

Beach Haven - Renew Coastal Connections Kaipātiki Local Board



24 November 2021

Project background

- The Kaipātiki Local Board approved the Kaipātiki Connections Network Plan in 2012 as the guiding document for expanding and enhancing the walking and cycling network within Kaipātiki. This plan was updated in 2016 to reflect recently completed or planned projects, stakeholder feedback and budget availability.
- The connection between Shepherds Park and Tui Park was included as a priority project in the Kaipātiki Connections Network Plan 2016 (KCNP). The section of the KCNP relevant to this project is provided as Attachment A to this report. The extracted map is included as Map 1 below. At the time of adopting the KCNP, the local board also approved progressing the planning of the connection between Shepherds Park and Tui Park along with 22 other projects (resolution number KT/2016/1).
- Local Board approved stage one works on 20/10/2021 KT/2021/179 and to report back on stage two

Beach Haven – Renew Coastal Connections

Total Budgets as at 20/11/21

Financial Year	Description of stages	Budget
FY21/22	Stage one (Engineers Estimate \$300,000 required rest will be carried over to complete stage two)	\$500,000
FY22/23	Stage two works – which includes platform, two bridges and realignment of paths	\$1,900,000
FY23/24	Stage two works – which includes platform, two bridges and realignment of paths	\$150,000
FY24/25	Stage three – to be discussed at later stage.	\$1,966,229

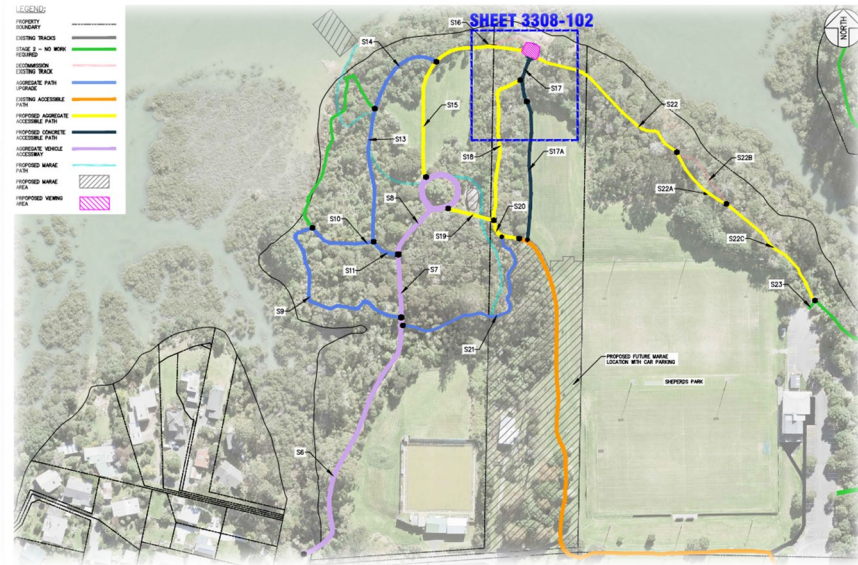
An aerial photograph of a residential area with several paths highlighted in green and orange. A red line is visible at the top left. A white box with black text is overlaid on the right side of the map. Labels for 'Kaipātiki Board', 'Paragon Avenue', 'Melba Street', 'Lancaster Road', 'Ranch Avenue', and 'Edonn Avenue' are visible. A large green area on the left is labeled 'Shepherds Park'.

**Northern Bridge Lancaster Road
- Hellyers Creek Res - Bridge**

**Southern Bridge
Shepherds Park to
Paragon Ave - Bridge**

Beach Haven Stage One Update

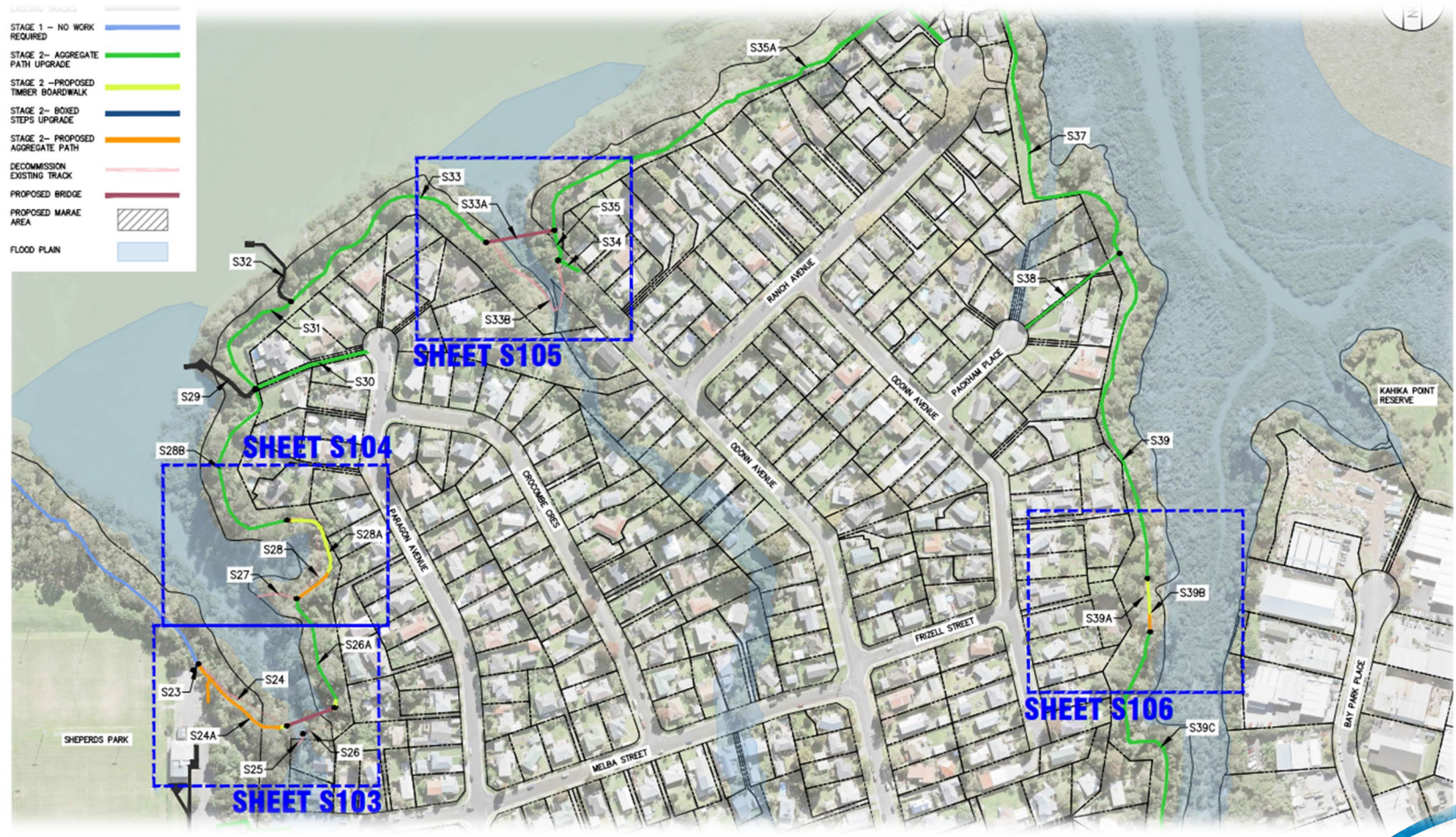
- The tender is under negotiation with tender and dates to be confirmed when physical works can commence
- The platform will be part of stage two works as requires consent



Beach Haven Stage Two Works for discussion

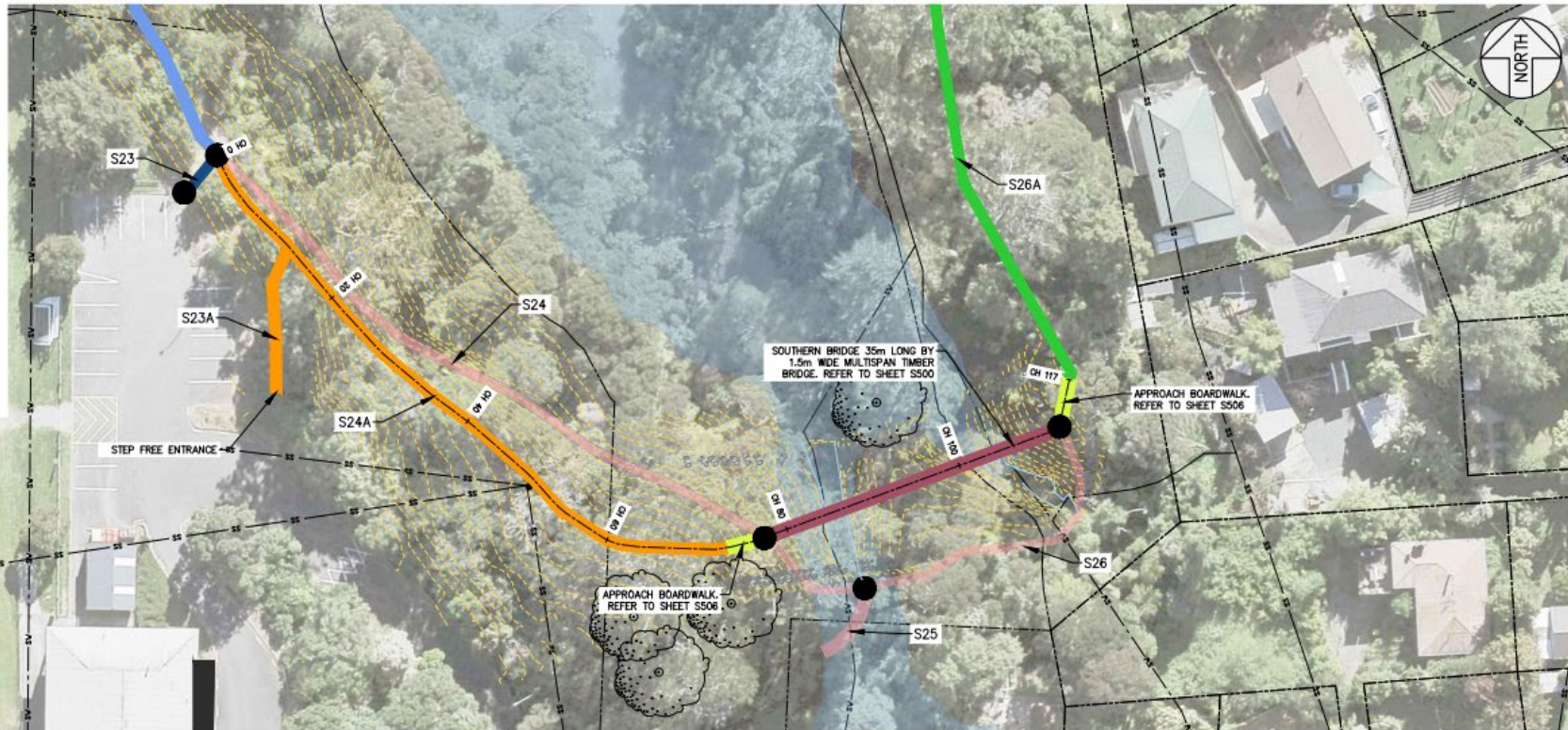
- Resource consent is required for this project and is underway
- What is in stage two?
 - Platform
 - Realignment of paths to make it more accessible and where possible step free.
 - Proposed 55m long by 1.5m wide coastal timber boardwalk (this is further out due to the salt marsh.
 - Timber boardwalks
 - Aggregate upgrades and proposed new paths
 - Southern Bridge 35m long by 1.5m wide multi-span timber bridge (Melba and Paragon Avenue
 - Northern Bridge 45m long by 1.5m wide multi-span timber bridge (Lancaster road)

- STAGE 1 - NO WORK REQUIRED —
- STAGE 2 - AGGREGATE PATH UPGRADE —
- STAGE 2 - PROPOSED TIMBER BOARDWALK —
- STAGE 2 - BOXED STEPS UPGRADE —
- STAGE 2 - PROPOSED AGGREGATE PATH —
- DECOMMISSION EXISTING TRACK —
- PROPOSED BRIDGE —
- PROPOSED MARAE AREA
- FLOOD PLAN



LEGEND:

- PROPERTY BOUNDARY
- EXISTING CONTOUR
- STAGE 1 - NO WORK REQUIRED
- STAGE 2 - AGGREGATE PATH UPGRADE
- STAGE 2 - PROPOSED TIMBER BOARDWALK
- STAGE 2 - BOXED STEPS UPGRADE
- STAGE 2 - PROPOSED AGGREGATE PATH
- PROPOSED BRIDE
- DECOMMISSION EXISTING TRACK
- EXISTING STORMWATER
- EXISTING WASTEWATER
- EXISTING MANHOLE
- FLOOD PLAN



3 PROPOSED SOUTHERN BRIDGE SITE PLAN- CH0 - CH17
SCALE: 1:500



LEGEND:

- PROPERTY BOUNDARY
- EXISTING CONTOUR
- STAGE 2- AGGREGATE PATH UPGRADE
- STAGE 2- PROPOSED TIMBER BOARDWALK
- STAGE 2- PROPOSED AGGREGATE PATH
- DECOMMISSION EXISTING TRACK
- EXISTING STORMWATER SW
- EXISTING WASTEWATER WS
- FLOOD PLAN



53m LONG BY 1.5m WIDE
TIMBER BOARDWALK.
REFER TO SHEET S506

5 SITE PLAN- CHO - CH84
SCALE: 1:250 A3





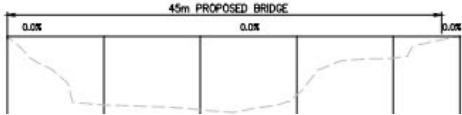
LEGEND:

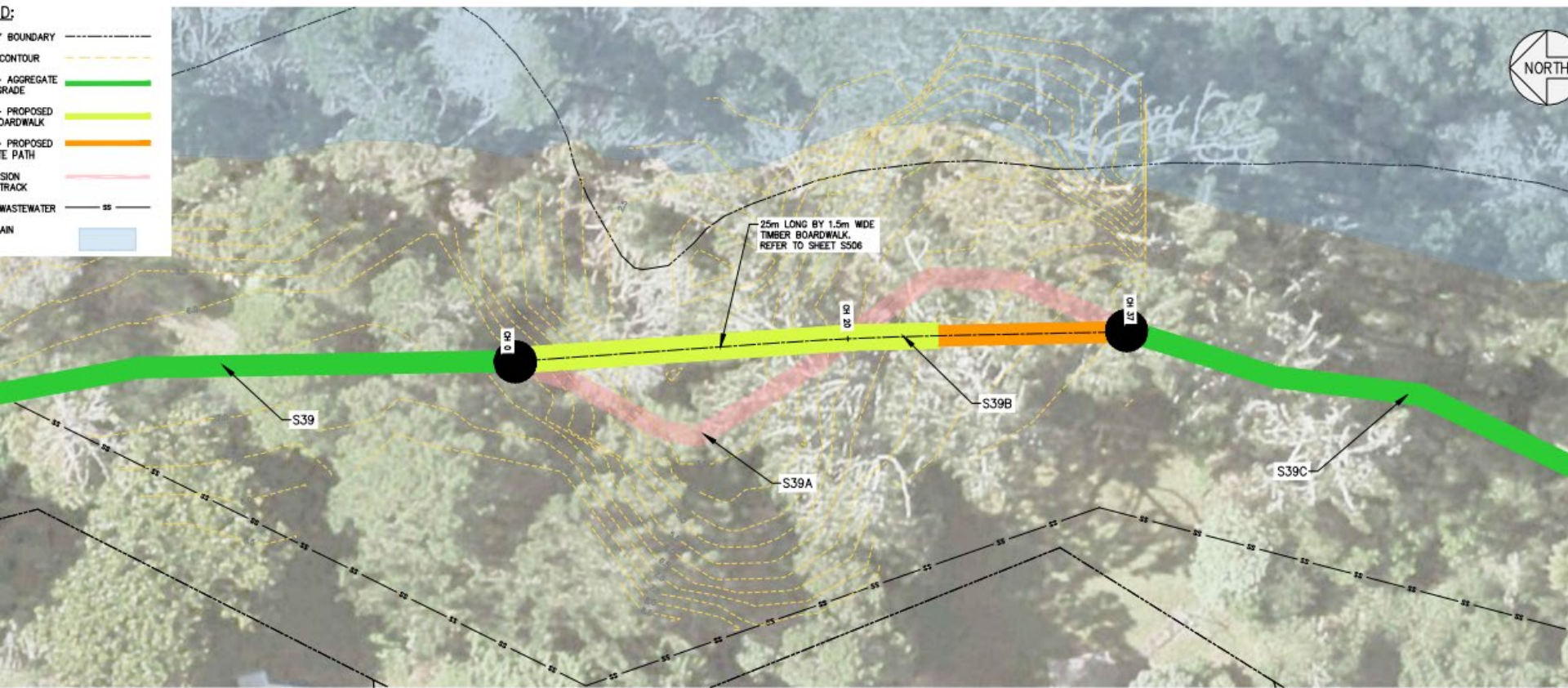
- PROPERTY BOUNDARY
- EXISTING CONTOUR
- STAGE 2- AGGREGATE PATH UPGRADE
- STAGE 2- PROPOSED TIMBER BOARDWALK
- PROPOSED BRIDGE
- DECOMMISSION EXISTING TRACK
- EXISTING STORMWATER
- EXISTING WASTEWATER
- FLOOD PLAIN



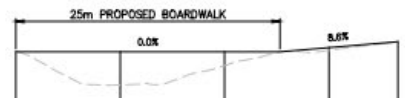
NORTHERN BRIDGE 45m LONG BY 1.5m WIDE MULTISPAN TIMBER BRIDGE. REFER TO SHEET S303

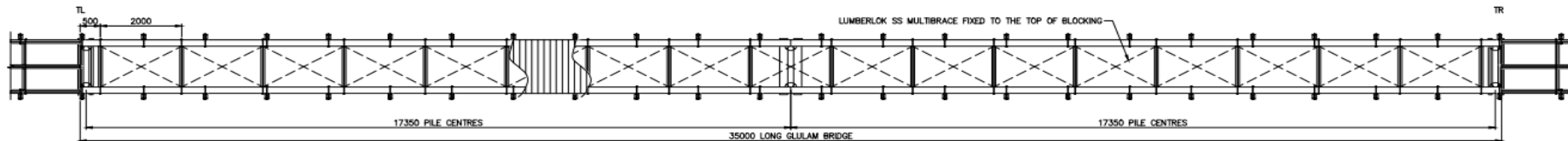
7 PROPOSED NORTHERN BRIDGE SITE PLAN - CHO - CH 47
SCALE: 1:250 ● A3





9 SITE PLAN - CHO - CH37
SCALE: 1:250 ● A3

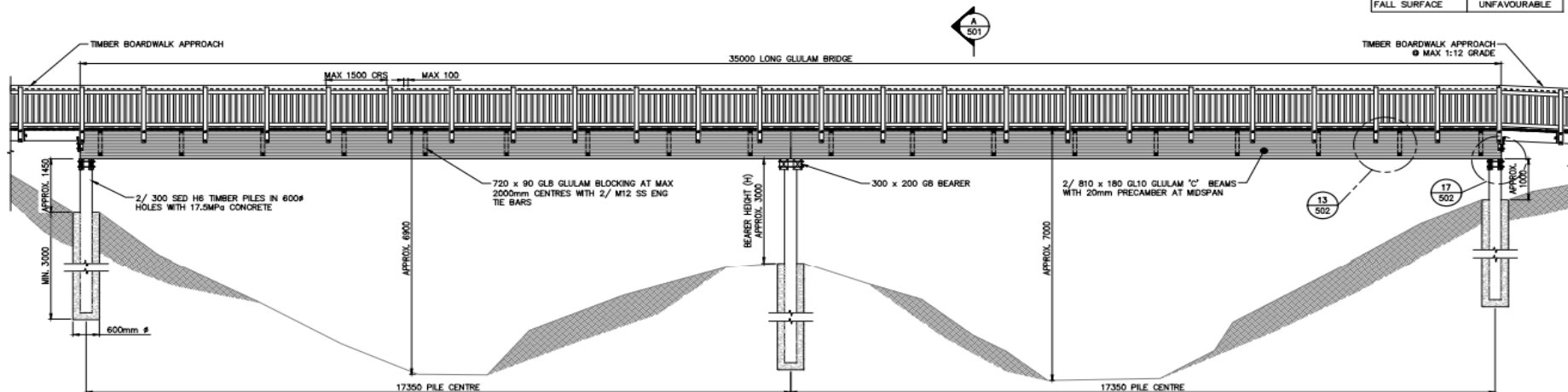




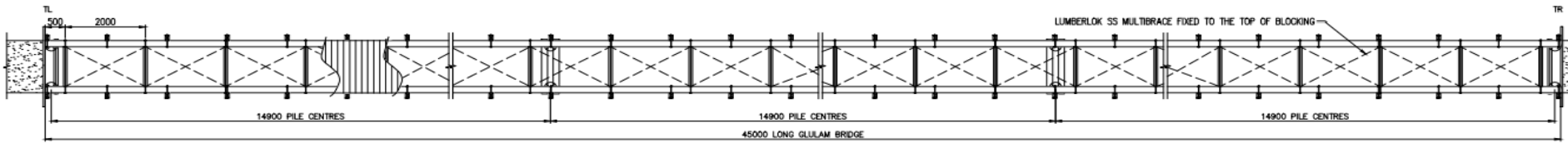
11 SOUTHERN BRIDGE PLAN
SCALE: 1:100 @ A3

THIS STRUCTURE IS DESIGNED IN ACCORDANCE WITH SNZ HB 8630:2004 "TRACKS AND OUTDOOR VISITOR STRUCTURES"

DESIGN DETAILS	
SITE USER GROUP	SST
BARRIER DESIGN LOAD	0.75kN/m
DECK DESIGN LOAD	4.0kPa
EFFECTIVE FALL HEIGHT	>3.0m
FALL SURFACE	UNFAVOURABLE



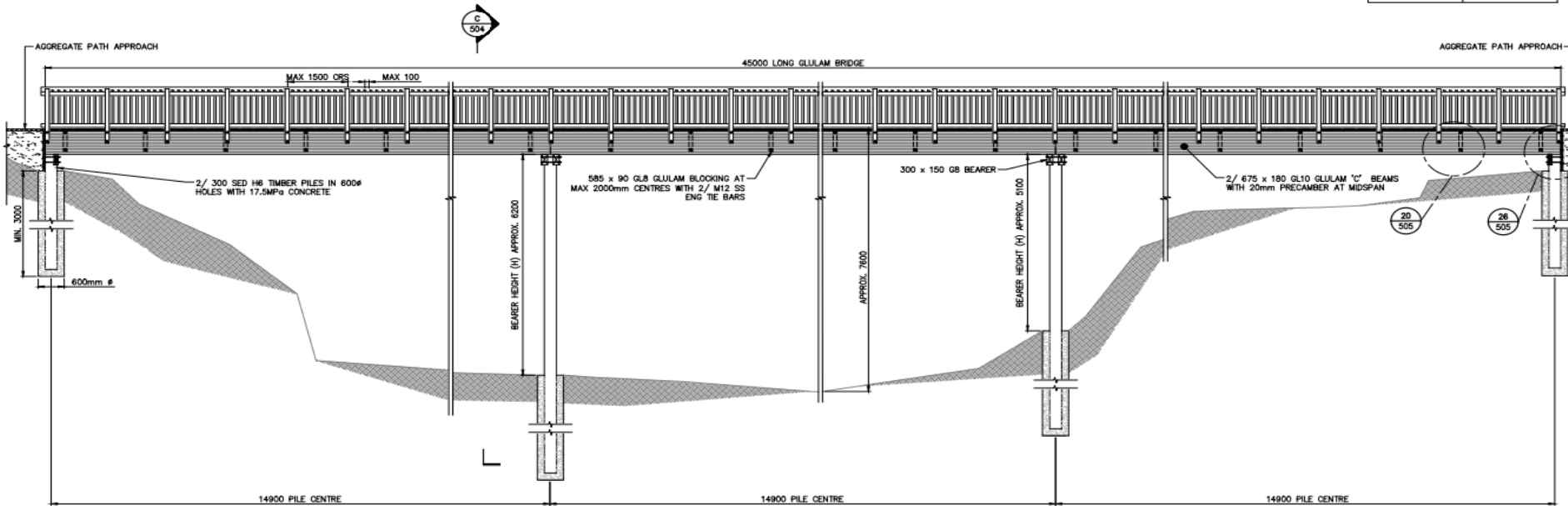
12 SOUTHERN BRIDGE ELEVATION
SCALE: 1:100 @ A3



18 NORTHERN BRIDGE PLAN
SCALE: 1:100 @ A3

THIS STRUCTURE IS DESIGNED IN ACCORDANCE WITH SNZ HB B630:2004 "TRACKS AND OUTDOOR VISITOR STRUCTURES"

DESIGN DETAILS	
SITE USER GROUP	SST
BARRIER DESIGN LOAD	0.75kN/m
DECK DESIGN LOAD	4.0kPa
EFFECTIVE FALL HEIGHT	>3.0m
FALL SURFACE	UNFAVOURABLE



19 NORTHERN BRIDGE ELEVATION
SCALE: 1:100 @ A3

35 Paragon to 101 Lancaster Road



Note:
Levels for visualisations have been
obtained from Auckland Council
GeoMaps

Existing Site Photo



Engagement and consultation

The stakeholder group includes:

Bike Kaipatiki

Bike Auckland

Accessible Parks

Uruamo Maranga Ake Marae Trust
Board

Friends of Shepherds Park

Scout group

Cedar Centre

Kaipatiki Projects

Beach Haven Guides

Beach Haven Bowl Club

Kaipatiki Facilities Trust

Birkenhead United Football Club.

Beach Haven Placemaking group

Beach Haven Residents Association

Next steps

- Local board to provide their feedback on stage two
- Talk to the stakeholders and gain their feedback
- Complete detailed design from local board feedback
- Report to come to the next business meeting to approve stage two works
- Continue with resource consent for approval
- Put out for tender
- Award
- Physical works to start in FY2022



The Year Ahead Kaipātiki Local Board

November 2021

Let's go there 

Agenda

1. Welcome and introductions
2. Purpose of workshop
3. Road Corridor Renewals
4. Road Safety
5. Network Optimisation
6. General discussion
7. Next steps

Purpose of today's workshop

An ongoing conversation:

- 1 Review and discuss priority work programmes, including 21/22 FY update and review of proposed 22/23 programme
- 2 Next Steps
 - LBs to review presentation detail and provide further feedback by mid December 2021
 - AT to review feedback and make adjustments to work programmes
 - AT to present finalised 22/23 programmes in March/April 2022



Engagement

AT's work programmes will be developed with different engagement approaches based on the weightings of:

- Strategic alignment
- Data dependance
- Customer feedback

Engagement approach	Commitment
Inform	We will keep you informed.
Consult	We will keep you informed, listen to and acknowledge concerns and aspirations, and provide feedback on how your input influenced the decision. We will seek your feedback on drafts and proposals.
Involve	We will work with you to ensure that your concerns and aspirations are directly reflected in the alternatives developed and provide feedback on how your input influenced the decision.
Collaborate	We will work together with you to formulate solutions and incorporate your advice and recommendations into the decisions to the maximum extent possible.
Empower	We will implement what you decide.



Programmes of Work Engagement Process

Programme	Summary	Programme Engagement
Local Board Transport Capital Fund	Local Board identified projects.	Empower
Community Programmes	Addressing community concerns for safety and accessibility	Involve
Active Modes (New footpaths) (Minor Cycling and Micromobility)	Delivers new sections of footpath which complete gaps in the network or enhances existing cycle facilities to connect people with key destinations.	Involve
Auckland Cycling Programme (Urban Cycleways Programme)	Delivering cycle facilities to connect town centres, public transport schools and employment. Urban Cycleways involved development of 4 key cycle networks.	Consult
Metro Operations and Infrastructure	Improving bus related infrastructure and services	Consult
Road Safety	Addressing high risk corridors and intersections, for our most vulnerable road users and setting safe and appropriate speeds across the network.	Inform
Parking	Addressing strategic and community initiated parking management improvements	Consult
Network Optimisation	Making best use of the existing roading network by increasing the capacity for people and freight movement, considering all modes	Inform
Road Corridor Renewals	Maintenance of assets within the road corridor	Inform
Capital Projects	Major projects	Inform
Streets for People (Innovating Streets for People, Regional Streets for People)	Focused on reducing transport emissions through encouraging mode shift to active modes, using temporary/semi-permanent infrastructure and non infrastructure initiatives.	Collaborate
Community Transport Programmes	Road safety behavior change initiatives	Consult
Sustainable Mobility, Customer Experience	Promoting active modes, improving safety and encouraging mode shift targeted at schools and communities and workplaces.	Consult

The 2022 / 2023 Programme

- The programme was initiated to ensure locally important transport projects are given appropriate priority, and to provide local boards with more direct ability to influence local transport projects.
- Budget of \$20m per annum has been split between local boards as per previous year allocations.

The Process

At the beginning of the electoral term, the Elected Member Relationship Team work with Local Boards to identify a long list of potential projects to be funded over their political term (3 financial years).

These projects are then assessed by AT to confirm they meet the funding criteria and are feasible. A rough order of cost is also calculated.

The results are then conveyed to the Board who resolve whether to proceed with the project or not. A three year works programme is then developed and agreed with Local Boards. Changes to this programme may only occur by Local Board resolution.

Kaipātiki: \$2,095,366 over 2yrs

AT are preparing responses to LB questions before prioritisation decisions can be made.





Focus areas

Introduction

Road renewals are undertaken when assets reach the end of their service life. This means that overall, the assets are in a range of condition from excellent (new) through to poor (old).



Outcomes

Road assets comprising road carriageway, footpaths, drainage, streetlights, traffic signals, bridges etc meet desired levels of service and are renewed in a timely, efficient and sustainable manner.



Potential Challenges

The existing renewal budgets are very constrained, but the renewal spend is forecast to increase significantly over the next 10 years to meet the needs of the network.

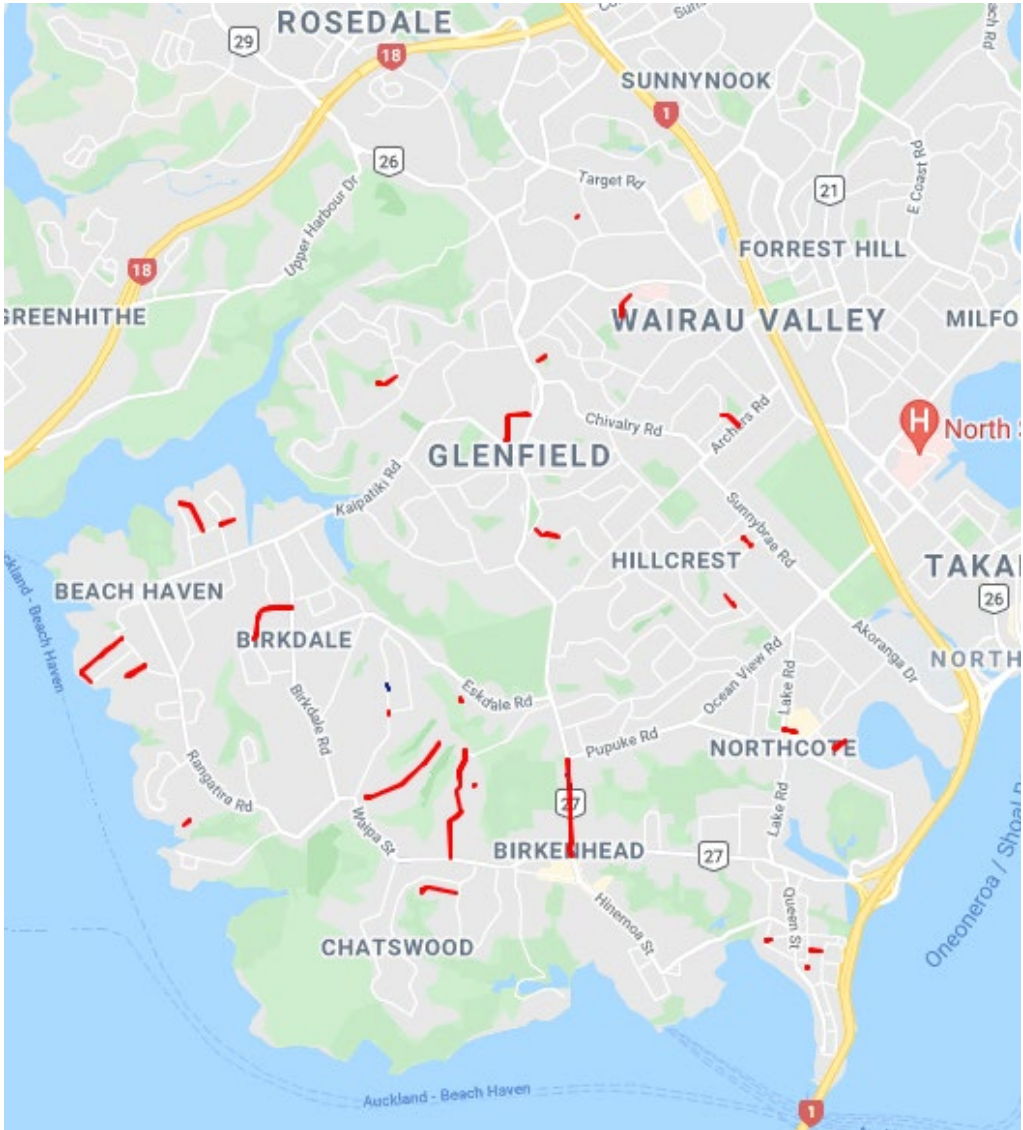
Road Corridor Renewals



2021 / 2022



	Road renewals
	Footpath renewals





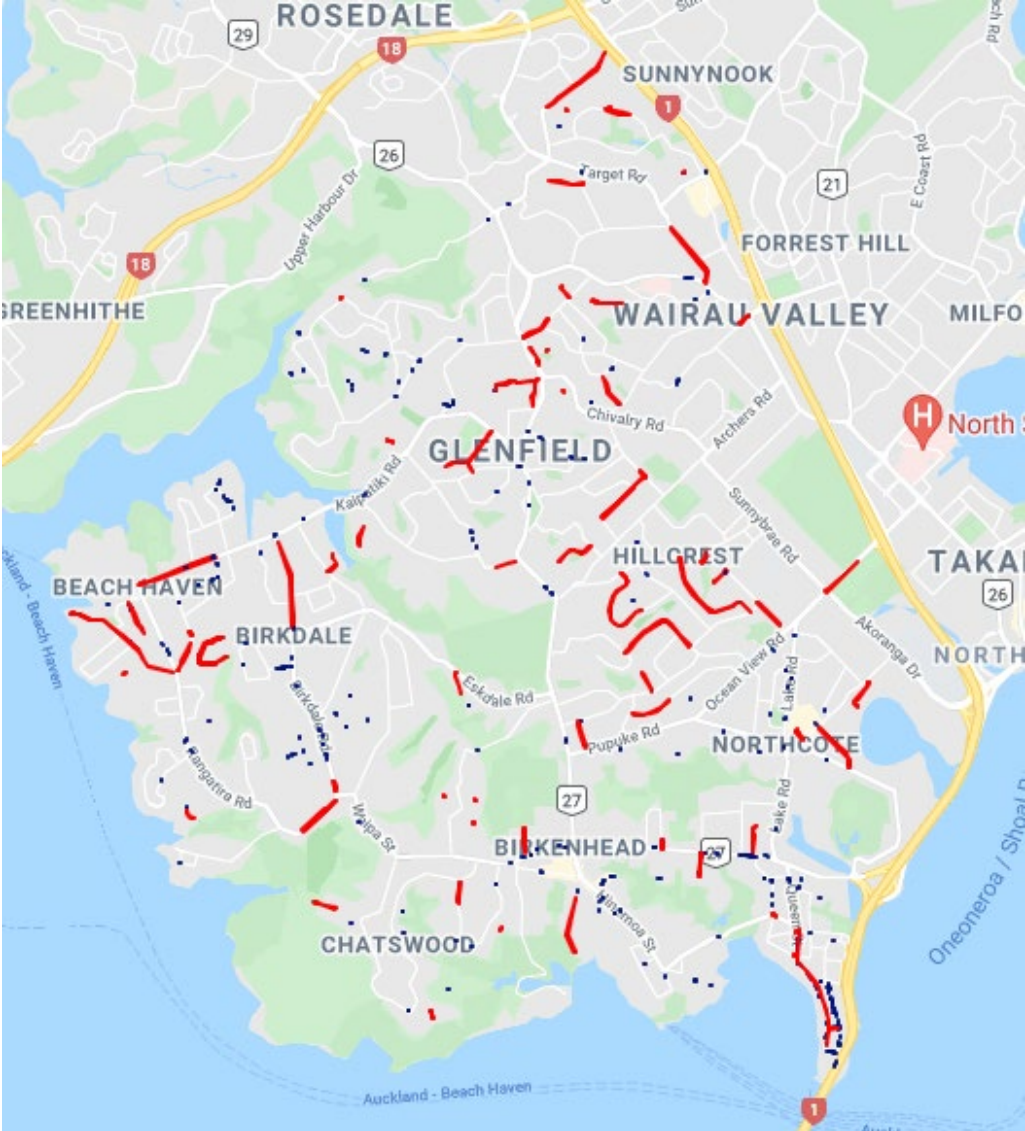
	Road renewals
	Footpath renewals





Road	
ARAHIA ST	RICHMOND AVE
BARLOW PL	ROSEBERRY AVE
BIRKENHEAD AVE	ROSS AVE
BRYERS PL	SISPARA PL
CROCOMBE CRES	SUNNYFIELD CRES
DAKOTA AVE	TOTARA GR
FLAXDALE ST	TYE RD
FRIZELL ST	VERBENA RD
HOBBY AVE	VERRAN RD
KEITH SMITH AVE	VIENNA PL
LOCKET RD	VINCENT RD
MERTON AVE	WANITA PL
MILTON RD	WOODCOTE DR
PEARN CRES	
POAKA PL	

Footpaths
BIRKENHEAD AVE
CROCOMBE CRES
VERBENA RD





	Road renewals
	Footpath renewals



Introduction

The Road Safety Programme is developed in partnership with Waka Kotahi and is aligned to the national Road to Zero Programme. The programme is focused on addressing high risk corridors and intersections, areas with the highest risk for our most vulnerable road users and making sure that we are setting a safe and appropriate speed across the network.



Outcomes

- Reducing Death and Serious Injuries (DSI) on our roads across the region.
- Having a network that is safer for all road users and creates a safe and appropriate speed environment at our areas of greatest risk.

Planning Process

Road Safety projects have been identified through numerous tools that assess the network for risk. The primary source of information has been developed by Waka Kotahi and the Road Safety programme to ensure that it is aligned with the national Road to Zero programme. All of our safety projects need to be endorsed by Waka Kotahi to be eligible for funding.

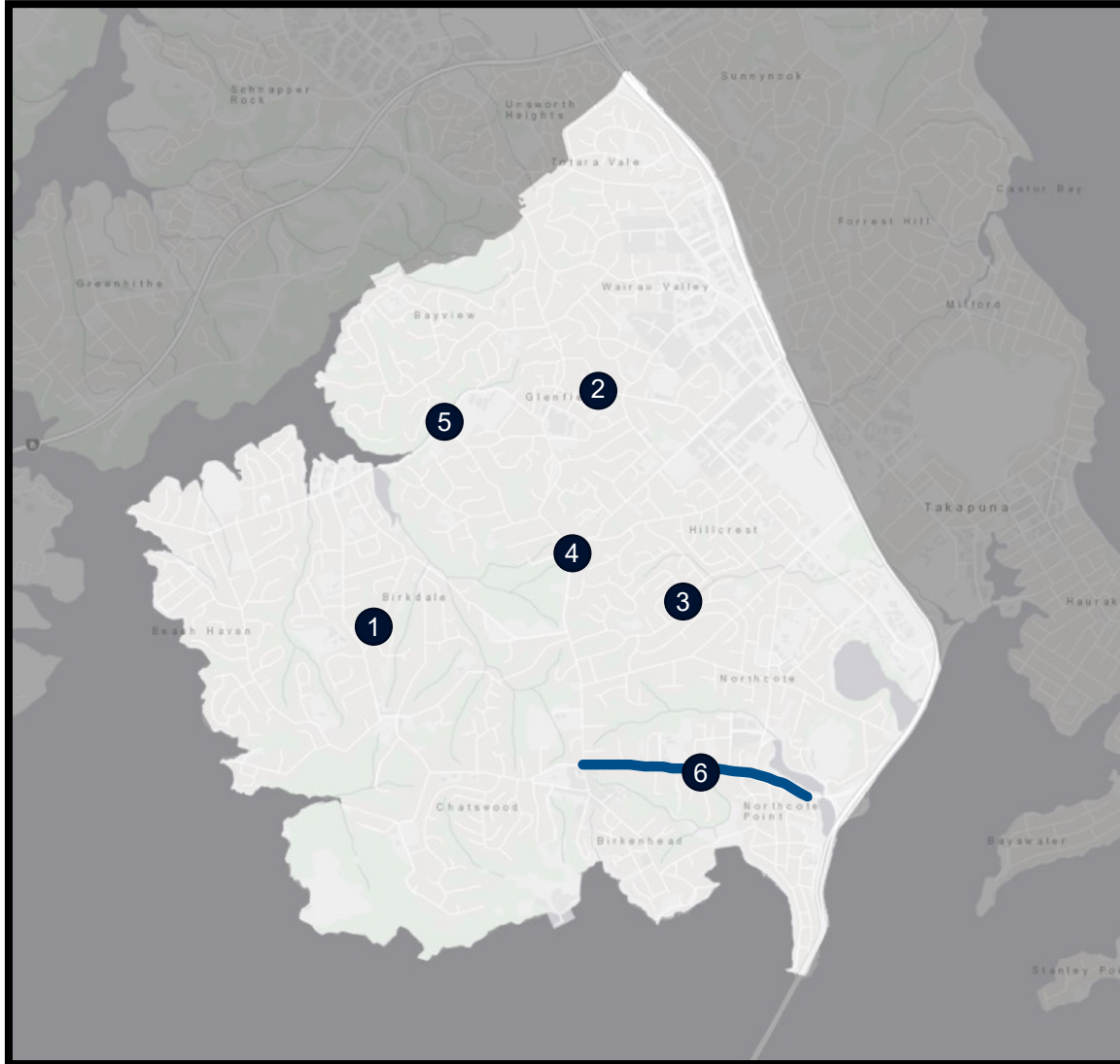
Project Title	Project Description	Status
Verran Primary School	Speed limit review	Investigation
Seaview Road High Friction Surfacing	Installation of high friction surfacing	Construction
Ellice Rd and Wairau Rd Intersection Improvement	Intersection improvements (Signalisation)	Design
2-10 Chartwell Ave Pedestrian Improvements	Raised table at existing pedestrian crossing and associated improvements	Investigation
85 Sunnynook Road Crossing Upgrade	Pedestrian crossing upgrade	Design
26 Chartwell Ave Pedestrian Improvements	Raised table at existing pedestrian crossing and associated improvements	Investigation
Glenfield Rd Midblock Pedestrian Improvements	New pedestrian facilities	Design
97 Pupuke Road Pedestrian Improvements	Swedish raised table	Design



Project Title	Project Description	Status
Birkdale Primary	Speed limit review and safety improvements	Investigation
Glenfield Primary School	Speed limit review and safety improvements	Investigation
Willow Park School	Speed limit review and safety improvements	Investigation
Glenfield Rd and Coronation Rd Intersection	Raised Safety Platform and new pedestrian crossings	Investigation
60 Kaipatiki Rd (Bend)	Bend improvement (mixture of skid resistance and delineation improvements)	Investigation
Onewa Road Corridor Improvements near Birkenhead Avenue	New pedestrian crossing facilities and other complementary safety improvements	Investigation

Red line indicates likely level of funding, below the line projects to be progressed in following years





Legend

- | | |
|---|--|
| 1 | Birkdale Primary- Speed limit review and safety improvements |
| 2 | Glenfield Primary School- Speed limit review and safety improvements |
| 3 | Willow Park School- Speed limit review and safety improvements |
| 4 | Glenfield Rd / Coronation Rd Intersection- Raised Safety Platform + Pedestrian Crossing |
| 5 | 60 Kaipatiki Rd- Corridor Improvements |
| 6 | Onewa Road Corridor Improvements - Speed calming and new pedestrian crossing facilities |

Introduction

The Network Optimisation Programme utilises the region's existing road network to raise the number of people and freight that travel through key routes and corridors as well as offset increasing levels of congestion.

We are currently in the process of developing a Programme Business Case with NZTA.

Planning Process

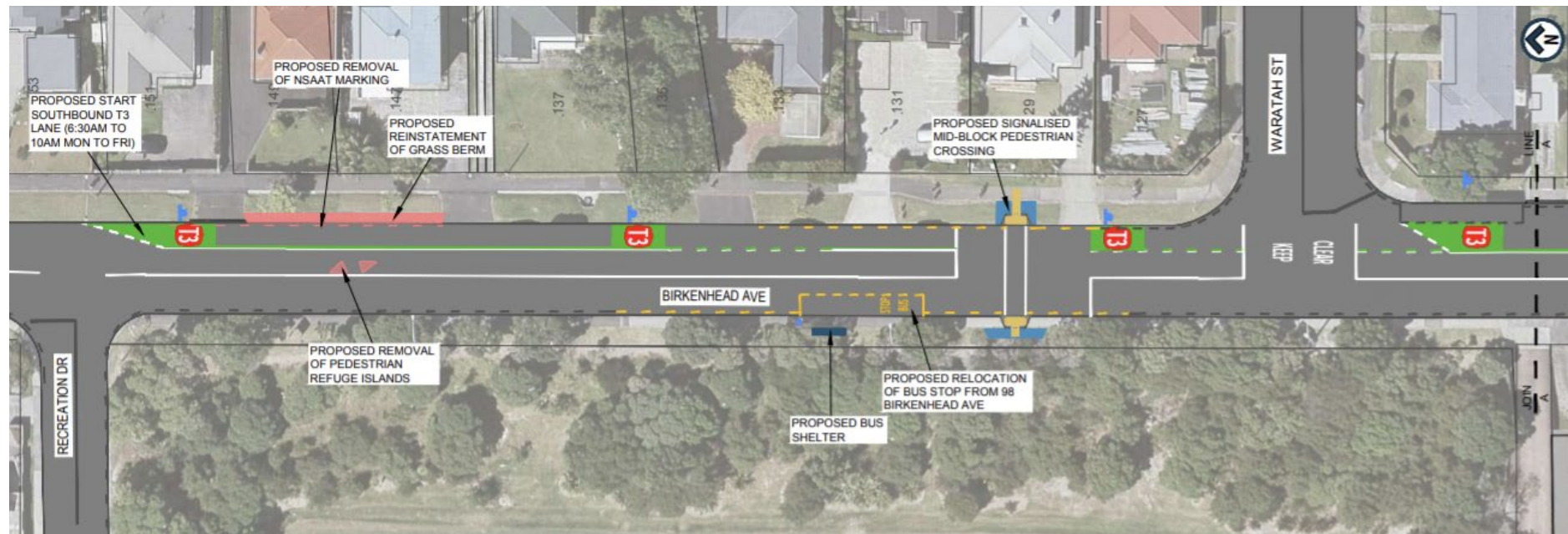
Network deficiencies are identified based on network performance reporting and measured against the Network Operating Plan. Network deficiencies (pain points) are then investigated and solutions prioritised to be included in the Optimisation Programme.


Outcomes

Solves known deficiencies (along with NZTA) on network performance for:

- Freight movements on the key freight network
- Bus movements on key public transport corridors (in conjunction with Metro Services)
- Walking (within activity centres) and cycling on key corridors
- General traffic

Project Title	Project Description	Status
Birkenhead T3 lane	Corridor improvements	In construction, to be completed in the current FY
Onewa Rd clearway from Birkenhead Ave to Queen St	Corridor improvements	Investigation





General discussion

**Next Steps &
Thank you.**



Supplementary programme information

Introduction

The Community Programme is a combination of 3 sub-programmes: Minor Improvements, Regional Improvements and the Community Safety Fund (CSF). The programmes focus on addressing community concerns for safety and accessibility.

Outcomes

- Delivers outcomes to respond to customer and stakeholder queries
- Improves the operation and safety of the network
- Responds to emerging issues in development areas
- Addresses safety concerns raised by the community and prioritised by the Local Board (CSF)

Planning Process

Regional improvement and minor improvement projects are typically identified while investigating safety or operational concerns raised by the community. The projects are then prioritised based on safety, mode choice, efficiency, community and cost.

Potential Challenges

- Community acceptance of infrastructure interventions
- Community expectation for rapid safety interventions
- CSF – final year of funding



Community Programme

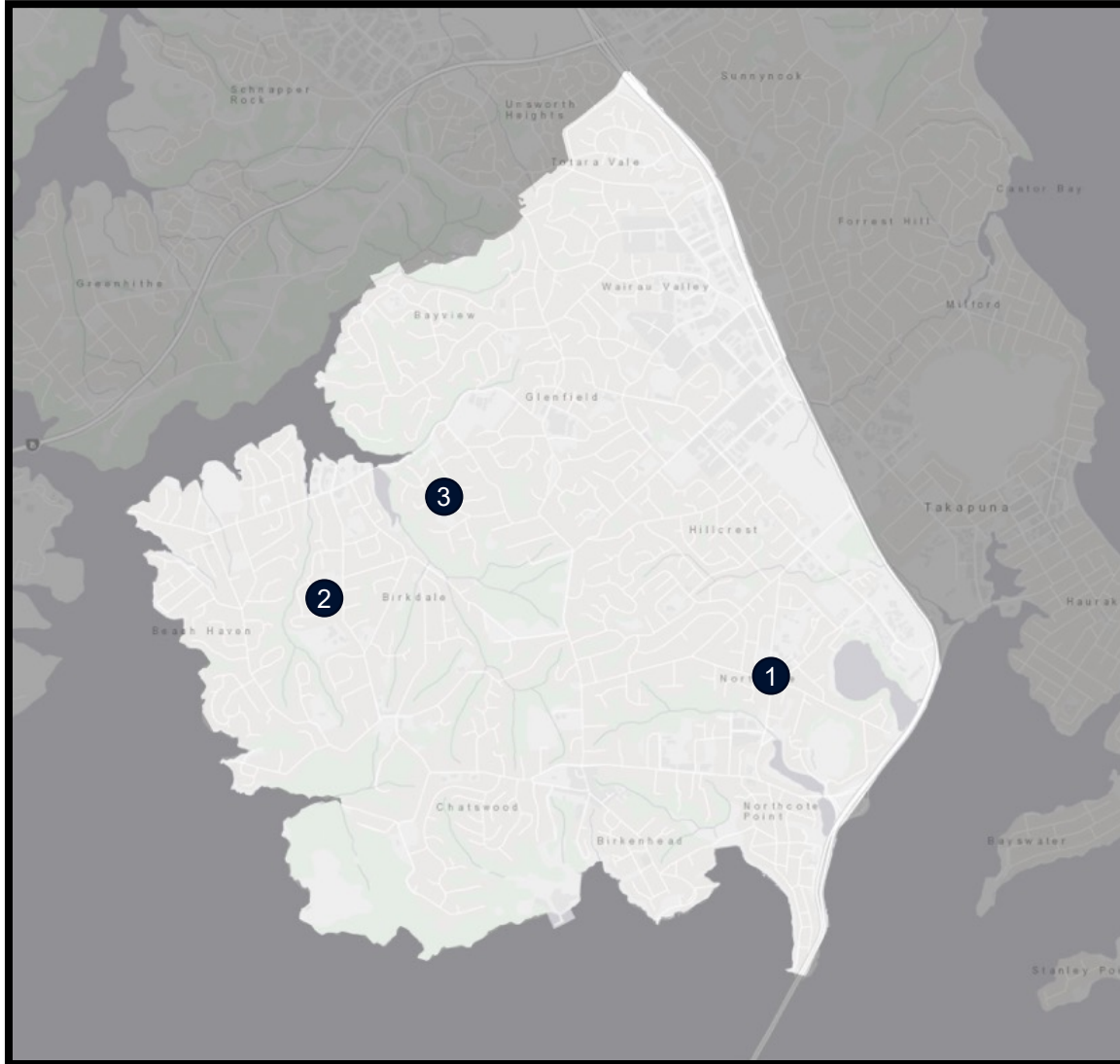
2021/2022 projects update

Project Title	Project Description	Status
Rangatira Road – Kauri Park School crossing	Rangatira Road at top entrance to Kauri Park School	Construction
27 College Rd Raised zebra crossing	#27 College Rd, Northcote	Design
Hinemoa St Pedestrian improvements	182 Hinemoa Street, Birkenhead	Design
Raised Zebra Crossing at 76 Exmouth Road, Northcote	76 Exmouth Road, Northcote	Design
Wairau Rd - View Rd Pedestrian & Intersection Improvements	Wairau Road / View Road, Glenfield	Investigation

2022/2023 proposed projects

Project Title	Project Description	Status
Lake Rd / Raleigh Rd / Exmouth Rd Roundabout Improvements	Roundabout Improvements	Investigation
Tiri Tiri Rd Pedestrian Improvements	Raised Zebra Crossing	Investigation
54 Seaview Rd Raised Zebra Crossing	Raised zebra crossing	Investigation





Legend

- | | |
|---|---|
| 1 | Lake Rd / Raleigh Rd / Exmouth Rd Roundabout - Roundabout Improvements |
| 2 | Tiri Tiri Rd Pedestrian Improvements - Raised Zebra Crossing |
| 3 | 54 Seaview Rd Crossing - Raised Zebra Crossing |



Auckland Cycling Programme

The PBC sets out the strategic investment programme for cycling in Auckland. The Cycling & Micromobility Programme Business Case review is currently developing a programme for the allocated RLTP \$306M funding.

Outcomes

The Business Case focuses on increasing cycling and access to opportunities, reducing deaths and serious injuries, and increasing the rate of delivery.

Planning Process

Prioritisation of preferred option focuses on:

- Access to schools, RTN stations, Metropolitan Centres
- Connections to the regional cycle network (Future Connect)
- Connections to the existing/committed cycle networks
- Routes that require no/little kerb movement in order to deliver more and faster

Urban Cycleways Programme

The Urban Cycle Programme (UCP), initiated in July 2015, forms a part of the AT cycling programme and commits the organisation to the completion of four new cycle networks. These networks / packages are:

- Western Connections
- City Centre Connections
- PT Links Connection
- Eastern Connections

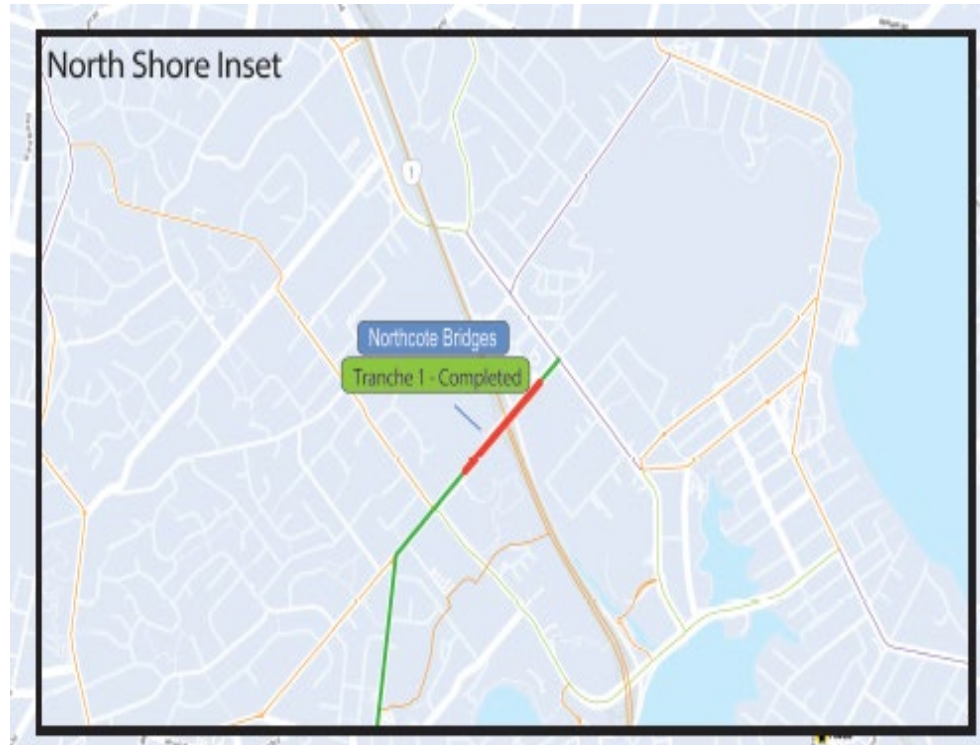
Delivery of UCP

The delivery of UCP is segregated into two tranches,

- **Tranche 1:** 12 projects are complete, with 2 projects due to complete between December 21 and March 22
- **Tranche 2:** 5 projects currently in detailed design and consultation phase. All are due for completion by 2024.



Urban Cycleways Programme



Project	Update
Northcote Safe Cycle Route	<p>This project is to construct safe cycle access along the 5.2 km route of Northcote Road, Lake Road, Onewa Road and Queen Street on Auckland's North Shore. It will provide various combinations consisting of on-road cycle lanes, shared cyclists/pedestrian paths and 'Sharrow' marking where possible and as appropriate for cyclists and pedestrians safety and priority.</p> <p>Northcote over bridge completed.</p>



Introduction

Public transport infrastructure is predominantly delivered under the PT Minor Works Programme. This programme is tasked with delivering a range of public transport infrastructure minor works for bus, marine and rail.

This includes:

- Bus stop infrastructure improvements
- Removal of redundant bus infrastructure
- Localised bus priority (bus tracking & route improvements)
- Public transport wayfinding upgrades
- Station facilities upgrades

Outcomes

To support and enhance the efficiency, reliability and customer amenity of all of AT's public transport services.

Planning Process

Projects are typically identified:

- From public transport operator requests
- Customer requests
- Bus performance assessments
- Changes in bus routes resulting in redundant infrastructure



Public Transport Minor Projects

2021 - 2023 Projects

Project Title	Project Description	Status
Bus Lane Improvement - Raleigh Rd Bus Lane / Sunnybrae Road T2 Transit Lane	New eastbound bus lane on Raleigh Rd between Ocean View Rd and Lake Rd. Including two signalised pedestrian crossings, removal of 4 existing bus stops and installation of 3 new bus stops. T2 transit lane for section of Sunnybrae Road also scheduled for construction in the same timeframe – construction to start March 2022.	Constuction planning
Bus stop upgrades - new shelters, new stops	Chivalry Rd – Glenfield (upgrade two pairs of bus stops, add two shelters), 79 Wairau Rd - Wairau Valley (new shelter), 243 Rangatira Rd – Beach Haven (relocation and renew shelter), 77 Hinemoa St – Birkenhead (new stop, upgrade opposite side of road), Onewa Rd – Northcote/Birkenhead (5 new shelters)	Design / Construction
Bus stop upgrade – line marking	Spinella Dr – Bayview (5 stops), Manuka Rd – Bayview (4 sites), Athena Dr – Totara Vale (2 stops)	Design
Bus stop upgrade – relocation	127 Sunset Rd – Totara Vale	Design
Bus shelter renewals	Opp 120 Birkdale Rd - Birkdale	Construction
Northcote Wharf	3x additional piles on the berth face and upgraded fendering system to be installed in Jan/Feb 2022. Works are being planned so as to minimise disruption to passenger services wherever possible	Construction

Introduction

The Parking Programme is both proactive and a community- initiated programme. It provides parking management to address key customer issues. It also focus on AT's congestion points to deliver on the AT Parking Strategy and the commitments set out in the SOI and Enterprise Business Plan.

Outcomes

- Enhances customer experience
- Supports mode-shift

Planning Process

The Parking Design Team collates feedback from the public, including Local Boards and Business Associations.

Projects are then assessed and prioritised by parking demand, 2012 Auckland Plan, community expectations, parking studies, and proximity to frequent and rapid transit network.

Potential Challenges

- Community acceptance to parking changes
- Operate on legislative network
- Technical Solutions lead time



Parking

2021 / 2022 Update

Project Title	Project Description	Status
Hinemoa Street	Broken Yellow Line Installation	To be initiated
McQuoid Place	Broken Yellow Line Installation	To be initiated
Uppingham Crescent	Broken Yellow Line Installation	To be initiated
Woodall Place	Broken Yellow Line Installation	Design
Eban Ave	P120 controls	To be initiated
Kororo Street	Broken Yellow Line Installation	To be initiated



Road Safety Programme

Community Transport road safety behavior change programmes address 8 high risk themes contributing towards death and serious injury crashes, in communities across Auckland.

Including: Young Drivers, Alcohol and Drugged Driving, Speed, Motorcycles and Motor scooters, Older Road Users, Intersections, Restraints, and Distractions.



Te Ara Haepapa (The Journey)

Te Ara Haepapa is a programme developed to address the high proportion of Maori involved in deaths and serious injury road crashes in Tamaki Makaurau.

What's happening in your local area

Young Drivers aged 16-24

- Online theory workshops for young
 - advice on how to stay safe when driving
 - advice on how to prepare for the next stage of their graduated licence
 - a free 'mock' licence test, with a qualified instructor after attending a restricted or full licence workshop

Speed

- Back to School & Slow Down Around Schools campaign and activities beginning Term 2

Restraints

- Whitiki – Online child restraints training workshops



Community Transport Programmes

Kaipātiki Local Board

26 Travelwise schools / 24 Walking School buses*

- | | |
|---|--|
| <ul style="list-style-type: none">• Bayview School *• Beach Haven School *• Belmont Primary School *• Birkdale Intermediate - Ngā Hua O Te Purapura Pai• Birkdale North School *• Birkdale Primary School -Te Puawaitanga O Te Purapura Pai• Birkenhead College• Birkenhead School *• Chelsea Primary School *• Glenfield Primary• Glenfield College• Glenfield Intermediate | <ul style="list-style-type: none">• Hato Petera College• Kauri Park School *• Manuka Primary School *• Marlborough Primary School *• Northcote College• Northcote Intermediate• Northcote Primary *• Onepoto School• St Mary's School *• Sunnybrae Normal School• Target Road School• Verran Primary School *• Willow Park Primary *• Windy Ridge Primary * |
|---|--|

Travelwise School Programme

This programme aims to encourage active modes of travel; walking, cycling, scootering and using public transport or a school bus.

Walking School Buses

A walking school bus provides young children with a safe way to walk to school through a structured, adult-supervised walking group that takes them from near their homes to their school.

Of 280,000 children in Auckland 46% use Active modes at Travelwise schools

288 Walking School buses

3750 Children across the region



Active Modes

Promoting active modes, improving safety and encouraging mode shift through cycle skills training, events, activations and campaigns targeted at schools and communities.

The cycling programme aims to get more people cycling more often, build community capacity for cycling and encourage the use of cycling infrastructure.

Local Board Involvement

We would love the Local Board to help us shape our programme locally by:

- *Identifying new community groups, schools and large businesses interested in working with us.*
- *Promoting our programme of activities to increase uptake.*

Travelwise Choices

Working with large businesses and organisations (over 100 staff) to increase travel by sustainable modes.

Targeted engagement including city centre businesses, DHBs, tertiary institutes and new movers.



Sustainable Mobility, Customer Experience

The following is a list of regional activities that will be tailored to local board areas, aligning with the existing cycle network and planned cycle infrastructure improvements.

Projects	Updates
Community Partnerships	Support community groups with the design, delivery and/or funding of their bike related activities.
Community Bike Fund	A contestable grant for community groups to deliver events and projects that encourage more people to ride bikes. Groups can apply for up to \$5,000.
School Cycle Training	Deliver Grade 1 (basic cycle skills) and Grade 2 (on road riding skills) at a selection of schools.
Bikes in Schools	Support the national programme through training teachers at participating schools.
Kids Learn 2 Ride	Community based drop-in sessions teaching bike skills to children (locations TBC).
Adult Bike Skills	Courses including Bronze (off-road skills), Silver (quiet road skills), Gold (on-road skills) and bike maintenance. Sessions available on demand.
Bike Burbs	In partnership with Bike Auckland provide capacity building support to community Bike Burbs.
Bike Hubs	Support the establishment and running of community-based bike hubs..
Events and Activations	Provide walking & cycling focused activities at community events and deliver a series of 'pit stops' on popular cycle routes to encourage safe cycling.
Auckland Bike Challenge	Individuals and teams can compete to log bike rides during the month of February.
Travelwise Choices Awards	Celebrating businesses who are doing great things in the sustainable travel space.



Introduction

Streets for People is made up of the following tactical urbanism programmes:

1. Innovating Streets 1.0 (2021) – Programme funded 90% by Waka Kotahi, monitoring & evaluation underway
2. Regional Streets for People (RSfP), established through Council's Climate Action Plan in the LTP. Focused on reducing transport emissions through encouraging mode shift to active modes, using temporary/semi-permanent infrastructure trials and non infrastructure initiatives outside the city centre.
3. Innovating Streets 2.0 Anticipated new programme from Waka Kotahi, focused on climate action anticipated later this year.

Planning Process

Innovating Streets for People: Waka Kotahi funded (90%), first tranche already complete/underway, another tranche will be announced in November.

Regional Streets for People : Auckland Council funded (90%), AT delivered, applications open until the end of November, \$3 million fund, focused on climate action, trying to encourage mode shift to active modes.

