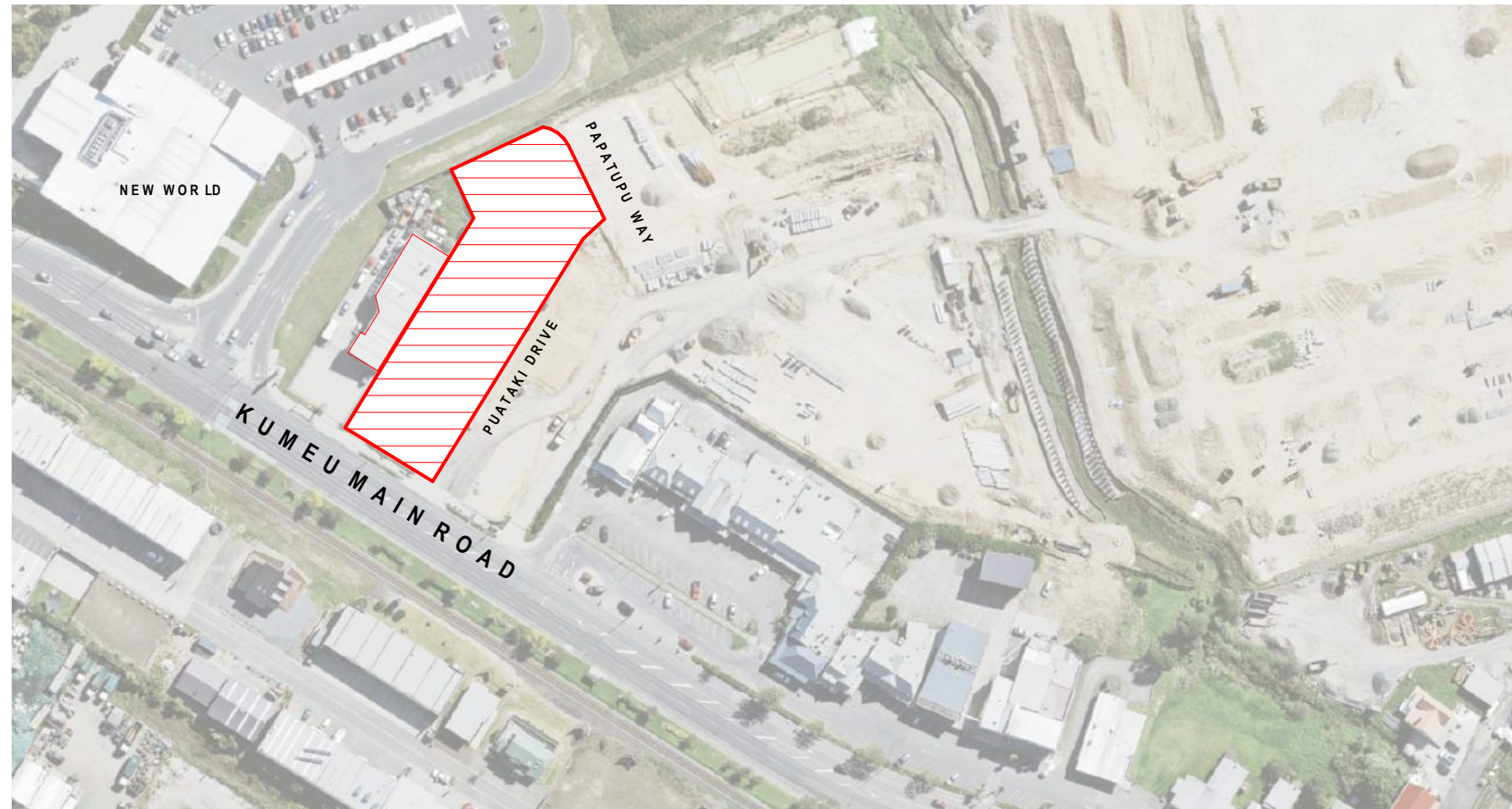
Traffic Engineer  
**TEAM TRAFFIC**  
 Tel: 09 838 3888

Civil / Drainage Engineer  
**MSC CONSULTING GROUP**  
 Tel: 09 486 2210

Urban Designer  
**TRANSURBAN**  
 Tel: 09 309 2555

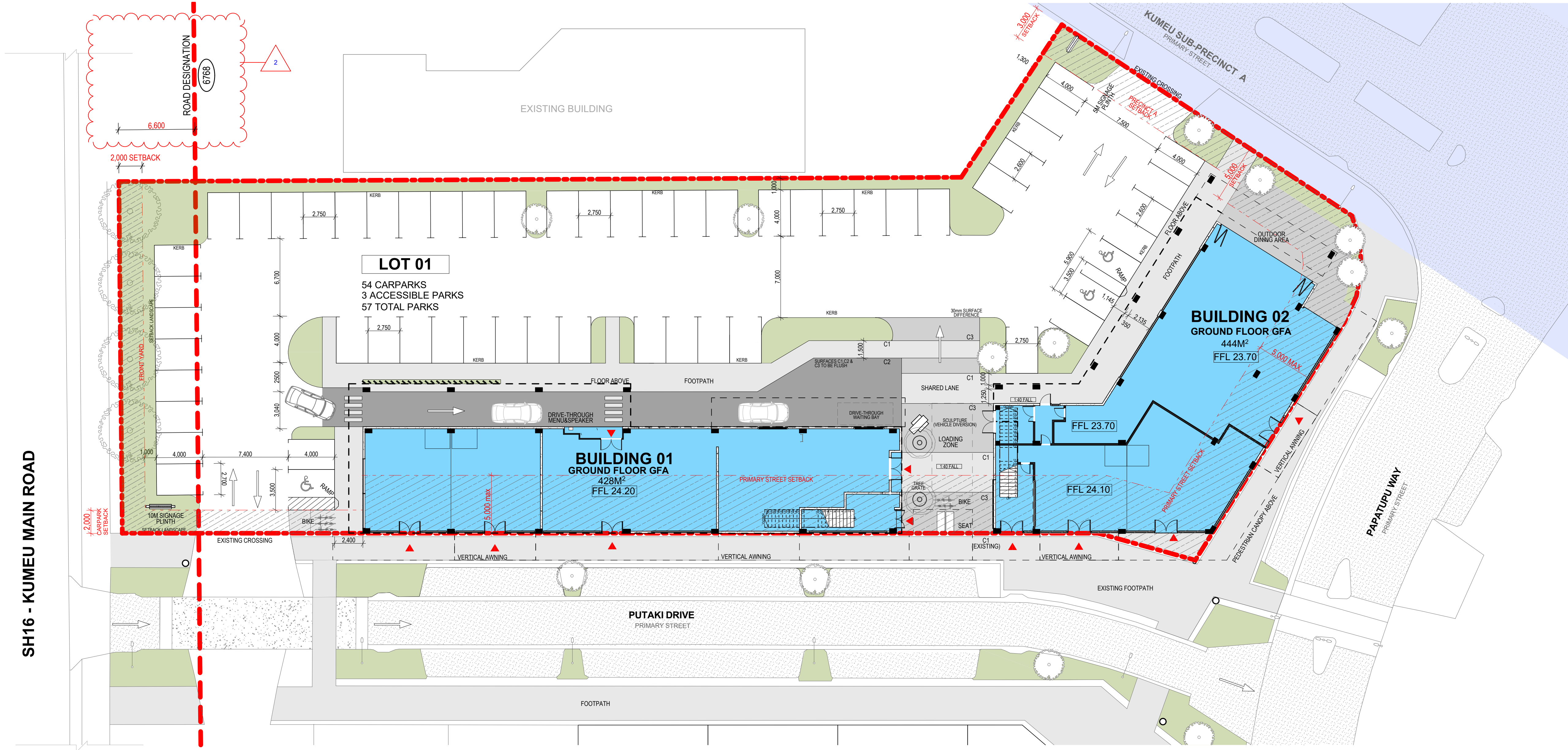
Landscape Architect  
**SHAFAER DESIGN**  
 Tel: 09 421 1412





Location  
NTS





**SITE PLAN**  
 1:200@A1

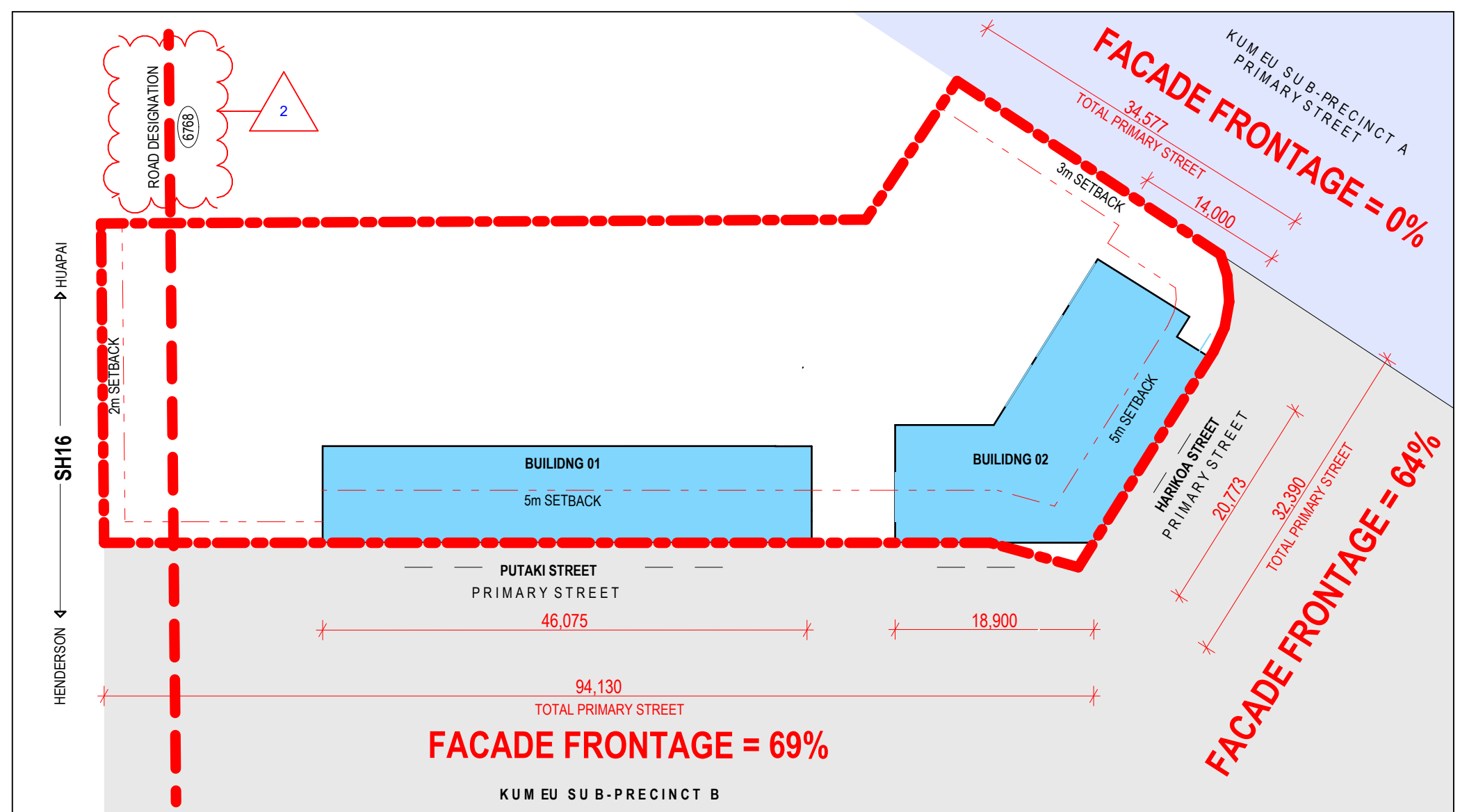
REFER TO LANDSCAPE ARCHITECTS DRAWINGS FOR PLANTING PLAN

**SITE INFORMATION**

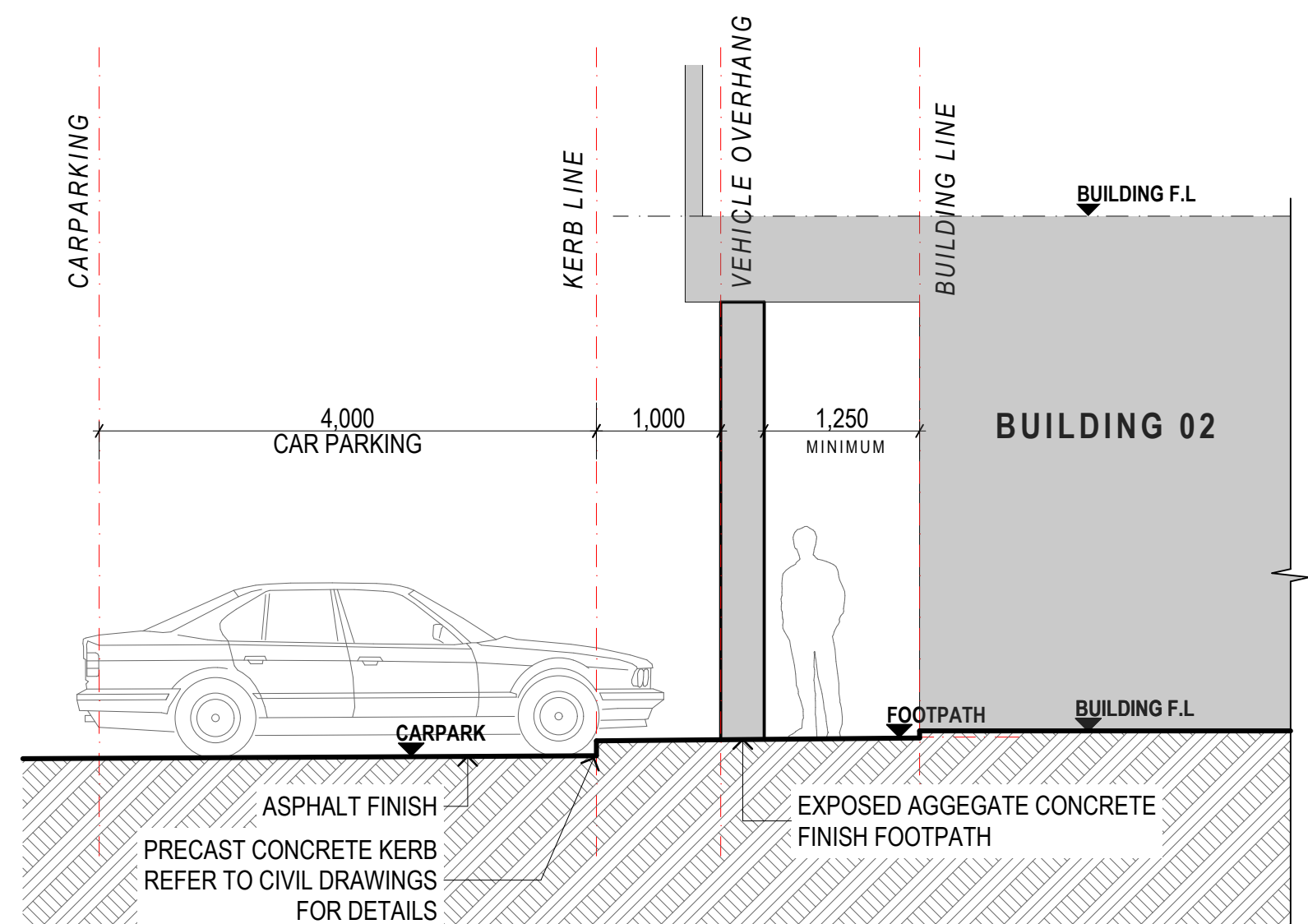
LEGAL DESCRIPTION:	LOT 3, DP 81928
WIND ZONE:	HIGH WIND ZONE
ZONE:	TOWN CENTRE MIXED USE, KUMEU SUB-PRECINCT B
SITE AREA:	3300m <sup>2</sup>
<b>CARPARKING</b>	<b>TOTAL 57</b>
SITE CARPARKS:	54 (3 ACCESSIBLE CARPARKS)
<b>BICYCLE PARKING</b>	<b>5</b>

**SITE AREAS**

SITE AREA	3345m <sup>2</sup>	
<b>BUILDING COVERAGE</b>		
BUILDING 01	548	
BUILDING 02	561	
	1,199m <sup>2</sup>	36%
<b>IMPERMEABLE PAVING &amp; CARPARK</b>	1,641m <sup>2</sup>	49%
<b>PERMEABLE LANDSCAPE</b>	460m <sup>2</sup>	14%



**FAÇADE FRONTAGE**  
 1:500@A1



**CARPARKING FOOTPATH DETAIL - BUILDING 2**  
 1:50@A1



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**NZIA PRACTICE**

RevID	ChID	Issue Date	Notes
A	1	28/06/2019	RETROSPECTIVE ISSUE 16-05-2019: ISSUED FOR RC
B	2	28/06/2019	S92 RC RFL: ROAD DESIGNATION ADDED



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**

For: **MATVIN GROUP LTD**

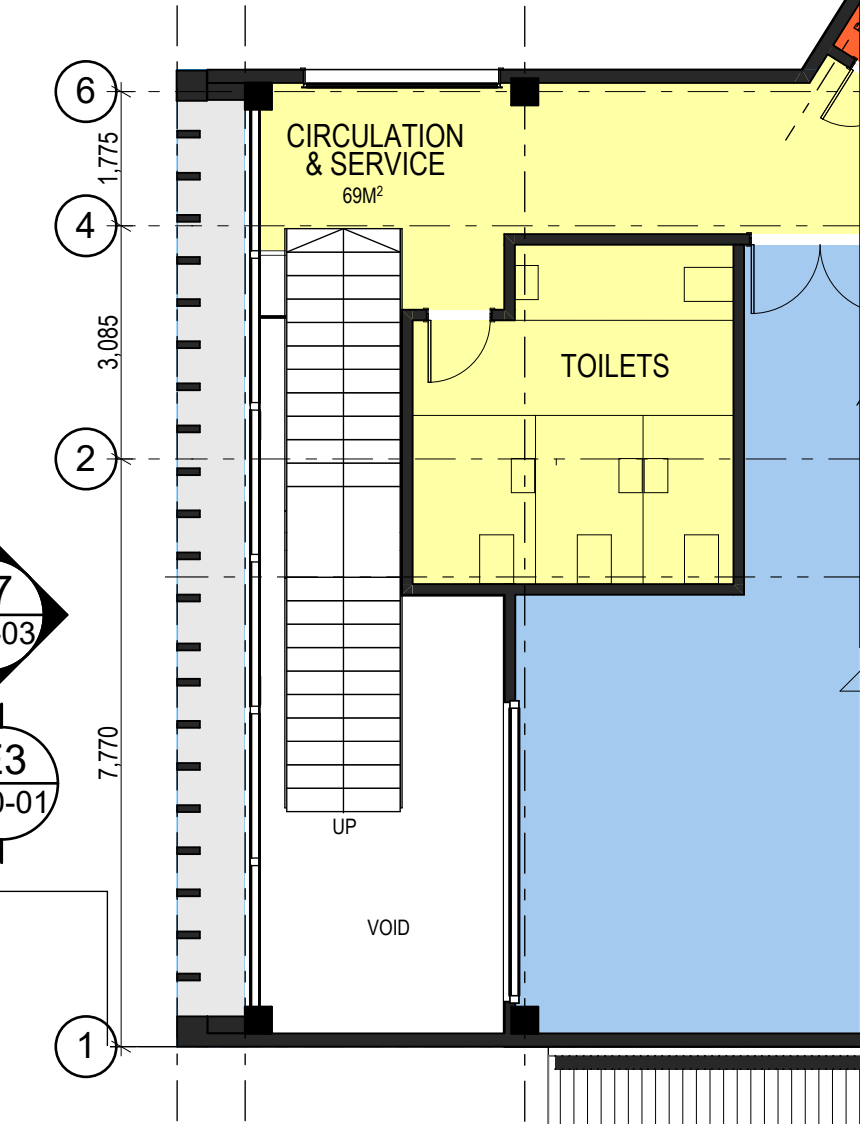
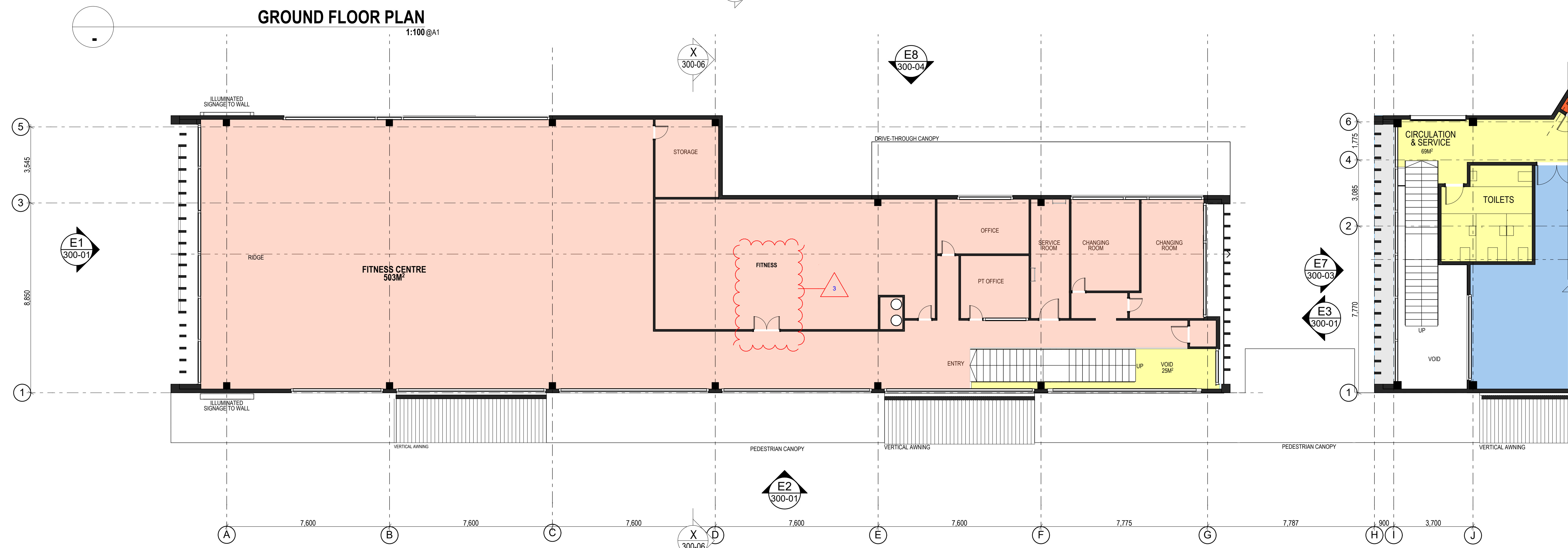
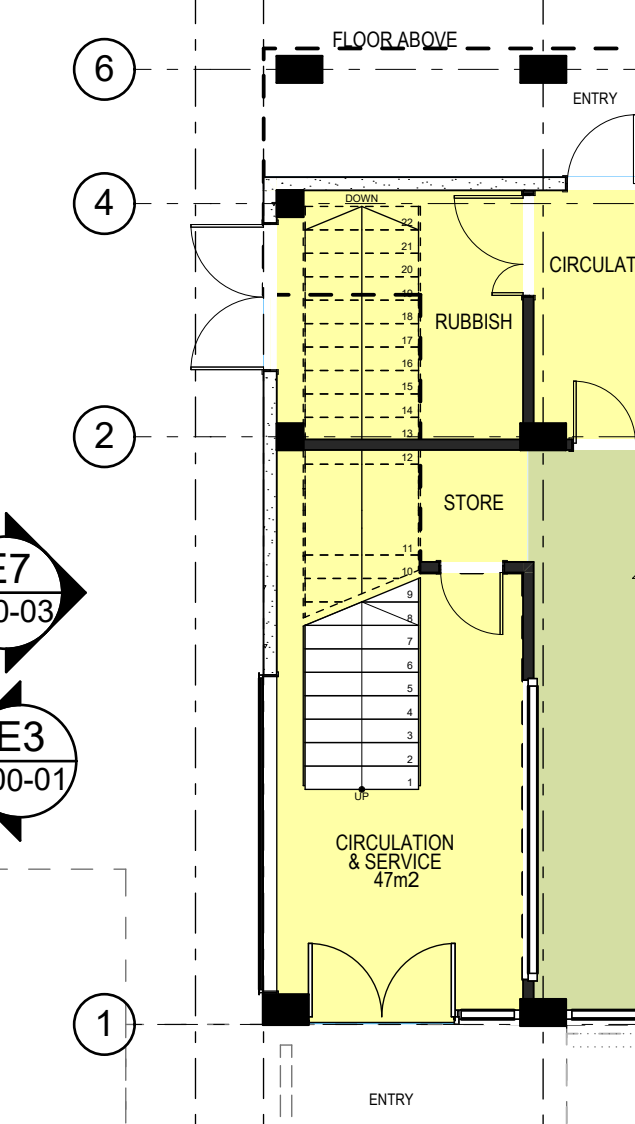
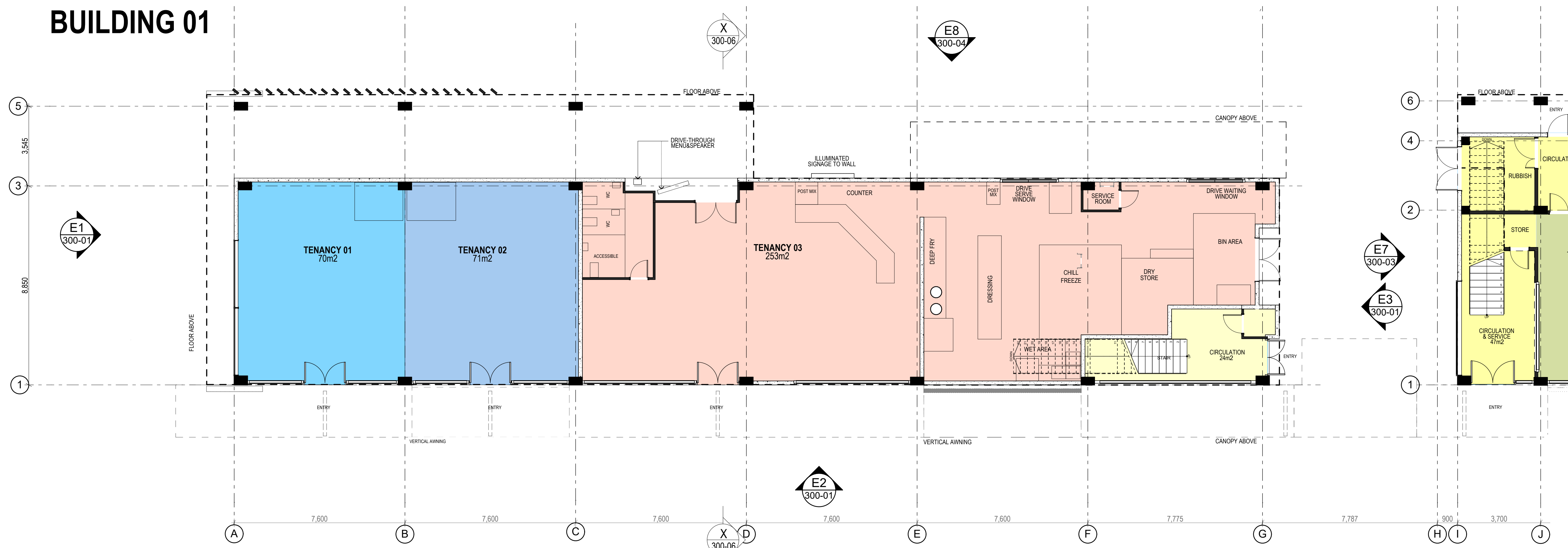
Drawing Title: **SITE PLAN**

**RESOURCE CONSENT**

Project No:	<b>19001</b>	Drawing No:	<b>100-01</b>
Scale @ A1:	1:200, 1:500, 1:50		
Scale @ A3:	1:400, 1:1000, 1:100		
Date:	28/06/2019	Revision:	<b>B</b>



# BUILDING 01



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**NZIA PRACTICE**

RevID	ChID	Issue Date	Notes
A	1	28/06/2019	RETROSPECTIVE ISSUE 16-05-2019; ISSUED FOR RC
B	3	28/06/2019	S92 RC RFI; ROOM NAME REVISED



Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
 For: **MATVIN GROUP LTD**

## BUILDING 01 FLOOR PLANS

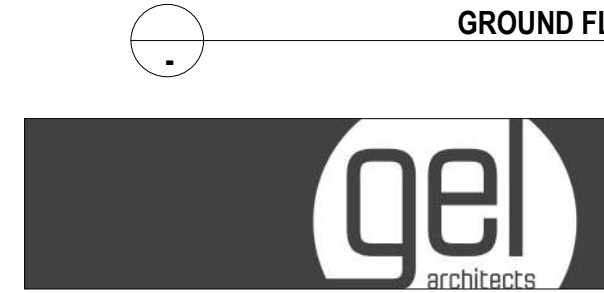
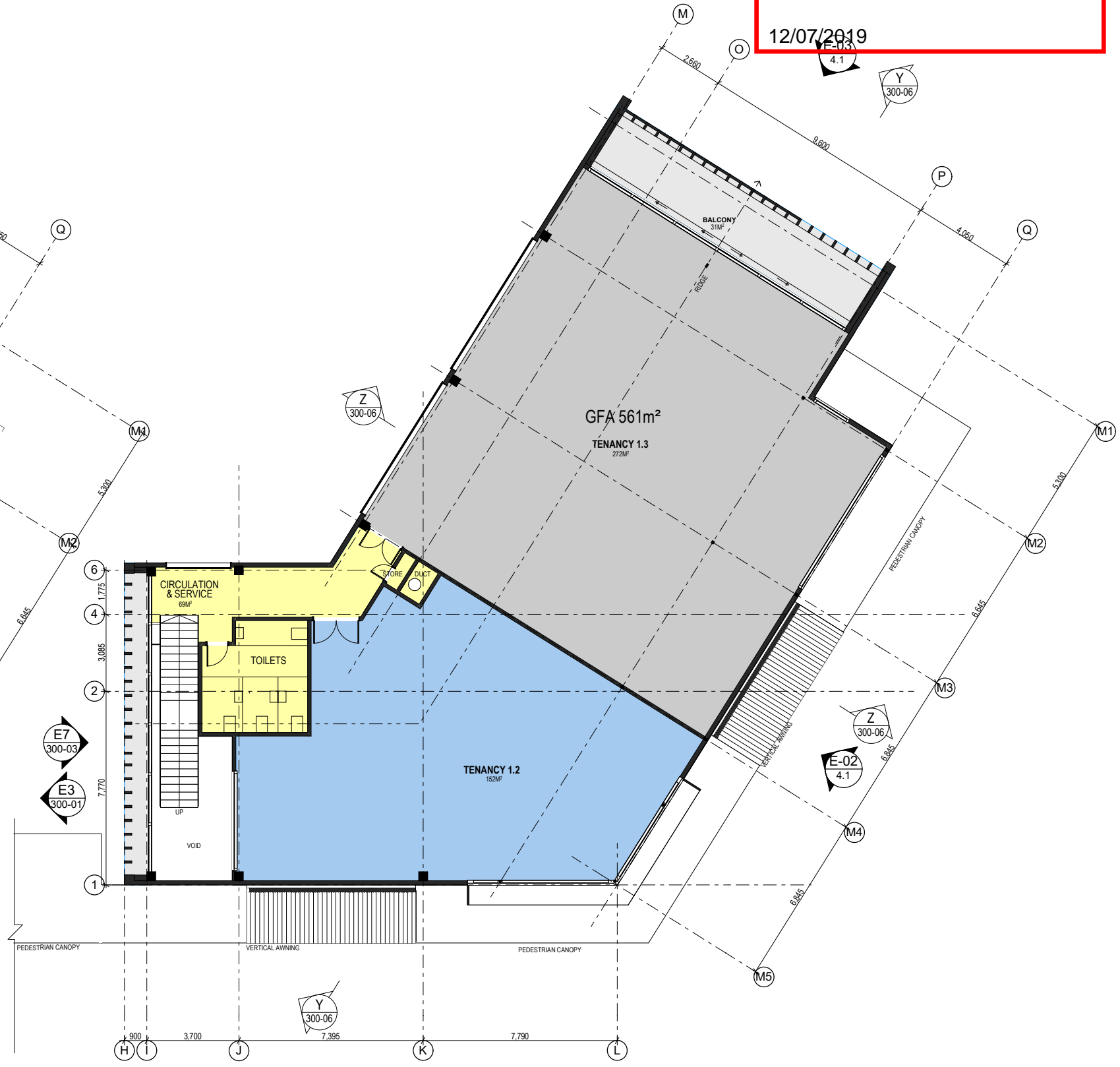
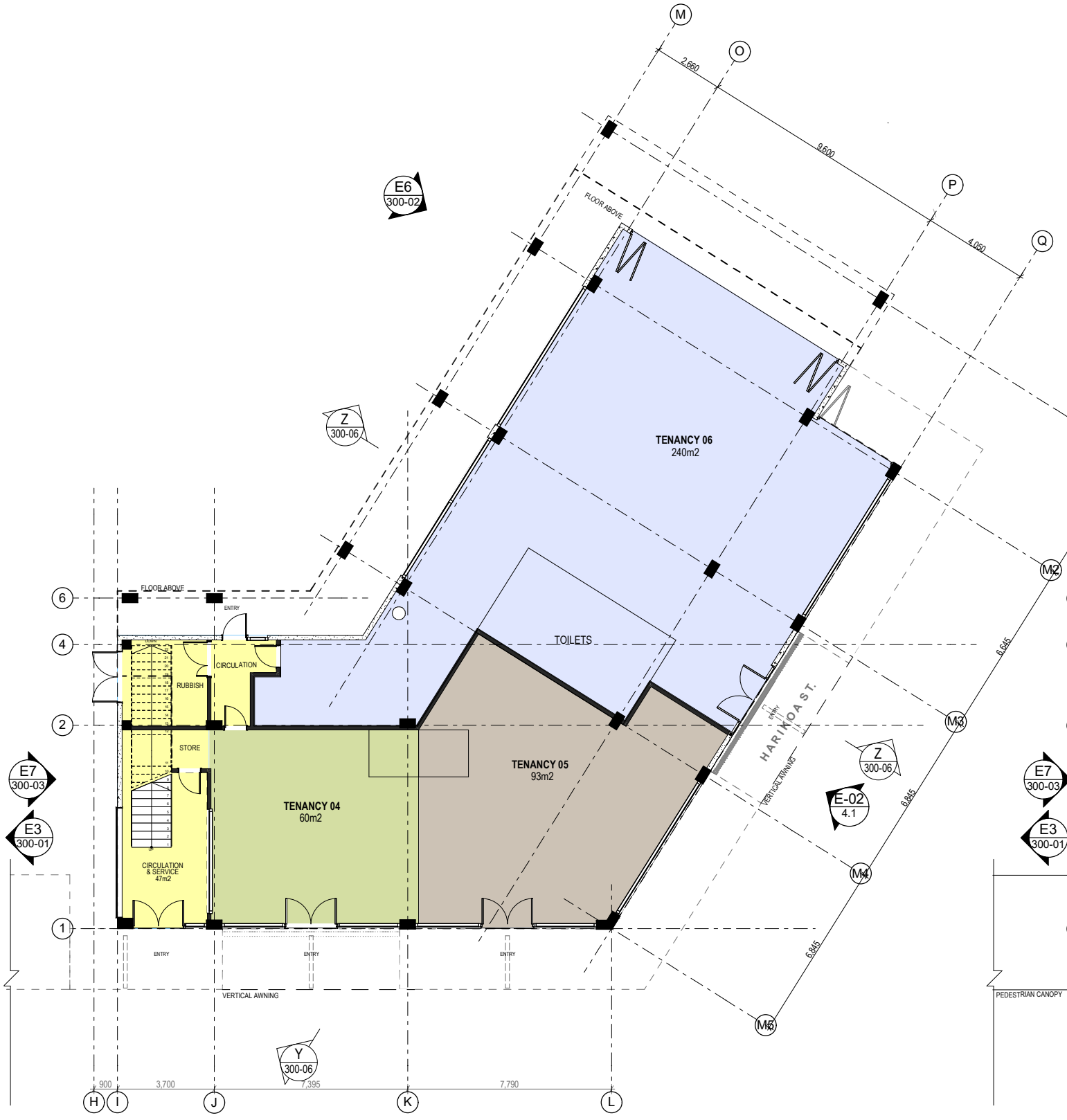
RESOURCE CONSENT	
Project No:	<b>19001</b>
Scale @ A1:	1:100
Scale @ A3:	1:200
Date:	28/06/2019
Drawing No:	<b>200-01</b>
Revision:	<b>B</b>



# BUILDING 02



**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019



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NZIA PRACTICE

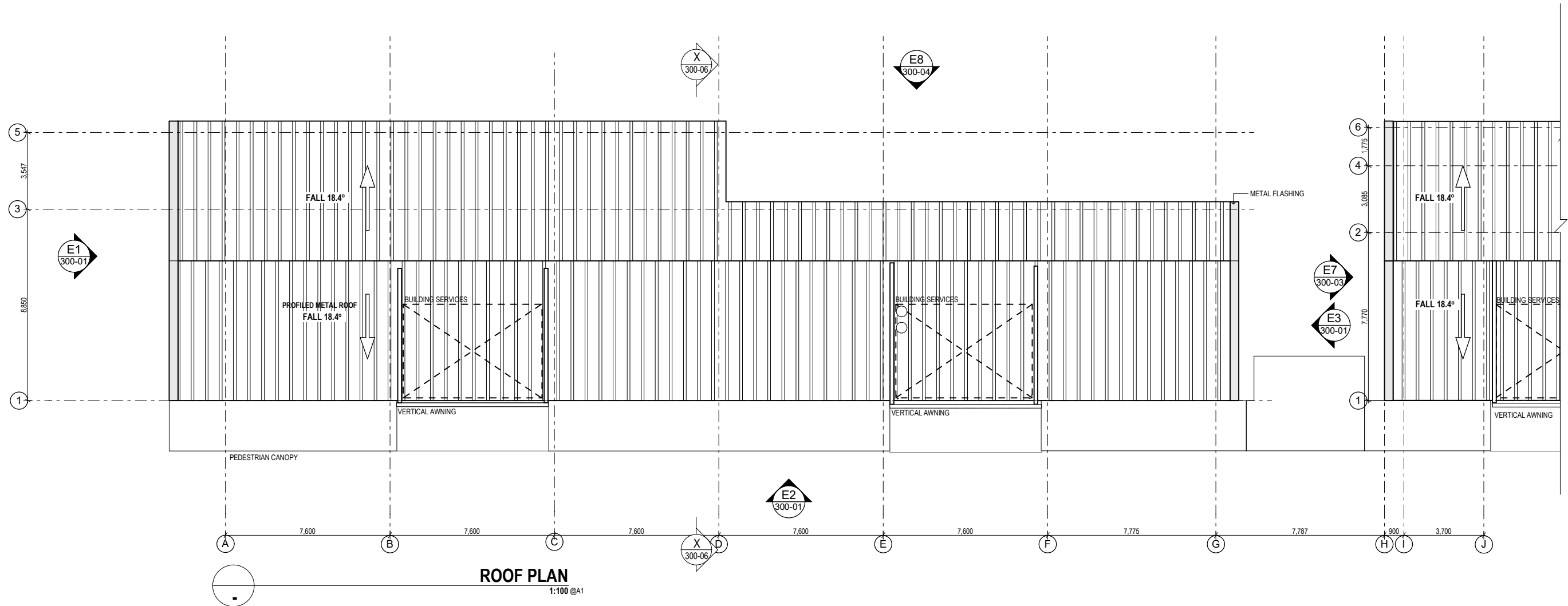
Revisions:  
 Rev: Date: Notes:



Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
 For: 61 MATVIN GROUP LTD

Drawing Title: **BUILDING 02 FLOOR PLANS**

RESOURCE CONSENT	
Project No: 19001	Drawing No: 200-02
Scale @ A1:	1:200
Scale @ A3:	
Date: 16/05/2019	Revision: A



**ROOF PLAN**  
1:100 @A1



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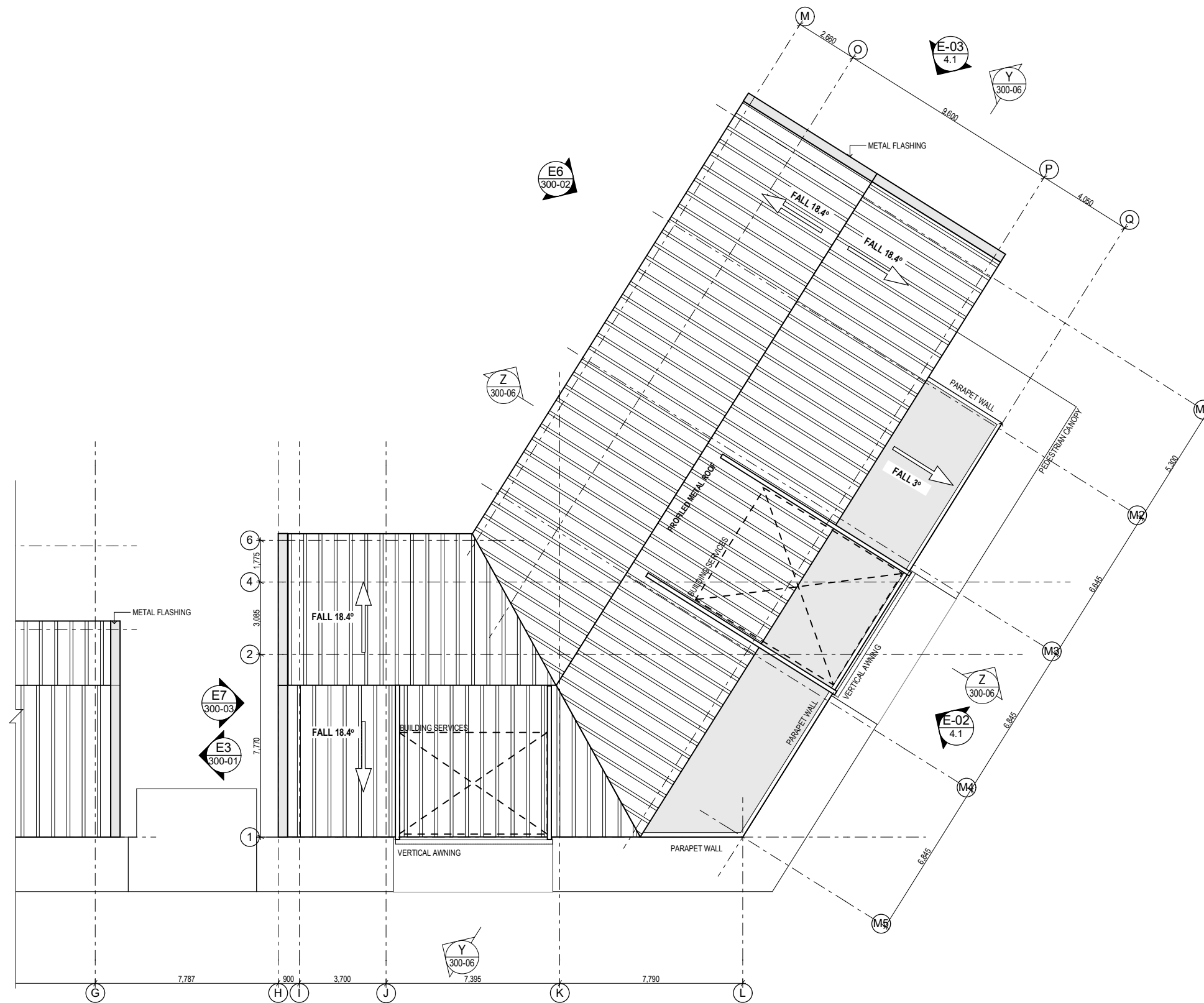
Revisions:  
Rev: Date: Notes:



Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
For: 62 MATVIN GROUP LTD

Drawing Title: **ROOF PLAN - BUILDING 1**

Project No: 19001		Drawing No: 200-03	
Scale @ A1: 1:100		Scale @ A3: 1:200	
Date: 16/05/2019	Revision: A		



**ROOF PLAN**  
1:100@A1



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Revisions:  
Rev: Date: Notes:

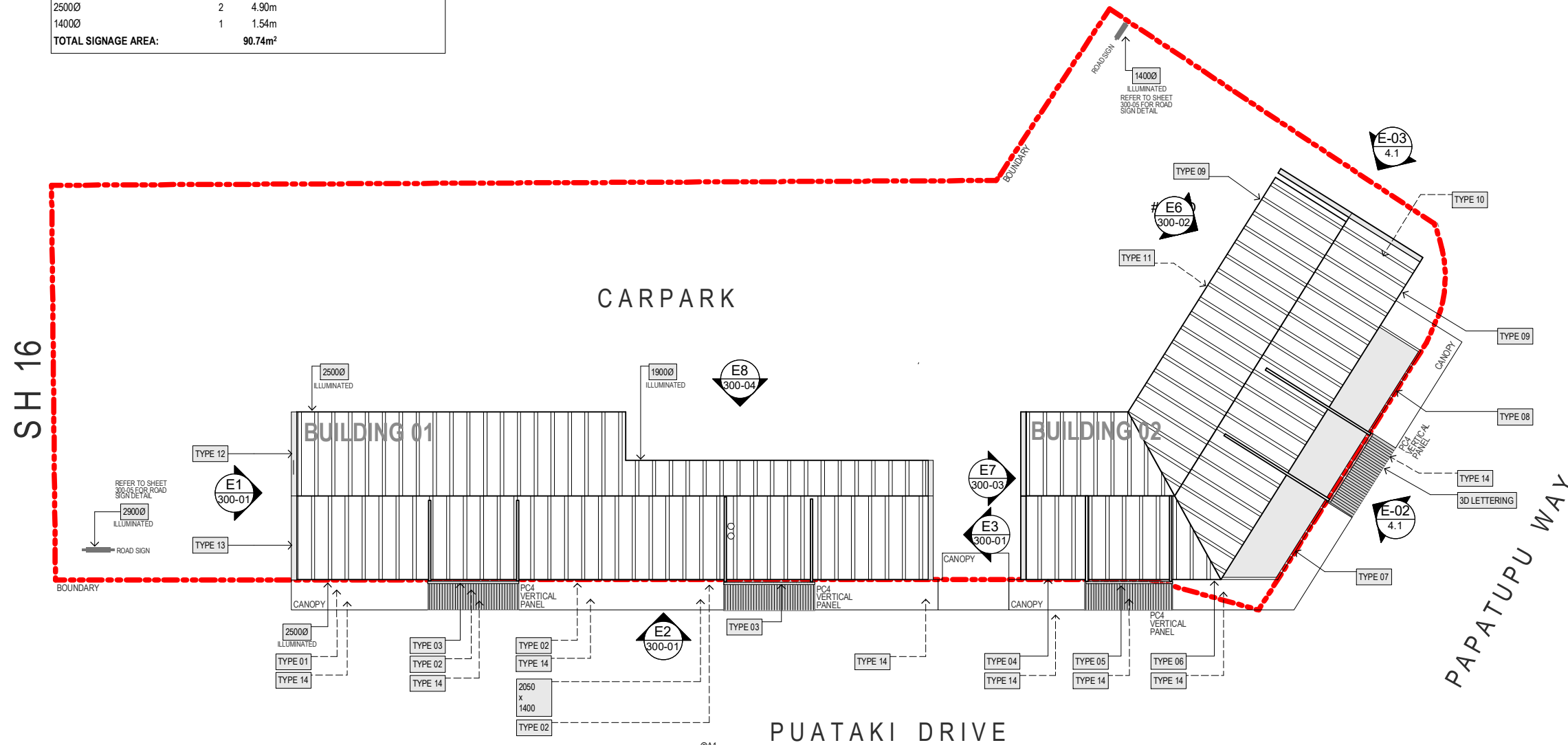


Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
For: **63 MATVIN GROUP LTD**

Drawing Title: **ROOF PLAN - BUILDING 2**

Project No: <b>19001</b>		Drawing No: <b>200-04</b>	
Scale @ A1: 1:100			
Scale @ A3: 1:200			
Date: 16/05/2019	Revision: <b>A</b>		

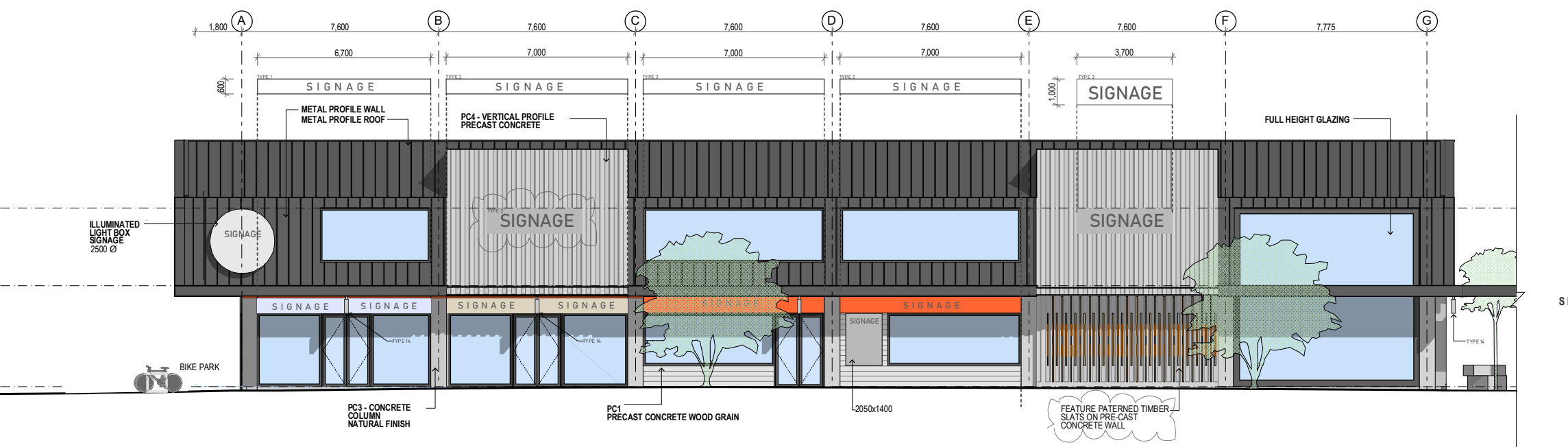
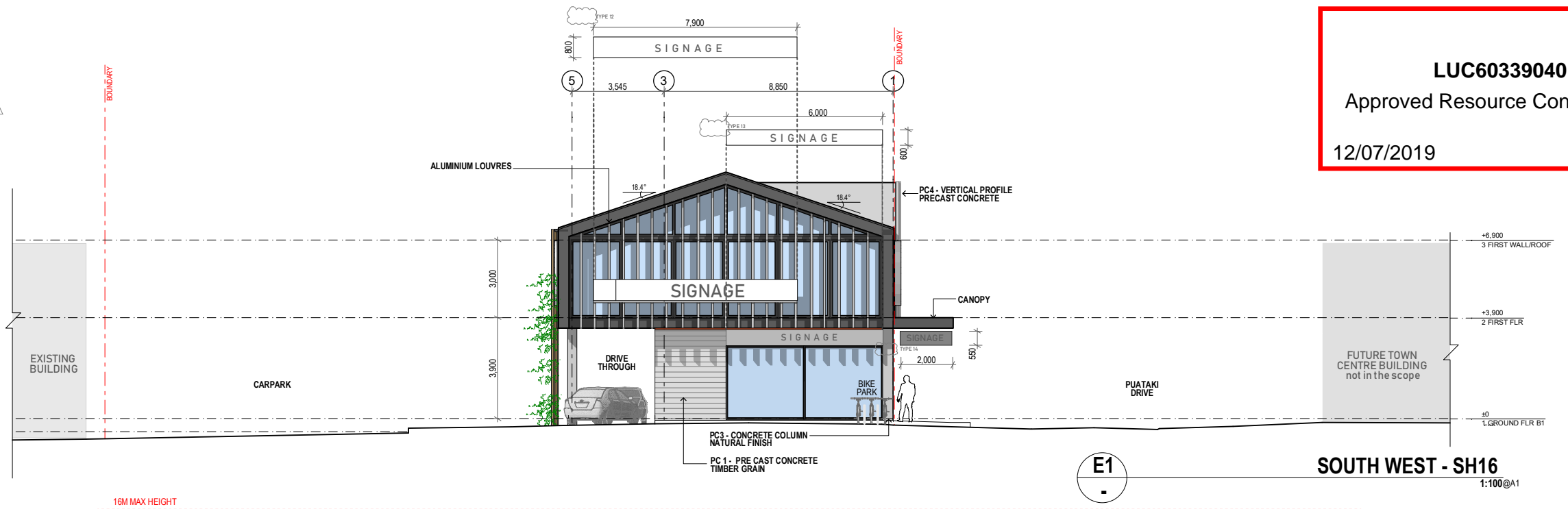
SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 8 6200 x 600(mm)	1	3.7	NON ILLUMINATED (SHOP FRONT)
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	LIGHT BOX (UNDER CANOPY)
2900Ø	1	6.5m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
2500Ø	2	4.90m	
1400Ø	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	



**SIGNAGE PLAN**  
1:200

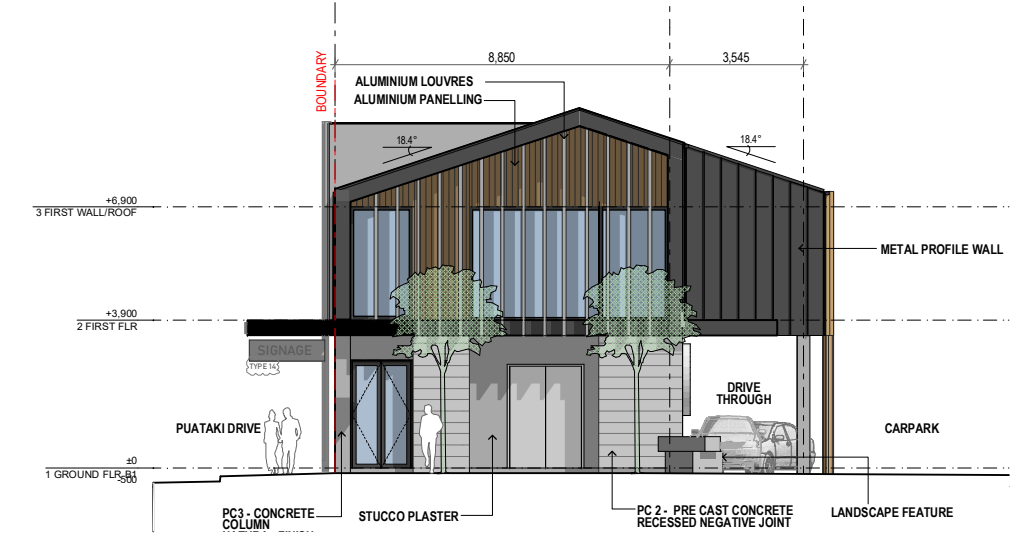
**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019

SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 8 6200 x 600(mm)	1	3.7	NON ILLUMINATED (SHOP FRONT)
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	LIGHT BOX (BUILDING FACADE)
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	NON ILLUMINATED (SHOP FRONT)
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	LIGHT BOX (UNDER CANOPY)
2900Ø	1	6.5m <sup>2</sup>	NON ILLUMINATED (SHOP FRONT)
2500Ø	2	4.90m <sup>2</sup>	
1400Ø	1	1.54m <sup>2</sup>	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	



**CONCRETE FINISHES**

<b>PC1</b> PRECAST CONCRETE HORIZONTAL TIMBER GRAIN	<b>PC3</b> NATURAL PRECAST CONCRETE
<b>PC2</b> PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT	<b>PC4</b> VERTICAL AWNING RIB PRE CAST CONCRETE



**E2 SOUTH EAST ELEVATION- Puataki Drive**  
 1:100@A1

**E3 NORTH EAST**  
 1:100@A1



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Revisions:  
 Rev: B Date: 19.06.19 Notes: RC - SECTION 92



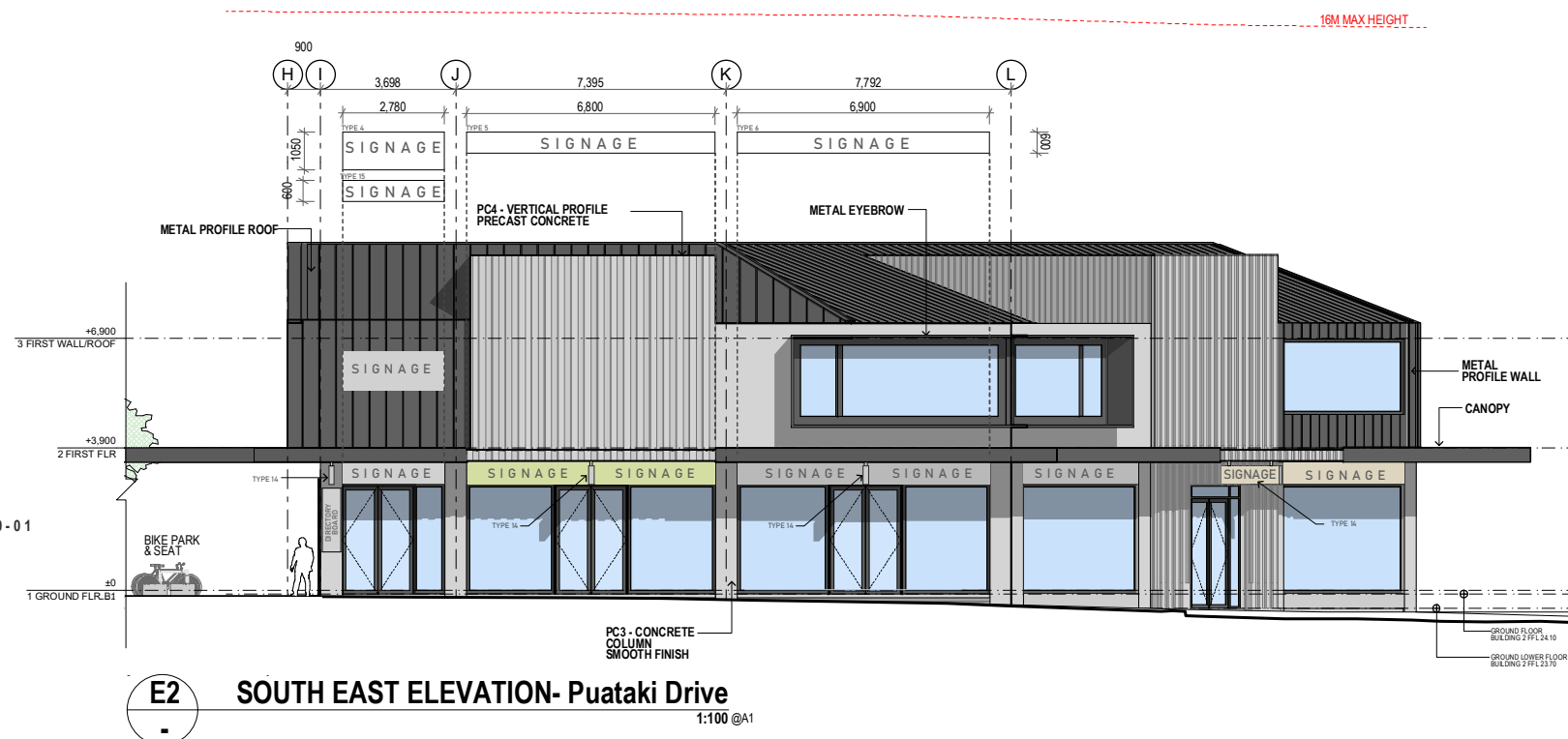
Project: **Lot 1 - 102-104 MAIN ROAD KUMEU**  
 For: **65 MATVIN GROUP LTD**

Drawing Title: **ELEVATIONS 1**

Project No:	19001	Drawing No:	300-01
Scale @ A1:	1:100, 1:50	Scale @ A3:	1:200
Date:	19/06/2019	Revision:	B

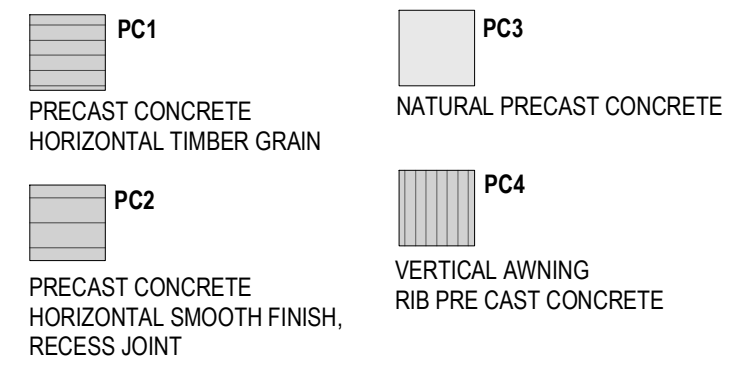


SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (PC4 CONCRETE PANEL)			
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (UNDER CANOPY)			
NON ILLUMINATED (SHOP FRONT)			
29000	1	6.5m <sup>2</sup>	
25000	2	4.90m	
14000	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	

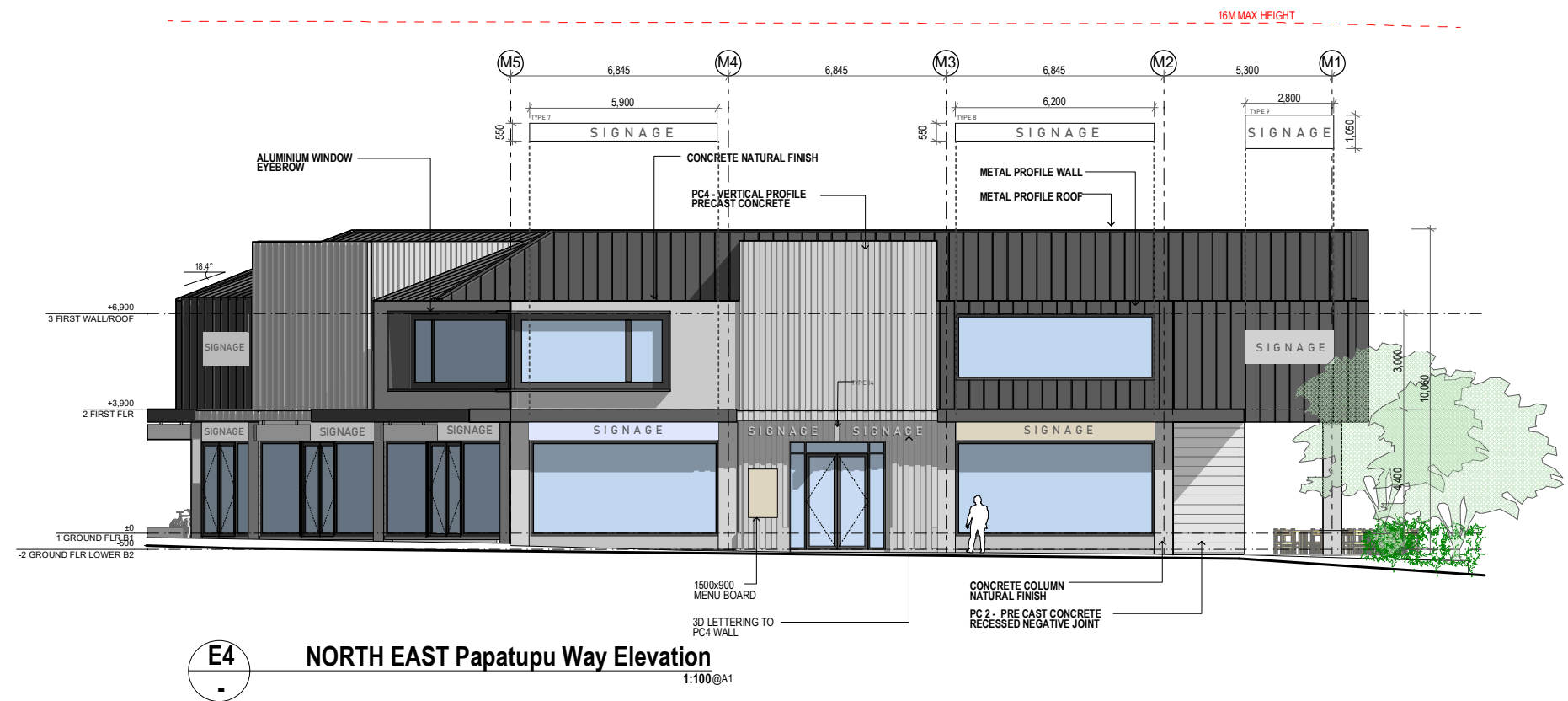


**E2 SOUTH EAST ELEVATION- Puataki Drive**  
1:100 @A1

CONCRETE FINISHES



Auckland Council  
**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019



**E4 NORTH EAST Papatupu Way Elevation**  
1:100 @A1



**E5 NORTH WEST**  
1:100 @A1



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Revisions:  
 Rev: B Date: 19.06.19 Notes: RC - SECTION 92



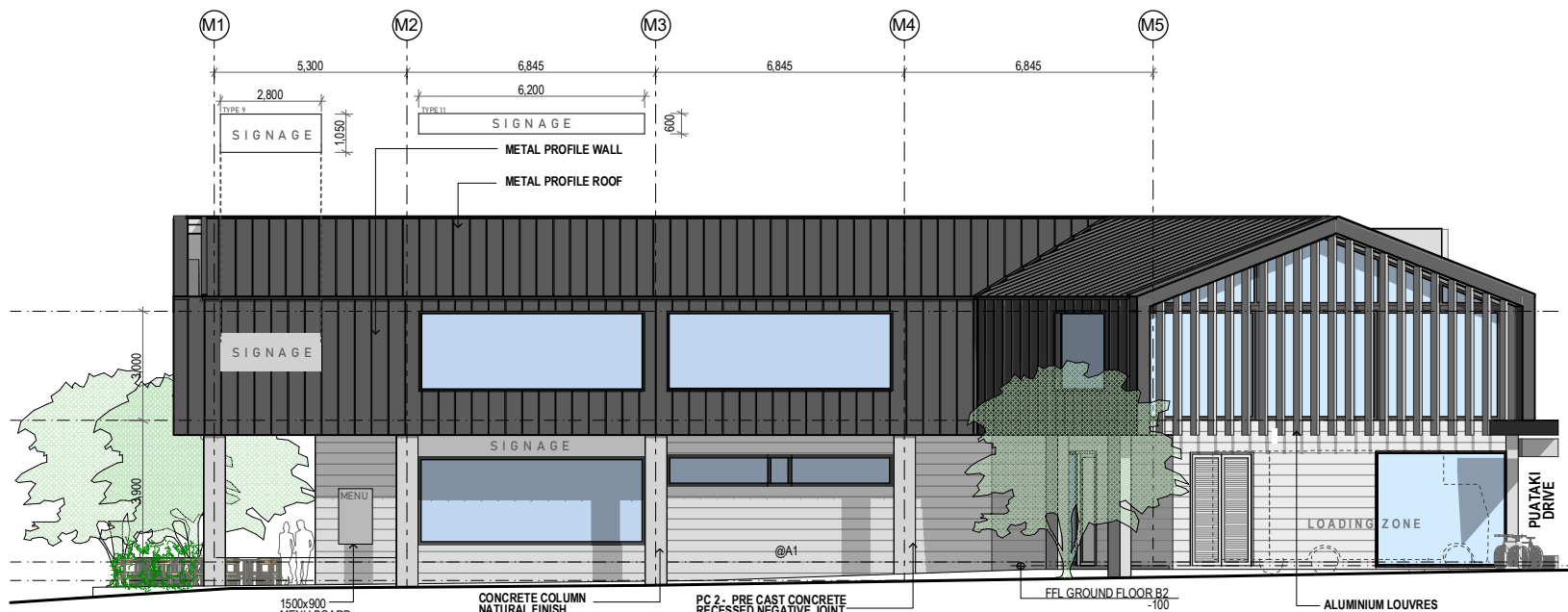
Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
 For: 66 MATVIN GROUP LTD

Drawing Title: **ELEVATIONS 2**

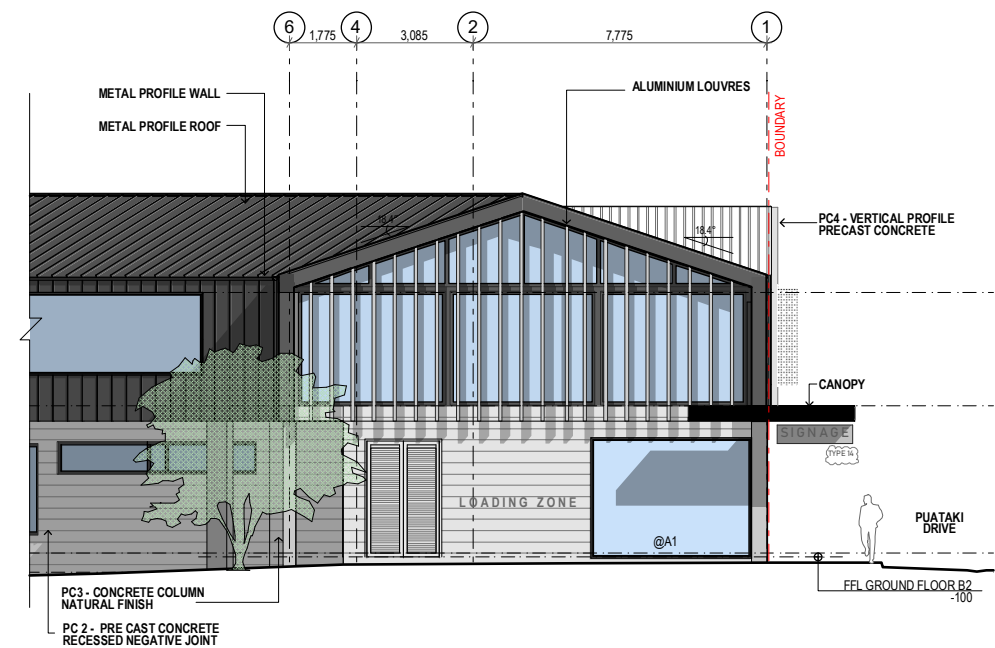
RESOURCE CONSENT	
Project No: 19001	Drawing No: 300-02
Scale @ A1: 1:100, 1:50	
Scale @ A3: 1:200	
Date: 19/06/2019	Revision: B



SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
29000	1	6.5m <sup>2</sup>	
25000	2	4.90m	
14000	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	



E6 SOUTH WEST 1:100 @A1



E7 SOUTH WEST 1:100 @A1

CONCRETE FINISHES

- PC1**  
PRECAST CONCRETE HORIZONTAL TIMBER GRAIN
- PC2**  
PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT
- PC3**  
NATURAL PRECAST CONCRETE
- PC4**  
VERTICAL AWNING RIB PRE CAST CONCRETE

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Revisions:  
Rev: B Date: 19.06.19 Notes: RC - SECTION 92




Project: Lot 1 - 102-104 MAIN ROAD KUMEU  
For: 67 MATVIN GROUP LTD

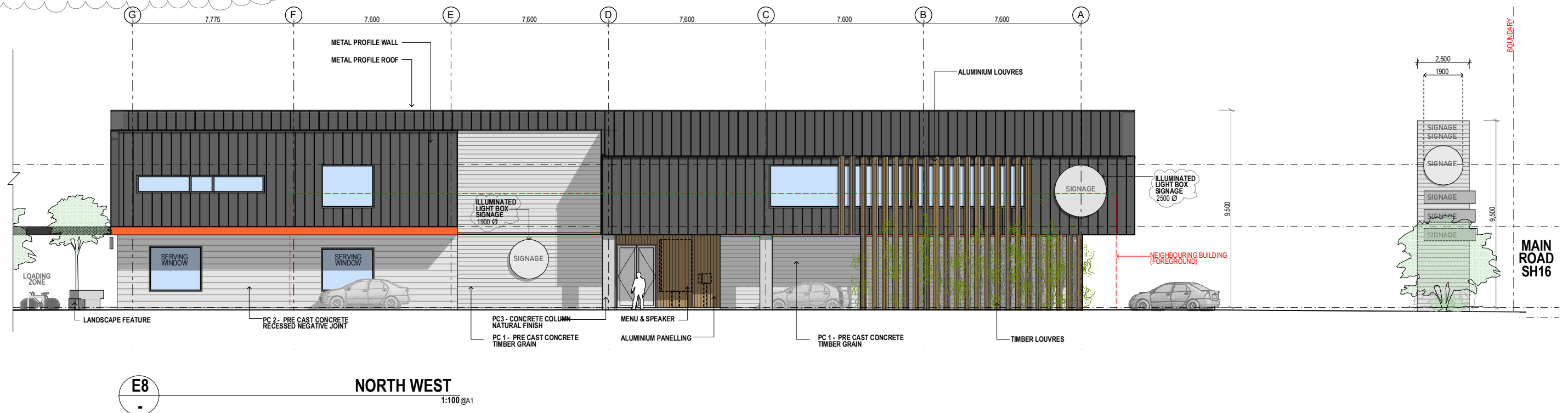
Drawing Title: ELEVATIONS 3

Project No: 19001		Drawing No: 300-03	
Scale @ A1: 1:100, 1:50			
Scale @ A3: 1:200			
Date: 19/06/2019	Revision: B		

**BUILDING 01**





SIGNAGE BUILDING 1:			
PROPOSED SIZE (LxH)	QUANTITY	AREA	ILLUMINATED
TYPE 1 6700x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 2 7000 x 600(mm)	3	12.6m <sup>2</sup> (4.2m <sup>2</sup> )	
TYPE 3 3700 x 1000(mm)	2	7.4m <sup>2</sup> (3.7m <sup>2</sup> )	
NON ILLUMINATED (SHOP FRONT)			
NON ILLUMINATED (SHOP FRONT)			
LIGHT BOX (PC4 CONCRETE PANEL)			
SIGNAGE BUILDING 2:			
TYPE 4 2700 x 1050(mm)	1	2.8m <sup>2</sup>	
TYPE 5 6800 x 600(mm)	1	4.0m <sup>2</sup>	
TYPE 6 6900 x 600(mm)	1	4.10m <sup>2</sup>	
TYPE 7 5900 x 600(mm)	1	3.50m <sup>2</sup>	
TYPE 8 6200 x 600(mm)	1	3.7	
TYPE 9 2800 x 1050(mm)	2	6.0m <sup>2</sup> (3.0m <sup>2</sup> )	
TYPE 10 9400 x 600(mm)	1	5.6m <sup>2</sup>	
TYPE 11 6200 x 600(mm)	1	3.7m <sup>2</sup>	
TYPE 12 7900 x 800(mm)	1	6.3m <sup>2</sup>	
TYPE 13 6000 x 600(mm)	1	3.6m <sup>2</sup>	
TYPE 14 2000 x 550(mm)	8	8.8m <sup>2</sup> (1.1m <sup>2</sup> )	
TYPE 15 2780 x 600(mm)	1	1.7m <sup>2</sup>	
NON ILLUMINATED (SHOP FRONT)			
2900Ø	1	6.5m <sup>2</sup>	
2500Ø	2	4.90m	
1400Ø	1	1.54m	
<b>TOTAL SIGNAGE AREA:</b>		<b>90.74m<sup>2</sup></b>	



  
**LUC60339040**  
 Approved Resource Consent Plan  
 12/07/2019



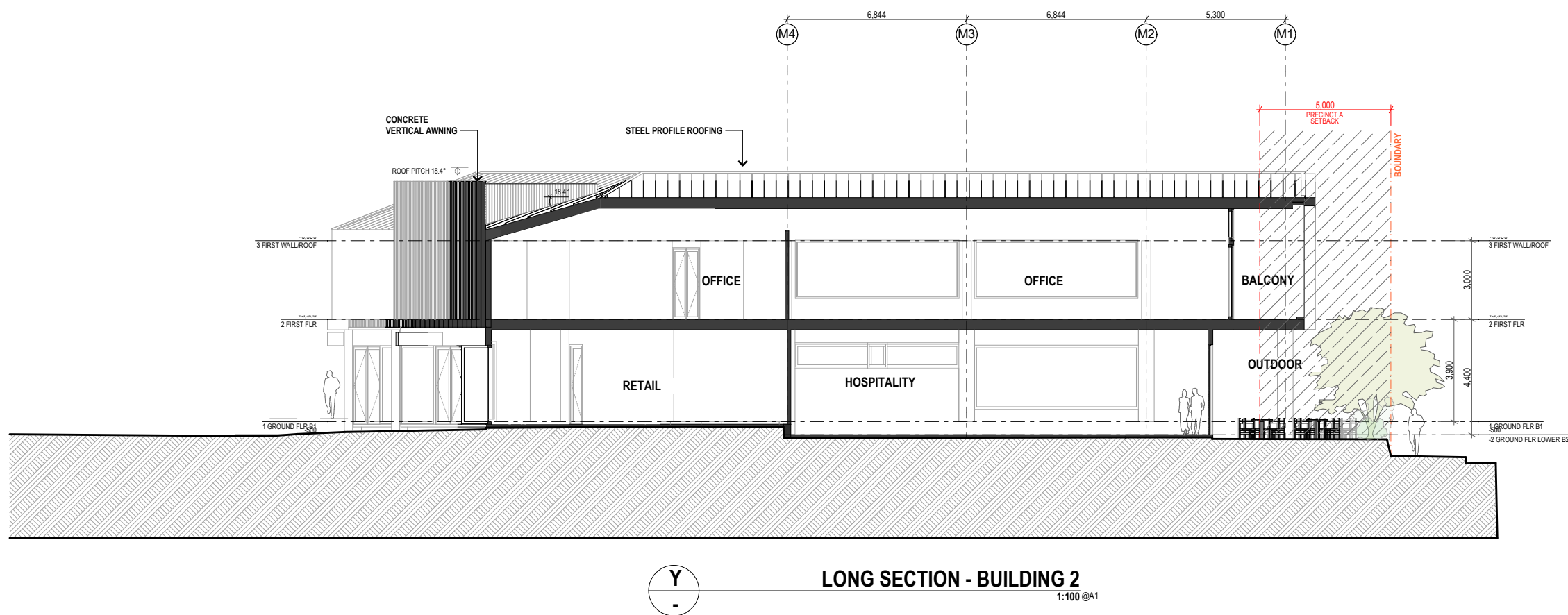
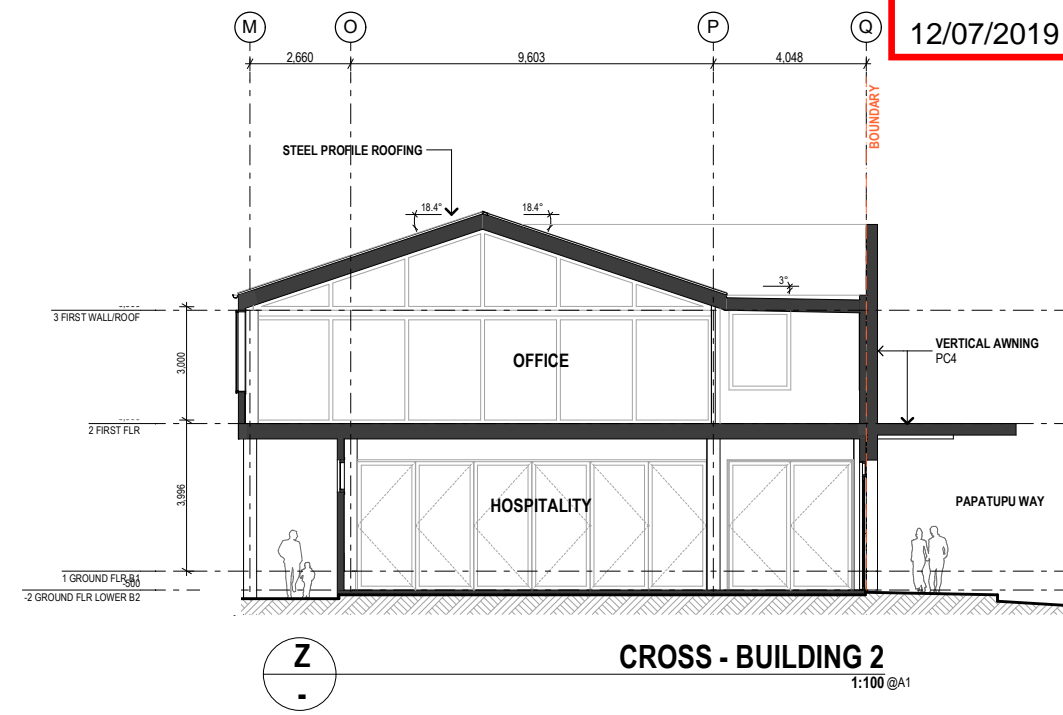
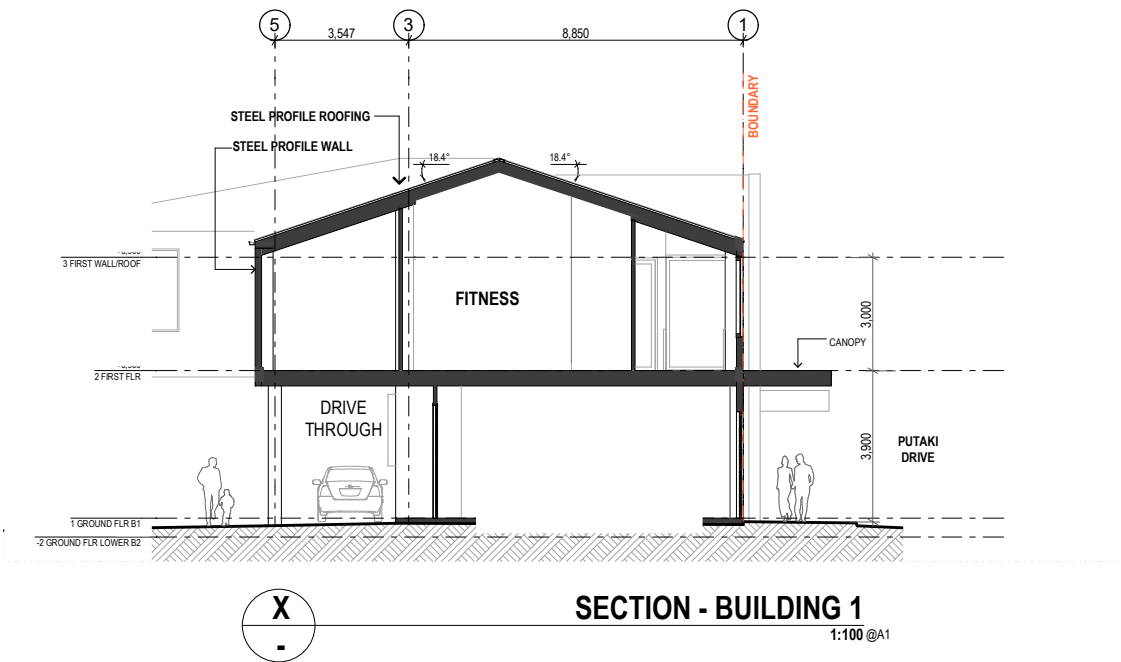
**E8** NORTH WEST  
1:100 @A1

**CONCRETE FINISHES**

 <b>PC1</b> PRECAST CONCRETE HORIZONTAL TIMBER GRAIN	 <b>PC3</b> NATURAL PRECAST CONCRETE
 <b>PC2</b> PRECAST CONCRETE HORIZONTAL SMOOTH FINISH, RECESS JOINT	 <b>PC4</b> VERTICAL AWNING RIB PRE CAST CONCRETE

	Do not scale drawings. Verify all dimensions on site before preparing Shop Drawings or commencing work. Any doubt, ambiguity or omission on any part of the drawings or specifications should be referred to the Architect for clarification prior to commencement of work. This drawing is COPYRIGHT and remains the property of Gel Architects Ltd.	Revisions: Rev: B Date: 19.06.19 Notes: RC - SECTION 92		Project: Lot 1 - 102-104 MAIN ROAD KUMEU	<b>ELEVATIONS 4</b>	Project No: 19001	Drawing No: 300-04
				For: 68 MATVIN GROUP LTD		Scale @ A1: 1:100, 1:50	Date: 19/06/2019

RESOURCE CONSENT



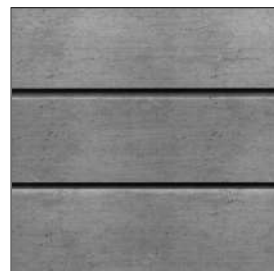


**EXTERNAL CLADDING LEGEND**

**CONCRETE**



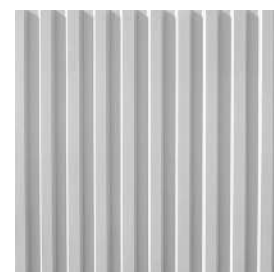
**PC1**  
PRECAST CONCRETE  
HORIZONTAL TIMBER GRAIN



**PC2**  
PRECAST CONCRETE  
HORIZONTAL SMOOTH FINISH,  
RECESS JOINT



**PC3**  
NATURAL PRECAST CONCRETE



**PC4**  
VERTICAL AWNING  
RIB PRE CAST CONCRETE



PROFILED METAL ROOFING  
& WALL (WRAPPED)  
(CHARCOAL GREY)

**CLADDING PRODUCTS**



VERTICAL ALUMINIUM -  
COLOURED

**SCREENS, LOUVRES**

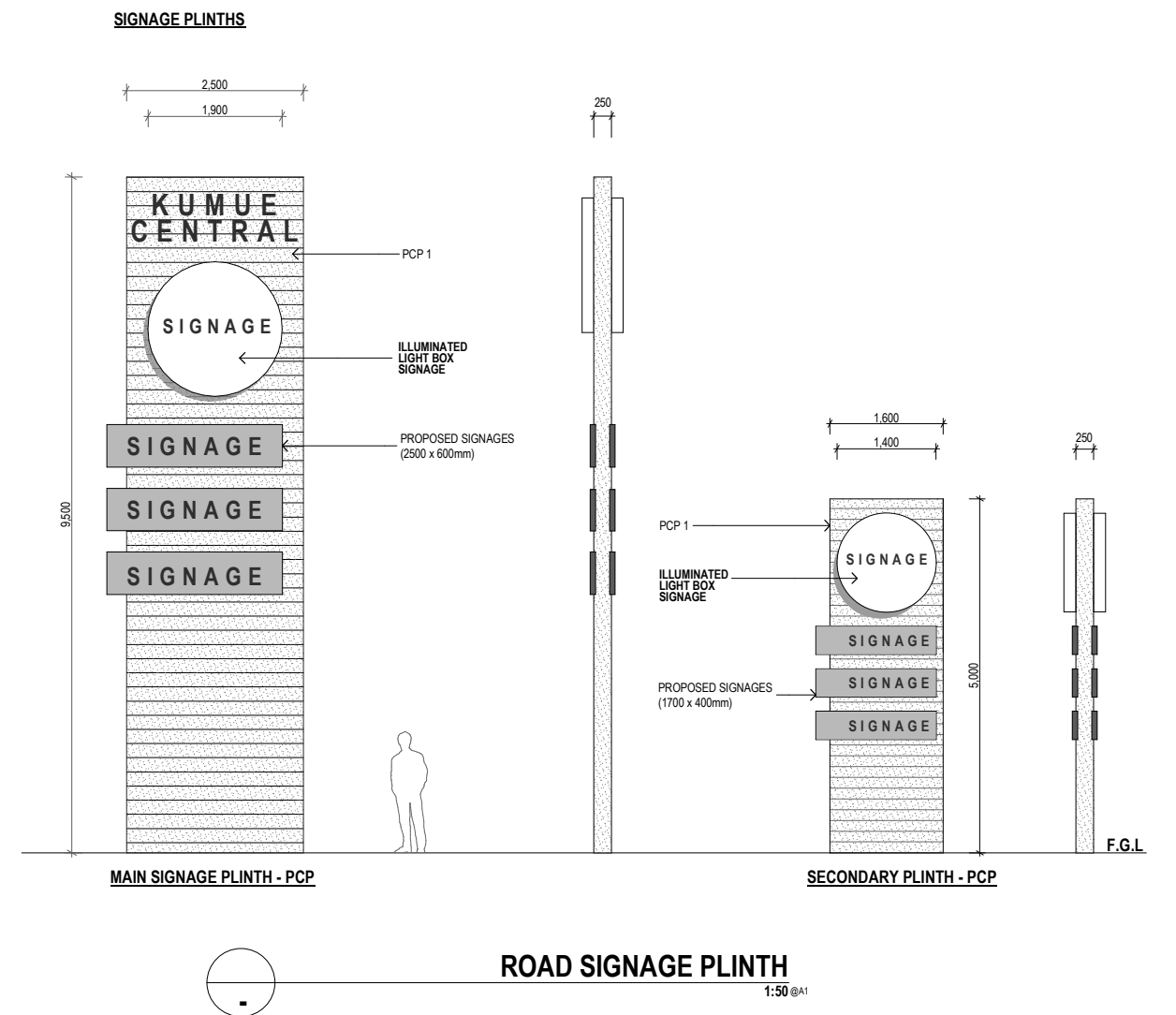


VERTICAL ALUMINIUM LOUVRES  
(DARK GREY OR BLACK)

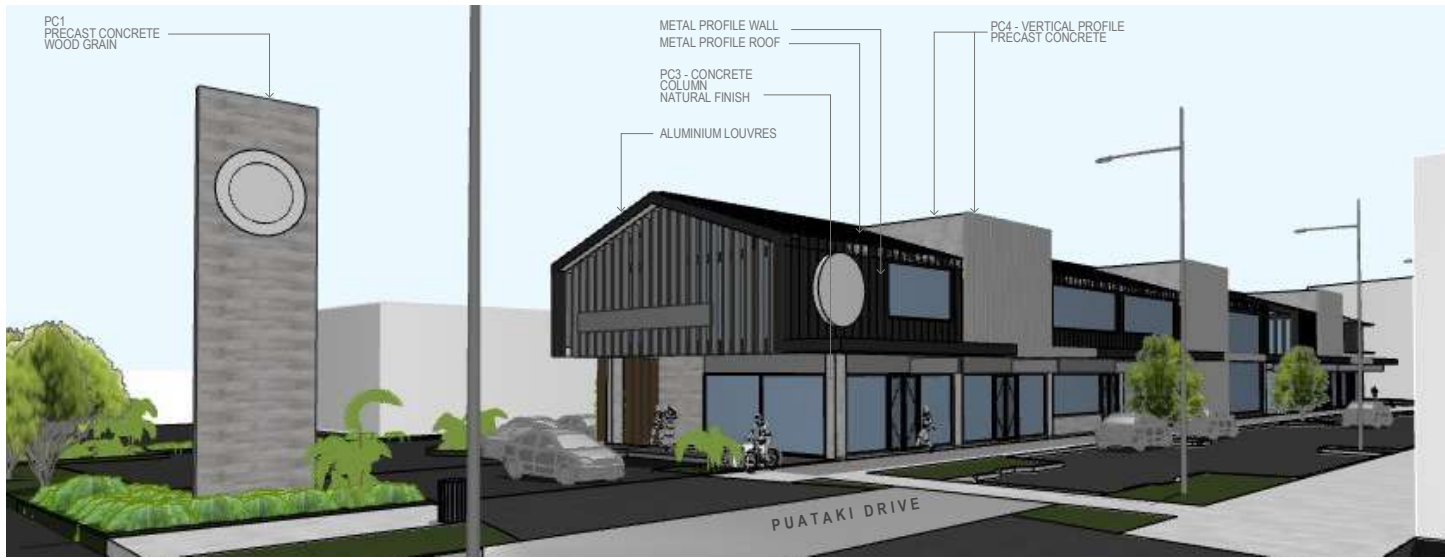
**PEDESTRIAN CANOPY & SOFFITS**



FIBRECEMENT FLUSH PANEL  
SOFFIT







○ ————— **PUATAKI DRIVE**  
1:77.4369



○ ————— **FROM SH16**  
1:100



○ ————— **PUATAKI DRIVE & PAPATUPU WAY**  
1:62.9800



○ ————— **PAPATUPU WAY**  
 1:75.3345



○ ————— **SECONDARY CARPARK ENTRY**  
 1:100





AERIAL 1 PUTAKI DRIVE  
1:85.3391



AERIAL 02 PUTAKI DRIVE & PAPATUPU WAY  
1:58.1387





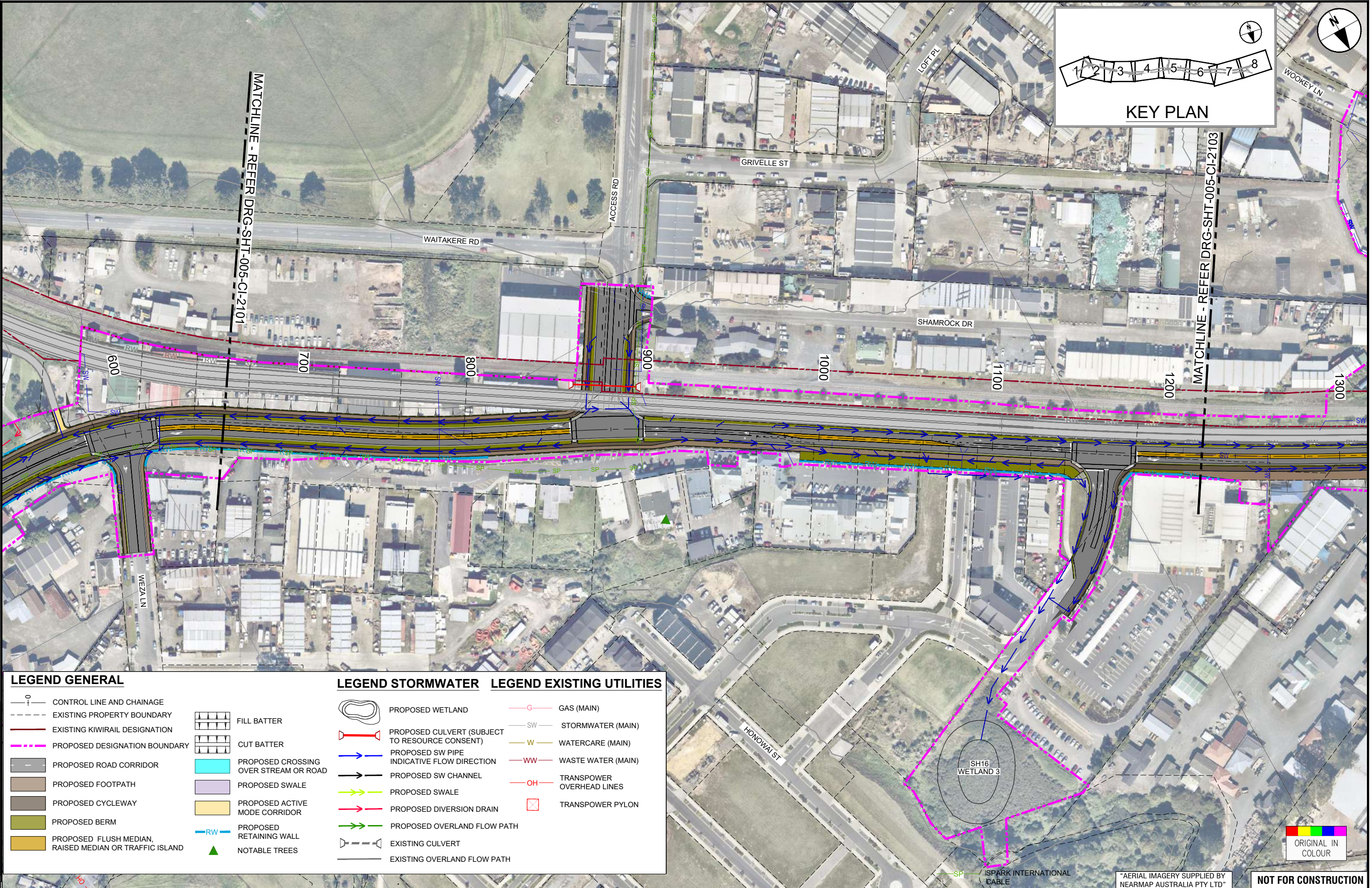
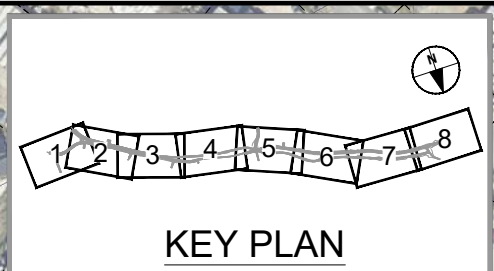
## Attachment C:

General Arrangement Plan for the NOR S2





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**LEGEND GENERAL**

- CONTROL LINE AND CHAINAGE
- EXISTING PROPERTY BOUNDARY
- EXISTING KIWI RAIL DESIGNATION
- PROPOSED DESIGNATION BOUNDARY
- PROPOSED ROAD CORRIDOR
- PROPOSED FOOTPATH
- PROPOSED CYCLEWAY
- PROPOSED BERM
- PROPOSED FLUSH MEDIAN, RAISED MEDIAN OR TRAFFIC ISLAND
- FILL BATTER
- CUT BATTER
- PROPOSED CROSSING OVER STREAM OR ROAD
- PROPOSED SWALE
- PROPOSED ACTIVE MODE CORRIDOR
- PROPOSED RETAINING WALL
- NOTABLE TREES

**LEGEND STORMWATER**

- PROPOSED WETLAND
- PROPOSED CULVERT (SUBJECT TO RESOURCE CONSENT)
- PROPOSED SW PIPE INDICATIVE FLOW DIRECTION
- PROPOSED SW CHANNEL
- PROPOSED SWALE
- PROPOSED DIVERSION DRAIN
- PROPOSED OVERLAND FLOW PATH
- EXISTING CULVERT
- EXISTING OVERLAND FLOW PATH

**LEGEND EXISTING UTILITIES**

- GAS (MAIN)
- STORMWATER (MAIN)
- WATERCARE (MAIN)
- WASTE WATER (MAIN)
- TRANSPOWER OVERHEAD LINES
- TRANSPOWER PYLON

ORIGINAL IN COLOUR

ORIGINAL SIZE A1

ISSUED FOR NOTICE OF REQUIREMENT LODGEMENT	JDT	DEC 2022	DESIGN REVIEW	R. KUBALA	16.07.2021
DRAFT ISSUE FOR REVIEW	VDLT	JULY 2021	APPROVED	B. BUSNARDO	07.12.2022
REVISIONS	DATE				

SURVEYED	N/A	
DRAWN	V. DELA TORRE	16.07.2021
DRAWING CHECK	J. DELA TORRE	16.07.2021
DESIGN	R. BELTRAN	16.07.2021
DESIGN REVIEW	R. KUBALA	16.07.2021
APPROVED	B. BUSNARDO	07.12.2022



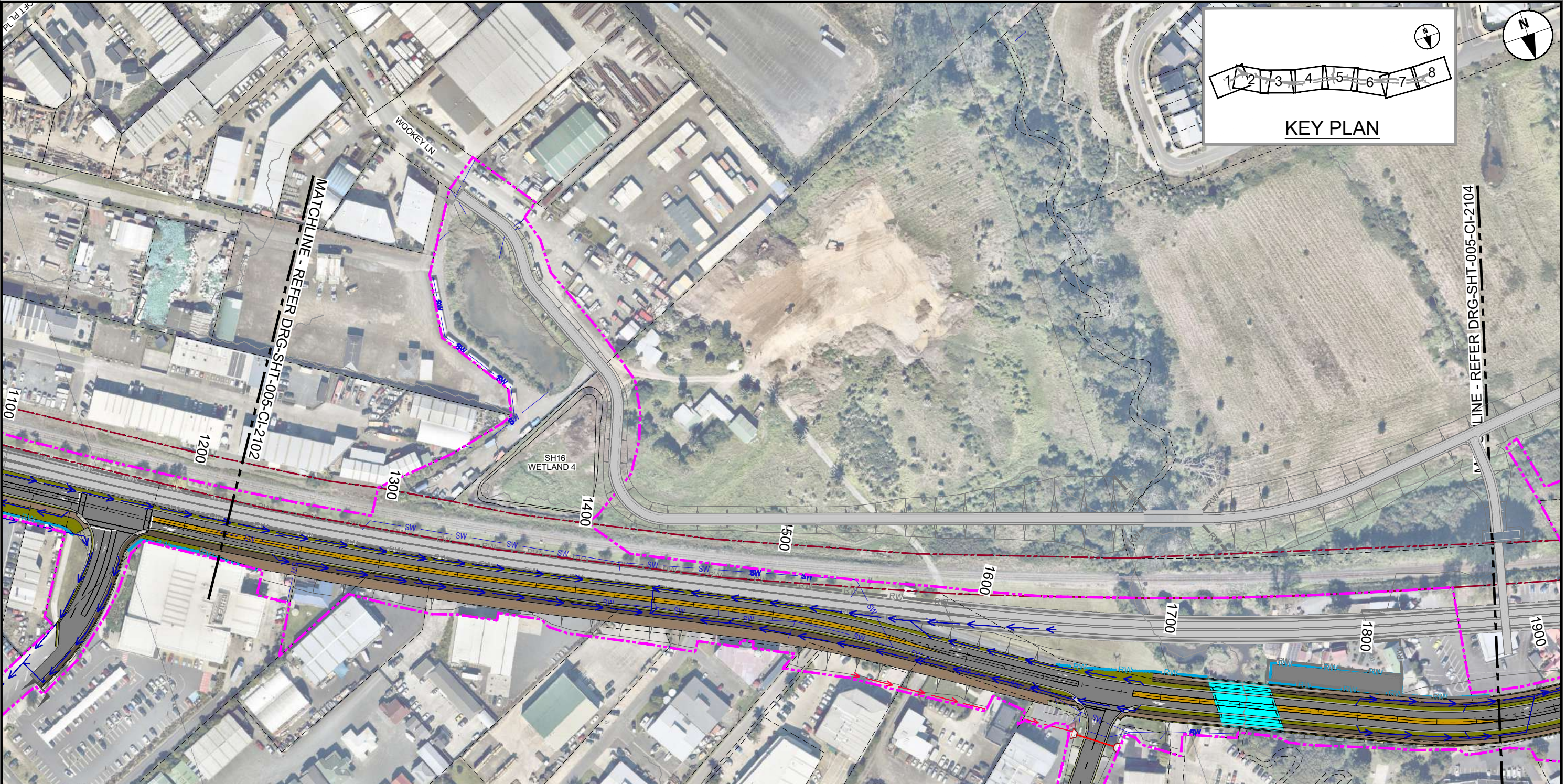
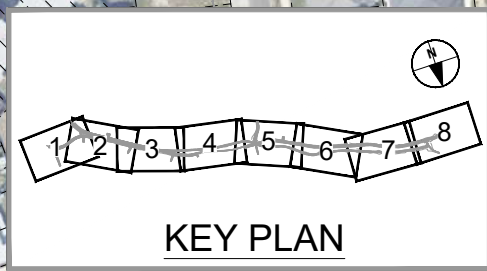
**SUPPORTING GROWTH PROGRAMME**  
**NORTH WEST STRATEGIC - SH16 MAIN ROAD UPGRADE (NoR S2)**  
 Drawing Title: GENERAL ARRANGEMENT LAYOUT PLAN  
 SHEET 2 OF 8

Drawing Date: 07.12.2022		FOR LODGEMENT	
A1 Scale: 1:1000	A3 Scale: 1:2000		
Discipline: CIVIL			
Drawing No: SGA-DRG-NWE-005-CI-2102		Revision: B	

NOT FOR CONSTRUCTION



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**LEGEND GENERAL**

- CONTROL LINE AND CHAINAGE
- EXISTING PROPERTY BOUNDARY
- EXISTING KIWI RAIL DESIGNATION
- PROPOSED DESIGNATION BOUNDARY
- PROPOSED ROAD CORRIDOR
- PROPOSED FOOTPATH
- PROPOSED CYCLEWAY
- PROPOSED BERM
- PROPOSED FLUSH MEDIAN, RAISED MEDIAN OR TRAFFIC ISLAND
- FILL BATTER
- CUT BATTER
- PROPOSED CROSSING OVER STREAM OR ROAD
- PROPOSED SWALE
- PROPOSED ACTIVE MODE CORRIDOR
- PROPOSED RETAINING WALL
- NOTABLE TREES

**LEGEND STORMWATER**

- PROPOSED WETLAND
- PROPOSED CULVERT (SUBJECT TO RESOURCE CONSENT)
- PROPOSED SW PIPE INDICATIVE FLOW DIRECTION
- PROPOSED SW CHANNEL
- PROPOSED SWALE
- PROPOSED DIVERSION DRAIN
- PROPOSED OVERLAND FLOW PATH
- EXISTING CULVERT
- EXISTING OVERLAND FLOW PATH

**LEGEND EXISTING UTILITIES**

- GAS (MAIN)
- STORMWATER (MAIN)
- WATERCARE (MAIN)
- WASTE WATER (MAIN)
- TRANSPOWER OVERHEAD LINES
- TRANSPOWER PYLON

ORIGINAL SIZE A1

REV	ISSUED FOR NOTICE OF REQUIREMENT LODGEMENT	JDT	DEC 2022	DESIGN REVIEW	R. KUBALA	16.07.2021
A	DRAFT ISSUE FOR REVIEW	VDLT	JULY 2021	APPROVED	B. BUSNARDO	07.12.2022
B						

SURVEYED	N/A	
DRAWN	V. DELA TORRE	16.07.2021
DRAWING CHECK	J. DELA TORRE	16.07.2021
DESIGN	R. BELTRAN	16.07.2021
DESIGN REVIEW	R. KUBALA	16.07.2021
APPROVED	B. BUSNARDO	07.12.2022



**SUPPORTING GROWTH PROGRAMME**  
**NORTH WEST STRATEGIC - SH16 MAIN ROAD UPGRADE (NoR S2)**  
 Drawing Title: GENERAL ARRANGEMENT LAYOUT PLAN  
 SHEET 3 OF 8

Drawing Date: 07.12.2022		FOR LODGEMENT	
A1 Scale: 1:1000	A3 Scale: 1:2000		
Discipline: CIVIL			
Drawing No: SGA-DRG-NWE-005-CI-2103		Revision: B	

ORIGINAL IN COLOUR

NOT FOR CONSTRUCTION

"AERIAL IMAGERY SUPPLIED BY NEARMAP AUSTRALIA PTY LTD"



## Form 21 Submission on requirement for designation that is subject to public notification or limited notification by a territorial authority

*Sections 168A, 169, 181, 189A, 190, and 195A, Resource Management Act 1991*

**To:** Auckland Council  
**Name of submitter:** Yvonne and Gayo Vodanovich

This is a submission on a notice of requirement from Auckland Transport for a designation (the **notice of requirement**) for Trig Road North (NoR W1).

We are not trade competitors for the purposes of [section 308B](#) of the Resource Management Act 1991.

**The specific parts of the notice of requirement that our submission relates to are:**

The location and levels of the designated works on and adjacent to 26 Trig Road.

The nature of the effects of works on 26 Trig Road, both permanently and during construction.

The duration and timing of construction effects.

Compliance with construction noise and vibration standards.

The inefficient proposed intersections of Trig, Hobsonville and Luckens Roads.

The extended lapse period.

**Our submission is:**

We oppose the Notice of Requirement.

**The reason for our views are:**

We own and reside at 26 Trig Road, Whenuapai. The property comprises 951m<sup>2</sup> and has the legal description Lot 40 DP 41172. It is contained in RoT NZ10D/299.

The Indicative Design and Designation Plans forming part of the Notice of Requirement show the footprint of the Designation extending into 26 Trig Road with a fill batter encroaching onto the property, close to an existing structure. Discussions with the requiring authority have indicated that the designation may be temporary and for construction purposes only, but that is not supported by the need to construct a permanent fill batter to support the road on 26 Trig Road. Other works are also shown passing through the property.

26 Trig Road is zoned Future Urban Zone and it is contemplated that it will be live-zoned for development in the coming years. When 26 Trig Road is developed, any reduction in allotment size to accommodate the roading works may impact the ability to achieve a sensible development or to comply with standards for development in the Unitary Plan. Such a requirement of land should be avoided. Similarly, the design of the road must ensure that there are no stormwater, flooding or other adverse effects on 26 Trig Road.

Any imposition of such impediments to redevelopment onto 26 Trig Road would be inefficient and inconsistent with sustainable management of natural and physical resources.

A resident of the property has high medical needs and is regularly attended to by ambulance. It is essential that vehicular access to the property is maintained at all times so that emergency medical assistance can be rendered when required.

Given the proximity of indicated works to living environments, it is essential that construction effects, particularly noise and vibration are managed to levels that will maintain residential amenity during construction.

The intersections connecting Luckens, Hobsonville and Trig Roads are inefficient both in terms of the land required and the traffic engineering outcomes. It would be better to have a traditional four-point intersection, which would avoid these inefficiencies.

The extended lapse period blights properties for far too long and will make redevelopment difficult and inefficient.

**We seek the following recommendation or decision from the territorial authority:**

- Ensure that the designation does not intrude upon 26 Trig Road. | 2.1
- Ensure that the levels and location of the finished Trig Road do not cause any adverse effects on 26 Trig Road including but not limited to issues related to access, runoff and stormwater. | 2.2
- Ensure that finalised street lighting does not cause undue glare or exceedance of any relevant standards for light spill at 26 Trig Road. | 2.3
- Ensure that vehicular access to 26 Trig Road is maintained at all times throughout the construction period. If there is any disruption to vehicular access, provide alternative wheelchair-accessible accommodation that can be accessed by ambulance at all times for the residents of 26 Trig Road. | 2.4
- Ensure that noise and vibration are managed to meet construction noise and vibration standards throughout the construction period. | 2.5
- Ensure that the intersection of Trig, Luckens and Hobsonville Road is formed to create a simple four-way intersection to maximise efficiency. | 2.6
- If any of the construction or permanent effects on 26 Trig Road are unable to be appropriately addressed through design of the designation or condition, recommend that the Notice of Requirement be declined. | 2.7
- Provide for a standard lapse period. | 2.8

**We wish to be heard in support of our submission.**

**If others make a similar submission, we will consider presenting a joint case with them at a hearing.**



Signature of person authorised to sign on behalf of submitter

Date: 22 April 2022

Electronic address for service of submitter: [janette@campbell.legal](mailto:janette@campbell.legal)

Telephone: 021 446 585  
Postal address Bankside Chambers  
PO Box 1571  
Auckland 1140

Contact person: Janette Campbell, Barrister

## Form 21 Submission on requirement for designation that is subject to public notification or limited notification by a territorial authority

*Sections 168A, 169, 181, 189A, 190, and 195A, Resource Management Act 1991*

**To:** Auckland Council  
**Name of submitter:** Yvonne and Gayo Vodanovich

This is a submission on a notice of requirement from Auckland Transport for a designation (the **notice of requirement**) for Trig Road North (NoR W1).

We are not trade competitors for the purposes of [section 308B](#) of the Resource Management Act 1991.

### **The specific parts of the notice of requirement that our submission relates to are:**

The location and levels of the designated works on and adjacent to 24-26 Trig Road.

The nature of the effects of works on 24-26 Trig Road, both permanently and during construction.

The duration and timing of construction effects.

Compliance with construction noise and vibration standards.

The inefficient proposed intersections of Trig, Hobsonville and Luckens Roads.

The extended lapse period.

### **Our submission is:**

We oppose the Notice of Requirement.

### **The reason for our views are:**

We own and reside at 24-26 Trig Road, Whenuapai. 24 Trig Road comprises 978m<sup>2</sup> and has the legal description Lot 1 DP 180500. It is contained in RoT NA111C/539. 26 Trig Road comprises 951m<sup>2</sup> and has the legal description Lot 40 DP 41172. It is contained in RoT NZ10D/299.

The Indicative Design and Designation Plans forming part of the Notice of Requirement show the footprint of the Designation extending into 24 and 26 Trig Road with a fill batter encroaching onto the property, close to an existing structure. Discussions with the requiring authority have indicated that the designation may be temporary and for construction purposes only, but that is not supported by the need to construct a permanent fill batter to support the road on 24-26 Trig Road. Other works are also shown passing through the property.

24-26 Trig Road is zoned Future Urban Zone and it is contemplated that it will be live-zoned for development in the coming years. When 24-26 Trig Road is developed, any reduction in allotment size to accommodate the roading works may impact the ability to achieve a sensible development or to comply with standards for development in the



Unitary Plan. Such a requirement of land should be avoided. Similarly, the design of the road must ensure that there are no stormwater, flooding or other adverse effects on 24-26 Trig Road. Any imposition of such impediments to redevelopment onto 26 Trig Road would be inefficient and inconsistent with sustainable management of natural and physical resources.

A resident of the property has high medical needs and is regularly attended to by ambulance. It is essential that vehicular access to the property is maintained at all times so that emergency medical assistance can be rendered when required.

Given the proximity of indicated works to living environments, it is essential that construction effects, particularly noise and vibration are managed to levels that will maintain residential amenity during construction.

The intersections connecting Luckens, Hobsonville and Trig Roads are inefficient both in terms of the land required and the traffic engineering outcomes. It would be better to have a traditional four-point intersection, which would avoid these inefficiencies.

The extended lapse period blights properties for far too long and will make redevelopment difficult and inefficient.

**We seek the following recommendation or decision from the territorial authority:**

Ensure that the designation does not intrude upon 24-26 Trig Road.

Ensure that the levels and location of the finished Trig Road do not cause any adverse effects on 24-26 Trig Road including but not limited to issues related to access, runoff and stormwater.

Ensure that finalised street lighting does not cause undue glare or exceedance of any relevant standards for light spill at 24-26 Trig Road.

Ensure that vehicular access to 24-26 Trig Road is maintained at all times throughout the construction period. If there is any disruption to vehicular access, provide alternative wheelchair-accessible accommodation that can be accessed by ambulance at all times for the residents of 24-26 Trig Road.

Ensure that noise and vibration are managed to meet construction noise and vibration standards throughout the construction period.

Ensure that the intersection of Trig, Luckens and Hobsonville Road is formed to create a simple four-way intersection to maximise efficiency.

If any of the construction or permanent effects on 24-26 Trig Road are unable to be appropriately addressed through design of the designation or condition, recommend that the Notice of Requirement be declined.

Provide for a standard lapse period.

**We wish to be heard in support of our submission.**

If others make a similar submission, we will consider presenting a joint case with them at a hearing.



Signature of person authorised to sign on behalf of submitter

Date: 22 June 2022

Electronic address for service of submitter: [janette@campbell.legal](mailto:janette@campbell.legal)

Telephone: 021 446 585  
Postal address: Bankside Chambers  
PO Box 1571  
Auckland 1140

Contact person: Janette Campbell, Barrister