

## I553 Takapuna 2 Precinct

### I553.1 Precinct Description

The Takapuna 2 Precinct applies to a site at 48 Esmonde Road, Takapuna, which is located adjacent to the estuary of Shoal Bay. The site comprises an area of 2.1566 hectares.

The precinct benefits from the existing amenity, landscape and ecological values that the adjacent coastal margin provides and is strategically located adjacent to Esmonde Road, which provides multi-modal transport connections to the Takapuna Metropolitan Centre and the city centre. These features support the intensity of development and residential liveability of the precinct.

The purpose of the precinct is to provide for the comprehensive and integrated redevelopment of the site. The precinct enables a new residential community comprising a mixture of accommodation types and supporting activities within a unique urban setting of high quality. The precinct also sets aside a 20 metre wide coastal margin that is to become a public esplanade reserve at the time of subdivision.

The precinct seeks to protect the ecological functions and water quality of the coastal margin, while also enhancing the landscape and open space amenity values of the area. This is achieved through requirements for the provision of publicly-accessible open space zoned land that incorporate established trees, planting (including coastal planting), visual corridors, shared pedestrian cycle paths, walkways and informal recreation and play areas. The proposed development pattern for the precinct facilitates a potential boardwalk connection to Francis Street.

The zoning of the land within the precinct comprises Residential - Terrace Housing and Apartment Buildings zone and Public Open Space – Conservation zone (the coastal margin).

A range of building heights are enabled to recognise the favourable size, location and topography of the precinct. The precinct provisions provide for a variety of heights up to 16 storeys and ensure that building modulation to the skyline is achieved. The precinct requires the provision of lower buildings around the edge of the site with increasing building heights towards the centrally located tallest structure/s.

The precinct seeks to manage any adverse effects of vehicle trips on the adjacent transportation network and encourage the use of non-car based trips. The existing intersection will be upgraded prior to development occurring on the site. Transport controls are provided for within the precinct to manage effects on the capacity of the transport network to accommodate the planned growth.

Development of this precinct will be guided by the following precinct plans:

- Precinct Plan 1 – Site Features
- Precinct Plan 2 – Building height and coverage

All relevant overlay, Auckland-wide and zone provisions apply in this precinct unless otherwise specified below.

## **I553.2 Objectives [dp]**

- (1) The Takapuna 2 Precinct develops as a vibrant and diverse residential community which:
  - (a) responds positively to its immediate surrounds and coastal setting with high quality architectural and urban design responses;
  - (b) provides a range of accommodation types including integrated residential development;
  - (c) is in keeping with the planned urban built character of the precinct, enabling buildings between four and 16 storeys in height;
  - (d) takes advantage of the site's proximity to the frequent public transport network;
  - (e) enables a limited range and scale of ancillary non-residential activities to support residents of the precinct.
  
- (2) The Takapuna 2 Precinct functions in a way that:
  - (a) links pedestrian and cycling facilities within and around the precinct, to enhance recreation and connectivity with the wider environment including the potential Francis Street boardwalk connection;
  - (b) ensures that the landscape and ecological values of the coastal margin are recognised and protected from inappropriate use and development;
  - (c) ensures that development is integrated with improvements to the transport network and mitigation measures are implemented to ensure the safe, effective and efficient operation of the transport network and support for active and public transport choices;
  - (d) ensures that the intensity of development is appropriate for the adjacent transport network; and,
  - (e) ensures that adverse effects of stormwater runoff within the precinct are avoided, remedied or mitigated to maintain water quality and preserve the *mauri* of the Waitemata Harbour.
  
- (3) Takapuna 2 Precinct avoids significant adverse effects on the:
  - (a) Safe, efficient and effective operation of the surrounding transport network;
  - (b) amenity of neighbouring zones and sites;
  - (c) function and amenity of Business – Metropolitan or Town Centre zones.

Objective H6.2(2) of the Residential - Terrace House and Apartment Building Zone does not apply in this Precinct. Otherwise, all other relevant overlay, Auckland-wide and zone objectives apply in this precinct.

### **I553.3 Policies [dp]**

- (1) Ensure comprehensive, integrated high quality development of the precinct in general accordance with Precinct Plans 1 and 2 that:
  - (a) provides for development in a variety of building forms and heights;
  - (b) enables the efficient and effective use of land;
  - (c) achieves the planned urban built character of the precinct;
  - (d) protects the character and amenity of the coastal margin, and
  - (e) provides high-quality urban built character and on-site amenity.
- (2) Achieve an integrated high quality urban form that:
  - (a) supports the safe, effective and efficient operation of the transport network;
  - (b) connects well with public transport and pedestrian and cycleway networks; and
  - (c) promotes alternatives to, and reduces dependency on, private motor vehicles as a means of transport while taking into account the maximum number of dwellings and non-residential floorspace anticipated for the precinct.
- (3) Enable a limited range and scale of non-residential activities to support residents and service the needs of the precinct while ensuring that:
  - (a) the activities will avoid, remedy or mitigate adverse effects on residential amenity of the precinct, car-based trips or generate adverse traffic effects; and
  - (b) the scale and intensity of commercial activities within the precinct will not have an adverse effect on the role, function and viability of the Takapuna Metropolitan Centre or any town centre.
- (4) Require that new buildings:
  - (a) achieve a high-density high quality urban built character of four to 16 storey buildings in identified locations in a variety of visually interesting forms;
  - (b) are appropriate in scale to, and interface with the internal pedestrian network, the coastal margin and the public frontage along Esmonde Road;
  - (c) provide a transition in building height down towards the coastal margin and Esmonde Road to reinforce the land-form/topography of the precinct and recognise that buildings will be viewed from all sides of the precinct - providing "frontage" in perimeter form;
  - (d) achieve a minimum development of four levels around the coastal margin to ensure that screening of the taller buildings within the centre of the precinct is achieved;
  - (e) are located and designed to maintain the identified Visual Corridors through the precinct and key open space and pedestrian connections to the locality;

- (f) are set back from Esmonde Road to provide space for a public shared pedestrian cycling facility within the site along the street frontage as shown in Precinct Plan 1.
  - (g) to employ mitigation measures to avoid bird strike through the use of nano UV-reflective coatings or patterns on windows or other visual cues for birds to identify hazards and reduce bird-strike incidents. (Refer: Bird Friendly Best Practise: Glass 2016: Bird-Friendly Development Guidelines. Toronto. 54 pp.).
  - (h) to employ measures to minimise the risk of bird strike resulting from increased night lighting.
  - (i) achieve reasonable internal noise levels for noise sensitive spaces for the protection of residential amenity from both business/commercial activities within the precinct and from elevated traffic noise from Esmonde Road and the nearby motorway.
- (5) Promote the use and enjoyment of the Open Space – Conservation Zone and internal open space and plaza areas for residents and visitors by:
- (a) developing and enabling appropriate recreation opportunities throughout the precinct, including a potential board walk along the edge of the coastal margin (esplanade reserve);
  - (b) creating a network that links open spaces and plazas of the precinct with the wider environment including a potential boardwalk to Francis Street and creating an easement in gross to ensure 24 hour public access through the precinct from Esmonde Road to the future boardwalk.
- (6) Ensure that the ecological and landscape values of the future esplanade reserve (Open Space - Conservation Zone) are recognised and protected from the effects of inappropriate use and development.
- (7) Ensure that the effects of stormwater runoff within the precinct are mitigated to maintain water quality and preserve the *mauri* of the Waitemata Harbour.
- (8) Ensure that the safety, efficiency and effectiveness of the adjoining transport network is maintained, taking into account the anticipated maximum number of dwellings and non-residential floorspace enabled by the precinct, by requiring intersection improvements that are aligned to the level of congestion caused by vehicles entering and exiting the precinct.
- (9) Ensure access points are restricted in accordance with Precinct Plan 1 to achieve a well-connected pedestrian, cycle and road network that provides for all modes of transport and facilitates active modes.
- (10) Require the applicant/developer to consider alternative methods to support movement to and from the precinct and encourage behaviour change away from private vehicles to other transport modes, including by way of a bus shuttle service to Takapuna or other locations where this is practicable and can be legally secured, such that the traffic generated by activities in the precinct does not exceed 420 vehicles per peak hour; and require an Integrated Transport Assessment to the satisfaction of Auckland Transport.

Policy H6.3(2) of the Residential – Terrace Housing and Apartment Building Zone does not apply in this precinct. Otherwise, all other relevant overlay, Auckland-wide and zone policies apply in this Precinct.

#### I553.4 Activity table

All relevant overlay, Auckland-wide and zone activity tables apply unless the activity is specifically provided for by a rule in Activity Table I553.4.1 or Table I553.4.2 below in which case the activity status of the precinct applies.

Activity Table I553.4.1 and Table I553.4.2 specify the activity status of land use, development and subdivision activities in the Takapuna 2 Precinct pursuant to sections 9(3) and 11 of the Resource Management Act 1991. Where there is a blank box in the table, the zone rules apply.

**Table I553.4.1 Activity table- THAB zoned land**

Activity		Activity Status
<b>Use</b>		
Residential		
A1	Dwellings	
A2	Integrated residential development	P
A3	Visitor accommodation	P
Commercial activities		
A4	Commercial activities and Healthcare facilities (excluding Drive through) of up to 200m <sup>2</sup> gross floor area per tenancy that comply with Standard I553.6.13 – Commercial GFA and location control.	P
A5	Commercial activities and Healthcare facilities (excluding Drive through) of more than 200m <sup>2</sup> gross floor area per tenancy that comply with Standard I553.6.13 – Commercial GFA and location control.	RD
A6	Commercial activities and Healthcare facilities (excluding Drive through) that do not comply with Standard I553.6.13 – Commercial GFA and location control.	D
A7	Care centres	D
Development		
A8	New buildings comprising up to three dwellings	P
A9	All other new buildings	RD
A10	External additions to existing buildings	RD
A11	Accessory buildings	RD
A12	Development that exceeds Standard I553.6.11 Maximum On Site Parking	RD
A13	Development which complies with Table I553.6.12.1 and Standard I553.6.12(1) Transport infrastructure development requirements	RD
A14	Development which exceeds the 273 dwellings or short term visitor accommodation units or 1,257m <sup>2</sup> of non-residential activity occupation thresholds but still generates less than 420 vehicle movements per any peak hour in Table I553.6.12.2 and Standard I553.6.12(2) Transport review thresholds	RD

Activity		Activity Status
A15	Development which does not comply with Table I553.6.12.1 and Standard I553.6.12(1) Transport infrastructure development requirements	D
A16	Development where the traffic generated by all activities in the precinct exceeds 420 vehicle movements per any peak hour.	D
A17	Buildings (other than street furniture and lighting poles) within an identified Visual Corridor on Precinct Plan 1.	D
A18	Any development, including vehicle access to Esmonde Road not otherwise listed in Table I553.4.1 that is not in accordance with Precinct Plan 1.	D
A19	Development that does not comply with Standard I553.6.8 Stormwater.	D
A20	Buildings that are less than four (4) storeys high above RL10. that do not comply with Standard I553.6.4.	D
<b>Community</b>		
A21	Recreation and leisure activities (including community spaces and gyms) within ground floor non-residential areas, identified on Precinct Plan 1	P

**Table I553.4.2 Activity table - Open Space – Conservation zoned land**

Activity		Activity Status
A1	Public amenities	
A2	Parks infrastructure	
A3	Recreation trails	
A4	Accessory buildings	

### **I553.5 Notification**

- (1) Any application for resource consent for a restricted discretionary, discretionary or non-complying activity listed in Table I55.4.1 and I553.4.2 Activity tables above will be subject to the normal tests for notification under the relevant sections of the Resource Management Act 1991.
- (2) When deciding who is an affected person in relation to any activity for the purposes of section 95E of the Resource Management Act 1991 the Council will give specific consideration to those persons listed in Rule C1.13(4).

### **I553.6. Standards**

All permitted and restricted discretionary activities listed in Table I553.4.1 must comply with the following standards.

The overlay, zone, and Auckland-wide standards apply in this precinct in addition to the following standards, except as outlined below:

The following standards in the Residential – Terrace Housing and Apartment Buildings Zone do not apply to land in the Takapuna 2 Precinct:

- H6.6.5. Building height;
- H6.6.6. Height in relation to boundary;
- H6.6.7. Alternative height in relation to boundary within the Residential – Terrace Housing and Apartment Buildings Zone;
- H6.6.8. Height in relation to boundary adjoining lower intensity zones;
- H6.6.10. Maximum impervious area;
- H6.6.11. Building coverage;
- H6.6.16. Front, side and rear fences and walls

The following standards in the Residential – Terrace Housing and Apartment Buildings Zone do apply in the Takapuna 2 Precinct:

- H6.6.2. Home occupations;
- H6.6.9. Yards
- H6.6.12. Landscaped area;
- H6.6.13. Outlook space;
- H6.6.14. Daylight;
- H6.6.15. Outdoor living space;
- H6.6.17. Minimum dwelling size

Standard E27.6.1 Trip generation does not apply to commercial activities or healthcare facilities that do not exceed a total of 1,257m<sup>2</sup> gross floor area within the precinct.

All standards in the Open Space – Conservation Zone apply to the Open Space – Conservation zoned land in the Takapuna 2 Precinct.

### **1553.6.1. Building Height and Building Length**

Purpose: To ensure development is consistent with the planned outcomes identified on Precinct Plan 2 and to make efficient use of the site, by:

- focusing greater building height within the precinct in identified locations that are set back from the coastal margin and Esmonde Road;
  - providing a cascade of building heights focussing the greatest height and density through the centre of the precinct; and,
  - controlling the length of buildings to manage building dominance effects.
- (a) Buildings in Area 1 (Outer) must not exceed 7 storeys above RL 5 and no part of the building shall exceed RL30m in height.
  - (b) Buildings in Area 2 (Intermediate) must not exceed 10 storeys above RL 5 and no part of the building shall exceed RL41m in height.
  - (c) Buildings in Area 3 (Inner) must not exceed 16 storeys above RL 5 and no part of the building exceeds RL62m in height.
  - (d) The maximum length of any building at the maximum storey heights set out in (a) to (c) above must not exceed 35m measured along any building façade facing any site boundary.

### **I553.6.2 Maximum impervious area**

Purpose: To provide for the intensive use of the majority of the precinct in recognition of the long coastal margin (esplanade reserve) and its capacity to mitigate the effects of stormwater runoff from the precinct's planned urban character of multi-storey buildings surrounded by open space.

- (1) The maximum impervious area in Areas 1, 2 and 3 as shown on the Takapuna 2 Precinct Plan 2 must not exceed:
  - (a) Area 1 (Outer) - 90% of the Area 1 area.
  - (b) Area 2 (Intermediate) - 95% of the Area 2 area.
  - (c) Area 3 (Inner) - 100% of the Area 3 area.

### **I553.6.3 Building coverage**

Purpose: To ensure development is consistent with the planned outcomes identified on Precinct Plan 2 and manage the extent of buildings on a site to achieve the planned urban character of buildings surrounded by open space and ensure a varied and visually interesting skyline of built forms.

- (1) The maximum building coverage in Area 1 (Outer) must not exceed 60% of Area 1 provided that, the allowable coverage for each storey height (within that 60%) is as follows:
  - a. Up to RL23 (5 storeys) - 100%.
  - b. Up to RL26.5 (6 storeys) – 90%
  - c. Up to RL 30 (7 storeys) – 40%
- (2) The maximum building coverage in Area 2 (Intermediate) must not exceed 60% of Area 2 provided that, the allowable coverage for each storey height (within that 60%) is as follows:
  - a. Up to RL30 (7 storeys) - 100%.
  - b. Up to RL41 (8-10 storeys) – 35%
- (3) The maximum building coverage in Area 3 (Inner) must not exceed 60% in Area 3 provided that, the allowable coverage for each storey height (within that 60%) is as follows:
  - a. Up to RL41 (10 storeys) - 100%.
  - b. Up to RL62 (11-16 storeys) – 35%

Note: All RL levels must be confirmed by a registered surveyor.

### **I553.6.4. Minimum building height**

Purpose: To ensure that buildings provide a minimum number of storeys to ensure that the efficient use of the precinct is achieved and that the outer buildings of the precinct deliver a cascade of heights to break up the form of the taller buildings located in the centre of the precinct as viewed from outside the precinct.



- (1) Buildings in Areas 1, 2 and 3 must be a minimum height of 4 storeys above ground level (RL10).

Note: All RL levels must be confirmed by a registered surveyor.

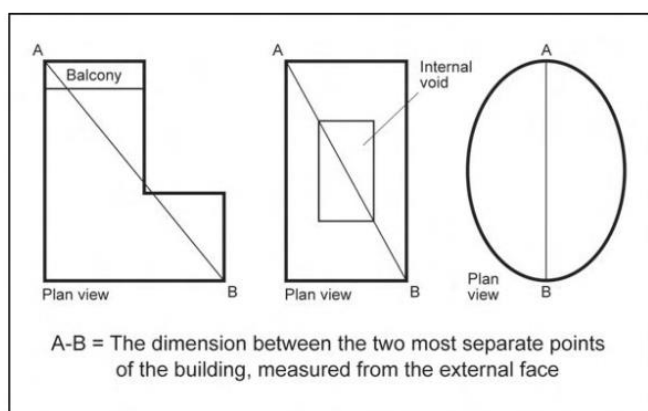
### 1553.6.5. Maximum building dimension and separation

Purpose: To ensure that buildings over 19m in height:

- are not overly bulky in appearance and avoid significant visual dominance effects;
- allow adequate sunlight and daylight access to Esmonde Road, public and internal open space and nearby sites;
- provide adequate sunlight and outlook around and between buildings; and
- mitigate adverse wind effects.

- (1) The maximum plan dimension of that part of the building above 19m must not exceed 55m.
- (2) The maximum plan dimension is the horizontal dimension between the exterior faces of the two most separate points of the building.
- (3) The part of a building above 19m must be located at least 6m from any side or rear boundary of the site.

**Figure 1 - Maximum tower dimension plan view**



### 1553.6.6. Wind

Purpose: mitigate the adverse wind effects generated by tall buildings potentially affecting the amenity of Esmonde Road, the Open Space – Conservation zoned land or any areas of open space within the precinct accessible to the general public.

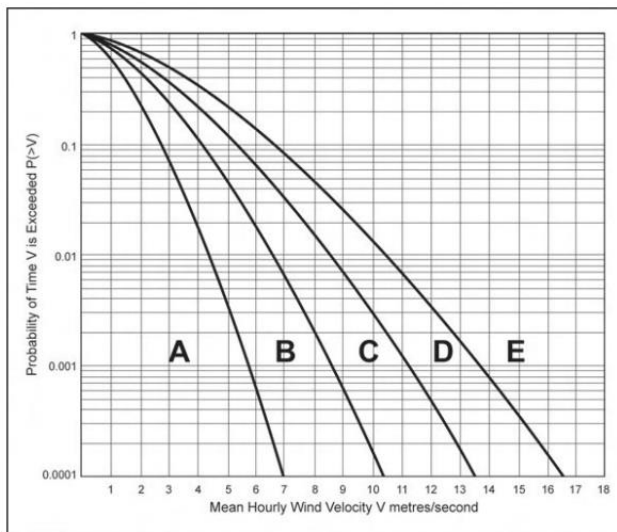
- (1) A new building exceeding 25m in height and additions to existing buildings that increase the building height above 25m must not cause:
  - a) the mean wind speed around it to exceed the category for the intended use of the area as set out in Table 1 and Figure 2 below;
  - b) the average annual maximum peak 3-second gust to exceed the dangerous level of 25m/second; and

- c) an existing wind speed which exceeds the controls of Standard I553.6.6(1)(a) or Standard I553.6.6(1)(b) above to increase.
- (2) A report and certification from a suitably qualified and experienced person, showing that the building complies with Standard I553.6.6(1) above, will demonstrate compliance with this standard.
- (3) If the information in Standard I553.6.6(2) above is not provided, or if such information is provided but does not predict compliance with the rule, a further wind report including the results of a wind tunnel test or appropriate alternative test procedure is required to demonstrate compliance with this standard.

**Table 1 Categories**

Category	Description
Category A	Areas of pedestrian use or adjacent dwellings containing significant formal elements and features intended to encourage longer term recreational or relaxation use i.e. public open space and adjacent outdoor living space
Category B	Areas of pedestrian use or adjacent dwellings containing minor elements and features intended to encourage short term recreation or relaxation, including adjacent private residential properties
Category C	Areas of formed footpath or open space pedestrian linkages, used primarily for pedestrian transit and devoid of significant or repeated recreational or relaxational features, such as footpaths not covered in categories A or B above
Category D	Areas of road, carriage way, or vehicular routes, used primarily for vehicular transit and open storage, such as roads generally where devoid of any features or form which would include the spaces in categories A - C above.
Category E	Category E represents conditions which are dangerous to the elderly and infants and of considerable cumulative discomfort to others, including residents in adjacent sites. Category E conditions are unacceptable and are not allocated to any physically defined areas of the city

**Figure 2 - Wind environment control**



Derivation of the wind environment control graph:

The curves on the graph delineating the boundaries between the acceptable categories (A-D) and unacceptable (E) categories of wind performance are described by the Weibull expression:

$$P(>V) = e^{-(v/c)^k}$$

where V is a selected value on the horizontal axis, and P is the corresponding value of the vertical axis:

and where:

$P(>V)$  = Probability of a wind speed V being exceeded;

e = The Napierian base 2.7182818285

v = the velocity selected;

k = the constant 1.5; and

c = a variable dependent on the boundary being defined:

A/B, c = 1.548

B/C, c = 2.322

C/D, c = 3.017

D/E, c = 3.715

#### **1553.6.7. Front, side and rear fences and walls**

Purpose: To ensure that where fences and walls are provided, they:

- enable privacy for dwellings and outdoor living spaces, whilst maximising opportunities for passive surveillance of Esmonde Road, the spaces between buildings or the adjoining Area O (future esplanade reserve); and
- minimise visual dominance effects to Esmonde Road or adjoining public places.

(1) Fences or walls or a combination of these structures (whether separate or joined together) must not exceed the height specified below when measured from the ground level at the boundary:

- (a) There must be no fences or walls along the length of Esmonde Road within 4 metres of the road boundary.
- (b) On land /boundaries that adjoin or are within 3 metres of the esplanade reserve (Open Space – Conservation Zone):
  - (i) Fences or walls or a combination of these structures must not exceed 1.0m in height.

- (c) Fences or walls or a combination of these structures internal to the precinct (not within 4 metres of Esmonde Road or within 3 metres of the esplanade reserve):
  - (i) Fences or walls or a combination of these structures must not exceed 1.8m in height;
  - (ii) Any fence on a rear boundary that faces onto a rear lane must be at least 50 percent visually open, as viewed perpendicular to the boundary.

#### **I553.6.8. Stormwater**

Purpose: To ensure that stormwater in the precinct is managed and, where appropriate, treated, to ensure the health and ecological values of the coastal environment are maintained.

- (1) All land use development must be managed in accordance with a Stormwater Management Plan approved by the stormwater network utility operator.

#### **I523.6.9. Coastal planting**

Purpose: To ensure that the amenity, water quality and ecology of the coastal environment within the precinct is maintained or enhanced through coastal planting; and to ensure the enhancement of the landscape values of the coastal margin.

- (1) The coastal margin (Area O – Open Space – Conservation Zone) identified on Precinct Plan 1 must be planted in accordance with a Council approved planting plan, using eco-sourced native vegetation, consistent with the local biodiversity and habitat in accordance with Appendix 16 Guideline for native revegetation plantings.
- (2) The plan required by (1) above must accompany any application for the development or the first subdivision for the site, with planting to be completed prior to issue of new titles.
- (3) Planting within the coastal margin does not preclude the provision of pedestrian and shared walkways and passive recreational spaces in accordance with Precinct Plan 1.

#### **I553.6.10. Visual Corridors**

Purpose: To ensure that the identified visual corridors are provided through the precinct to the coast, open space and pedestrian connections to the locality.

- (1) The visual corridors must be provided as indicated on precinct plan 1 and have a minimum width of 5 metres clear of buildings (other than street furniture and lighting poles).

#### **I553.6.11. Maximum On-site parking**

Purpose: To ensure the safety, efficiency and effectiveness of the internal and local road network and to discourage single occupancy car-based trips to and from the precinct.

- (1) The maximum number of long term or short term parking spaces (inclusive of any stacked parking) within the precinct must not exceed 321.

<b>Activity/parking type</b>	<b>Car parking spaces</b>
Residential dwellings	301
Commercial activities and healthcare facilities	20
<b>Total:</b>	<b>321</b>

### **I553.6.12. Transport infrastructure development thresholds**

Purpose: To ensure that the precinct maintains the safe and efficient operation of the local transport network.

- (1) Any application that involves the construction of 1 or more dwellings, or 1 or more visitor accommodation units, or any non-residential activity must meet the requirements specified in Table I553.6.12.1 Integrated transport infrastructure development upgrade requirements:

**Table I553.6.12.1 Integrated transport infrastructure development upgrade requirements**

<b>Occupation threshold</b>	<b>Transport infrastructure required in order to exceed the occupation threshold</b>
1 dwelling or any non-residential activity.	Provision of a private shuttle bus between the site and Takapuna for residents, to encourage behaviour change away from private vehicle and towards public transport.
	Provision of the pedestrian/cycle connection along the extent of Esmonde Road identified on Precinct Plan 1.
	Provision of a Parking and Traffic Management Plan.
	Provision of an upgrade to the signalised intersection of the site access with Esmonde Road to improve pedestrian and cyclist safety and amenity and increase traffic capacity as required to support 420 vehicle movements generated by the precinct in any peak hour: <ul style="list-style-type: none"> <li>• removal of the left turn slip lanes from Esmonde Road (westbound) and from the site access road</li> <li>• addition of separate left and right turn lanes (with at least 12m of queue length and 3m taper (total of 15m)) on the site approach to the intersection</li> <li>• addition of a separate left turn lane (with at least 21 m queue length) on the Esmonde Road (westbound) approach to the intersection</li> <li>• provision of dual pedestrian and cyclist signalised crossings on the site access and Esmonde Road (eastbound) arms of the intersection.</li> </ul>

- (2) Any application that involves the construction of dwellings, visitor accommodation, or any non-residential activities that will result in the total cumulative number of dwellings, visitor accommodation, or any non-residential activities within the precinct either constructed or consented exceeding the occupation thresholds but still generating less than 420 vehicle movements per any peak hour specified in Table I553.6.12.2 Transport review thresholds must meet the following requirements:

**Table I553.6.12.2 Transport review thresholds**

<b>Occupation threshold</b>	<b>Transport assessment required in order to exceed the occupation threshold</b>
273 dwellings or short term visitor accomodation units; or 1,257m <sup>2</sup> of non-residential activity	<p>A Transport Assessment is required to ensure the traffic generation of the existing and proposed development is less than the traffic generation threshold detailed below:</p> <p>(a) 420 vehicles movements per any peak hour.</p> <p>The Transport Assessment must include details of:</p> <ul style="list-style-type: none"> <li>• Surveyed traffic volumes entering and exiting the precinct at the signalised intersection;</li> <li>• Resultant traffic generation rate of the precinct (trips / dwelling);</li> <li>• Predicted traffic generated by any proposed development that will generate vehicle trips, and the specific traffic generating characteristics of those activities; and</li> <li>• Traffic generation analysis demonstrating the predicted cumulative traffic generation of all existing and proposed development within the precinct is less than the traffic generation thresholds referenced above, being 420 vehicles movements per any peak hour</li> </ul> <p>Note: All applications where the number of vehicle movements exceeds 420 vehicles per hour in any peak hour are required to be accompanied by an Integrated Transport Assessment as required in the Special Information Requirements.</p>

**I553.6.13. Commercial GFA and location control**

Purpose: To enable commercial activities and healthcare facilities in identified locations on Precinct Plan 1 without compromising the role, function and viability of existing centres.

- (1) Commercial activities and healthcare facilities must be located in areas shown on Precinct Plan 1.
- (2) The area to be used for commercial and/or healthcare purposes must have its public access and frontage to Esmonde Road or the open space plaza area of the precinct.
- (3) The total gross floor area of all commercial activities and healthcare facilities within the precinct must not exceed 1,257m<sup>2</sup>, provided that retail activities must be limited to 1,200m<sup>2</sup>.

### I553.6.14. Noise

Purpose: To ensure appropriate noise levels for noise sensitive spaces for the protection of residential amenity from business activities within the precinct and by elevated road noise from Esmonde Road and the nearby motorway.

- (1) All buildings containing noise sensitive spaces (as defined in AUP (OP) J1) must be designed, constructed and maintained to not exceed the following internal noise levels:

Unit	Time	Ambient Noise Limit
Bedrooms and sleeping areas	Between 10:00pm to 7:00am	35 dB $L_{Aeq,T}$ or NC 30
Other noise sensitive spaces	At all other times	40 dB $L_{Aeq,T}$

- (2) Where the noise levels in I553.6.14(1) can only be achieved when windows and/or external doors to rooms are closed, those rooms must have installed a mechanical ventilation and/or air conditioning system which does not generate a noise level greater than 35 dB  $L_{Aeq}$  in bedrooms and 40 dB  $L_{Aeq}$  in other spaces when measured 1m from the diffuser at the minimum air flows required to achieve the design temperatures and air flows in (i) or (ii) below or an alternative temperature control system approved by the Council.
- (i) an internal temperature no greater than 25 degrees celsius based on external design conditions of dry bulb 25.1 degrees celsius and wet bulb 20.1 degrees celsius; or

#### Note 1

Mechanical cooling must be provided for all habitable rooms (excluding bedrooms) provided that at least one mechanical cooling system must service every level of a dwelling that contains a habitable room (including bedrooms).

- (ii) a high volume of outdoor air supply to all habitable rooms with an outdoor air supply rate of no less than:
- six air changes per hour (ACH) for rooms with less than 30 per cent of the façade area glazed; or
  - 15 air changes per hour (ACH) for rooms with greater than 30 per cent of the façade area glazed; or
  - three air changes per hour for rooms with facades only facing south (between 120 degrees and 240 degrees) or where the glazing in the façade is not subject to any direct sunlight.

### I553.8 Assessment – restricted discretionary activities

#### I553.8.1. Matters of discretion

The council will restrict its discretion to all the following matters when assessing a restricted discretionary activity resource consent application, in addition to the matters specified for the relevant restricted discretionary activities in any relevant overlay, zone, and Auckland-wide provisions:

- (1) new buildings, additions and alterations to existing buildings and accessory buildings:
  - (a) consistency with precinct plans;
  - (b) building design and external appearance;
  - (c) consistency with precinct plans;
  - (d) shading;
  - (e) landscaped open space;
  - (f) transport;
  - (g) travel plans and integrated transport assessments; and
  - (h) infrastructure.
  
- (2) Commercial activities and Healthcare facilities (excluding Drive through) of more than 200m<sup>2</sup> gross floor area per tenancy that comply with Standard I552.6.13 – Commercial GFA and location control, effects on:
  - (a) residential amenity;
  - (b) transport; and
  - (c) travel plans and integrated transport assessments.
  
- (3) Any development that does not comply with standard I553.6.11 Maximum On-Site Parking:
  - (a) effects on the transport network.
  
- (4) Any development that complies with Table I553.6.12.1 and Standard I553.6.12(1) Transport infrastructure development thresholds:
  - (a) the operation, including but not limited to the type, capacity and frequency, of a private shuttle bus between the site and Takapuna for residents;
  - (b) the design of the pedestrian/cycle connection along the extent of Esmonde Road identified on Precinct Plan 1;
  - (c) the Parking and Traffic Management Plan; and
  - (d) the design of the intersection upgrade so that pedestrian and cyclist safety and amenity is improved; and so that traffic capacity supports 420 vehicle movements in any peak hour while ensuring adequate performance of through traffic movements on Esmonde Road.
  
- (5) Any development which exceeds the 273 dwellings or short term visitor accommodation units or 1,257m<sup>2</sup> of non-residential activity occupation thresholds but still generates less than 420 vehicle movements per any peak hour in Table I553.6.12.2 and Standard I553.6.12(2) Transport review thresholds:
  - (a) the adequacy and the recommendations in the Transport assessment.
  - (b) the contribution of alternatives to mitigating overall traffic effects.
  
- (4) Any development that does not comply with the following standards I553.6.1. Building Height and Building Length, I553.6.2 Maximum impervious area, I553.6.3 Building coverage, I553.6.5. Maximum building dimension and separation, I553.6.6. Wind, I553.6.7. Front, side and rear fences and walls, I523.6.9. Coastal planting, I553.6.10. Visual Corridors, I553.6.14. Noise



### **I553.8.2. Assessment criteria**

The council will consider the relevant assessment criteria below for restricted discretionary activities in addition to the assessment criteria specified for the relevant restricted discretionary activities in the overlay, zone, and Auckland-wide provisions.

(1) New buildings, additions and alterations to existing buildings and accessory buildings:

(a) Consistency with precinct plans:

- (i) whether the development or land use is in accordance with Precinct Plans 1 and 2.

(b) Building design and external appearance:

the extent to which building design and layout achieves:

- (i) a character and appearance that will ensure a high standard of amenity for residents and visitors;
- (ii) a design of buildings that contributes to the local streetscape and a sense of place by responding positively to the planned form and character of the surrounding area and the coastal setting;
- (iii) a silhouette of the buildings as viewed from areas surrounding the site in a way that positively contributes to the Takapuna skyline;
- (iv) clearly defined public frontages and entrances that address Esmonde Road and open space and plaza areas to positively contribute to the public realm and pedestrian safety, including the any necessary setback along Esmonde Road for pedestrian and amenities;
- (v) for mixed use buildings, a separate pedestrian entrance for residential uses;
- (vi) a coherent scheme including proposed building heights for the whole precinct to demonstrate an overall design strategy that contributes positively to the visual quality of the precinct and the locality;
- (vii) buildings that are designed to:
  - a) avoid long, unrelieved frontages and excessive bulk and scale when viewed from Esmond Road and open spaces;
  - b) visually break up their mass into distinct elements to reflect a human scale;
  - c) provide view corridors through the site towards the coastal margin in general accordance with Precinct Plan 1; and
  - d) use techniques such as recesses, variation in building height, length, and roof form, horizontal and vertical rhythms, and facade modulation and articulation;

- (viii) a variety of architectural detail at ground and middle levels including maximising doors, windows and balconies overlooking the Esmond Road and open spaces;
- (ix) roof profiles that are designed as part of the overall building form and contribute to the architectural quality of the skyline as viewed from both ground level and the surrounding area. This includes integrating plant, exhaust and intake units and other mechanical and electrical equipment into the overall rooftop design;
- (x) colour variation and landscaping, used in conjunction with building articulation, that achieves overall high design quality;
- (xi) glazing is provided along Esmonde Road and open space frontages and the benefits it provides in terms of:
  - a) the attractiveness and pleasantness of the street and public open space and the amenity for people using or passing through that street or space;
  - b) the degree of visibility that it provides between the street and public open space and the building interior; and
  - c) the opportunities for passive surveillance of the street and public open space from the ground floor of buildings.
- (xii) buildings that use quality, durable and easily maintained materials and finishes on the façade, particularly at street level;
- (xiii) measures are used to minimise the risk of bird strike resulting from increased night lighting, including consideration of the following:
  - a) all internal lights should be downward facing with minimal horizontal spill, and external lights should be shielded with no horizontal spill (National Light Pollution Guidelines for Wildlife Including Marine Turtles, Seabirds and Migratory Shorebirds, Commonwealth of Australia 2020. 111 pp.).
  - b) window screens and tinted windows can reduce light being seen at night by birds. Vegetation should be planted to screen roosting and breeding areas from the building (National Light Pollution Guidelines for Wildlife Including Marine Turtles, Seabirds and Migratory Shorebirds, Commonwealth of Australia 2020. 111 pp.).
  - c) lighting should only be used as necessary and at a low intensity.
  - d) the spectral range should avoid lights rich in blue light (400 – 500 nm).
- (xiv) signage that is designed as an integrated part of the building façade;

- (xv) development that integrates mātauranga and tikanga into the design of new buildings and public open spaces;
- (xvi) design that recognises the functional requirements of the intended use of the building; and
- (xvii) design that contributes to the avoidance of conflict between residential and non-residential activities within the precinct.
- (xviii) additionally, for residential development:
  - the mechanical repetition of unit types is not encouraged, where this would detract from the architectural form of the building.
  - balconies are designed as an integral part of the building.
  - external walkways/breezeways should generally be avoided unless a high design resolution is achieved;
- (xix) the use of materials, colour finishes, and glazing reduces glare having regard to this coastal location;
- (xx) internal living areas at all levels within a building maximise outlook onto open spaces and proposed public open space and Esmonde Road;
- (xxi) any otherwise unavoidable blank walls are enlivened by methods which may include artwork, māhi toi, articulation, modulation and cladding choice to provide architectural relief;
- (xxii) parking areas located within buildings are not directly open and/or visible from open spaces or Esmonde Road

(c) Shading:

the extent to which:

- (i) the location and design of buildings ensures a reasonable level of sunlight access (measured at the Equinox) to residential units (principal living rooms and private outdoor spaces) and open space areas, taking into consideration site and building orientation and the planned built character of the precinct.

(d) Landscaped area (both soft and hard landscaping):

the extent to which:

- (i) landscaping treatment within any open space or plaza areas responds to and acknowledges the natural landscape character of the adjoining coastal margin (future esplanade reserve zoned Open Space – Conservation);
- (ii) the design of hard and soft landscaping integrates with and appropriately enhances the design and configuration of buildings

and the amenity of publicly accessible areas for the various users of the precinct;

- (iii) the design of hard and soft landscaping along Esmonde Road provides for pedestrian and cycle movements and includes high quality of soft landscape elements to assist in stitching both sides of the Esmonde Road corridor together.

(e) Transport:

the extent to which:

- (i) vehicle access is designed and located to complement the road function and hierarchy;
- (ii) appropriate provision is made for:
  - a) pedestrian, cycle and vehicle movements;
  - b) car parking (while minimising reliance on private vehicle use);
  - c) infrastructure services;
  - d) the ability to connect to Francis Street with a pedestrian link ; and,
  - e) capacity of the roading network;
- (iii) pedestrian/cycle networks minimise potential conflicts between vehicles and cyclists and encourage a walkable neighbourhood to reduce vehicle dependency and ensure local accessibility to community facilities, open space areas, public transport facilities and non-residential activities;
- (iv) prior to the first occupation of the site, the provision of a private shuttle bus between development within the Takapuna 2 Precinct and the Takapuna Metropolitan Centre:
  - a) achieves the intended purpose of encouraging behaviour change away from private vehicles and towards public transport;
  - b) is privately funded, operated, managed and, where not provided directly by the developer, is secured through an appropriate legal mechanism such as (but not limited to) a Body Corporate or residents' association to ensure an effective level of service;
  - c) provides a level of service to support residents at any given stage of development of the Precinct; and
  - d) is necessary taking into consideration other transport options and modes available to the residents of and visitors to the precinct.

(f) Travel plans and integrated transport assessments:

the extent to which:

- (i) proposed developments and travel plans prepared in support of a proposal are consistent with the analysis and recommendations of any existing integrated transport assessment applying to the proposed development and/or precinct.

(g) Infrastructure and open space areas

the extent to which:

- (i) access and plazas create high quality open spaces and incorporate quality amenity features such as tree planting and footpath paving.
- (ii) there is consistency with the Stormwater Management Plan and/or relevant network discharge consent.
- (iii) coastal margin is supported through landscaping comprising predominantly native species, to contribute to the amenity of the precinct and to support ecological function.
- (iv) Trees located within the esplanade reserve that are affected by development areas are protected from development works.
- (v) open spaces and plazas are provided so that they are:
  - a) readily visible and accessible by adopting methods such as a generous street frontage or bordering onto yards of sites and front faces of buildings that are clear of visual obstructions;
  - b) located to provide visual relief;
  - c) integrated with surrounding development including;
  - d) sized and developed according to community and neighbourhood needs; and
- (vi) the esplanade shared path and all other walkways within the precinct are designed to be:
  - a) suitable and safe for regular share pedestrian and cycle use;
  - b) easily visible and accessible; and
  - c) linked to the public walkway and cycleway network outside the precinct.

(2) Commercial activities and Healthcare facilities that comply with Standard I553.6.13 – Commercial GFA and location control:

(a) Effect on residential amenity.

(b) Transport:

- (i) The extent that traffic generation and trip movements to and from the activity may create adverse effects on the:
  - a) capacity of roads giving access to the site;
  - b) safety of road users including cyclists and pedestrians;

- c) effective, efficient and safe operation of the arterial road network; and
- d) the planned urban built character of the precinct.

(c) Travel plans and integrated transport assessments:

- (i) The extent to which proposed travel plans prepared in support of a proposal are consistent with the analysis and recommendations of any existing integrated transport assessment for the proposed development and/or precinct.

(3) Any activity or development which does not comply with standards I553.6.11 Maximum On-site parking.

the extent to which:

- (a) the trip characteristics of the proposed activities on the site enable additional parking spaces without creating adverse effect on the roading network;
- (b) the effects of the vehicle movements associated with the additional parking spaces on the safe and efficient operation of the adjacent transport network, including public transport and the movements of pedestrians, cyclists and general traffic;
- (c) the adequacy and accessibility of public transport and its ability to serve the proposed activity;
- (d) a bus shuttle service to the Takapuna Metropolitan Centre is in place and provides for the transportation needs of residents and visitors;
- (e) mitigation measures are proposed to provide the additional parking, which may include measures such as by entering into a shared parking arrangement with another site or sites in the immediate vicinity; or
- (f) the demand for the additional parking can be adequately addressed by management of existing or permitted parking. Depending on the number of additional parking spaces proposed, the number of employees, and the location of the site, this may be supported by a travel plan outlining measures and commitments for the activity or activities on-site to minimise the need for private vehicle use and make efficient use of any parking provided.

(4) Any development which exceeds the 273 dwellings or short term visitor accommodation units or 1,257m<sup>2</sup> of non-residential activity occupation thresholds but still generates less than 420 vehicle movements per any peak hour in Table I553.6.12.2 and Standard I553.6.12(2) Transport review thresholds:

(a) Effects on the transport network:

- (i) Whether subdivision and/or development has adverse effects on the safety, efficiency and effectiveness of the operation and safety of the transport network, having particular regard to:

- a) Safety, efficiency and effectiveness effects of general traffic on existing and future pedestrians, active mode users, public transport operations of Esmonde Road;
- b) safety effects on existing and future users of the transport network on Esmonde Road.

(b) Contribution of alternatives to overall traffic effects:

- (i) Whether other transport network upgrade works to those identified in Table I553.6.12.1 and Standard I553.6.12(1) Transport infrastructure development thresholds can be undertaken, or other measures are proposed that mitigate the transport effects of the proposed subdivision and/or development; and
- (ii) The extent to which (if any) staging of subdivision or development may be required due to the co-ordination of the provision of transport infrastructure.

(4) Any activity or development that does not comply with standard I553.6.1. Building Height and Building Length, I553.6.2 Maximum impervious area, I553.6.3 Building coverage, I553.6.5. Maximum building dimension and separation, I553.6.6. Wind, I553.6.7. Front, side and rear fences and walls, I523.6.9. Coastal planting, I553.6.10. Visual Corridors, I553.6.14. Noise

- (a) any policy which is relevant to the standard;
- (b) the purpose of the standard;
- (c) the effects of the infringement of the standard;
- (d) the effects on the urban built character of the zone;
- (e) the effects on the amenity of neighbouring sites;
- (f) the effects of any special or unusual characteristic of the site which is relevant to the standard;
- (g) the characteristics of the development;
- (h) any other matters specifically listed for the standard; and
- (i) where more than one standard will be infringed, the effects of all infringements.

### **I553.9 Special information requirements**

In addition to the general information that must be submitted with a resource consent application (refer C1.2(1) Information requirements for resource consent applications), applications for the activities listed below must be accompanied by the additional information specified:

#### Integrated Transport Assessment

- (1) Any subdivision resource consent application, or land use resource consent application for any development where the peak hour trip generation exceeds

420 vehicles movements per any peak hour, must be accompanied by an integrated transport assessment for the precinct.

#### Commercial GFA details

- (2) Any application for commercial activities or healthcare facilities shall be accompanied by details of existing and proposed gross floor areas of individual premises for these activities and facilities within the precinct, so as to confirm compliance with standard I552.6.13. Commercial GFA and location control.

#### Private Shuttle Service

- (3) Where residential dwellings are proposed as part of any application, information shall be provided to confirm that the private shuttle bus required under Standard I553.6.12 Transport infrastructure development thresholds is in place and operating.

#### Arboricultural Assessment

- (4) Any resource consent involving any tree trimming or alteration and/or works within the drip line of trees over 3 metres in height that are located within the esplanade reserve and overlapping the development areas shall be accompanied with an arboricultural assessment of the effects on the trees and tree works / protection methodology to minimise any adverse effects on the trees.

#### Acoustic Assessment

- (5) An acoustic assessment shall be prepared by a suitably qualified and experienced acoustic professional to calculate noise levels arising from both business/commercial activities within the precinct and from traffic on Esmonde Road and the nearby motorway to inform the minimum façade noise reductions required to achieve the internal noise levels specified in I553.6.14(1) for noise sensitive spaces. The acoustic design must be based on cumulative noise from business/commercial activities within the precinct and traffic noise (i.e. from the road network). Furthermore, where the internal noise levels can only be achieved when windows and/or external doors to rooms are closed, those rooms shall have installed a mechanical ventilation and/or air conditioning system designed in accordance with the requirements specified in Standard I553.6.14(2).

#### Note:

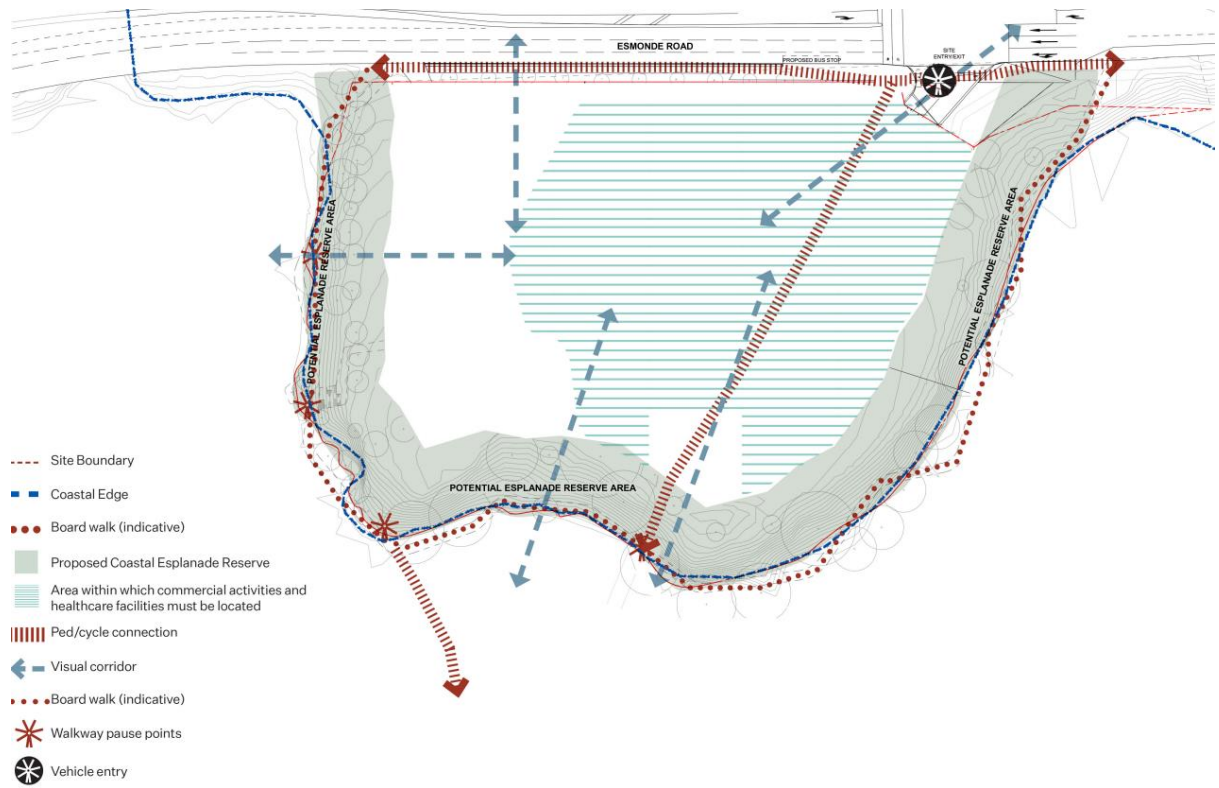
At the time the building consent application is lodged the consent holder will be required to provide written certification from a suitably qualified and experienced acoustic professional to the Council confirming that the building has been designed to ensure internal noise levels in bedrooms and other noise sensitive spaces specified in I553.6.14 will be met. Written certification will need to be in the form of a report.

#### **I553.10 Precinct plans**

- Precinct Plan 1 – Site features
- Precinct Plan 2 – Building height and coverage.



## Precinct Plan 1 – Site features



## Precinct Plan 2 – Building height and coverage



	Maximum Impervious	Maximum Building Coverage	Maximum Coverage Heights	Maximum Heights	Minimum Heights
Area 1	90%	60%	a. Up to RL23 (5 storeys) - 100%. b. Up to RL26.5 (6 storeys) - 90%. c. Up to RL 30 (7 storeys) - 40%	Must not exceed 7 storeys above RL 5 and no part of the building shall exceed RL30m in height	4 storeys above ground level (RL10).
Area 2	95%	60%	a. Up to RL30 (7 storeys) - 100%. b. Up to RL41 (8-10 storeys) - 35%	Must not exceed 10 storeys above RL 5 and no part of the building shall exceed RL41m in height	4 storeys above ground level (RL10).
Area 3	100%	60%	a. Up to RL41 (10 storeys) - 100%. b. Up to RL62 (11-16 storeys) - 35%	Must not exceed 16 storeys above RL 5 and no part of the building exceeds RL62m in height	4 storeys above ground level (RL10).

*Note: All RL Levels must be confirmed by a registered surveyor.*